

Report to Planning and Environment Committee

To: Chair and Members
Planning & Environment Committee

From: Scott Mathers, MPA, P.Eng.,
Deputy City Manager, Planning and Economic Development

Subject: 2857082 Ontario Inc.
3510-3524 Colonel Talbot Road
Public Participation Meeting

Date: July 25, 2022

Recommendation

That, on the recommendation of the Director, Planning & Development, with respect to the application of 2857082 Ontario Inc. relating to the property located at 3510-3524 Colonel Talbot Road,

the proposed by-law attached hereto as Appendix "A" **BE INTRODUCED** at the Municipal Council meeting on August 2, 2022 to amend Zoning By-law No. Z.-1, in conformity with the Official Plan for the City of London as amended in part (a) above, to change the zoning of the subject property **FROM** an Urban Reserve (UR4) Zone, **TO** a Holding Residential R8 Special Provision (h-18*R8-4()) Zone;

IT BEING NOTED that the following urban design and site plan matters were raised during the application review process for consideration by the Site Plan Approval Authority:

- i) Encourage the applicant to return to the Panel once the development is at the detailed design and site plan submission stage;
- ii) Relocate the transformer to a less prominent location away from the street frontage along Pack Road;
- iii) Ensure parking areas visible from the street are screened in order to reduce their visual impact along both streetscapes;
- iv) Provide safe and effective direct pedestrian linkages to Colonel Talbot Road and Pack Road from the building;
- v) Provide an appropriately sized and located common outdoor amenity area for the number of units proposed;
- vi) The proposal should take into consideration any existing significant mature trees on the site and along property boundaries.
- vii) Ensure that the proposed building/built form is oriented to both Colonel Talbot Road and Pack Road and establishes a pedestrian-oriented built edge with street oriented units;
- viii) Ensure the building is appropriately scaled and located on the site to provide visual interest and enclose the street;
- ix) Extend the building façade along the perimeter of both Colonel Talbot Road and Pack Road to have a more efficient use of land and foster an enclosed pedestrian-oriented streetscape;
- x) Ensure that the proposed building has regard for its corner location. The massing/ articulation or other architectural features should emphasize the intersection;
- xi) Ensure development is designed in a main street format with buildings at the street edge with high proportions of vision glazing and principal entrances oriented to the street;
- xii) Locate the principal residential building entrance (lobby) at the intersection of Colonel Talbot Road and Pack Road or an alternative location close to intersection along either of the public streets. Differentiate the residential lobby entrance from the commercial unit entrances with architectural features such as canopies, signage, lighting, increase in glazing, double doors, framing, materials, etc.;

- xiii) Commercial units proposed along Colonel Talbot Road should be designed with a human-scale rhythm and include a store-front design with high proportion of vision glass, appropriately scaled signage, canopies and lighting, double doors, and an increased ground floor height;
- xiv) Ensure the top of the building is designed and distinguished through an articulated roof form, step-backs, cornices, material change and/or other architectural details and explore opportunities to screen/integrate the mechanical and elevator penthouses into an architecture of the building;
- xv) Setback for parking needs to be sufficient to allow for tree plantings.

Executive Summary

Summary of Request

The owner has requested an amendment to Zoning By-law Z.-1 to change the zoning of the subject lands to permit the construction of a mixed-use (commercial/office and residential) apartment building with a maximum height of four (4) storeys, 435 square metres of commercial/office space on the first floor, and a maximum mixed-use density of 87 units per hectare. The requested Holding Residential R8 Special Provision (h-18*R8-4(□)) Zone would permit apartment buildings, handicapped person's apartment buildings, lodging house class 2, stacked townhouses, senior citizen apartment buildings, emergency care establishments, and continuum-of-care facilities.

The requested zoning special provision would permit: bake shop, commercial recreation establishment, convenience store, food stores, office - business, office – service, office – professional, personal service establishments, pharmacy, retail store, and restaurant as additional permitted uses restricted to the first floor, provide for a maximum of 435 square metres of total gross floor area for commercial/office uses; and permit a reduced minimum front and exterior side yard depth of 1.0 metres, whereas 8 metres is required; a maximum height of 15.0 metres, whereas 13.0 metres maximum is required; a density of 87 units per hectare, whereas 75 units per hectare maximum is permitted; and 60 parking spaces maximum, whereas 75 parking spaces are required.

Purpose and Effect of Recommended Action

The purpose and effect of the recommended action is to permit the development of a mixed-use four (4) storey, 37-unit apartment building with a maximum of 60 surface parking spaces. Special provisions establishing a minimum front and exterior side yard depth of 1.0 metres; a maximum height of 15.0 metres (or 4 storeys); a mixed-use density of 87 units per hectare; and non-residential uses shall be located on the ground floor and shall occupy no more than 435 square metres of total gross flood area. The recommendation also includes site design matters that were raised during the application review process.

