

## Report to Planning and Environment Committee

**To:** Chair and Members  
Planning & Environment Committee

**From:** Scott Mathers, MPA, P. Eng.,  
Deputy City Manager, Planning and Economic Development

**Subject:** Bonaventure Crossings (London) Limited (c/o Effort Trust)  
161 Bonaventure Drive  
City File: Z-9574 Ward 2  
Public Participation Meeting

**Date:** March 27, 2023

## Recommendation

That, on the recommendation of the Director, Planning and Development, the following actions be taken with respect to the application of Bonaventure Crossings (London) Limited (c/o Effort Trust) relating to the property located at 161 Bonaventure Drive.

- (a) the proposed by-law attached hereto as Appendix "A" **BE INTRODUCED** at the Municipal Council meeting on April 4, 2023 to amend Zoning By-law No. Z-1, in conformity with The London Plan for the City of London, to change the zoning of the subject property **FROM** Highway Service Commercial/Restricted Service Commercial (HS1HS4 /RSC2/RSC3/RSC4) Zone to a Residential R9 Special Provision (R9-7(\_)\*H30) Zone.
- (b) The Site Plan Approval Authority **BE REQUESTED** to consider the following through the site plan process:
  - i) Reduce the number of surface parking spaces to accommodate more amenity space;
  - ii) Remove the parking area that is adjacent to Dundas Street and address the corner through a landscape treatment and outdoor amenity space;
  - iii) Screen any surface parking exposed to the public street or residential units with enhanced landscaping, including low landscape walls, shrubs and street trees;
  - iv) Provide a centrally located and adequately sized outdoor amenity space;
  - v) Consent to remove any boundary trees is required prior to final site plan approval; and
  - vi) Differentiate the main building entrance from ground floor units. Incorporate patios or forecourt spaces that spill out into the setback to further activate the space and provide additional amenity space for residents.

## Executive Summary

### Summary of Request

The applicant has requested to rezone the subject site to permit the development of an 8-storey apartment building containing 125 units, which is equivalent to a density of 232 units per hectare.

### Purpose and Effect of Recommended Action

The purpose and effect of the recommended action is to rezone the subject site to a Residential R9 Special Provision (R9-7(\_)\*H30) Zone to permit the proposed 8-storey, 125 unit apartment building. The following special provisions would facilitate the development: a maximum residential density of 232 units per hectare, a minimum interior yard setback of 12.0 metres, a minimum rear yard setback of 16.0 metres, a minimum exterior side yard setback of 2.0 metres, a minimum parking area setback

from the north lot line of 30 metres, and no access shall be provided along Bonaventure Drive within the first 60 metres of the intersection, south of the Dundas Street frontage.

### **Rationale of Recommended Action**

1. The recommended amendment is consistent with the Provincial Policy Statement, 2020, which encourages the regeneration of settlement areas and land use patterns within settlement areas that provide for a range of uses and opportunities for intensification and redevelopment. The PPS directs municipalities to permit all forms of housing required to meet the needs of all residents, present and future.
2. The recommended amendment conforms to the in-force policies of The London Plan including, but not limited to, Key Directions and Urban Corridors Place Type and will facilitate a built form that contributes to achieving a compact, mixed-use city.
3. The recommended amendment would permit a development at an intensity that is appropriate for the site and the surrounding neighbourhood.
4. The recommended amendment facilitates the development of an underutilized property within the Built-Area Boundary through an appropriate form of infill development.

### **Linkage to the Corporate Strategic Plan**

Building a Sustainable City – London’s growth and development is well planned and sustainable over the long term.

### **Climate Emergency**

On April 23, 2019, Council declared a Climate Emergency. Through this declaration the City is committed to reducing and mitigating climate change. Refer to Appendix C for further details on the characteristics of the proposed application related to the City’s climate action objectives.

### **Analysis**

#### **1.0 Background Information**

##### **1.1 Previous Reports Related to this Matter**

None.

##### **1.2 Planning History**

None.

##### **1.3 Property Description**

The subject site is located at the southwest corner of the Bonaventure Drive and Dundas Street intersection in the Argyle Planning District. The site has an irregular shape, is 0.54 hectares in size with 19 metres of frontage along Dundas Street and 145 metres of frontage along Bonaventure. Currently, the site is undeveloped consisting of a predominately landscaped open area in the form of a maintained lawn. A London Transit bus stop and bust stop shelter is located along the Bonaventure Drive frontage.



Figure 1: 161 Bonaventure Drive, facing north on Bonaventure Drive (Google Image, June 2021)



Figure 2: 161 Bonaventure Drive, facing south on Dundas Street (Google Image, June 2015)

### 1.3 Current Planning Information

- The London Plan Place Type –Urban Corridor Place Type fronting a Neighbourhood Connector (Bonaventure Drive) and intersecting with a Civic Boulevard (Dundas Street)
- Existing Zoning – Highway Service Commercial/Restricted Service Commercial (HS1/HS4 /RSC2/RSC3/RSC4)

### 1.4 Site Characteristics

- Current Land Use – Underutilized landscaped area
- Frontage – 19.0 Metres
- Depth – 118.7 Metres
- Area – 0.445 Hectares
- Shape – Irregular

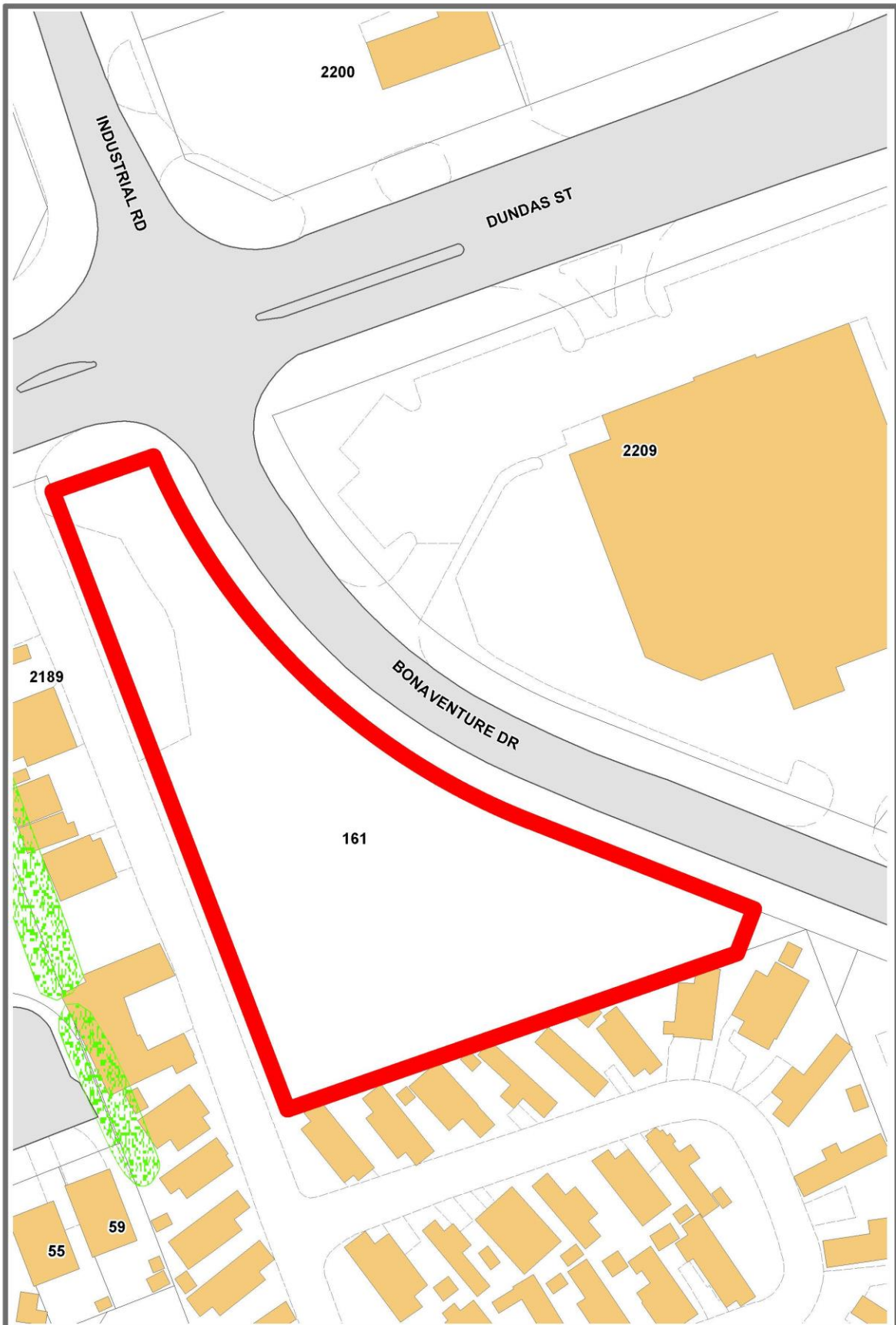
### 1.5 Surrounding Land Uses

- North – Dundas Street corridor, commercial/industrial uses
- East – Commercial, low-density residential, single detached dwellings
- South – Low Density Residential/ mobile park home dwellings
- West – Low Building and Medium Density Residential/ mobile home park dwellings/ townhouse development

## **1.6 Intensification**

The total of 125 residential units represents intensification within the Built-Area Boundary.

## 1.7 Location Map



### LOCATION MAP

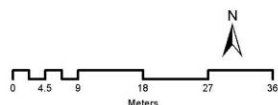
Address: 161 Bonaventure Drive

File Number: Z-9574

Planner: Olga Alchits

Date: 2023/2/13

Corporation of the City of London  
Prepared By: Planning and Development



Scale 1:4000

### Legend

-  Buildings
-  Assessment Parcels
-  Driveways/Parking Lots
-  Draft Approved Subdivisions

## 2.0 Discussion and Considerations

### 2.1 Development Proposal

In December of 2022, the City of London accepted a complete application that proposed the development of an 8-storey apartment building containing 125 units, made up of one- and two- bedroom layouts, which is equivalent to 232 units per hectare.

