

Report to Planning and Environment Committee

To: Chair and Members
Planning & Environment Committee

From: Scott Mathers MPA, P. Eng.,
Deputy City Manager, Planning and Economic Development

Subject: 1154 Hamilton Road
City File: Z-9569 Ward 1
Public Participation Meeting

Date: March 27, 2023

Recommendation

That, on the recommendation of the Director, Planning and Development, the following actions be taken with respect to the application of Lynphyl Homes Limited (c/o Monteith Brown Planning Consultants) relating to the property located at 1154 Hamilton Road:

- (a) The proposed by-law attached hereto as Appendix "A" **BE INTRODUCED** at the Municipal Council meeting on April 4, 2023, to amend Zoning By-law No. Z.-1, in conformity with The London Plan, to change the zoning of the subject property **FROM** a Neighbourhood Shopping Area Special Provision (NSA4(4)) Zone **TO** a Residential Special Provision (R5-7(_)) Zone.
- (b) **IT BEING NOTED** that the following Site Plan matters have been raised through the application review process for consideration by the Site Plan Approval Authority:
 - i) Provide pedestrian connections throughout the site and wrought-iron fencing (or similar fencing type) and a gate along the area nearest the Hamilton Road and Gore Road intersection to access the sidewalk connections at the intersection.
 - ii) Provide a minimum driveway length of 6.0 metres where a driveway abuts a sidewalk, and a minimum of 2.1 metres where a barrier-free parking stall abuts a sidewalk.
 - iii) Consider alterations to current parking space configurations to provide a more substantial and usable amenity space and better screen any parking exposed to the public street.

Executive Summary

Summary of Request

The applicant has requested to rezone the subject site from a Neighbourhood Shopping Area Special Provision (NSA4(4)) Zone to a Residential Special Provision (R5-7(_)) Zone to permit a stacked townhouse development consisting of 3, 3-storey stacked townhouses with a total of 34 dwelling units. Special Provisions are required to permit reduced front, rear, and interior side yard setbacks as well as to reduce the maximum encroachments for balconies and non-structural architectural features.

Purpose and the Effect of Recommended Action

The purpose and effect of the recommended Zoning By-law amendment is to rezone the lands to a Residential Special Provision (R5-7(_)) Zone to permit a 3-storey stacked townhouse development consisting of 34 dwelling units. Changes to the current permitted land uses and development regulations are summarized below.

Rationale of Recommended Action

1. The recommended amendment is consistent with the *Provincial Policy Statement, 2020*, which encourages the regeneration of settlement areas and

land use patterns within settlement areas that provide for a range of uses and opportunities for intensification and redevelopment. The PPS directs municipalities to permit all forms of housing required to meet the needs of all residents, present and future;

2. The recommended amendment conforms to the policies of The London Plan, including but not limited to the Key Directions, City Building policies, and the Neighbourhoods Place Type policies;
3. The recommended amendment would permit an appropriate form of development at an intensity that is appropriate for the site and the surrounding neighbourhood;
4. The recommended amendment facilitates the development of a vacant, underutilized site within the Built-Area Boundary.

Analysis

1.0 Background Information

1.1 Property Description

The subject lands are located on the north side of Hamilton Road, slightly east of the Hamilton Road and Gore Road y-intersection within the Hamilton Road Planning District. The site is approximately 0.605 hectares in size with a frontage of approximately 128 metres along Hamilton Road. The surrounding area primarily consists of a mix of residential uses and open spaces, including Kiwanis Park South and Pottersburg Park. The subject site is also adjacent to a London Hydro, hydro corridor.

Currently, the subject site contains a vacant single storey building previously used for a variety of commercial purposes, with the remainder of the site covered in surface level pavement. At present, there are two points of vehicular access to the site from Hamilton Road as well as access to public transit services. Public sidewalks are currently not available along this section of Hamilton Road. An Environmental Assessment is underway for the redevelopment of the Hamilton Road and Gore Road y-intersection, which anticipates installation of sidewalks into this area.

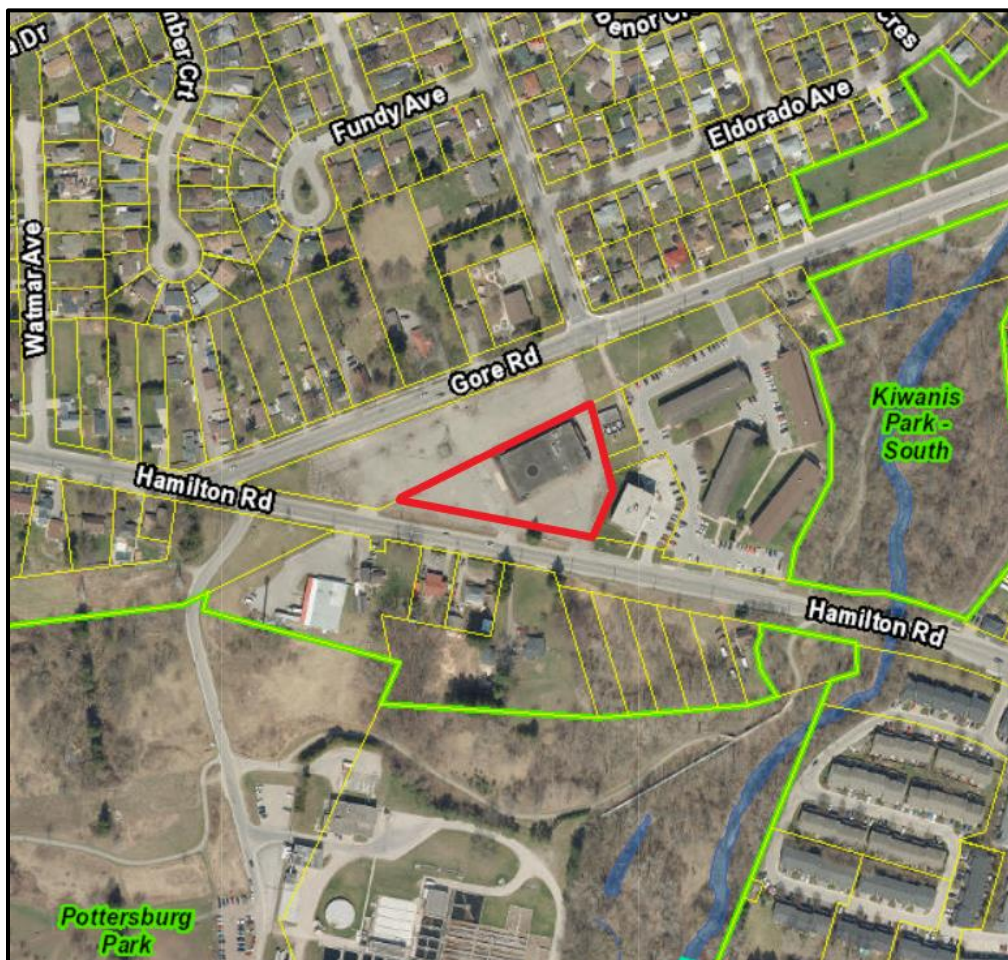


Figure 1. Aerial Photo of 1154 Hamilton Road and surrounding lands



Figure 2. Streetview of 1154 Hamilton Road (view of the subject lands, facing southeast from Gore Road)



Figure 3. Streetview of 1154 Hamilton Road (view of the subject lands, facing northeast from Hamilton Road)

1.2 Current Planning Information

- The London Plan Place Type – Neighbourhoods fronting a Civic Boulevard
- Existing Zoning – Neighbourhood Shopping Area (NSA4(4)) Special Provision Zone
- 0.605 hectares in size with a frontage of approximately 128 metres

1.3 Site Characteristics

- Current Land Use – Vacant Commercial Building
- Frontage – 128 metres
- Area – 6,050 metres square (0.605 hectares)
- Lot Coverage – 29 percent
- Shape – Irregular