Rationale of Recommended Action

1. The recommended Zoning By-law Amendment is consistent with the Provincial Policy Statement, 2020, which encourages development to occur within settlement areas and land use patterns that provide for a range of uses and opportunities that will meet the needs of current and future residents;
2. The recommended zoning conforms to the in-force policies of *The London Plan*, including, but not limited to, the Neighbourhood Place Type, City Building and Design, Our Tools, and all other applicable *London Plan* policies;
3. The recommended amendment conforms to the policies of the Southwest Area Secondary Plan, Schedule 9, North Lambeth Residential Neighbourhood.
4. The recommended amendment would permit development at an intensity that is appropriate for the site and the surrounding neighbourhood.

Linkage to the Corporate Strategic Plan

Building a Sustainable City – London’s growth and development is well planned and sustainable over the long term.

Climate Emergency

On April 23, 2019, Council declared a Climate Emergency. Through this declaration the City is committed to reducing and mitigating climate change by encouraging intensification and growth at appropriate locations. This includes efficient use of existing urban lands and infrastructure. It also includes aligning land use planning with transportation planning to facilitate transit-supportive developments and encourage active transportation

Analysis

1.0 Background Information

1.1 Previous Reports Related to this Matter

None.

1.2 Property Description

The subject site is comprised of two lots located at the southeast corner of the intersection of Colonel Talbot Road and Pack Road. The site has a frontage of 86 metres along Colonel Talbot Road, a depth of 56 metres along Pack Road, and a total lot area of 4874 square metres. The site currently consists of two single detached residential dwellings and a detached accessory structure that would be demolished and removed. The remaining lands are generally flat and consist of manicured lawn.

Colonel Talbot Road is an arterial road/civic boulevard with an average annual daily traffic volume of 13,000 vehicles per day. Pack Road is also an arterial road/civic boulevard with an average daily traffic volume of 2,500 vehicles per day. The intersection is signalized with dedicated left turn lanes on Colonel Talbot Road in both directions. Public sidewalks are currently available along the west side of Colonel Talbot Road and on the north side of Pack Road.

The surrounding neighbourhoods consist primarily of low density residential/single detached housing, open space, and future residential development. The subject property is also directly adjacent to a plan of subdivision (39T-16509) that is planned for residential development to the east, and a registered plan of subdivision (39T-14504) that is currently under construction to the west.



Figure 1 – Street view of 3510 Colonel Talbot Road (view from corner of Colonel Talbot Road and Pack Road)

Figure 2 – Street view of 3524 Colonel Talbot Road (view from Colonel Talbot Road)

1.3 Current Planning Information (see more detail in Appendix E)

- 1989 Official Plan Designation – Multi-Family, Medium Density Residential
- The London Plan Place Type – Neighbourhoods Place Type fronting two Civic Boulevards
- Southwest Area Secondary Plan (North Lambeth Residential Neighbourhood) – Medium Density Residential
- Existing Zoning – Urban Reserve (UR4) Zone

1.4 Site Characteristics

- Current Land Use – two single detached dwellings
- Frontage – 86 metres (Colonel Talbot Road)
- Depth – 56 metres
- Area – 4874 square metres
- Shape – rectangular/irregular

1.5 Location Map



1.6 Surrounding Land Uses

- North – SWM/residential
- East – existing/future residential
- South – existing/future residential
- West – future commercial/residential

2.0 Discussion and Considerations

2.1 Development Proposal

In March 2022, the applicant submitted a zoning by-law amendment application for a four (4) storey, mixed use apartment building with 37 residential units and 430 square metres of commercial/office space on the ground floor, with a total of 63 parking spaces, to be provided through surface parking, at a mixed-use density of 87 units per hectare. The concept is shown in Figure 3. Building rendering and elevations are shown in Figures 4, 5, 6 and 7.