The design of the building positions and orients the building mass to the Bonaventure Drive Street frontage and transitions the building height from eight storeys adjacent to the street frontage to seven storeys internal to the site toward Dundas Street. Common indoor amenity space is incorporated on the ground level, whereas outdoor common area is proposed to be located in the southwest corner of the site, measuring 65 square metres in area. The Site Plan provides for 55 surface parking stalls, inclusive of 4 barrier free spaces, as well as a structured parking facility accommodating 102 underground stalls including two barrier free spaces and storage space. Two vehicular access points are proposed from Bonaventure Drive with a main drive-aisle and internal walkway providing access to the building internal to the site. The site concept plan is shown in Figure 3, and building renderings are shown in Figure 4 and 5.

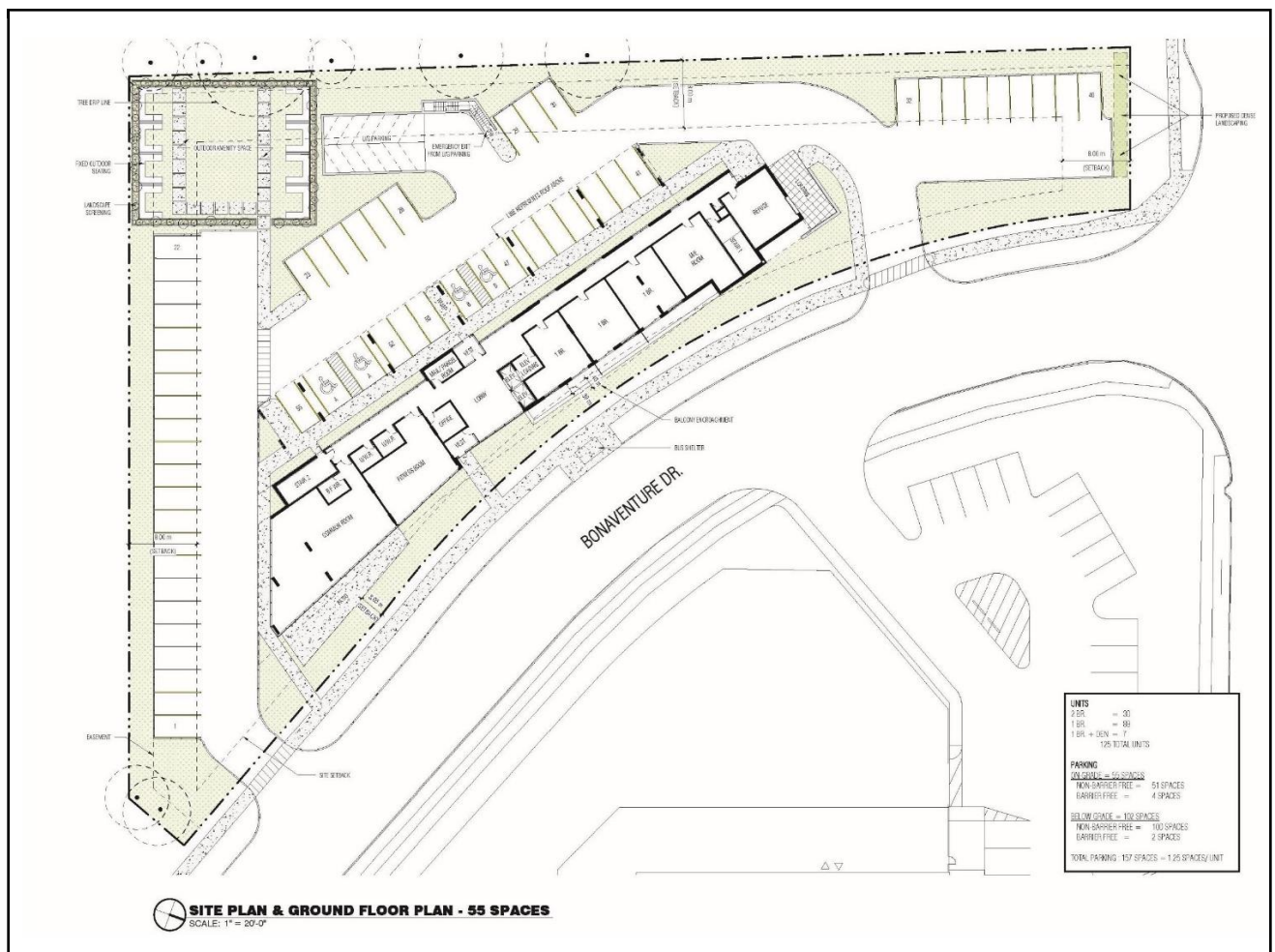


Figure 3: Site Concept



Figure 4: Rendering; view from Bonaventure Drive



Figure 5: Rendering; view from Bonaventure Drive

## 2.2 Requested Amendment

The applicant is requesting a Residential R9 Special Provision (R9-7(\_)) Zone, which permits apartment buildings. Special provisions are being requested for:

- maximum height of 30 metres;
- increased residential density of 232 units per hectare whereas 150 units per hectare is the maximum;
- a reduced interior yard setback of 12 metres, whereas 28 metres is required;
- a reduced rear yard setback of 16.0 metres, whereas 28.0 metres is required;
- a reduced minimum exterior side yard setback of 2.0 metres, whereas 9.0 metres is required; and
- to permit parking in the front yard and exterior yard.

## 2.3 Community Engagement (see more detail in Appendix B)

Members of the public were given an opportunity to provide comments on this application in response to the notice of application given on December 21, 2022.

5 emails were received from 6 members of the public:

The public's concerns generally identified the following:

- Increased traffic
- View obstruction, privacy

### **3.0 Financial Impact/Considerations**

There are no direct municipal financial expenditures associated with this application.

## **4.0 Key Issues and Considerations**

### **4.1 Issue and Consideration #1: Provincial Policy Statement, 2020 (PPS)**

#### *Provincial Policy Statement, 2020*

The Provincial Policy Statement 2020 provides policy direction on matters of provincial interest related to land use and development. The PPS encourages an appropriate affordable and market-based range and mix of residential types, including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons to meet long-term needs (1.1.1b)). The PPS also promotes the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs (1.1.1e)).

The PPS directs settlement areas to be the focus of growth and development. Land use patterns within settlement areas shall be based on densities and a mix of land uses which: efficiently use land and resources; are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion; minimize negative impacts to air quality and climate change, and promote energy efficiency; prepare for the impacts of a changing climate; support active transportation and are transit-supportive, where transit is planned, exists or may be developed (1.1.3.2). Land use patterns within settlement areas shall also be based on a range of uses and opportunities for intensification and redevelopment (1.1.3.2).

The policies of the PPS direct planning authorities to identify appropriate locations and promote opportunities for transit-supportive development, accommodating a significant supply and range of housing options through intensification and redevelopment where this can be accommodated, taking into account existing building stock or areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs (1.1.3.3).

The PPS is supportive of development standards which facilitate intensification, redevelopment and compact form (1.1.3.4). Planning authorities are further directed to permit and facilitate all housing options required to meet the social, health, economic and well-being requirements of current and future residents as well as all types of residential intensification, including additional residential units and redevelopment (1.4.3b)). Densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed, are promoted by the PPS (1.4.3d)).

Consistent with the PPS, the proposed apartment building, as recommended, will contribute to the existing range and mix of housing types in the area, which consists primarily of low density residential in the form of mobile homes, single detached dwellings, with some medium density residential uses to the west in the form of townhouse dwellings. Further, this development will provide choice and diversity in housing options for both current and future residents. The recommended amendment facilitates the development of an underutilized site within a settlement area and the



increased intensity on the site will make use of existing transit services, nearby recreational, institutional, shopping and entertainment service uses. The subject site is of a size and configuration capable of accommodating a more intensive development on an underused site. As such, the proposed Zoning By-Law amendment to permit the development of the proposed apartment building is consistent with the 2020 Provincial Policy Statement.

#### **4.2 Issues and Consideration #2: The London Plan Key Directions and Specific Policy Area – Dundas Street Transitional Segment**

The London Plan provides Key Directions (54\_) that must be considered to help the City effectively achieve its vision. These directions give focus and a clear path that will lead to the transformation of London that has been collectively envisioned for 2035. Under each key direction, a list of planning strategies is presented. These strategies serve as a foundation to the policies of the plan and will guide planning and development over the next 20 years. Relevant Key Directions are outlined below.

The London Plan provides direction to build a mixed-use compact city by:

- Planning to achieve a compact, contiguous pattern of growth – looking “inward and upward”
- Planning for infill and intensification of various types and forms to take advantage of existing services and facilities and to reduce our need to grow outward; and
- Ensure a mix of housing types within our neighbourhoods so that they are complete and support aging in place. (Key Direction #5, Directions 2, 4 and 5).

The London Plan also provides direction to building strong, healthy and attractive neighbourhoods for everyone by:

- Integrating affordable forms of housing in all neighbourhoods (Key Direction #7).
- Design complete neighbourhoods by meeting the needs of people of all ages, incomes and abilities, allowing for aging in place and accessibility to amenities, facilities and services (Key Direction #7).

The London Plan also provides direction to make wise planning decisions by:

- Plan for sustainability- balance economic, environmental, and social considerations in all planning decisions (Key Direction #8, Direction1).