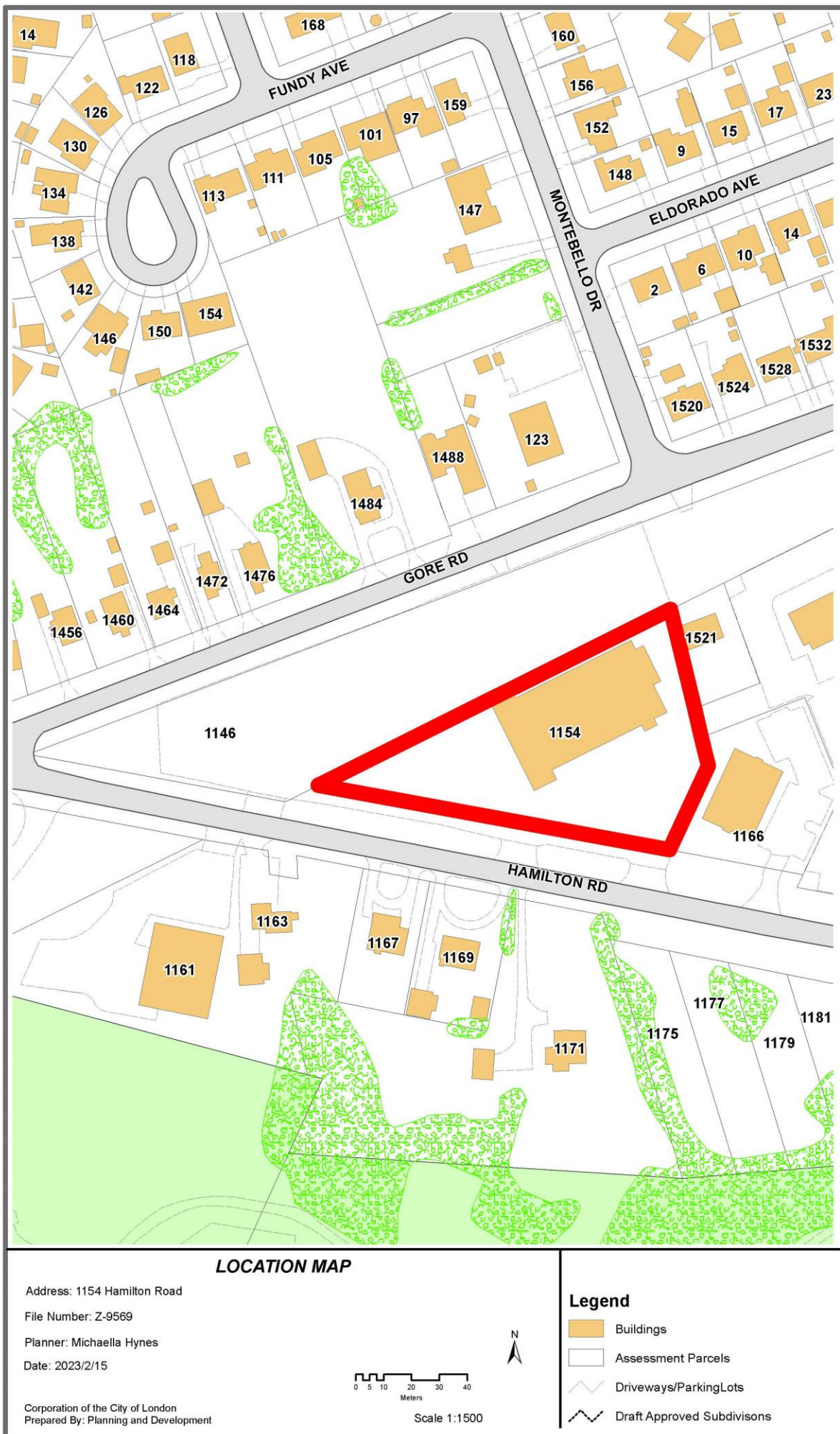
1.4 Surrounding Land Uses

- North – Residential and Neighbourhood Facility
- East – Residential and Open Space
- South – Residential and Open Space
- West – Residential and Open Space

1.5 Intensification

- The proposed development represents residential intensification within the Built-Area Boundary through the addition of 34 new dwelling units.
- The proposed development will not represent residential intensification within the Primary Transit Area.

1.6 Location Map



Description of Proposal

2.1 Development Proposal

The proposed stacked townhouse development consists of three, 3-storey stacked townhouses containing 34 residential units at a density of up to 57 units per hectare (UPH) and a height of 12.0 metres. Buildings 'A' and 'B' (fronting Hamilton Road) will contain 10 dwelling units each while Building 'C' will contain 14 dwelling units. Each unit is anticipated to have a patio/balcony to accommodate private outdoor amenity space in addition to a shared amenity space.

Vehicular access is provided via Hamilton Road leading to 68 vehicular parking spaces that can be accessed by way of integrated/attached garages and individual driveways. An additional 5 visitor parking stalls are included within the site design. Pedestrian access onto Hamilton Road is proposed to be provided via an interior sidewalk network which will extend to Hamilton Road on the west side of the development.

The site design has taken into consideration the Environmental Assessment with respect to the Hamilton Road and Gore Road intersection, whereby the southwestern corner of the property is proposed for amenity space. The proposed site layout provides sufficient space for future City acquisition of lands to accommodate the proposed roundabout option.

A site plan, floor plans, elevations and renderings of the proposed development are shown in Figures 5-10 below.

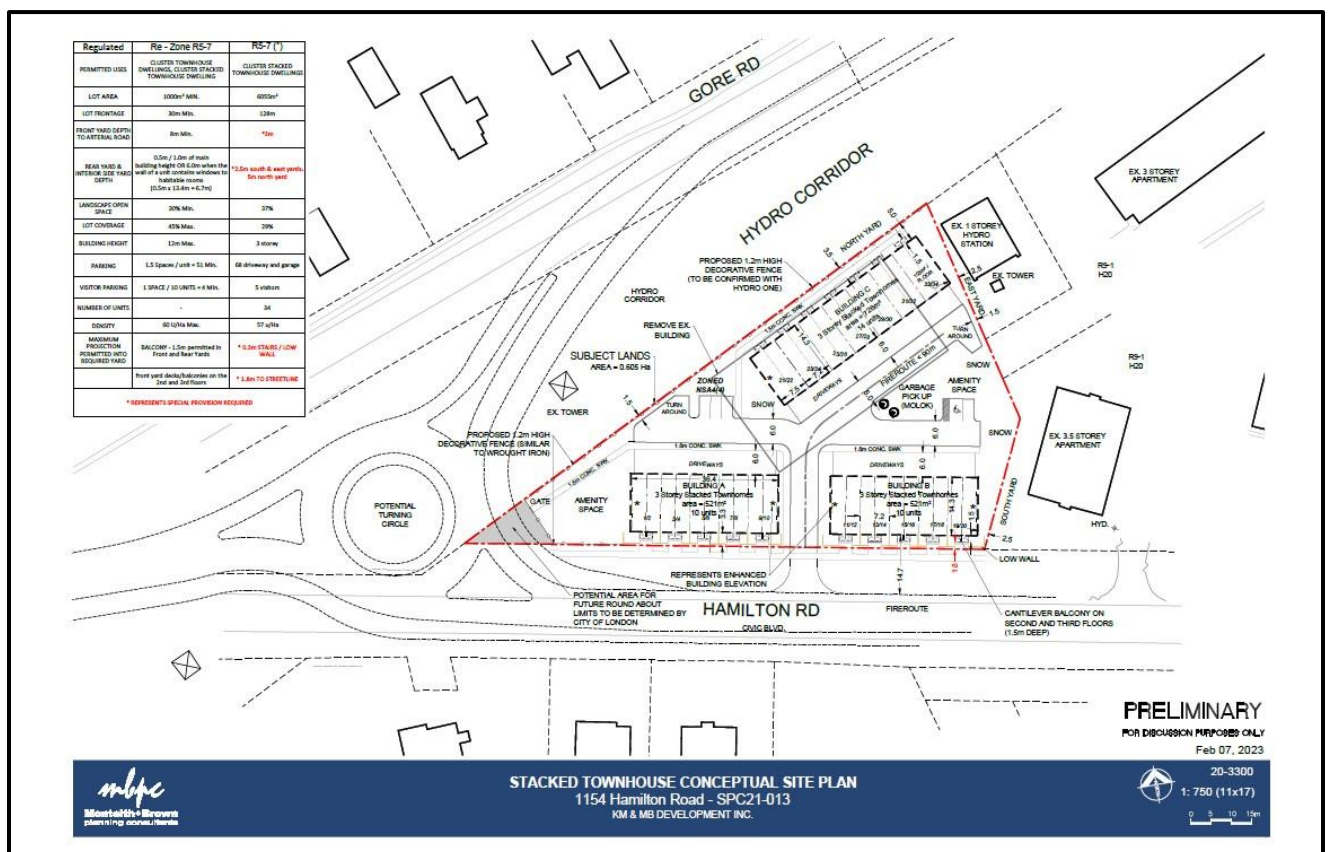


Figure 5. Conceptual Site Plan



Figure 6. Conceptual Rendering A1 (front of building facing Hamilton Road)



Figure 7. Conceptual Rendering A2 (rear of building facing Gore Road)