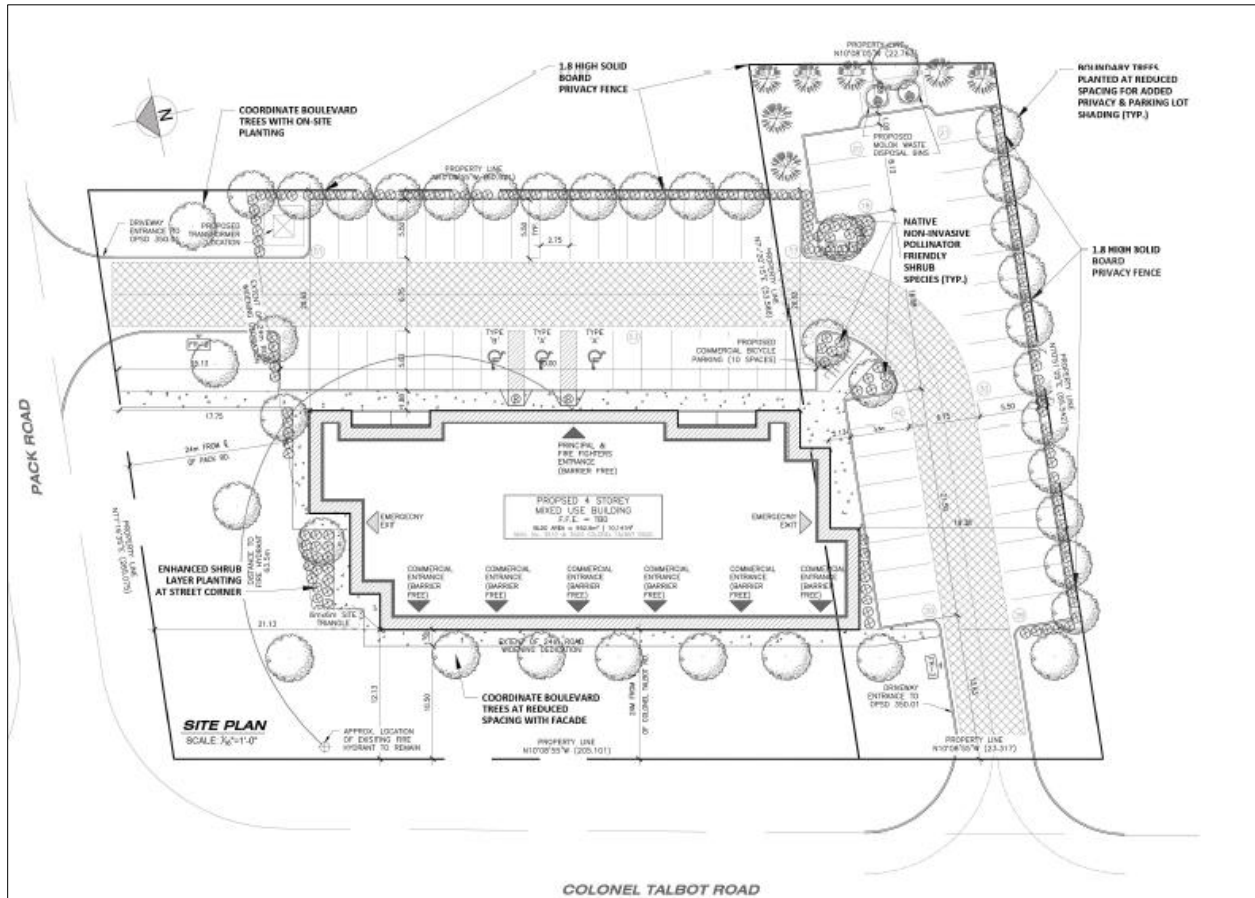


Figure 3 – Original Site Concept



Figure 4 - Building Rendering looking southeast from Colonel Talbot Road



Figure 5 - West Elevation along Colonel Talbot Road



Figure 6 - East elevation



Figure 7 - South and North elevation

Based on comments from staff, the applicant submitted a revised site concept plan, shown as Figure 8. This revised plan includes a small increase in the commercial/office area (from 430 square metres to 435 square metres), front and exterior yard setback from Colonel Talbot and Pack Road of 1.0m (previous concept showed 0 m setback), increased parking area setbacks from the south (3.0m), a 1.5m parking area setback to the east property line (previously 0 metres), 60 parking spaces, and additional amenity area.