The rezoning supports these Key Directions by proposing a development that achieves a form of residential intensification that builds inward and upward, resulting in compact growth and takes advantage of the existing services and facilities, specifically along Dundas Street as well as all available municipal services. Further, the proposed 8-storey, 125-unit apartment building development contributes to the mix of housing options within the neighbourhood, allowing residents to age in place while providing a more intrinsically affordable housing option in the community.

Policy 853\_ and Map 7 of The London Plan identify that the subject site is in the Dundas Street Transitional Segment and is subject to Transitional policies. These policies are intended to recognize the current development pattern along certain segments of the Corridor Place Types and guide development in these areas to allow, on a transitional basis, proposal that do not generally fulfil the long-term vision for the place types (854\_, 855\_). These policies support additional uses than what is prescribed in the Place Type, specifically, large-scale retail and service uses. As the apartment building, that is the subject of this rezoning is a permitted use within the Urban Corridor Place Type, these additional transitional policies are not relevant to the proposed rezoning.

#### **4.3 Issue and Consideration #3: Use**

The subject site is in the Urban Corridor Place Type which contemplates a range of residential, retail, service, office, cultural, recreational and institutional uses (837\_). The London Plan further directs Urban Corridors to be places that encourage intensification over the life of the Plan so that they can mature to support higher-order transit at some point in the future beyond 2035 (828\_). The proposed apartment building is a permitted

use in this location in The London Plan providing a form of intensification that helps realize the vision set out for the Urban Corridor Place Type. The proposal will provide a residential land use with convenient access to nearby goods and services in a walkable environment, and convenient access to higher order transit.

The London Plan supports the development of a variety of residential types, with varying locations, size, affordability, tenure, design and accessibility so that a broad range of housing requirements are satisfied (830.11). The recommended amendment will result in 125 residential apartment units contributing to the variety of housing options available in the area.

#### **4.4 Issue and Consideration #4: Intensity**

The London Plan provides direction to sustain, enhance and revitalize our downtown, main streets, and urban neighbourhoods to build a mixed-use, compact City (59.3). The subject site is a vacant, underutilized parcel within a notable intersection fronting a bus transit route and other amenities. The objective of The London Plan is to provide and support opportunities for the development of vacant or underutilized properties, and to strengthen the existing corridor. The proposed development supports this objective.

The London Plan places an emphasis on growing “inward and upward” to achieve a compact form of development with a greater focus on encouraging and supporting growth within the existing built-up areas of the City. The Urban Corridor Place Type policies encourage intensification along these corridors, while managing and mitigating impacts on adjacent, lower-intensity residential areas (832\_). The unique shape of the lot requires creativity when determining the location of the building and site functions in order to ensure that any impacts to neighbouring properties are mitigated. The building is proposed to be positioned close to Bonaventure Drive and central to the street frontage to provide adequate separation distance and setbacks from abutting properties as well as providing visual screening such as dense landscaped areas, landscape buffers, and private fencing between land uses.

Table 9 of the Plan further prescribes that the height in the Urban Corridor Place Type is a minimum of 2 storeys, a standard maximum of 8 storeys, and an upper Maximum of 10 storeys. The rezoning proposal is requesting an 8-storey apartment building, which is a permitted height and aligns with the standard maximum for this place type. Given the above-mentioned location of the building combined with the proposed setbacks and opportunity for landscaping, staff are satisfied the proposed intensity and scale of development is in conformity of The London Plan.

#### **4.5 Issue and Consideration #4: Form**

Within the Urban Corridor Place Type, and according to the urban design considerations the following relevant design policy criteria will be considered (841\_):

1. Buildings should be sited close to the front lot line, to create a pedestrian-oriented street along Corridors and provide appropriate setback from properties that are adjacent to the rear lot line.
2. The mass of large buildings fronting the street should be broken down and articulated at grade so that they support a pleasant and interesting pedestrian environment. Large expanses of blank wall not be permitted to front the street, and windows, entrances, and other building features that add interest and animation to the street will be encouraged.
3. Development should be designed to implement transit-oriented design principles.
4. Buildings and the public realm will be designed to be pedestrian, cycling and transit-supportive through consideration of building orientation, location of entrances, clearly marked pedestrian pathways, widened sidewalks, cycling infrastructure and general site layout that reinforces pedestrian safety and easy navigation.

The proposed site plan is consistent with The London Plan and conforms to the Urban Corridor Design policies in the following ways:

- the building is positioned and oriented to Bonaventure Drive frontage which reinforces the building line along the street and helps activate the streetscape;
- the position of the building allows for an adequate separation distance between the apartment building and residential properties adjacent to the rear and interior side yards.

Further, the design transitions the building height from eight storeys adjacent to the street frontage to seven storeys internal to the site and towards the Dundas Street frontage. A transit stop is located directly to the front of the subject site allowing for convenient access to public transit. The development also includes walkway connections to the existing sidewalk on Bonaventure Drive, which further connects to the existing sidewalk along Dundas Street. The design elements support connectivity to transit routes and supportive uses proximate to the site, placing importance on pedestrian safety and easy navigation. As such the development is in line with the vision of the Urban Corridor Place Type that supports the development of a variety of residential types, with varying locations, size, affordability, tenure, design and accessibility so that broad range of housing requirements are satisfied along these corridors.

#### **4.6 Issues and Consideration #5: Zoning**

The proposed apartment building requires special provisions to facilitate the development. The following is an analysis of the request and staff's response:

- A maximum height of 30.0 metres or 8 storeys and a density of 232 units per hectare.

The requested 8-storeys or 30.0 metres in height is considered appropriate based on Planning Staff's review of the proposal and aligns with the standard maximum height for the Urban Corridor Place Type. Staff have no concern with the density being sought as the site can accommodate the development and provides adequate parking, landscaped open space, outdoor amenity space, indoor amenity space and provide adequate separation with the abutting uses.

- Minimum Interior side yard setback of 12.0 metres and a maximum rear yard setback of 16.0 metres.

The intent of interior yard and rear yard setbacks is to provide adequate separation and mitigate potential impacts between the proposed development and adjacent properties, while also providing access to the rear and interior yards. In this case the west interior side yard setback is 12.0 metres and abuts the rear yard of a mobile park development at 2189 Dundas Street. The 12.0 metre rear yard setback is for the northwest corner of the building, the setback increases as the development moves south to the site. Amenity space is provided at the southwest corner to increase the buffering between the mobile park homes. The rear yard setback is 16.0 metres and also abuts the rear yards of the mobile homes. This setback applies to the narrower end of the building, which is also reduced to 7 storeys in height, to mitigate any impacts of the reduced setbacks in the rear yards of the abutting property. Staff are satisfied that the 12.0 metre and 16.0 metre setbacks provide adequate separation between the future apartment building and abutting lots. Privacy issues will be mitigated through the spatial separation, landscaping, tree planning and fencing.

- Minimum exterior yard setback of 2.0 metres.

In this case, because the subject site is irregularly shaped and abuts the lot line to both Bonaventure Drive and Dundas Street, the exterior yard is identified as being Bonaventure Drive, although the future apartment building fronts onto this street making Bonaventure Drive the active lot frontage. The London Plan encourages buildings to be positioned with minimal setbacks to public rights-of

ways to create a street wall/edge that provides a sense of enclosure within the public realm (259\_). The reduced exterior yard setback of 2.0 metres is appropriate for the site as it provides a minimal setback helping create a street wall/edge and activate the streetscape along Bonaventure Drive.

- Parking Area Setback from north lot line (minimum) 30 metres.

The applicant's site plan locates the surface parking lot on the north portion of the site and has requested special provisions to permit parking in the front and exterior yard. The London Plan policies direct parking facilities to minimize associated impacts on the public realm by strategically locating and screening these parking areas (272\_). Staff do not support a surface parking lot along Dundas Street and have placed a special provision in the by-law to locate the parking lot away from the street. Measures to screen the surface parking are also identified for consideration by the Site Plan Approval Authority.

- No access shall be provided along Bonaventure Drive within the first 60 metres of the intersection, south of the Dundas Street frontage.

The north access location is not supported by Transportation Staff. The preferred location for access is to be aligned with the existing access at 2209 Dundas Street or removed completely with only one access location on the southerly portion of the site. As per City's Access Management Guidelines, a private driveway cannot be located closer than 60.0 metres from the Arterial intersection. As such, a special provision has been placed in the Zoning By-law to implement this.

#### **4.7 Issues and Consideration #6: Public Concerns**

As noted in the public engagement section of this report, 5 emails were received from 6 members of the public. The public's concerns were related to the following matters:

##### *Increased Traffic*

The Transportation Department provided comment which state that the applicant is required to submit a Transportation Impact Assessment at the Site Plan Control review stage to evaluate the impact the development will have on the transportation infrastructure in the area and provide recommendations for mitigation measures.

Bonaventure Drive is identified as a Neighbourhood Connector and Dundas Street is identified as Civic Boulevard (Map 3- Street Classifications). Neighbourhood Connectors are intended to prioritize pedestrian movement and move low to medium volumes of cycle, transit and vehicle movement with an average daily traffic volume of 4,500 vehicles per day, whereas Civic Boulevards are intended to prioritize pedestrian, cycle and transit movements and move medium to high volumes of vehicular traffic with an average daily traffic volume of 22,000 vehicles per day. Additionally, Bonaventure Drive and Dundas Street are integrated into the City's cycling and walking route system as identified in Map 4- Active Mobility Network and the development is within convenient walking distance of London Transit bus stops located along Bonaventure Drive and Dundas Street. Staff are satisfied that the recommended special provision, which allows for a single access, only, to the proposed development along Bonaventure Drive, would accommodate the additional traffic of the proposed development and will not significantly affect the capacity of the local roads.