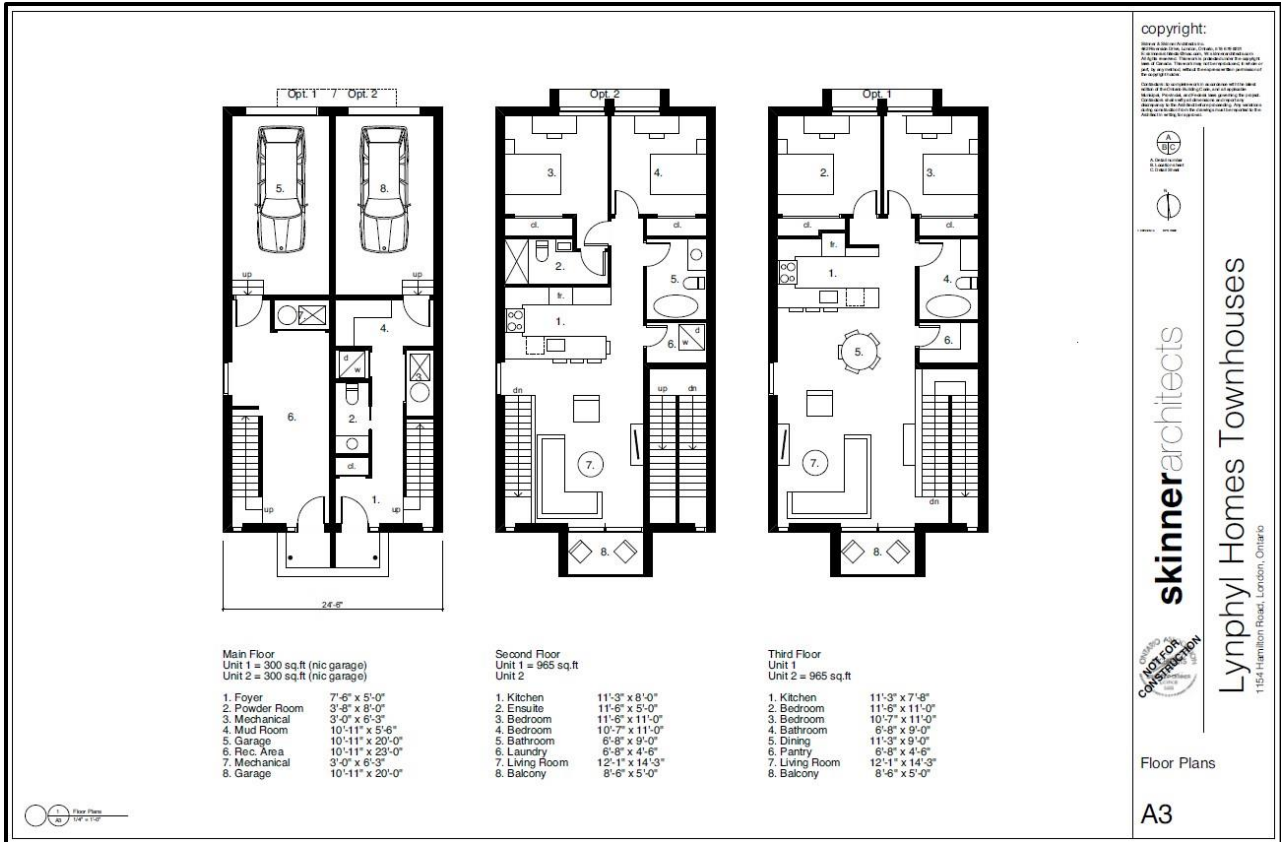


Figure 8: Floor Plan A3



Figure 9: Elevations A4 (North and South elevations)

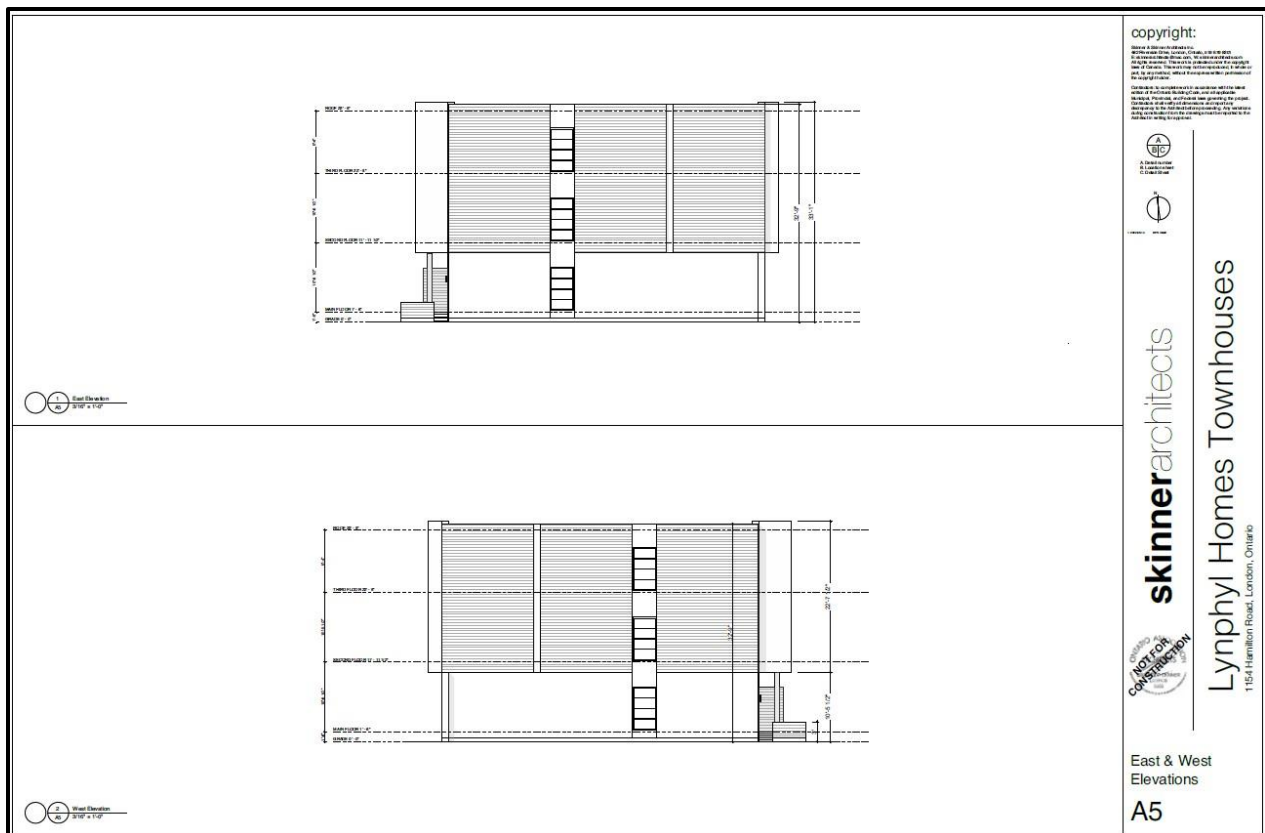


Figure 10: Elevations A4 (East and West elevations)

2.2 Requested Amendment

The applicant has requested to rezone of the subject site from a Neighbourhood Shopping Area Special Provision (NSA4(4)) Zone to a Residential Special Provision (R5-7(_)) Zone to permit a stacked townhouse development consisting of three, 3-storey stacked townhouses with a total of 34 dwelling units.

Special Provisions are also requested to permit:

- A minimum front yard depth of 3.0 metres;
- A minimum rear yard depth of 5.0 metres;
- A minimum southerly interior side yard depth of 2.5 metres;
- A minimum easterly interior side yard depth of 2.5 metres;
- A maximum encroachment for non-structural architectural features of 0.3 metres;
- A maximum balcony encroachment in the front yard provided the projection is not closer than 1.8 metres to the lot line.

2.3 Community Engagement (see more detail in Appendix B)

The public was provided with opportunities to provide comments and input on the application. There was 1 public response received during the community consultation period. No concerns with the proposed development were expressed by the public.

2.4 Internal and Agency Comments (see more detail in Appendix B)

The application and associated materials were circulated for internal comments and public agencies to review. Comments received were considered in the review of this application and are addressed in Section 4.0 of this report.

3.0 Climate Emergency

On April 23, 2019, Council declared a Climate Emergency. Through this declaration the City is committed to reducing and mitigating climate change. Refer to Appendix C for further details on the characteristics of the proposed application related to the City's climate action objectives.

4.0 Financial Impacts

There are no direct municipal financial expenditures with this application.

5.0 Key Issues and Considerations

4.1 Issue and Consideration #1 – Provincial Policy Statement (PPS), 2020

The PPS provides policy direction on matters of provincial interest related to land use planning and development. In accordance with Section 3 of the Planning Act, all planning decisions “shall be consistent with” the PPS.

Section 1.1 of the PPS encourages healthy, livable, and safe communities which are sustained by promoting efficient development and land use patterns which sustain the financial well-being of the province and municipalities over the long term (1.1.1 a). The PPS directs settlement areas to be the focus of growth and development, further stating that the vitality and regeneration of settlement areas is critical to the long-term economic prosperity of our communities (1.1.3). As well, the PPS directs planning authorities to provide for an appropriate range and mix of housing options and densities required to meet projected requirements of current and future residents of the regional market area (1.4.1).