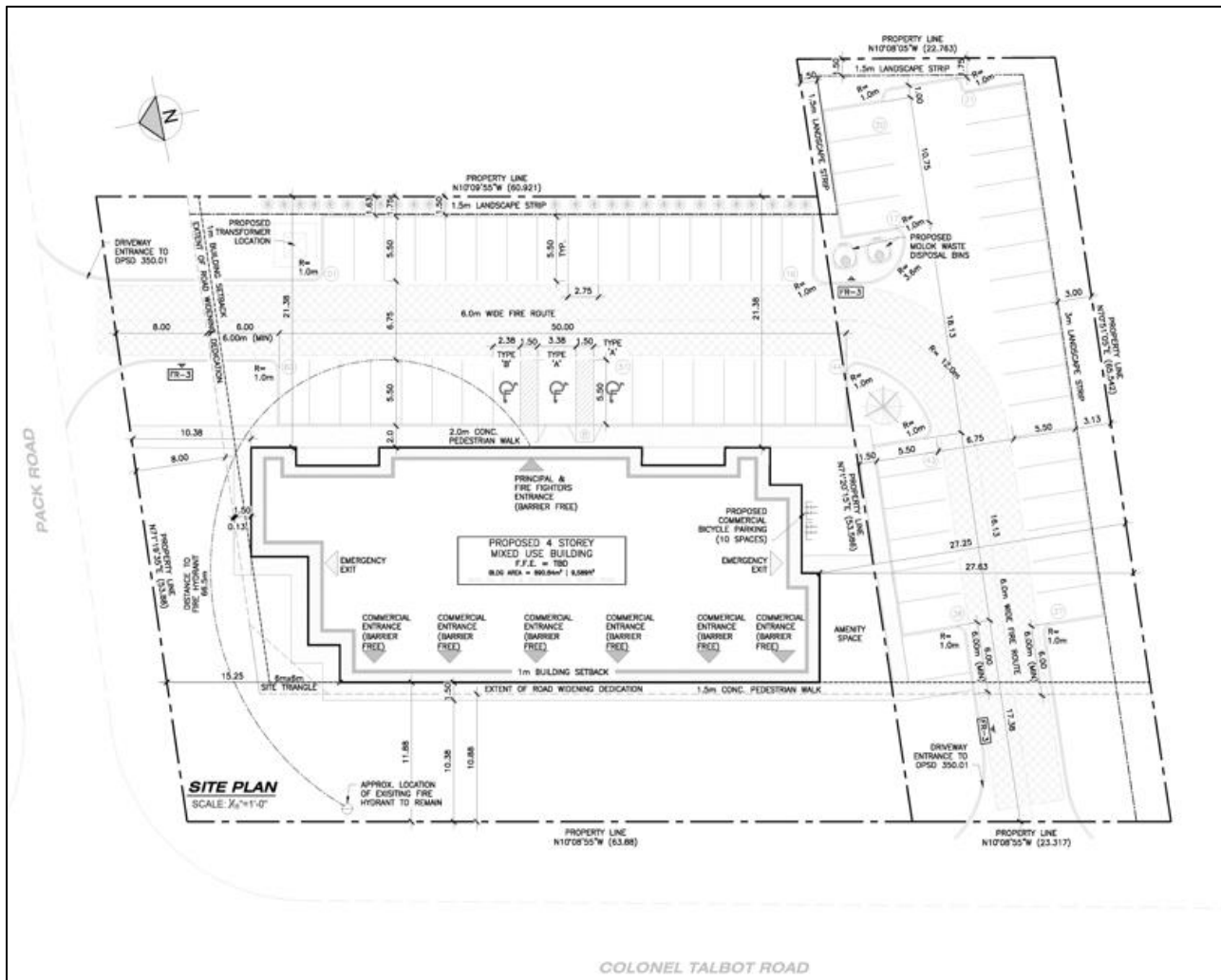


Figure 8 - Revised Concept Plan (June 2022)

2.5 Requested Amendment

The applicant is requesting a Residential R8 Special Provision (R8-4(_)) Zone, with special provisions as follows:

- additional permitted commercial uses limited to the first floor, up to a maximum of 435 square metres of ground floor space;
- a maximum mixed-use density of 87 units per hectares, whereas 75 units per hectare maximum is permitted;
- a maximum height of approximately 15.0 metres, whereas 13.0 metres maximum is required (up to a maximum of 4 storeys);
- a reduced minimum front and exterior side yard depth of 1.0 metres, whereas 8 metres is required; and
- 60 parking spaces maximum, whereas 75 parking spaces are required.

2.6 Community Engagement (see more detail in Appendix C)

Written responses were received from, or on behalf of, 1 household (two individuals).

The public's concerns generally dealt with the following matters:

- The development not being appropriate for the area
- Privacy
- Construction impacts
- Traffic
- Noise
- Impact on property values
- Environmental impacts/bird impact
- Grading concerns/stormwater

2.4 Policy Context (see more detail in Appendix D)

Provincial Policy Statement, 2020

The Provincial Policy Statement (PPS), 2020 provides policy direction on matters of provincial interest related to land use planning and development. In accordance with Section 3 of the Planning Act, all planning decisions “shall be consistent with” the PPS.

Section 1.1 of the PPS encourages healthy, livable and safe communities which are sustained by promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term. The PPS directs settlement areas to be the focus of growth and development, further stating that the vitality and regeneration of settlement areas is critical to the long-term economic prosperity of our communities (1.1.3). As well, the PPS directs planning authorities to provide for an appropriate range and mix of housing options and densities required to meet projected requirements of current and future residents of the regional market area (1.4.1).

The London Plan

The London Plan provides Key Directions (54_) that must be considered to help the City effectively achieve its vision. These directions give focus and a clear path that will lead to the transformation of London that has been collectively envisioned for 2035. Under each key direction, a list of planning strategies is presented. These strategies serve as a foundation to the policies of the plan and will guide planning and development over the next 20 years. Relevant Key Directions are outlined below.

The London Plan provides direction to build a mixed-use compact city by:

- Implementing a city structure plan that focuses high-intensity, mixed-use development at strategic locations;
- Planning to achieve a compact, contiguous pattern of growth – looking “inward and upward”;
- Planning for infill and intensification of various types and forms to take advantage of existing services and facilities and to reduce our need to grow outward; and,
- Ensure a mix of housing types within our neighbourhoods so that they are complete and support aging in place. (Key Direction #5, Directions 1, 2, 4 and 5).

The London Plan also provides direction to build strong, healthy and attractive neighbourhoods for everyone by:

- Integrating affordable forms of housing in all neighbourhoods (Key Direction #7, Direction 10).

Lastly, The London Plan provides direction to make wise planning decisions by:

- Planning for sustainability – balance economic, environmental, and social considerations in all planning decisions. (Key Direction #8, Direction 1).

Policy 916_3 of the Neighbourhoods Place Type identifies key elements for achieving the vision for neighbourhoods, which includes a diversity of housing choices allowing for affordability and giving people the opportunity to remain in their neighbourhoods as they age if they choose to do so. Furthermore, policy 918_2 states that neighbourhoods will be planned for diversity and mix and should avoid the broad segregation of different housing types, intensities, and forms.