##### *View obstruction/Loss of privacy/Noise*

The applicant has made efforts to ensure that the placement, orientation and design of the new development on the site appropriately responds to surrounding land uses in a manner that reduce shadow and privacy issues.

The proposed interior and rear setbacks measuring 12.0 metres and 16.0 metres, respectively, provide an appropriate separation between the proposed residential building and existing dwellings, which will serve to mitigate the concerns of the public. In addition, through the installation of landscaping and fencing, these design features also serve to provide privacy and soft screening between the existing residential uses abutting to the west and south. Overall, the proposed plan provides for sufficient space that can accommodate enhanced, robust landscaping, which will provide the necessary building separation and space that allows for the identified screening for the adjacent residents.

## **Conclusion**

The recommended amendment is consistent with the Provincial Policy Statement, 2020 and conforms to the in-force policies of The London Plan, including but not limited to the Key Directions and the Urban Corridor Place Type. The recommended amendment will facilitate the development of an underutilized site within the Built-Area Boundary with a land use, intensity and form that is appropriate for the site.

**Prepared by:** Olga Alchits  
Planner I, Planning Implementation

**Reviewed by:** Mike Corby, MCIP, RPP  
Manager, Planning Implementation

**Recommended by:** Heather McNeely, MCIP, RPP  
Director, Planning and Development

**Submitted by:** Scott Mathers, MPA, P. Eng.  
Deputy City Manager, Planning and Economic  
Development

Copy:

Britt O'Hagan, Manager, Current Development

Michael Pease, Manager, Site Plans

Ismail Abushehada, Manager, Development Engineering

## Appendix A

Bill No. (number to be inserted by Clerk's Office)  
2023

By-law No. Z.-1-23\_\_\_\_\_

A by-law to amend By-law No. Z.-1 to rezone an area of land located at 161 Bonaventure Drive.

WHEREAS Bonaventure Crossings (London) Limited (c/o Effort Trust) has applied to rezone an area of land located at 161 Bonaventure Drive, as shown on the map attached to this by-law, as set out below;

AND WHEREAS this rezoning conforms to the Official Plan;

THEREFORE the Municipal Council of The Corporation of the City of London enacts as follows:

- 1) Schedule "A" to By-law No. Z.-1 is amended by changing the zoning applicable to lands located at 161 Bonaventure Drive, as shown on the attached map comprising part of Key Map No. A108, from a Highway Service Commercial/Restricted Service Commercial (HS1HS4 /RSC2/RSC3/RSC4) Zone to a Residential R9 Special Provision (R9-7( )\*H30) Zone
- 2) Section Number 13.4 of the Residential R9 (R9-7) Zone is amended by adding the following Special Provision:
  - ) R9-7( ) 161 Bonaventure Drive
    - a) Regulations
      - i) Density (Maximum) 232 Units per hectare
      - ii) Interior Side Yard Depth (Minimum) 12.0 metres (39.3 feet)
      - iii) Exterior Side Yard Depth (Minimum) 2.0 metres (6.5 feet)
      - iv) Rear Yard Depth (Minimum) 16.0 metres (52.4 feet)
      - v) Parking Area Setback From north lot line (Minimum) 30 metres (98.4 feet)
      - vi) No access shall be provided along Bonaventure Drive within the first 60 metres of the intersection, south of the Dundas Street Frontage.

The inclusion in this By-law of imperial measure along with metric measure is for the purpose of convenience only and the metric measure governs in case of any discrepancy between the two measures.

This By-law shall come into force and be deemed to come into force in accordance with Section 34 of the *Planning Act, R.S.O. 1990, c. P13*, either upon the date of the passage of this by-law or as otherwise provided by the said section.

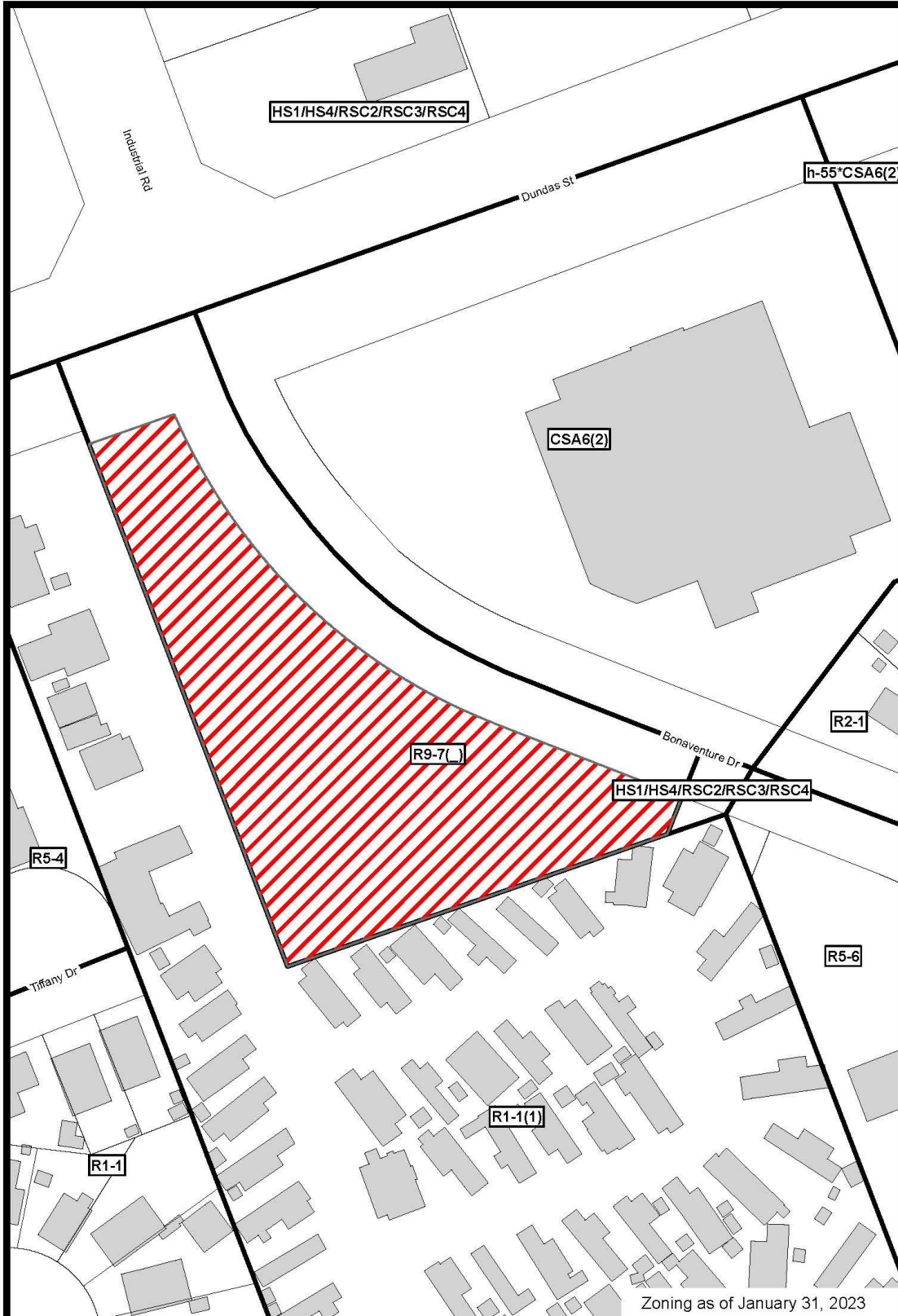
PASSED in Open Council on April 4, 2023.

Josh Morgan  
Mayor

Michael Schulthess  
City Clerk

First Reading – April 4, 2023  
Second Reading – April 4, 2023  
Third Reading – April 4, 2023

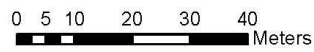
AMENDMENT TO SCHEDULE "A" (BY-LAW NO. Z-1)



File Number: Z-9574  
Planner: OA  
Date Prepared: 2023/2/13  
Technician: JI  
By-Law No: Z-1-

SUBJECT SITE 

1:1,000





## Appendix B – Public Engagement

### Community Engagement

#### Notice of Application:

**Public liaison:** On December 21, 2022, Notice of Application was sent to surrounding property owners and tenants in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on December 22, 2022. A “Planning Application” sign was also posted on the site.

5 Replies from 6 individuals were received.

#### **Nature of Liaison:**

**161 Bonaventure Drive** –The purpose and effect of this zoning change is to permit an 8-storey apartment building containing 125 units with 157 parking spaces. Possible change to Zoning By-law Z.-1 **FROM** Highway Service Commercial (HS1), (HS4), and Restricted Service Commercial (RSC2), (RSC3), (RSC4) Zones **TO** a Residential R9 Special Provision (R9-7\_) Zone to permit an 8-storey apartment building. The following special provisions are being requested: a maximum height of 30 metres, an increased residential density of 232 units per hectare whereas 150 is the maximum, a reduced minimum rear yard setback of 16.0 metres, whereas 28.0 meters is required, a reduced minimum exterior side yard setback of 2.0 metres, whereas 9.0 metres is required and to permit parking in the front yard and exterior yard. The city may consider additional special provisions for this site. File: Z-9574 Planner: O. Alchits.

**Responses:** A summary of the various comments received include the following:

#### **Concern for:**

- Increased traffic
- View obstruction, privacy

**Public Responses:** 5 (see below)

#### **Comment 1:**

We the people who live in this neighbourhood, are opposed to this proposal. This end would like a few more stores etc. This area needs no apartment buildings or anything of the kind. Opposed is not a strong enough word really.

April D.

#### **Comment 2:**

Hello,

I live in a townhouse on Bonaventure Dr.

First of all, thank you for providing us with the "Notice of Planning Application".

We have some questions in regards to the planned construction of 161 Bonaventure Dr.