The policies of the PPS direct planning authorities to identify appropriate locations and promote opportunities for transit-supportive development to accommodating a significant supply and range of housing options through intensification and redevelopment where it can be accommodated. The PPS also considers existing building stock or areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs (1.1.3.3) and is supportive of development standards which facilitate intensification, redevelopment, and compact form (1.1.3.4).

Planning authorities are further directed to permit and facilitate all housing options required to meet the social, health, economic and well-being requirements of current and future residents as well as all types of residential intensification, including additional residential units and redevelopment (1.4.3b)). Densities for new housing which efficiently use land, resources, infrastructure, public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed, are promoted by the PPS (1.4.3d)). Lastly, the PPS encourages long-term economic prosperity to be supported by promoting opportunities for economic development and community investment-readiness (1.7.1 a)).

The recommended amendment is in keeping with the PPS 2020 as it will permit a compatible use within the surrounding context and will contribute to providing an appropriate range and mix of housing options and densities essential to meeting the projected requirements for current and future residential. The recommended amendment contributes to a land use pattern that makes efficient use of an underutilized parcel within an established settlement area and represents an appropriate form of residential intensification.

The proposed redevelopment is considered a more efficient use of the subject site as the proposal will accommodate a medium-density residential development where a remnant commercial use currently exists. Further, the subject site is of a size and configuration capable of accommodating the proposed intensity of development while making use of existing active and public transportation, and nearby recreational, institutional, and commercial services. The proposal will also facilitate a development that will contribute to a greater range of housing options that meets a diversity of social, health, economic and well-being requirements of current and future residents. Lastly, the proposed development has the potential to encourage an increase in long-term social, economic, and environmental prosperity within the neighbourhood.

4.2 Issue and Consideration #2 – Key Directions, Use, Intensity and Form

The London Plan

The London Plan provides Key Directions (54_) that must be considered to help the City effectively achieve its vision. These directions give focus and a clear path that will lead to the transformation of London that has been collectively envisioned for 2035. Under each key direction, a list of planning strategies is presented. These strategies serve as a foundation to the policies of the Plan and will guide planning and development over the next 20 years. Relevant Key Directions are outlined below.

The London Plan provides direction for building a mixed-use compact city for London's future by:

- Planning for infill and intensification of various types and forms to take advantage of existing services and facilities and to reduce our need to grow outward (Direction 4)
- Ensuring a mix of housing types within our neighbourhoods so that they are complete and support aging in place (Direction 5)

The London Plan also provides direction to build strong, healthy, and attractive neighbourhoods for everyone by:

- Thinking “big picture” and long-term when making planning decisions – considering the implications of a short-term and/or site-specific planning decision within the context of this broader view. (Key Direction #8, Direction 3)
- Integrating affordable forms of housing in all neighbourhoods (Key Direction #7, Direction 10).
- Ensuring new development is a good fit within the context of an existing neighbourhood. (Key Direction #8, Direction 9)

The area surrounding the subject lands primarily consists of a mix of residential uses and open spaces with some commercial uses that cater to the surrounding community. The proposed amendment supports these Key Directions by permitting the redevelopment of an underutilized parcel to allow a form of residential intensification that would contribute to the mix of housing options in the neighbourhood. The proposed development would maximize the use of the land by accommodating an appropriate residential density within the neighbourhood thereby allowing existing residents to age in place whilst taking advantage of existing municipal services and facilities.

The site is located within the Neighbourhoods Place Type of The London Plan with frontage onto a Civic Boulevard (Hamilton Road) as identified on Map 1 – Place Types and Map 3 – Street Classifications. Permitted uses within Neighbourhoods Place Type at this location include a broad range of residential uses that include stacked townhouses at a standard maximum height of 4-storeys (The London Plan, Table 10 and 11).

When proposing residential intensification projects within the Neighbourhoods Place Type, the City will consider a variety of design policies including the intensity of the proposed development and the compatibility and fit of the proposal within the surrounding context (The London Plan, Policy 953_ 2 and 3). In order to determine whether a proposed residential intensification project is appropriate for the surrounding neighbourhood; the following design criteria are considered:

- Site layout within the context of the surrounding neighbourhood, considering such things as access points, driveways, landscaping, amenity areas, building location, and parking.
- Building and main entrance orientation.
- Building line and setback from the street.
- Character and features of the neighbourhood.
- Height transitions with adjacent development.
- Massing appropriate to the scale of the surrounding neighbourhood.

The proposed stacked townhouse development represents a permitted land use at an appropriate scale and intensity which is considered compatible with the surrounding properties. The development also represents an appropriate redevelopment of an underutilized parcel in favour of a residential use and intensity that maximizes the use of the land and existing municipal servicing and facilities while accommodating necessary site functions and features. Special provisions will be implemented to establish a building form that is pedestrian orientated in nature while complementing the surrounding neighbourhood.

Parking areas will be located internally and away from the street frontage to maintain the visual aesthetic and further encourage a pedestrian orientated streetscape. Additionally, the proposal includes internal pedestrian walkways, which through Site Plan Approval will establish a strong pedestrian circulation through the site and access to Hamilton Road. The southwestern corner of the site is also proposed as a large on-site landscaped open space to accommodate an appropriate amount of space for the shared amenity of all units, while also contributing to a significant increase in the overall landscaped open space on the site. This also provide opportunity for the City to possibly acquire additional lands for the proposed roundabout with respect to the Hamilton Road and Gore Road intersection.

As such, the proposed development is considered to implement the planned vision of the Neighbourhoods Place Type that intends to help establish an appropriate form and scale of residential intensification while complementing the character of the surrounding area. The proposed stacked townhouse development will also contribute to a mix of housing types, and provide choice and diversity in housing options for both current and future residents.

4.3 Issue and Consideration #3: Transportation and Parking

The applicant is proposing to incorporate 68 vehicular parking spaces and 5 visitor parking stalls (2 parking spaces per unit within driveways and private garages) on site for a total of 73 off-street parking spaces. Given that the City's Parking Standards requires that only 1 space be provided for each townhouse unit (Zoning By-Law, Section 4.19) the inclusion of 2 spaces per unit plus 5 additional visitor spaces can be considered more than sufficient to successfully accommodate the increase in vehicles anticipated for the site and assists in mitigating the risk of parking spilling over onto nearby residential streets. Slight reconfigurations of parking are proposed to be considered during the Site Plan Approvals process in an effort to increase the site's amenity area and provide pedestrian connections throughout the site. Furthermore, a Transportation Impact Analysis (TIA) was not warranted as no significant transportation or parking concerns were raised, and any increased traffic resulting from the additional 34 residential units are not anticipated to negatively impact the surrounding area.

Lastly, City Staff noted that the Hamilton Road and Gore Road Environmental Assessment has not yet been finished, and therefore, the roundabout shown on the site concept drawing does not reflect the footprint of the proposed roundabout, should the roundabout become the preferred option. Notwithstanding that the roundabout was taken into consideration when designing the site concept, it is noted that changes to the site design and layout may be required during the review of the Site Plan application.

4.4 Issue and Consideration #4: Zoning

The applicant has requested to rezone the lands from the existing Neighbourhood Shopping Area Special Provision (NSA4(4) Zone to a Residential Special Provision (R5-7(_)) Zone. The intent is to permit three, 3-storey stacked townhouse development consisting of 34 dwelling units.

The proposed Residential (R5-7) Zone variation provides for, and regulates, medium density residential development in the form of cluster townhouses and cluster stacked townhouses and permits a maximum height of 12.0 metres and a maximum density of 60 units per hectare (uph). Special provisions are outlined below.

A maximum front yard depth of 3.0 metres.

The intent of a front yard depth is to ensure sufficient space between buildings and the road to accommodate an appropriate site line, landscaping and in the event of future road widening while facilitating a pedestrian oriented development. In this case, the reduced front yard depth will facilitate a pedestrian oriented development by orienting the two building blocks towards the street along Hamilton Road, establishing a strong street edge. Staff are also of the opinion that the setback provides sufficient space for proper functionality, maintenance, landscaping, and road widening.

A rear yard setback of 5.0 metres and a south and east interior side yard setback of 2.5 metres

The intent of interior yard and rear yard setbacks is to provide adequate separation and mitigate potential impacts between the proposed development and adjacent properties, while also providing access to the rear and interior yards. In this case, the south interior side yard abuts an existing apartment building, while the east interior side yard and rear yard abut a hydro station and hydro corridor. Staff are of the opinion that the orientation of the building block closest the south interior side yard is sympathetic to the abutting apartment building, and that all setbacks provide adequate separation between the future development and abutting lots. Balconies are proposed to be oriented either towards the hydro corridor or Hamilton Road. Therefore, privacy concerns are not anticipated for the abutting apartment building.