1989 Official Plan

The subject site is designated Multi-Family, Medium Density Residential in accordance with Schedule ‘A’ of the 1989 Official Plan. The Multi-Family, Medium Density Residential designation permits multiple-attached dwellings, such as row houses or cluster houses; low-rise apartment buildings; rooming and boarding houses; emergency care facilities; converted dwellings; and small-scale nursing homes, rest homes and homes for the aged. Development shall be subject to height limitations in the Zoning By-law which are sensitive to the scale of development in the surrounding neighbourhood. Normally height limitations will not exceed four (4) storeys.

Southwest Area Secondary Plan

The subject site is located within the Southwest Area Secondary Plan North Lambeth Residential Neighbourhood. The North Lambeth Neighbourhood will provide for residential development of an intensity that is generally higher than achieved in other areas of the city but, is less than the intensity of the Bostwick Neighbourhood. The focus for new development is to be on a mix of low to mid-rise housing forms, ranging from single detached dwellings to low rise apartment buildings within individual subdivisions and throughout the neighbourhood (20.5.10 i)). The residential areas will develop as traditional suburban neighbourhoods, with characteristics similar to those found in the older areas of the city, reflecting a compact development, a diversity of building types, and walkable amenities to enhance the day to day living experience (20.5.10.ii)).

Generally, the objective of the Southwest Area Secondary Plan is to provide for a range of land uses including residential, open space, public, commercial, office and mixed-uses and community facilities. Mixed use developments are generally permitted within the Southwest Secondary Planning Area.

3.0 Financial Impact/Considerations

There are no direct municipal financial expenditures associated with this application.

4.0 Key Issues and Considerations

4.1 Issue and Consideration #1: Use

Provincial Policy Statement, 2020

The PPS encourages an appropriate affordable and market-based range and mix of residential types, including single-detached dwellings, additional residential units, multi-unit housing, affordable housing and housing for older persons to meet long-term needs (1.1.1b)). Land use patterns within settlement areas shall be based on densities and a mix of land uses which: efficiently use land and resources; are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion; minimize negative impacts to air quality and climate change, and promote energy efficiency; prepare for the impacts of a changing climate; support active transportation and are transit-supportive, where transit is planned, exists or may be developed (1.1.3.2).

The subject site is in the Neighbourhoods Place Type at the intersection of two Civic Boulevards (Colonel Talbot and Pack Road), as identified on Map 1 – Place Types and Map 3 – Street Classifications. Permitted uses within the Neighbourhoods Place Type at this location include a range of residential uses, such as stacked townhouses, fourplexes, and low-rise apartments. Secondary permitted uses at this location also include mixed-use buildings. (Table 10 – Range of Permitted Uses in Neighbourhoods Place Type). Service uses identified include neighbourhood-oriented services such as, but not limited to, personal services, restaurants, small-scale recreational uses, and public services (926_). The minimum permitted height is two (2) storeys, and the maximum permitted height is four (4) storeys. (Table 11 – Range of Permitted Heights in Neighbourhoods Place Type).

The subject site is located with the Medium Density Residential designation in the Southwest Area Secondary Plan (SWAP). New development within the North Lambeth Neighbourhood will focus on a mix of low to mid-rise housing forms, ranging from single detached dwellings to low rise apartment buildings. In addition to residential development, a limited range of convenience and personal service commercial uses, small-scale eat-in restaurants, civic and institutional uses such as parks, schools and churches, and live-work uses may also be permitted.

The subject site is designated Multi-Family, Medium Density Residential (MDR) in

accordance with Schedule 'A' of the 1989 Official Plan. The MDR designation contemplates multiple-attached dwellings, such as low-rise apartment buildings. Where there is a conflict or inconsistency between the parent policies or maps of The London Plan and/or the 1989 Official Plan and the policies or maps of a secondary plan, the secondary plan policies or maps will prevail in accordance with policy 1558_of The London Plan and policy 19.2.1iii) of the 1989 Official Plan.

Consistent with the PPS, The London Plan, and the Southwest Area Secondary Plan, the recommended mixed use low-rise apartment building will contribute to the existing range and mix of housing types in the area, which consists of mostly one and two-storey single detached dwellings to the north and west. The recommended zoning would permit medium density residential development in the form of a low-rise mixed-use apartment building containing 37 residential units and 435 square metres of commercial/office space on the ground floor, with a total of 60 parking spaces, at a mixed-use density of 87 units per hectare, and at a maximum height of four (4) storeys (15 meters/49.2 feet) through the recommended Special Provision Zone. Development of mixed-use forms with small-scale commercial or retail uses on the main floor and residential development above, is generally encouraged in the SWAP. The proposed apartment development would contribute to a mix of housing choices in a compact form and is street oriented, which also contributes to an active street front along both Colonel Talbot Road and Pack Road, creating a safe pedestrian environment that promotes connectivity. The recommended zoning is considered appropriate and provides a range of uses and dwelling types to the area.