- 1) How long is the construction of the building estimated to take?
- 2) Will the roadway entry to the building be from Dundas or from Bonaventure? If it will be off Bonaventure, will Bonaventure be widened to accomodate the heavier traffic? There is already an issue when the 2B bus stops at the stop (#281).
- 3) Speaking of buses, will the 2B get a turn-off area to that stop since it will obviously see heavier use with the new building?
- 4) Will this building have underground parking? There doesn't seem to be enough room for vehicles for 125 units (~ 500-900 people) if they were to all be parked beside the building.
- 5) Finally, what effect will this building have on property values in the area?

Thank you for your time.

Ewa & Justin Dawid

**Comment 3:**

I am curious if the application for the 125-apartment high rise is for a normal apartment or a London Housing Project?

Allan Lacoursiere

**Comment 4:**

Dear Mr. Lewis and Olga Alchits:

I am contacting you to voice my concerns over the proposed 8 storey apartment building at 161 Bonaventure Drive. I do understand the need for more affordable housing, however those who live in this established area and who will be most impacted, need to be considered also. I have lived here for 30 years and have a corner lot off Bonaventure Drive across from the school.

Having such high density infill will obviously create heavier traffic, noise and pollution through the neighbourhood, especially for those of us on the main artery of the subdivision. On a daily basis I witness cars speeding through the stop signs in front of the school, and hear them at night racing through the subdivision. This will only get worse with more housing density.

Noise is a huge concern for me as I enjoy being outdoors in my garden, opening my windows for fresh air. I resent the conclusion statement from akoustic engineering ltd. That people should just turn on their A/C in the summer to block out noise and hole themselves up in their apartments/homes. How is this healthy or environmentally friendly?

Those abutting this proposed massive white elephant, now also have to contend with added noise and having no privacy with 8 storeys of apartment dwellers looming over them, looking in their windows. The proposed green space is so minuscule it's an insult to any future tenant and their children. A parking lot is not a suitable space for kids to play.

I believe that town homes or at most a much smaller 3 storey apartment building is a more reasonable alternative to what is proposed currently. I know that I will be negatively impacted and I am upset about what you are planning to do to an established neighbourhood. I implore you to come to the site and hopefully see how oversized this apartment building is for the area.

A concerned homeowner.

Mona Philips

**Comment 5:**

Hello,

How do I go about contesting a zoning amendment? I have been speaking with almost all the residents near Bonaventure Dr., Simpson Cres., Moreau Cres. and even some down Carlyle Dr. and no one wants the apartments built there.

If you could provide any assistance I would greatly appreciate it.

Corbulo Olubroc

## **Departmental and Agency Comments**

### Ecology (January 11, 2023)

Confirmation that there are currently no ecological planning issues related to this property and/or associated study requirements.

### Major issues identified

- No Natural Heritage Features on, or adjacent to the site have been identified on Map 5 of the London Plan or based on current aerial photo interpretation.

### Ecology – complete application requirements

- None.

### Notes

- None.

### Engineering (January 18, 2023)

The City of London's Environmental and Engineering Services Department offers the following comments with respect to the aforementioned re-zoning application:

#### **Comments for the Re Zoning:**

- A Transportation Impact Assessment (TIA) will be required, the TIA will evaluate the impact the development will have on the transportation infrastructure in the area and provide recommendations for any mitigation measures. The TIA will need to be scoped with City staff prior to undertaking and be undertaken in general conformance with the City's TIA guidelines;
  - Vehicle turning diagrams (AutoTurn analysis) is required part of TIA for waste collection, loading, U/G parking ramp, and internal driveway aisle;
- North Access location is not acceptable to Transportation. Preferred location for access is to be aligned with the existing access at 2209 Dundas Street. As per City's Access Management Guidelines, a private driveway cannot be located closer than 60.0m from the Arterial intersection.
- 6m x 6m daylight triangle is required at the corner of Bonaventure and Dundas. This is not shown on the concept plan.

**The following items are to be considered during a future site plan application stage:**

#### **TRANSPORTATION COMMENTS:**

- A TMP is required for any work in the City ROW, including servicing, restoration, proposed access construction, etc. To be reviewed as part of a PAW submission;
- Ensure 1.5m clearance between proposed access and any hydro pole/signal poles/light standards and/or fire hydrant. Ensure 2.0m clearance for communication pedestals;
- Fully dimension access as per Access Management Guidelines, radii 6.0m, width 6.7m, and 8.0m minimum clear throat at property line;
- Ensure 6.7m wide internal drive aisle is maintained throughout development;
- Presently the width from centerline on Dundas St adjacent to this property is 18.288m as shown on Plan 33M-208. Therefore no additional widening would be required to attain 18m from centerline as per Z-1;
- Please note that a 6m x 6m daylight triangle is required at the widened limit of Bonaventure Dr and Dundas St.

#### **WATER COMMENTS**

- Water is available from the 300mm PVC watermain on Bonaventure.
- A water servicing report to be provided which includes water demand, fire protection and turnover calculations.
- Servicing to meet City of London design standards.

## **WASTEWATER COMMENTS**

- The municipal sanitary sewer available is the 200mm diameter sanitary on Dundas Street. City Plan 22378 shows “as-constructed” information related to the municipal sewer and PDC stub. Applicant to field verify size of the existing PDC; if it is 100mm diameter as city records indicate, applicant will be required to resize the PDC adequately for the proposed use.
- Area is tributary to Bonaventure Meadows Sanitary Drainage Area Plan (City plan no. 12619) Block 55 allocated 160p/ha. Applicant to provide servicing brief with maximum peak flow for the proposed development.
- Further comments may be forthcoming as part of a future application.

## **STORMWATER MANAGEMENT COMMENTS:**

### **Specific comment for this site**

- As per City as-constructed 12618 & 12625, the site at C=0.70 is tributary to the existing 375mm storm sewer on Bonaventure Drive. The applicant should be aware that any future changes to the C-value will require the applicant to demonstrate sufficient capacity in this pipe and downstream systems to service the proposed development as well as provide on-site SWM controls. On-site SWM controls design should include, but not be limited to required storage volume calculations, flow restrictor sizing, bioswales, etc.
- The number of proposed/existing parking spaces exceeds 29, the owner shall be required to have a consulting Professional Engineer confirming how the water quality will be addressed to the standards of the Ministry of the Environment, Conservation and Parks (MECP) with a minimum of 80% TSS removal to the satisfaction of the City Engineer. Applicable options are outlined in the Stormwater Design Specifications & Requirements Manual.
- The proposed land use of a high density residential triggers the application of design requirements of Permanent Private Storm System (PPS) as approved by Council resolution on January 18, 2010. A standalone Operation and Maintenance manual document for the proposed SWM system is to be included as part of the system design and submitted to the City for review.
- As per 9.4.1 of The Design Specifications & Requirements Manual (DSRM), all multi-family, commercial and institutional block drainage is to be self-contained. The owner is required to provide a lot grading plan for stormwater flows and major overland flows on site and ensure that stormwater flows are self-contained on site, up to the 100 year event and safely convey the 250 year storm event.
- Any proposed LID solutions should be supported by a Geotechnical Report and/or a Hydrogeological Assessment report prepared with a focus on the type(s) of soil present at the Site, measured infiltration rate, hydraulic conductivity (under field saturated conditions), and seasonal high groundwater elevation. Please note that the installation of monitoring wells and data loggers may be required to properly evaluate seasonal groundwater fluctuations. The report(s) should include geotechnical and hydrogeological recommendations of any preferred/suitable LID solution. All LID proposals are to be in accordance with Section 6 Stormwater Management of the Design Specifications & Requirements manual.
- This site plan may be eligible to qualify for a Stormwater Rate Reduction (up to 50% reduction) as outlined in Section 6.5.2.1 of the Design Specifications and Requirements manual. Interested applicants can find more information and an application form at the following: <http://www.london.ca/residents/Water/water-bill/Pages/Water-and-Wastewater-Rates.aspx>.
- An Operations and Maintenance manual should be provided as a separate report/manual identifying any implemented/constructed LIDs. For examples of such report contents please refer to the following website <https://cvc.ca/low-impact-development/lid-maintenance-monitoring/>

### **General comments for sites within Waubuno Subwatershed**

- The subject lands are located in the Waubuno Subwatershed and is tributary to the Crumlin Drain. The Owner shall provide a Storm/Drainage Servicing Report demonstrating compliance with the SWM criteria and environmental targets

identified in the Pottersburg Subwatershed Study that may include but not be limited to, quantity/quality control (80% TSS), erosion, stream morphology, etc.

- The Owner agrees to promote the implementation of SWM Best Management Practices (BMP's) within the plan, including Low Impact Development (LID) where possible, to the satisfaction of the City Engineer.
- The owner is required to provide a lot grading plan for stormwater flows and major overland flows on site and ensure that stormwater flows are self-contained on site, up to the 100 year event and safely conveys up to the 250 year storm event, all to be designed by a Professional Engineer for review.
- The Owner shall allow for conveyance of overland flows from external drainage areas that naturally drain by topography through the subject lands.
- Stormwater run-off from the subject lands shall not cause any adverse effects to adjacent or downstream lands.
- An erosion/sediment control plan that will identify all erosion and sediment control measures for the subject site and that will be in accordance with City of London and MECP standards and requirements, all to the specification and satisfaction of the City Engineer. This plan is to include measures to be used during all phases of construction. These measures shall be identified in the Storm/Drainage Servicing Report.

#### Heritage Planning (December 6, 2022)

This memo is to confirm that I have reviewed the following and find the report's (analysis, conclusions, and recommendations) to be sufficient to fulfill the archaeological assessment requirements for (Pre-Application Consultation- ZBA):

- Lincoln Environment Consulting Corp. *Stage 1-2 Archaeological Assessment of 161 Bonaventure Drive (...)* Middlesex County, Ontario (PIF P1289-0233-2022), April 2022.