A maximum encroachment of 0.3 metres to the lot line for a non-structural architectural feature and a maximum encroachment for a balcony into the front yard of 1.8 metres to the lot line

The intent of encroachment regulations is to ensure that there are no structural encroachments or overhangs which would impact on adjacent properties, and that stormwater runoff does not adversely affect the abutting lands. In this case, the increased encroachment for a non-structural feature (circled in blue) is for the purpose of providing a low wall at the corner of the front yard and south interior side yard, while the increased encroachment for the balconies (circled in green) will facilitate the private amenity of units. Staff are of the opinion that the low wall and balconies are appropriately situated for the design of the site and for the benefit of residents and are not anticipated to adversely affect abutting lands.

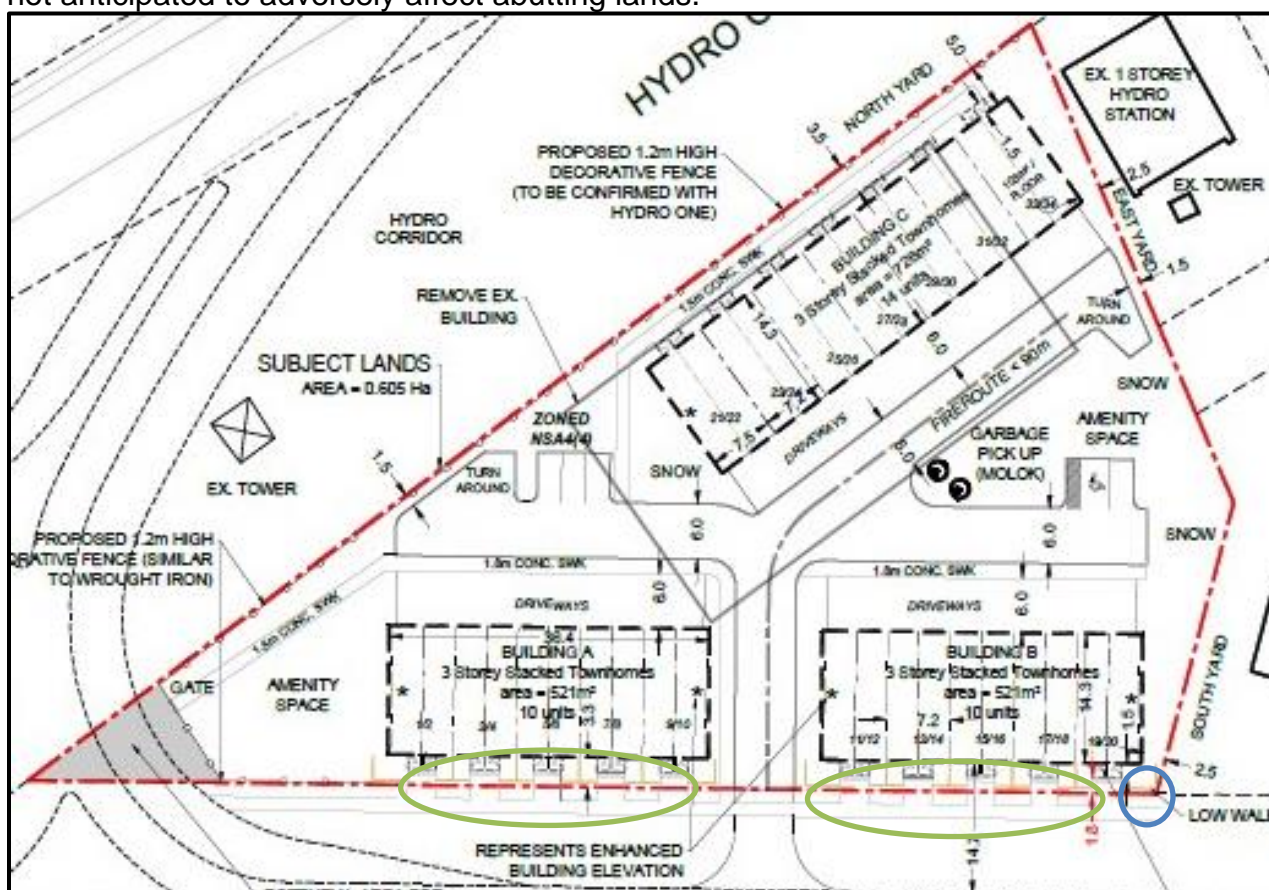


Figure 11: Site Plan denoting the two encroachment special provisions.

For these reasons, Planning and Development Staff are of the opinion that the proposed Residential Special Provision (R5-7(_)) Zone is appropriate for the site and would permit a specific development proposal at an appropriate scale and configuration that is compatible with the surrounding area.

Conclusion

The recommended amendment is consistent with the Provincial Policy Statement, 2020 and conforms to the policies of The London Plan for the Neighbourhoods Place Type. The recommended amendment would facilitate the redevelopment of an underutilized parcel and would help broaden the range and mix of housing options within the area to support the needs of current and future residents. The land use, intensity, and form are considered appropriate for the site and compatible with the surrounding context. As such, the proposed amendment is being recommended for approval.

Prepared by: **Michaella Hynes**
Planner I

Reviewed by: **Mike Corby, MCIP, RPP**
Manager, Planning Implementation

Recommended by: **Heather McNeely, MCIP, RPP**
Director, Planning and Development

Submitted by: **Scott Mathers, MPA, P. Eng**
**Deputy City Manager, Planning and Economic
Development**

Appendix A

Bill No. (number to be inserted by Clerk's Office)
2023

By-law No. Z.-1-_____

A by-law to amend By-law No. Z.-1 to
rezone an area of land located at 1154
Hamilton Road

WHEREAS Lynphyl Homes Limited has applied to rezone an area of land located at 1154 Hamilton Road, as shown on the map attached to this by-law, as set out below;

AND WHEREAS this rezoning conforms to the Official Plan;

THEREFORE the Municipal Council of The Corporation of the City of London enacts as follows:

- 1) Schedule "A" to By-law No. Z.-1 is amended by changing the zoning applicable to lands located at 1154 Hamilton Road, as shown on the attached map comprising part of Key Map No. A107, from a Neighbourhood Shopping Area Special Provision (NSA4(4)) Zone TO a Residential Special Provision (R5-7(_)) Zone.
- 2) Section Number 9.4 of the Residential (R5-7) Zone is amended by adding the following Special Provisions:

R5-7 () 1154 Hamilton Road

a) Regulations:

- i) Front Yard Depth (Minimum) 3.0 metres (9.8 feet)
- ii) Rear Yard Depth (Minimum) 5.0 metres (16.4 feet)
- iii) South Interior Side Yard Depth (Minimum) 2.5 metres (8.2 feet)
- iv) East Interior Side Yard Depth (Minimum) 2.5 metres (8.2 feet)
- v) Maximum Encroachment for a non-structural architectural feature no closer than 0.3 metres (0.98 feet) to the lot line.
- vi) Maximum Balcony Encroachment into the front yard provided the projection is no closer than 1.8 metres (5.9 feet) to the lot line.

The inclusion in this By-law of imperial measure along with metric measure is for the purpose of convenience only and the metric measure governs in case of any discrepancy between the two measures.

This By-law shall come into force and be deemed to come into force in accordance with Section 34 of the *Planning Act, R.S.O. 1990, c. P13*, either upon the date of the passage

of this by-law or as otherwise provided by the said section.