4.2 Issue and Consideration #2: Intensity

The PPS is supportive of development standards which facilitate intensification, redevelopment and compact form (1.1.3.4). Densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed, are promoted by the PPS (1.4.3d)).

The London Plan uses height as a measure of intensity in the Neighbourhoods Place Type. A minimum height of 2 storeys and a maximum height 4 storeys, is contemplated within the Neighbourhoods Place Type where a property has frontage on a Civic Boulevard. (Table 11 – Range of Permitted Heights in the Neighbourhoods Place Type). The intensity of development must be appropriate for the size of the lot (953_3.).

The SWAP also permits medium-density development with a maximum height of four (4) storeys and at a maximum density of 75 units per hectare. These policies are similar to the policies contained within the 1989 Official Plan.

The requested height of four (4) storeys is in keeping with the policies of The London Plan, but not the permitted density identified in the SWAP. Although the density is greater than what is permitted, the proposed height is considered appropriate for this location. The subject lands are of a size and configuration capable of accommodating a more intensive redevelopment.

More specifically, the proposed development of 37 new apartment units and 435 square metres of commercial/office space on the ground floor equates to 87 units per hectare. This is in keeping with the special policy within the SWAP which applies to these lands located on the east side of Colonel Talbot Road. Policy 20.5.4.1 iii) Residential Development Intensity Adjacent to Arterial Roads permits development at a minimum density of 30 units per hectare and a maximum density of 100 units per hectare. Building heights shall be a minimum of two storeys and a maximum of nine storeys. This policy is intended to focus intense, medium density housing forms along transit-oriented corridors. This would also support alternative modes of transportation, such as walking and bicycling.

The recommended base Residential R8 Special Provision (R8-4(*)) Zone includes provisions for additional density, height, and zoning considerations to implement the above policy, including parking facilities designed to minimize the visual impact, providing for enhanced amenity and recreation areas for the residents of the

development through rooftop patios, locating the building close to the street and ensuring a street-oriented building with functional front and main entrances to the building facing the street, and providing for safe and accessible pedestrian connections for the public and an enhanced pedestrian environment.

Floor area limits for retail, services and office uses are shown on Table 12 - Retail, Service and Office Floor Area Permitted in Neighbourhoods Place Type. The proposed 435m² of ground floor commercial/office uses are considered minor and appropriate, given the location of the development along a Civic Boulevard and Neighbourhood Connector.

4.3 Issue and Consideration #3: Form

The PPS is supportive of appropriate development standards which facilitate intensification, redevelopment and compact form (1.1.3.4). The PPS also identifies that long term economic prosperity should be supported by encouraging a sense of place by promoting a well-designed built form (1.7.1e)).

The London Plan encourages compact forms of development as a means of planning and managing for growth (7_, 66_). The London Plan encourages growing “inward and upward” to achieve compact forms of development (59_ 2, 79_). Within the Neighbourhoods Place Type, and according to the urban design considerations for residential intensification, compatibility and fit will be evaluated from a form-based perspective through consideration of the following: site layout in the context of the surrounding neighbourhood; building and main entrance orientation; building line and setback from the street; height transitions with adjacent development; and massing appropriate to the scale of the surrounding neighbourhood (953_ 2.a. to f.). Similar to the Planning Impact Analysis criteria within the 1989 Official Plan, the Our Tools section of The London Plan contains various considerations for the evaluation of all planning and development applications (1578_).

Development within the Medium Density Residential designation of the 1989 Official Plan and the SWAP shall have a low-rise form and a site coverage and density that could serve as a transition between low density residential areas and more intensive forms of development. Normally height limitations will not exceed four storeys. Appendix D of this report includes a complete Planning Impact Analysis addressing matters of both intensity and form.

The proposed R8-4() Zone requires a minimum lot area of 1000 square meters and a minimum lot frontage of 30 meters. These requirements are satisfied as the lands are approximately 4874 square meters and there are approximately 86.7 meters of lot frontage on Colonel Talbot Road.

The proposed development would be located close to the road, contributing to an active street front and pedestrian scale, while also increasing the separation between the building and the adjacent properties. The design contributes to a development form that will create an active street front and appropriate pedestrian scale. The recommended Zoning and Special Provisions would facilitate an alternative and appropriate form of development that will add new housing to the area.

Consistent with the PPS and conforming to the SWAP, 1989 Official Plan and The London Plan, the recommended use for subject property would optimize the development of the land and public investment in infrastructure in the area. Located within a developing area of the City, the development contributes to achieving more compact forms of growth in a homogenous area. The proposed apartment building represents a more compact form of development and efficient use of land and resources than the single detached dwellings that currently occupy the site.

The location and massing of the proposed building is consistent with urban design goals. The building is proposed to be situated close to the intersection of Pack Road and Colonel Talbot Road, defining the street edge and encouraging a street-oriented design, with ground floor entrances facing the streets.