Please be advised that heritage planning staff recognizes the conclusion of the report that states that: "no archaeological resources were identified during the Stage 2 archaeological assessment of the study area, and as such no further archaeological assessment of the property is recommended" (p 2).

An Ontario Ministry of Tourism, Culture and Sport (MTCS) archaeological assessment compliance letter has been received (without technical review), dates Apr 21, 2022 (MHSTCI Project Information Form Number P1289-0233-22, MHSTCI File Number 0016287).

Archaeological conditions can be considered satisfied for this application.

#### Landscape Architect (January 18, 2023)

1. Please advise the applicant that they must include in the Site Plan Application Documents - Proof of payment to Forestry Operations for the removal of 2 city trees growing in Bonaventure road allowance.

2. The Tree Preservation Plan has identified one boundary tree on south property line co-owned with 2189 Dundas St.. A boundary tree is the common property of the owners of the adjoining lands. Forestry Act 1998, c. 18, Sched. I, s. 21. Every person who injures or destroys a tree growing on the boundary between adjoining lands without the consent of the land owners is guilty of an offence under this Act. It is the responsibility of the developer to adhere to the Forestry Act legislation and to resolve any tree ownership issues or disputes. A letter of consent from tree's co-owner to be submitted to Development and Planning Staff at the Site Plan Application stage.

#### Site Plan (January 9, 2023)

1. A communal amenity space is required to provide quality of life elements for future residents.

2. Perimeter plantings are required in accordance with the SPCBL – Provide a separation 3.0m from parking areas to the edge of the property to allow for tree roots.
3. The pedestrian pathway to Dundas that duplicates the sidewalk should be removed.
4. Dimension all elements on plan to confirm regulations are met.
5. Reduce driveway connection to the rear to one.
6. Provide underground parking plan at application.
7. Further details on lighting, plantings, pedestrian circulation on site and snow storage are expected at application.
8. Provide more details on the approach for waste collection and set out locations for both garbage bulk bins and recycling carts.
9. The building design needs to pronounce elevation and front entrance/active ground floor.
10. While The London Plan contemplates the proposed form on sites located within the Urban Corridor Place Type, the form and site design policies of the plan must also be taken into consideration to ensure appropriate intensity. In this case, the following are design related issue that would need to be addressed/resolved by establishing the limits of development and implementing the limits with appropriate setbacks and other zoning provisions;
  - Locate the proposed building or the bulk of the massing closer to the intersection of Dundas Street & Bonaventure Drive, as this built form is intended to be located adjacent to the higher order street;
  - Orient the building to the Dundas Street frontage;
  - Provide a centrally-located and adequately-sized outdoor common amenity space;
  - The design of the site should have regard for the large boundary trees. This may require increased setbacks for the parking area in order to retain the trees and the low density residential.
11. Through the submission materials at the Zoning By-Law Application and subsequent Site Plan process the proposed building design should incorporate the following:
  - Design the building to have regard for its corner location. Building massing and articulation should address the corner of Dundas Street and Bonaventure Drive;
  - Include active ground floor uses such as: the principle building entrance, lobbies, common amenity areas, and street oriented residential units on the ground floor of street facing elevations in order to active the street edge.
  - Design any ground floor residential units located adjacent to a street frontage design their individual amenity spaces as open courtyards or front porches extending into the front setback to create a pedestrian-oriented streetscape. Provide direct walkway access from ground floor units to the public sidewalk;
  - Provide a primary entrance to the lobby on the street-facing elevation, and differentiate this entrance from the individual units through an increased proportion of glazing and appropriately scaled building mass;
  - Provide for a step-back above the 3rd or 4th stories along the street frontage in order to provide for a human-scale environment along the street;
  - Incorporate balconies/terraces along with variety of complimentary materials and textures to highlight different architectural elements (i.e. trim, framing, decorative masonry details, fenestration rhythm), and provide interest and rhythm on street facing elevations of the building to reduce the perceived elongated mass.
  - Design the building to include a high proportion of fenestration in order to add further interest and break-up the massing of the building. Increase the size & scale of proposed windows and use material change and articulation to break up the street facing facades;
  - Include all requirements of the Site Plan Control By-Law in the site design, in particular as it relates to parking (landscape islands, parking setbacks) and garbage pick-up (location). These changes may require a reduction in the number of parking spaces in order to ensure the functionality of the site

## Urban Design (First Submission – January 19, 2023)

The applicant is commended for the provision of an underground parking facility. Floor plans of the parking garage should be submitted. Include the circulation into and out of the parking garage in the primary vehicular circulation route. Further urban design comments may follow upon receipt of the plans.

The proposed built form for the ZBA related to **161 Bonaventure Drive** must be revised to receive support from Urban Design. The following Urban Design comments must be addressed:

- Orient the building to front onto Dundas Street and locate the proposed building closer to the intersection of Dundas Street & Bonaventure Drive.
  - Ensure there are zoning provisions for a maximum front yard setback from Dundas Street.
- Design the building to have regard for its corner location. Building articulation and massing should address the corner of Dundas Street and Bonaventure Drive.
  - The entrance should align with the bus stop along Bonaventure Drive and any secondary entrance oriented towards Dundas Street should include a direct pedestrian path to the higher-order right-of-way.
  - Remove the parking area that is adjacent to Dundas St and address the corner through a landscape treatment and outdoor amenity space.
- The height of the building is significantly taller than the surrounding mature neighborhood, and the location of the proposal is far from the Primary Transit Area. A lower height may be more appropriate for the site.
  - If the height is justified in this location, a zoning provision for a minimum side yard setback (west property line) should be included in the zoning to ensure the shadow impacts of the building are mitigated as much as possible.
- Provide for a step-back above the 2<sup>nd</sup> or 3<sup>rd</sup> stories along the street frontage to provide for a human-scale environment along the street. Alternatively, step down a portion of the building (eg. North half) to minimize the shadow and overlook impacts of a the long 8-storey built form.
- Provide a zoning provision for the minimum area of amenity space based on the number of residents anticipated (i.e., at grade and/or rooftop). Refer to the London Plan, Policy 295.
  - Outdoor amenity space should be located away from the parking lot, and include direct pedestrian connections to the proposed built form.
- Screen any surface parking exposed to the public street or residential units with enhanced landscaping, including low landscape walls, shrubs, and street trees. Refer to the London Plan, Policy 282.
  - Include zoning provisions for large, enhanced setbacks to accommodate trees and buffer landscape between the parking lot and the right-of-way/adjacent properties. Refer to the London Plan, Policy 224.

The following Urban Design comment should also be addressed through revisions or direction to the site plan authority:

- Include active ground floor uses such as: the principle building entrance, lobbies, common amenity areas, and street oriented residential units on the ground floor of street facing elevations to activate the street edge.
- Differentiate the main building entrance from the ground floor units. Incorporate patios or forecourt spaces that spills out into the setback to further activate the space and provide additional amenity space for residents.
  - Provide direct walkway access from ground floor units to the public sidewalk.
  - Use lockable front doors for ground floor units facing the public street to encourage walkability and access to the units from the sidewalk and to activate the streetscape.
- Incorporating a variety of materials, textures, and articulation to highlight different architectural elements and provide interest and rhythm, along the building (i.e., trim, framing, balconies, decorative masonry details, fenestration rhythm).

- Ensure that the design of the building includes high-quality durable materials.
- Provide easily accessible temporary bicycle parking facilities on-site. Refer to the London Plan, Policy 280.
- Consider providing benches, street furniture, street trees, and pet amenities on-site. Especially, next to the bus stop on Bonaventure Drive.
- Ensure that the development is “future ready”. Refer to the London Plan, Policy 729.
  - Consider including charging station for ebikes and electric vehicles within the proposed parking facilities.
  - Consider making the roof strong enough to hold solar panels and/or green roof infrastructure.
- Provide a full set of dimensioned elevations for all sides of the proposed built form, floor plans, and a rooftop plan. Include materials and colour labels. Further urban design comments may follow upon receipt of the elevations.
  - The rooftop plan and elevations should outline the placement of rooftop mechanical equipment and the proposed mechanical equipment screening. Refer to the London Plan, Policy 296.

London Hydro (January 9, 2023):

Servicing the above proposal should present no foreseeable problems. Any new and/or relocation of existing infrastructure will be at the applicant’s expense, maintaining safe clearances from L.H. infrastructure is mandatory. Note: Transformation lead times are minimum 16 weeks. Contact the Engineering Dept. to confirm requirements & availability.

Upper Thames River Conservation Authority (January 10, 2023):

The Upper Thames River Conservation Authority (UTRCA) has reviewed this application with regard for the policies within the Environmental Planning Policy Manual for the Upper Thames River Conservation Authority (June 2006), Section 28 of the *Conservation Authorities Act*, the *Planning Act*, the Provincial Policy Statement (2020), and the Upper Thames River Source Protection Area Assessment Report.

**CONSERVATION AUTHORITIES ACT**

The subject lands **are not** affected by any regulations (Ontario Regulation 157/06) made pursuant to Section 28 of the *Conservation Authorities Act*.

**DRINKING WATER SOURCE PROTECTION:** *Clean Water Act*

For policies, mapping and further information pertaining to drinking water source protection please refer to the approved Source Protection Plan at:

<https://www.sourcewaterprotection.on.ca/approved-source-protection-plan/>

**RECOMMENDATION**

The UTRCA has **no objections** or requirements for this application. Thank you for the opportunity to comment. If you have any questions, please contact the undersigned.