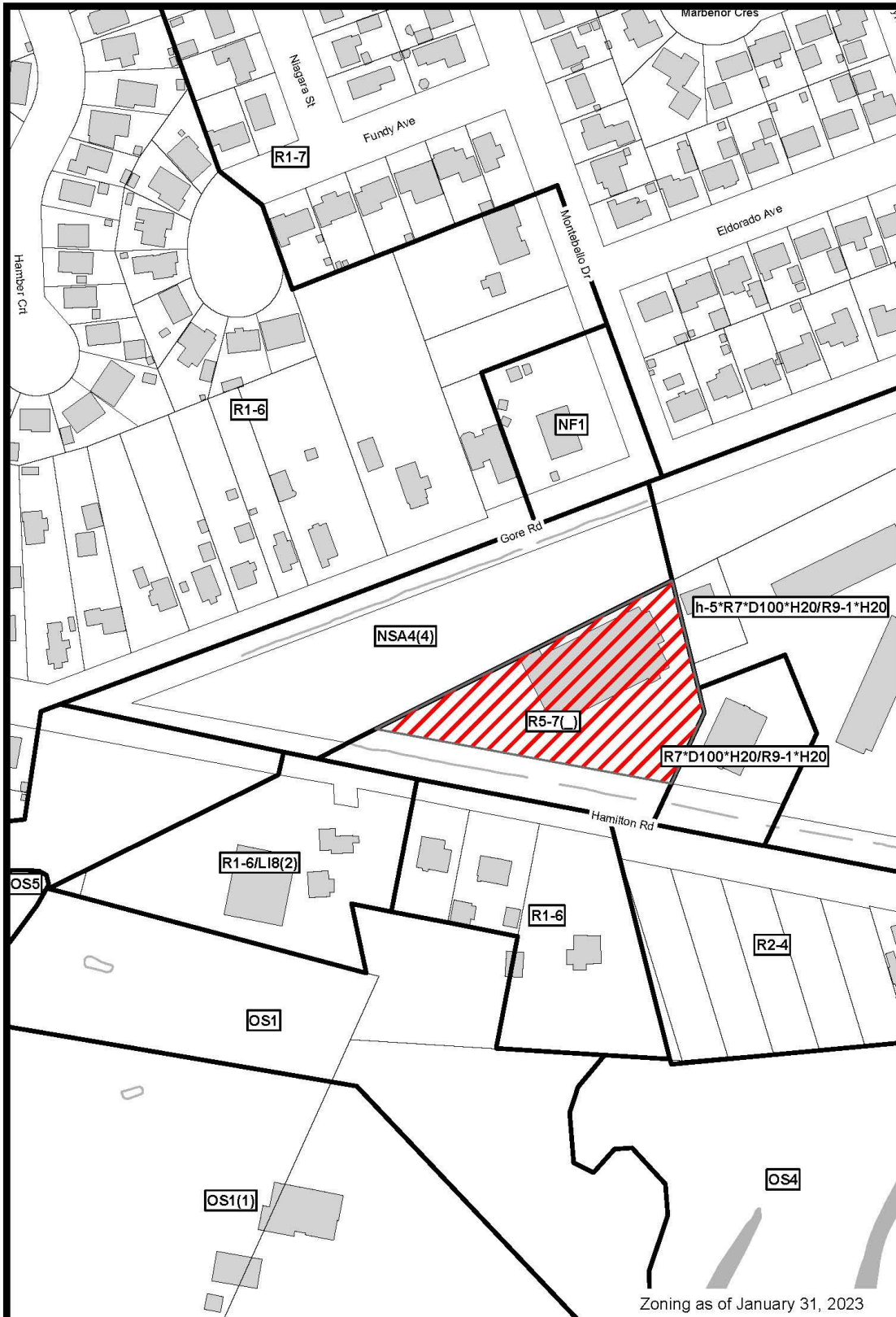
PASSED in Open Council on April 4, 2023

Josh Morgan
Mayor

Michael Schulthess
City Clerk

First Reading – April 4, 2023
Second Reading – April 4, 2023
Third Reading – April 4, 2023

AMENDMENT TO SCHEDULE "A" (BY-LAW NO. Z.-1)



Zoning as of January 31, 2023

File Number: Z-9569
 Planner: MH
 Date Prepared: 2023/2/15
 Technician: JI
 By-Law No: Z.-1-

SUBJECT SITE 

1:2,000

0 10 20 40 60 80 Meters



Appendix B – Public Engagement

Community Engagement

Public liaison: On Wednesday, November 30, 2022, Notice of Application was sent to property owners and tenants in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on Thursday, December 1, 2022. A “Planning Application” sign was also placed on the site.

Nature of Liaison: The purpose and effect of this zoning change is to permit three 3-storey stacked townhouse buildings with a total of 34 residential units and a density of 57 units per hectare. Special provisions are requested to permit a reduced front yard depth of 3.0 metres, whereas 8.0 metres is the minimum required; a reduced rear yard setback of 5.0 metres; whereas 6.0 is the minimum required; a reduced interior side yard setback of 2.5 metres, whereas 3.0 metres is the minimum required; and a reduced maximum projection of 0.3 metres into the front yard; whereas 1.5 metres is the maximum permitted. Possible change to Zoning By-law Z.-1 **FROM** a Neighbourhood Shopping Area NSA4 Special Provision (NSA4(4)) **To** Residential R5 Special Provision (R5-7(_)) Zone.

Public Responses: Replies were received from, or on behalf of 1 household.

1. Paul Walmsley

It sure will be a benefit to the neighborhood to have the derelict vacant building on the site replaced by the 34-unit townhouse buildings. 100% support from me.

Agency/Departmental Comments

November 30, 2022: London Hydro

- Servicing the above proposal should present no foreseeable problems. Any new and/or relocation of existing infrastructure will be at the applicant’s expense, maintaining safe clearance from L.H. infrastructure is mandatory. A blanket easement will be required. **Note:** Transformation lead teams are minimum 16 weeks. Contact Engineering Dept. to confirm requirements & availability.
- London Hydro has no objection to this proposal or possible official plan and/or zoning amendment. However, London Hydro will require a blanket easement.

December 8, 2022: Urban Design

- Consider an alternative building form, such as a low-rise apartment, on the site to better respond to the angular shape of the property and site constraints.
- Include direct access from the unit entrances to the proposed public sidewalk on Hamilton Road.
 - Design any proposed amenity spaces flanking this façade as open courtyards. Avoid a ‘rear-yard’ condition along the street.
- For end units that are visible from the street, provide enhanced architectural details, such as wrap-around porches, entrances and a similar number of windows, materials, and articulation as is found on the front elevation.
- The proposed buildings are highly visible from Gore Road, across the Hydro corridor.
 - Screen any parking exposed to the public street with a combination of low masonry walls (max 0.75m in height) and/or enhanced landscaping.
- Remove 4 parking spaces between Building B and C and provide a more substantial and usable amenity space.
- The turning radius for the end-unit parking space in Building A might be impeded by the proposed fence. Consider reducing one unit to provide adequate space, while increasing the amenity area.
- **Note:** Please confirm the height of the proposed fence along the Hydro One corridor

December 8, 2022: Landscape Architecture

- No comments to provide on this application. There are some boulevard trees that could be impacted by development, this can be addressed at the Site Plan Application stage.

December 16, 2022: Parks Planning

- Parkland dedication is required in the form of cash in lieu, pursuant to By-law CP-9 and will be finalized at the time of site plan approval.

December 20, 2022: UTRCA

- The subject lands **are not** affected by any regulations (Ontario Regulation 157/06) made pursuant to Section 28 of the *Conservation Authority Act*.

December 21, 2022: Ecology

- This is to confirm that there are currently no ecological planning issues related to this property and/or associated study requirements.
- No Natural Heritage Features on, or adjacent to the site have been identified on Map 5 of the London Plan or based on current aerial photo interpretation.

January 5, 2022: Engineering

Comments for the rezoning application:

- The Gore and Hamilton EA hasn't been finalized yet. We are waiting for Hydro One comments in order to confirm the preferred solution. Please note that the roundabout shown on the site concept drawing does not reflect the actual footprint of the proposed roundabout. If the roundabout becomes preferred option, it might impact the Building A location/size.

The following items are to be considered during a future site plan application stage:
Wastewater:

- The municipal sanitary sewer available for the subject lands is a 200mm diameter sanitary sewer on Hamilton Road via an existing 200mm diameter sanitary lateral at a manhole PT1119 at the existing easterly driveway access on Hamilton Rd to MH PT937.

Stormwater:

Specific comment for this site

- As per attached drawing No (1954), the site (at C=0.90) is tributary to the existing 600mm storm sewer on Hamilton Road.
- In order to service the proposed site the applicant will be required to extend the sewers on Hamilton Road to the East limit of their site; these works shall be in accordance with City Standards.
- The consulting engineer is to provide a hydraulic calculation for the 600mm storm sewer (e.g. storm sewer capacity analysis based on upstream/downstream tributary areas and run-off coefficients). to demonstrate the expected surplus capacity.
- The Developer shall be required to provide a Storm/Drainage Servicing Report demonstrating that the proper SWM practices will be applied to ensure on-site controls are designed to reduce/match existing peak flows from the 2 through 100-year return period storms.
- Any proposed LID solutions should be supported by a Geotechnical Report and/or hydrogeological investigations prepared with focus on the type of soil, it's infiltration rate, hydraulic conductivity (under field saturated conditions), and seasonal high ground water elevation. The report(s) should include geotechnical

and hydrogeological recommendations of any preferred/suitable LID solution. All LID proposals are to be in accordance with Section 6 Stormwater Management of the Design Specifications & Requirements manual. As per City of London standard, the proposed land use as a light industrial area is conducive for filtration.

- The number of proposed/existing parking spaces exceeds 29, the owner shall be required to have a consulting Professional Engineer confirming how the water quality will be addressed to the standards of the Ministry of the Environment, Conservation and Parks (MECP) with a minimum of 80% TSS removal to the satisfaction of the City Engineer. Applicable options could include, but not be limited to the use of oil/grit separators or any LID filtration/infiltration devices.
- The proposed land use of a medium density residential will trigger(s) the application of design requirements of Permanent Private Storm System (PPS) as approved by Council resolution on January 18, 2010. A standalone Operation and Maintenance manual document for the proposed SWM system is to be included as part of the system design and submitted to the City for review.
- As per 9.4.1 of The Design Specifications & Requirements Manual (DSRM), all multi-family, commercial and institutional block drainage is to be self-contained. The owner is required to provide a lot grading plan for stormwater flows and major overland flows on site and ensure that stormwater flows are self-contained on site, up to the 100-year event and safely convey the 250-year storm event.
- Additional SWM related comments will be provided upon future review of this site.