The parking area is located to the east and south of the building and does not extend into the exterior side yard beyond the building façade. Additional setbacks for the parking area along the southern property edge (3.0m) are recommended to ensure there is enough area to plant robust tree plantings, as per the landscape plan, and to protect trees/root zones located on the adjacent properties.

The proposed building is taller than the surrounding single detached dwellings. As previously discussed, the proposed building placement provides for a suitable separation between the proposed development and existing homes, mitigating compatibility concerns including loss of privacy. Sufficient space is available to provide for appropriate fencing and/or vegetative screening along the south and east property boundary.

Comments from Urban Design staff highlighted various considerations:

Site Layout

- This proposal should take into consideration the road widening allowance and ensure that any proposed sidewalk, entrances and canopies do not encroach.
- Relocate the transformer to a less prominent location away from the street frontage along Pack Road.
- To minimise visual impact of parking and make efficient use of land, relocate surface parking underground while allowing space for a greater amount of street-facing facades and appropriately sized amenity space.
- Ensure parking areas visible from the street are screened using a combination of landscaping and low-rise masonry walls (max. 0.75m, including exposed footings) in order to reduce their visual impact along both streetscapes.
- Provide safe and effective pedestrian linkages to Colonel Talbot Road and Pack Road.
 - Provide connections from the pedestrian walkway abutting the building with the road widening sidewalks and intersection for direct pedestrian access from the public sidewalk.
- Provide an appropriately sized and located common outdoor amenity area for the number of units proposed.
- Include all requirements of the Site Plan Control By-Law in the site design, in particular as it relates to parking (landscape islands, parking setbacks) and garbage collection/loading areas (location).
- The proposal should take into consideration any existing significant mature trees on the site and along property boundaries.

Building Design

- Ensure that the proposed building/built form is oriented to both Colonel Talbot Road and Pack Road and establishes a pedestrian-oriented built edge with street oriented units.
 - Ensure the building is appropriately scaled and located on the site to provide visual interest and enclose the street.
 - Extend the building façade along the perimeter of both Colonel Talbot Road and Pack Road to have a more efficient use of land and foster an enclosed pedestrian-oriented streetscape.
- Ensure that the proposed building has regard for its corner location. The massing/ articulation or other architectural features should emphasize the intersection.
- Ensure development is designed in a main street format with buildings at the street edge with high proportions of vision glazing and principal entrances oriented to the street.
- Locate the principal residential building entrance (lobby) at the intersection of Colonel Talbot Road and Pack Road or an alternative location close to intersection along either of the public streets. Differentiate the residential lobby entrance from the commercial unit entrances with architectural features such as canopies, signage, lighting, increase in glazing, double doors, framing, materials, etc.

- Consider flexibility in ground floor space programming to incorporate mixed uses such as street oriented residential or/and commercial units to be able to respond to the market conditions while maintaining the street orientation.
- Commercial units proposed along Colonel Talbot Road should be designed with a human-scale rhythm and include a store-front design with high proportion of vision glass, appropriately-scaled signage, canopies and lighting, double doors, and an increased ground floor height.
 - Consider delineating commercial unit signage to further provide human scaled rhythm and a defined separation of space/uses.
- Ensure the top of the building is designed and distinguished through an articulated roof form, step-backs, cornices, material change and/or other architectural details and explore opportunities to screen/integrate the mechanical and elevator penthouses into an architecture of the building.

Comments from the Urban Design Peer Review Panel highlighted the following:

- The Panel is generally in support of the rezoning application; however, encourage the applicant to return to the Panel once the development is at the detailed design and site plan submission stage
- The Panel notes that the internal pedestrian sidewalk is absent along the South façade of the building. The Panel suggests that the sidewalk continue along this façade, with adequate clearances for landscaping to create a less intimidating pedestrian experience.
- The Panel notes that the building sits on a highly visible 'gateway site', situated at the edge of the city. The building should respond to this landmark opportunity with a memorable and unique design.
- The Panel notes that the setback to the surface parking lot along the South property line is insufficient and would require the removal of existing trees and landscape long this edge. The Panel is concerned that this would create an inefficient privacy buffer between the residential property to the South, as well as create unfavourable wind conditions. Consider reducing the building footprint and shifting the building to the North to retain the existing South tree line and provide an adequate landscape buffer.
- The Panel notes some inconsistencies between drawings and renderings in reference to the corner condition. The Panel recommends that additional attention be given to animate the corner, such as wrapped balconies, a differentiation in height, and/or relocating the residential/retail entrance.

Comments from site plan on the revised concept include the following:

- Long-term bicycle parking is required. Short-term bicycle parking should be located further forward on the site to service the commercial uses and on a hard surface.
- Setback for parking needs to be sufficient to allow for tree plantings, 3.0m is considered a minimum standard.
- Parking peninsulas should be included in accordance with the SPCBL with tree plantings both within the parking area and within the peninsulas already provided.
- The plan should anticipate the location of the future City sidewalk.
- As an apartment building a garbage room and put-out location should be provided.
- A communal amenity area for residents of the building located away from the City right-of-way is required.