Yours truly,

UPPER THAMES RIVER CONSERVATION AUTHORITY

Mike Serra

Land Use Planner I

**Urban Design Peer Review Panel (UDPRP):**

- The applicant is to submit a completed “Urban Design Peer Review Panel Comments – Applicant Response” form that will be forwarded following the January 2023 UDPRP meeting. This completed form, with the revised drawings, must be submitted prior to the site plan application stage.



Urban Design Peer Review Comments and Applicants Responses (March 3, 2023)

<b>Comment:</b>
The panel commends the applicant for proposing a building with good massing and form, interesting architectural character, and a positive relationship with Bonaventure Drive. We appreciate the dynamic quality of the building facades and suggest the same attention to detail that has been put into the design of the building elevations should be applied to the site plan
<b>Applicant Response:</b>
Commented Noted.
<b>Comment:</b>
The panel recommends reducing the amount of surface parking to provide more open space, landscaping, and more meaningful outdoor amenity space. We suggest reorganizing site traffic and parking as follows: a) Reorganize underground parking to make it more efficient, to allow surface parking to be displaced if required. b) Delete the north access to Bonaventure drive (the City will not approve this access point due to its proximity to Dundas Street.) Consolidate a single driveway access, loading area, and ramp to underground parking, to the south side of the site. c) Consider integrating the underground parking ramp into the ground floor of the building. d) Locate surface parking behind buildings main façade to screen it from the street, both visually and for noise considerations. We recommend removing minimum three of the surface parking spaces proposed near the south driveway entrance to allow for adequate landscaping. e) Remove surface parking from the north side of the site. Including a parking lot along Dundas Street goes against the City's vision for future development of the street. We recommend reconsidering the north end of the site as per the comments below.
<b>Applicant Response:</b>
The proposed site layout has been development with consideration for the matters addressed above. • Parking Configuration: The parking area configuration has been designed to efficiently accommodate surface and underground stalls. No substantive changes are proposed to the parking layout. • North Access: It is proposed that the northern access would be a right-in and right-out configuration. This measure is intended to mitigate any traffic impacts that could potentially cause problems due to the proximity of the northern access to the intersection.
<b>Comment:</b>
The panel recommends moving and/or extending the building north, closer to Dundas Street to create a stronger urban edge. Consider a single loaded corridor that leads to the interior amenity space, relocated to the north side of the building, as a better solution. Retail space and outdoor patio space facing Dundas Street could also be considered. We suggest that the portion of the building extending to Dundas Street could be smaller in scale (1-2 stories) and could include a roof terrace. We recommend providing adequate setback (min 4.5M) to allow for substantial landscape treatment along the north edge of the site.
<b>Applicant Response:</b>
The request to relocate the building to the corner of the lot fronting Dundas Street, is not feasible for several reasons. Firstly, the limited setback requirements in the corner make such a move impractical. Additionally, there is a potential concern regarding that casting of shadows on the nearby trailers, which could lead to further issues and extreme proximity to the lot line. Furthermore, relocating the building to the corner of the lot would violate the guidelines set forth by MEPC per the noise and vibration study, as well as guidance

provided by our noise consultant. Based on the recommendation provided by our noise consultant in a memo, it has been advised that the construction of any residential dwelling in that corner should be avoided.

Finally, related Departmental comments from the City's Transportation and Urban Design groups present a conflict. If we were to attempt to accommodate both parties, it would likely result in circulation issues with vehicles, which could create additional problems if the building was moved to the corner of the lot.

**Comment:**

The panel notes that the proposed location for outdoor amenity space seems isolated. We recommend relocating the outdoor amenity space further to the north side of the site, ideally located adjacent to the indoor amenity space noted above and provided with

direct access to the building.

**Applicant Response:**

Suggestion will be taken into account as part of project planning to advance Site Plan Approval (SPA).

**Comment:**

The panel notes the following regarding setbacks, edge treatments, and landscape buffers:

a) Building setbacks appear to be appropriate but overhangs need to be shown and dimensioned to verify.

b) Show and dimension all building outlines, landscape setbacks, and easements. Easements generally don't allow any structures or significant landscape. Applicant to confirm. It appears the proposed outdoor amenity space is within the easement and surface and below grade parking directly abuts the easement.

c) Underground and surface parking to be set back minimum 3.0 meters to allow for adequate landscape buffer, including consideration of easements noted above. We recommend providing continuous landscaping and trees to buffer the south and west edges of the site.

d) We recommend providing a continuous tree canopy and increased common patio space with gardens and benches along Bonaventure Drive to further animate the street frontage.

e) We recommend providing direct sidewalk connections to private terraces and gardens for apartments along Bonaventure Drive.

**Applicant Response:**

Suggestions will be taken into account as part of the development of detailed landscape designs to advance the future SPA application.

**Comment:**

The approach to creating two adjoining angled volumes is appropriate for site geometry and context and has resulted in an interesting overall massing composition. However, the building appears to be top heavy. Consider 'un-topping' the building by removing the cantilevered soffit framing at the top the grey volume of the building.

**Applicant Response:**

Suggestion will be taken into account in conjunction with the development of detailed elevations to progress the SPA process.

**Comment:**

The panel suggest the drama of the cantilevered balconies shown on the south end of the building could be repeated on the north end.

**Response:**

Suggestion will be taken into account in conjunction with the development of detailed elevations to progress the SPA process.

**Comment:**

The panel notes the ground floor of the building appears to be compressed. We suggest

Upper floors of the building should be "lifted" by means such as increasing the floor to ceiling height of the ground floor, and/or reducing the height of the canopy/framing element above the ground floor.

**Response:**

Suggestion will be taken into account in conjunction with the development of detailed elevations to progress the SPA process.

## Appendix C – Climate Emergency Impact Summary

On April 23, 2019, Council declared a Climate Emergency. Through this declaration the City is committed to reducing and mitigating climate change. The following are characteristics of the proposed application related to the City's climate action objectives:

### Infill and Intensification

Located within the Built Area Boundary: Yes  
Located within the Primary Transit Area: No  
Net density change: 232 units/hectare  
Net change in affordable housing units: 0

### Complete Communities

New use added to the local community: Yes, residential  
Proximity to the nearest public open space: 300 metres  
Proximity to the nearest commercial area/use: 30 metres  
Proximity to the nearest food store: 900 metres  
Proximity to nearest primary school: Bonaventure Meadows Public School/ 290 metres  
Proximity to nearest community/recreation amenity: Argyle Arena/ 900 metres  
Net change in functional on-site outdoor amenity areas: N/a

### Reduce Auto-dependence

Proximity to the nearest London Transit stop: 0 metres  
Completes gaps in the public sidewalk network: No  
Connection from the site to a public sidewalk: Yes  
Connection from the site to a multi-use pathway: Yes  
Site layout contributes to a walkable environment: Yes  
Proximity to nearest dedicated cycling infrastructure: 0 metres  
Secured bike parking spaces: 125 spaces  
Secured bike parking ratio: 1:1  
New electric vehicles charging stations: N/A  
Vehicle parking ratio: 157 parking stalls/125 units (1.25 space per unit)

### Environmental Impacts

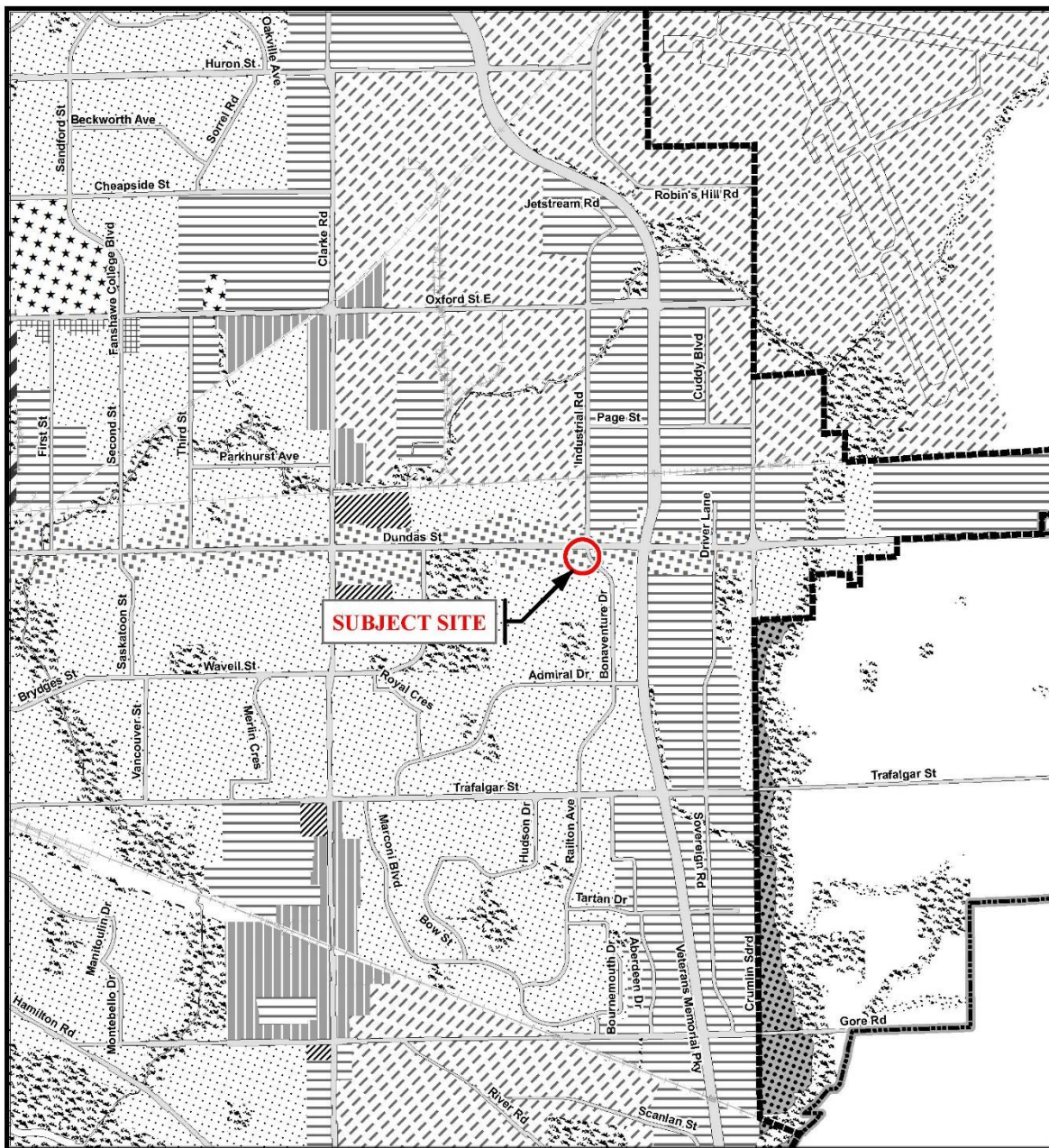
Net change in permeable surfaces: 0.53 hectares  
Net change in the number of trees: -23  
Tree Protection Area: No  
Landscape Plan considers and includes native and pollinator species: Yes  
Loss of natural heritage features: | No  
Species at Risk Habitat loss: No  
Minimum Environmental Management Guideline buffer met (Table 5-2 EMG, 2021): Yes