General comments for sites within Pottersburg Creek and Crumlin Drain Subwatershed

- The subject lands are located in the Pottersburg Creek and Crumlin Drain Subwatershed. The Owner shall provide a Storm/Drainage Servicing Report demonstrating compliance with the SWM criteria and environmental targets identified in the Pottersburg Creek and Crumlin Drain Subwatershed Study that may include but not be limited to, quantity/quality control (80% TSS), erosion, stream morphology, etc.
- The Owner agrees to promote the implementation of SWM Best Management Practices (BMP's) within the plan, including Low Impact Development (LID) where possible, to the satisfaction of the City Engineer.
- The owner is required to provide a lot grading plan for stormwater flows and major overland flows on site and ensure that stormwater flows are self-contained on site, up to the 100-year event and safely conveys up to the 250-year storm event, all to be designed by a Professional Engineer for review.
- The Owner shall allow for conveyance of overland flows from external drainage areas that naturally drain by topography through the subject lands.
- Stormwater run-off from the subject lands shall not cause any adverse effects to adjacent or downstream lands.
- An erosion/sediment control plan that will identify all erosion and sediment control measures for the subject site and that will be in accordance with City of London and MECP standards and requirements, all to the specification and satisfaction of the City Engineer. This plan is to include measures to be used during all phases of construction. These measures shall be identified in the Storm/Drainage Servicing Report.

Water:

- Water is currently available to the subject site via the municipal 300 mm watermain on Hamilton Road
- Due to existing structure being demolished, all existing water services (150mm CI water service to Gore Road) shall be abandoned as per City of London standards (cut and capped at the main)
- The servicing strategy for the site shall not create a regulated drinking water system.
- A water servicing report will be required addressing domestic demands, fire flows, water quality and future ownership of the development.

Transportation:

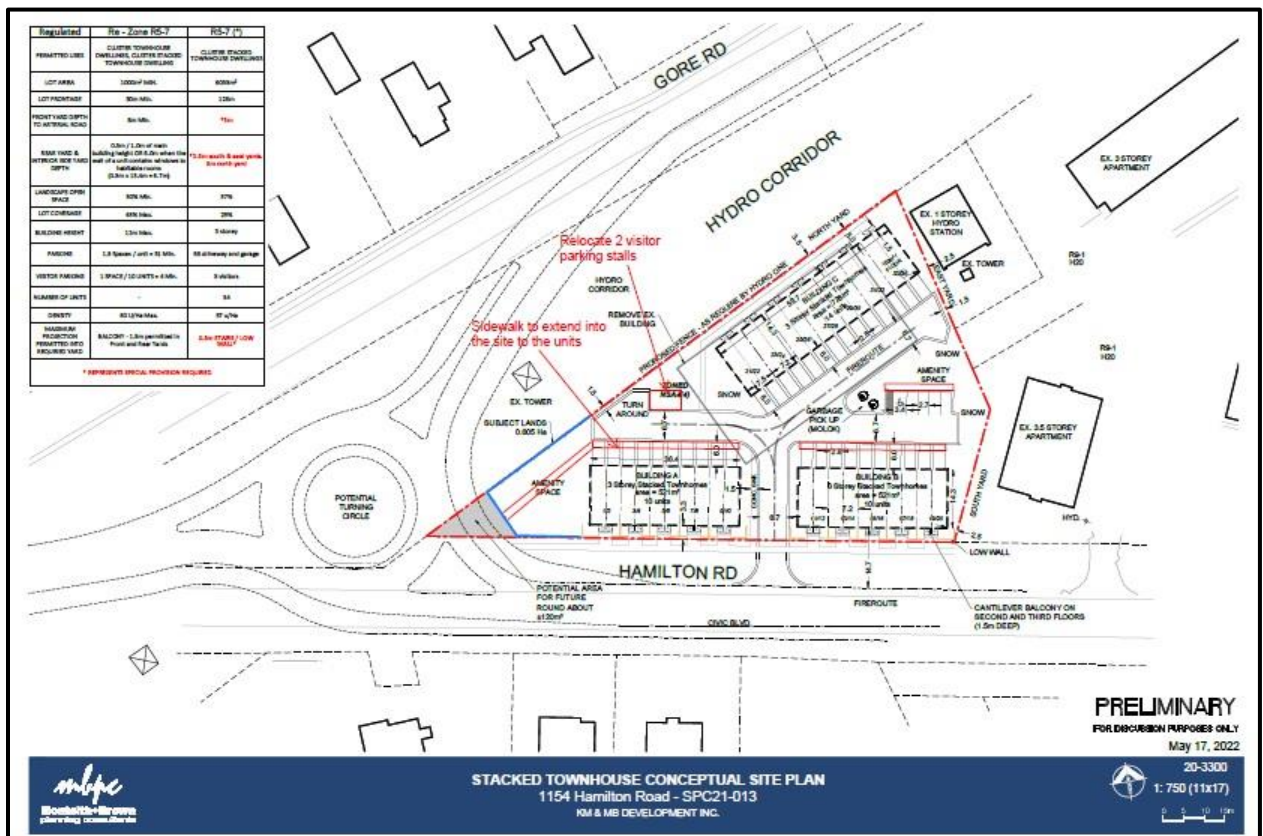
- Detailed comments regarding access design and location will be made through the site plan process.

January 5, 2022: Site Plan

- Pedestrian connections are required throughout the site. The sidewalk coming in off of Hamilton Road ends requiring pedestrians to walk along the road to meet the next sidewalk for Building C units. See attached red line drawing for pedestrian connection requirements.
- Where a driveway abuts a sidewalk, a minimum driveway length of 6.0m is to be maintained to accommodate any vehicle overhang.
- Sidewalks abutting the barrier-free parking stalls are to be a minimum of 2.1 metres to accommodate any vehicle overhang.
- The applicant is encouraged to relocate 2 of the visitor parking stalls to the north portion of the site to split up some of the visitor parking stalls and provide a larger amenity space.
- As indicated on the red-line drawing, the area in blue is to have wrought-iron fencing (or similar fencing type) with pedestrian connections and a gate to access the sidewalk connections at Hamilton and Gore Road.
 - Along the Hydro One Corridor and around the rest of the site, a board-on-board fence is preferred.
- Remove the spacing in between the driveways and provide one continuous driveway space. Typically, the small spaces are used for parking and no landscaping would survive.

For zoning/special provisions:

- A special provision is required for the proposed front yard decks/balconies on the 2nd and 3rd floors. The applicant is to provide a setback to the balconies to confirm the required special provision.



January 10, 2022: Heritage

- This memo is to confirm that I have reviewed the following and find the report’s (analysis, conclusions, and recommendations) to be sufficient to fulfill the archaeological assessment requirements for (Z-9569):

- Detritus Consulting Ltd. Stage 1 Archaeological Assessment, 1154 Hamilton [Road], London (PIF P389-0513-2020), February 3, 2021.
- Please be advised that heritage planning staff recognizes the conclusion of the report that states that: “[b]ased on the results of the Stage 1 assessment, it was determined that the entire Study Area has been subject to extensive and deep land alteration that has severely damaged the integrity of any archaeological sources [...]. Therefore, the Study Area retains no potential for the identification and recovery of archaeological resources. As such, a Stage 2 archaeological assessment is not recommended for the Study Area.” (p ii)
- An Ontario Ministry of Heritage, Sport, Tourism, and Culture Industries (MHSTCI) archaeological assessment compliance letter has also been received (without technical review), dated Aug 18, 2021 (MHSTCI Project Information Form Number P389-0513-2020, MHSTCI File Number 0013486).
- Archaeological conditions can be considered satisfied for this application.