### Construction

Existing structures on site: No  
Existing structures repurposed/adaptively reused: N/A  
Green building features: N/A  
District energy system connection: N/A

# Appendix D – Relevant Background

## The London Plan – Map 1 – Place Types



### Legend

- |                        |                          |   |
|------------------------|--------------------------|---|
| Downtown               | Future Community Growth  | Environmental Review                    |
| Transit Village        | Heavy Industrial         | Farmland                                |
| Shopping Area          | Light Industrial         | Rural Neighbourhood                     |
| Rapid Transit Corridor | Future Industrial Growth | Waste Management Resource Recovery Area |
| Urban Corridor         | Commercial Industrial    | Urban Growth Boundary                   |
| Main Street            | Institutional            |   |
| Neighbourhood          | Green Space              |   |

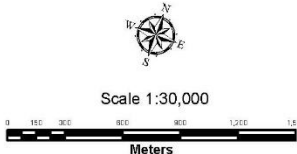
*This is an excerpt from Planning & Development's working consolidation of Map 1 - Place Types of the London Plan, with added notations.*

### CITY OF LONDON

Official Plan

LONDON PLAN MAP 1  
- PLACE TYPES -

PREPARED BY: Planning & Development



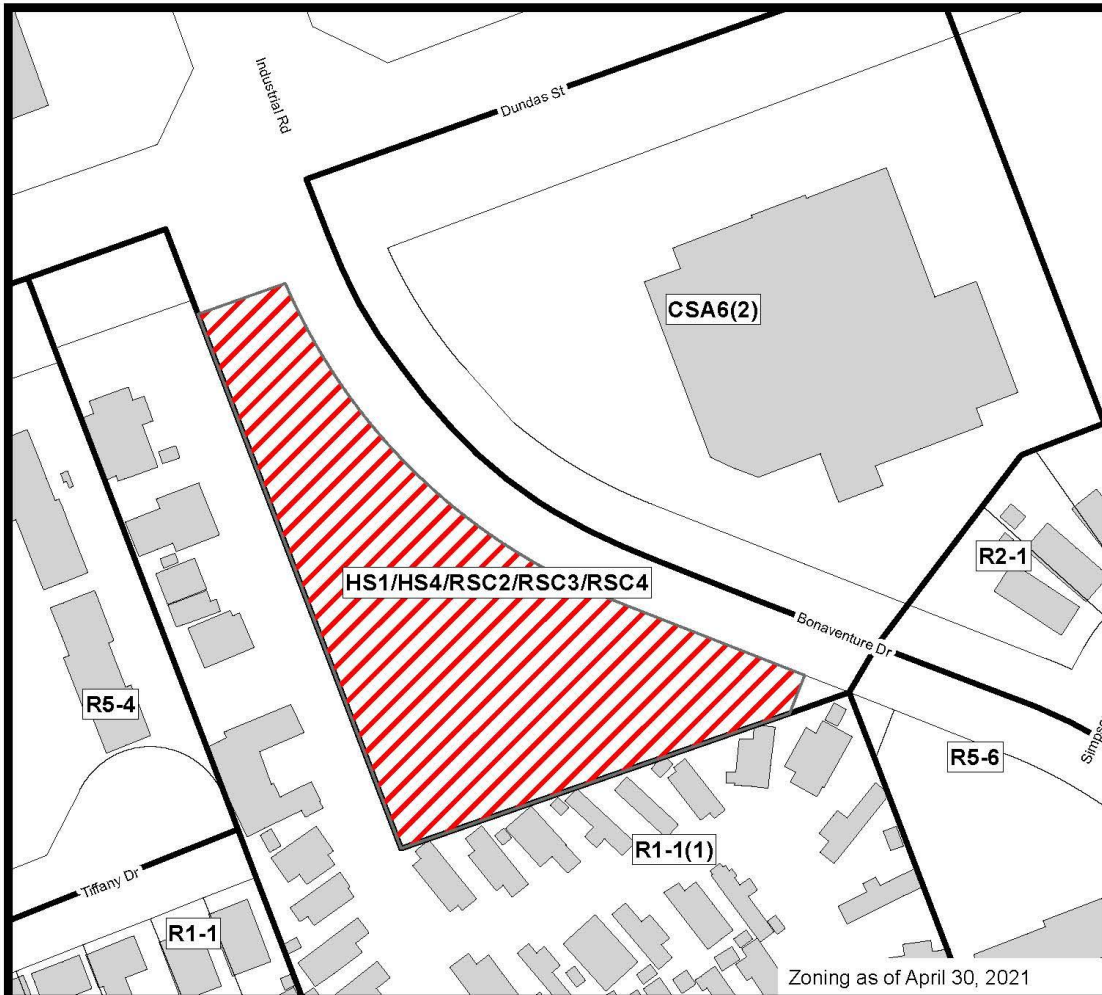
File Number: Z-9574

Planner: OA

Technician: JI

Date: 2023/2/13

# Zoning By-law Z.-1 – Zoning Excerpt



**COUNCIL APPROVED ZONING FOR THE SUBJECT SITE: HS1/HS4/RSC2/RSC3/RSC4**

## 1) LEGEND FOR ZONING BY-LAW Z-1

- |   |                                   |
|---|-----------------------------------|
| R1 - SINGLE DETACHED DWELLINGS            | RF - REGIONAL FACILITY            |
| R2 - SINGLE AND TWO UNIT DWELLINGS        | CF - COMMUNITY FACILITY           |
| R3 - SINGLE TO FOUR UNIT DWELLINGS        | NF - NEIGHBOURHOOD FACILITY       |
| R4 - STREET TOWNHOUSE                     | HER - HERITAGE                    |
| R5 - CLUSTER TOWNHOUSE                    | DC - DAY CARE                     |
| R6 - CLUSTER HOUSING ALL FORMS            |                                   |
| R7 - SENIOR'S HOUSING                     | OS - OPEN SPACE                   |
| R8 - MEDIUM DENSITY/LOW RISE APTS.        | CR - COMMERCIAL RECREATION        |
| R9 - MEDIUM TO HIGH DENSITY APTS.         | ER - ENVIRONMENTAL REVIEW         |
| R10 - HIGH DENSITY APARTMENTS             |                                   |
| R11 - LODGING HOUSE                       | OB - OFFICE BUSINESS PARK         |
|   | LI - LIGHT INDUSTRIAL             |
| DA - DOWNTOWN AREA                        | GI - GENERAL INDUSTRIAL           |
| RSA - REGIONAL SHOPPING AREA              | HI - HEAVY INDUSTRIAL             |
| CSA - COMMUNITY SHOPPING AREA             | EX - RESOURCE EXTRACTIVE          |
| NSA - NEIGHBOURHOOD SHOPPING AREA         | UR - URBAN RESERVE                |
| BDC - BUSINESS DISTRICT COMMERCIAL        |                                   |
| AC - ARTERIAL COMMERCIAL                  | AG - AGRICULTURAL                 |
| HS - HIGHWAY SERVICE COMMERCIAL           | AGC - AGRICULTURAL COMMERCIAL     |
| RSC - RESTRICTED SERVICE COMMERCIAL       | RRC - RURAL SETTLEMENT COMMERCIAL |
| CC - CONVENIENCE COMMERCIAL               | TGS - TEMPORARY GARDEN SUITE      |
| SS - AUTOMOBILE SERVICE STATION           | RT - RAIL TRANSPORTATION          |
| ASA - ASSOCIATED SHOPPING AREA COMMERCIAL |                                   |
| OR - OFFICE/RESIDENTIAL                   | "h" - HOLDING SYMBOL              |
| OC - OFFICE CONVERSION                    | "D" - DENSITY SYMBOL              |
| RO - RESTRICTED OFFICE                    | "H" - HEIGHT SYMBOL               |
| OF - OFFICE                               | "B" - BONUS SYMBOL                |
|   | "T" - TEMPORARY USE SYMBOL        |

## CITY OF LONDON

PLANNING SERVICES / DEVELOPMENT SERVICES

### ZONING BY-LAW NO. Z.-1 SCHEDULE A



THIS MAP IS AN UNOFFICIAL EXTRACT FROM THE ZONING BY-LAW WITH ADDED NOTATIONS

FILE NO:

Z-9574

OA

MAP PREPARED:

2023/2/13

JL

1:1,200

0 5 10 20 30 40 Meters