Appendix C – Climate Emergency

On April 23, 2019, Council declared a Climate Emergency. Through this declaration the City is committed to reducing and mitigating climate change. The following are characteristics of the proposed application related to the City's climate action objectives:

Infill and Intensification

Located within the Built Area Boundary: **Yes**
Located within the Primary Transit Area: **No**
Net density change: **57 units per hectare (34 units)**
Net change in affordable housing units: **N/A**

Complete Communities

New use added to the local community: **Residential**
Proximity to the nearest public open space: **approximately 46 metres**
Proximity to the nearest commercial area/use: **approximately 700 metres**
Proximity to the nearest food store: **approximately 1,100 metres**
Proximity to nearest primary school: **approximately 680 metres**
Proximity to nearest community/recreation amenity: **approximately 512 metres**
Net change in functional on-site outdoor amenity areas: **landscaped open space of 37 percent from zero.**

Reduce Auto-dependence

Proximity to the nearest London Transit stop: **On site**
Completes gaps in the public sidewalk network: **Yes**
Connection from the site to a public sidewalk: **Yes**
Connection from the site to a multi-use pathway: **Yes**
Site layout contributes to a walkable environment: **Yes**
Proximity to nearest dedicated cycling infrastructure: **approximately 147 metres**
Secured bike parking spaces: **N/A**
Secured bike parking ratio: **N/A**
New electric vehicles charging stations: **Unknown**
Vehicle parking ratio: **68 spaces, plus visitor parking is proposed**

Environmental Impacts

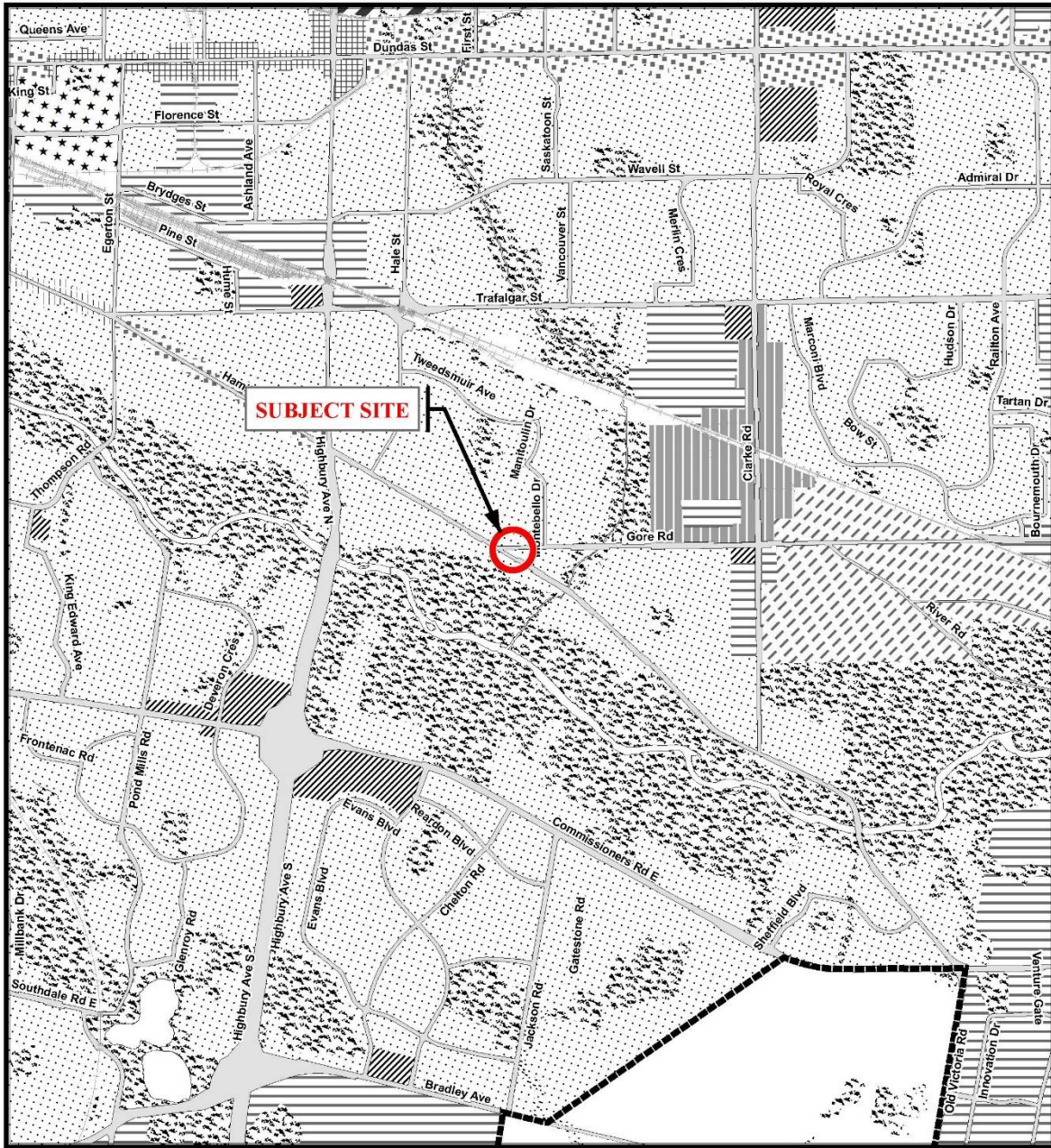
Net change in permeable surfaces: **Increased through application**
Net change in the number of trees: **Increased through application**
Tree Protection Area: **No**
Landscape Plan considers and includes native and pollinator species: **N/A**
Loss of natural heritage features: **No**
Species at Risk Habitat loss: **No**
Minimum Environmental Management Guideline buffer met (Table 5-2 EMG, 2021): **N/A**

Construction

Existing structures on site: **Yes**
Existing structures repurposed/adaptively reused: **No**
Green building features: **Unknown**
District energy system connection: **Yes**

Appendix D – Relevant Background

The London Plan – Map 1 – Place Types



Legend

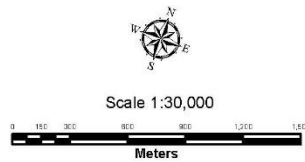
- | | | |
|------------------------|--------------------------|---|
| Downtown | Future Community Growth | Environmental Review |
| Transit Village | Heavy Industrial | Farmland |
| Shopping Area | Light Industrial | Rural Neighbourhood |
| Rapid Transit Corridor | Future Industrial Growth | Waste Management Resource Recovery Area |
| Urban Corridor | Commercial Industrial | Urban Growth Boundary |
| Main Street | Institutional | |
| Neighbourhood | Green Space | |

This is an excerpt from Planning & Development's working consolidation of Map 1 - Place Types of the London Plan, with added notations.

CITY OF LONDON
Official Plan

LONDON PLAN MAP 1
- PLACE TYPES -

PREPARED BY: Planning & Development



File Number: Z-9569
Planner: MH
Technician: JI
Date: 2023/2/15

Zoning By-law Z.-1 – Zoning Excerpt



COUNCIL APPROVED ZONING FOR THE SUBJECT SITE: NSA4(4)

1) **LEGEND FOR ZONING BY-LAW Z-1**

- | | |
|--|---|
| <ul style="list-style-type: none"> R1 - SINGLE DETACHED DWELLINGS R2 - SINGLE AND TWO UNIT DWELLINGS R3 - SINGLE TO FOUR UNIT DWELLINGS R4 - STREET TOWNHOUSE R5 - CLUSTER TOWNHOUSE R6 - CLUSTER HOUSING ALL FORMS R7 - SENIOR'S HOUSING R8 - MEDIUM DENSITY/LOW RISE APTS. R9 - MEDIUM TO HIGH DENSITY APTS. R10 - HIGH DENSITY APARTMENTS R11 - LODGING HOUSE
 DA - DOWNTOWN AREA RSA - REGIONAL SHOPPING AREA CSA - COMMUNITY SHOPPING AREA NSA - NEIGHBOURHOOD SHOPPING AREA BDC - BUSINESS DISTRICT COMMERCIAL AC - ARTERIAL COMMERCIAL HS - HIGHWAY SERVICE COMMERCIAL RSC - RESTRICTED SERVICE COMMERCIAL CC - CONVENIENCE COMMERCIAL SS - AUTOMOBILE SERVICE STATION ASA - ASSOCIATED SHOPPING AREA COMMERCIAL
 OR - OFFICE/RESIDENTIAL OC - OFFICE CONVERSION RO - RESTRICTED OFFICE OF - OFFICE | <ul style="list-style-type: none"> RF - REGIONAL FACILITY CF - COMMUNITY FACILITY NF - NEIGHBOURHOOD FACILITY HER - HERITAGE DC - DAY CARE
 OS - OPEN SPACE CR - COMMERCIAL RECREATION ER - ENVIRONMENTAL REVIEW
 OB - OFFICE BUSINESS PARK LI - LIGHT INDUSTRIAL GI - GENERAL INDUSTRIAL HI - HEAVY INDUSTRIAL EX - RESOURCE EXTRACTIVE UR - URBAN RESERVE
 AG - AGRICULTURAL AGC - AGRICULTURAL COMMERCIAL RRC - RURAL SETTLEMENT COMMERCIAL TGS - TEMPORARY GARDEN SUITE RT - RAIL TRANSPORTATION
 "h" - HOLDING SYMBOL "D" - DENSITY SYMBOL "H" - HEIGHT SYMBOL "B" - BONUS SYMBOL "T" - TEMPORARY USE SYMBOL |
|--|---|

CITY OF LONDON

PLANNING SERVICES / DEVELOPMENT SERVICES

**ZONING
BY-LAW NO. Z.-1
SCHEDULE A**



THIS MAP IS AN UNOFFICIAL EXTRACT FROM THE ZONING BY-LAW WITH ADDED NOTATIONS

FILE NO:

Z-9569

MH

MAP PREPARED:

2023/2/15

Jl

1:2,200

0 10 20 40 60 80
Meters