A reduced front and exterior side yard (1.0 m minimum) is recommended as part of the special provisions for the site, to place the building closer to the street in support of urban design goals. The requested reductions are considered appropriate in their context and are recommended as part of the R8-4 Zone.

4.4 Issue and Consideration #4: Neighbourhood & Agency Concerns

Comments provided on the application focused on impacts to adjacent properties, including:

- Privacy
- Construction impacts
- Traffic
- Noise
- Impact on property values
- Environmental impacts
- Grading concerns/stormwater

Privacy and Overlook

Members of the public expressed concerns about the height of the building leading to loss of privacy from people looking out their windows or using their terraces or balconies.

The proposed building setback from the south property line is 27.63m. The applicant is providing a 3.0m setback from the property line to the parking area along the south property line, to ensure robust plantings and trees can be accommodated as additional mitigation, as per the proposed landscape plan. The additional setback will also help to protect trees on the adjacent property by providing an adequate setback for the critical root zones. A 1.8m board on board fence is also proposed along the property line to further protect the adjacent property. No balconies are proposed along the southern building façade.

Construction Impacts

Comments on construction impacts adjacent to existing homes was made. Construction impacts will be temporary, and all traffic associated with construction will be limited to the main roads (Colonel Talbot/Pack).

Traffic Impacts and Parking Reduction

Concerns on the impact of this development on traffic were also raised.

As per the Transportation Impact Assessment provided for this application, it is forecast that the proposed development will generate 17 new trips in the AM peak hour (5 in and 12 out) and 73 trips during the PM peak hour (37 in and 36 out). Colonel Talbot Road and Pack Road are both Civic Boulevards that are intended to carry a higher amount of traffic volumes.

A parking justification analysis was undertaken as part of the Transportation Impact Assessment. It concluded the following:

- Based on the parking requirements in the City's Zoning By-Law, 79 parking spaces would be required for the development.
- A review of Zoning By-Law parking requirements from other municipalities showed that other municipalities have lower requirements for an apartment use, commonly at or near 1 space per unit.
- Similarly, it was found that the parking rates for commercial uses are commonly 1 space per 30m² in other municipalities.

Through the application review process it was determined that the recommended parking rate of 1 space per residential unit and 1 space per 30m² of commercial/office space, for a total of 51spaces is reasonable for the proposed development.

Noise

Nearby property owners were concerned with the level of noise being generated by the apartment use. The proposed development is not expected to negatively impact the surrounding adjacent properties. Activity areas associated with the residential and commercial uses will be concentrated within the building. Noise impacts are expected to be negligible and more consistent with noise patterns expected of a residential area.

Environmental Concerns

Residents indicated that destruction of many trees will occur on multiple properties, which will affect the environment overall. As per the tree preservation plan, several trees are to be removed from the subject site. However, any adjacent trees on adjacent

properties cannot be removed, and additional setbacks to ensure protection of adjacent critical root zones will be addressed at site plan. Additionally, all tree removals must take place between September 1 and April 1st to avoid disturbing nesting migratory birds. Trees may be removed outside this window only if a qualified bird specialist has been determined there are not nesting birds in the trees. This requirement is in accordance with the Migratory Birds Convention Act, 1994.

Grading and Impact on Stormwater Flows

Members of the public have expressed concerns about the impact of development with its inherent additional hard surfacing and increased surface runoff, and its affect on adjacent properties.

As part of the site plan process, grading will be addressed, and any surface or stormwater runoff will need to be contained on the subject lands. Stormwater is proposed to be contained in an underground storage tank. Further review of this solution will occur at site plan.

Archaeological Assessment

A Stage 1 and 2 Archeological Assessment was done for the subject lands. Although the report does not recommend any further study (Stage 3 and 4), heritage planning requires both the archaeological assessment and ministry compliance letter to be provided prior to sign off. The Ministry letter has not yet been received. In order to advance the application, a holding provision (h-18) is being added to ensure no site disturbance until all documents (including the Ministry sign off letter) have been provided, to the satisfaction of the City.

Conclusion

The recommended amendment is consistent with the Provincial Policy Statement, 2020 and conforms to the in-force policies of The London Plan, including but not limited to the Key Directions and the Neighbourhoods Place Type. Further, the recommended amendment is in conformity with the 1989 Official Plan and the Southwest Area Secondary Plan, including but not limited to the Medium Density Residential designation within the North Lambeth Neighbourhood. The recommended amendment will facilitate the development of new dwelling types within a newly developing area, with a land use, intensity, and form that is appropriate for the site.

Prepared by: Nancy Pasato, MCIP, RPP
Senior Planner, Development Services

Submitted by: Mike Corby, MCIP, RPP
Manager, Planning Implementation

Recommended by: Gregg Barrett, AICP
Director, Planning and Development

Submitted by: Scott Mathers, MPA, P.Eng.
Deputy City Manager, Planning and Economic
Development

Appendix A

Bill No. (number to be inserted by Clerk's Office)
2022

By-law No. Z.-1-22_____

A by-law to amend By-law No. Z.-1 to rezone an area of land located at 3510-3524 Colonel Talbot Road.

WHEREAS 2857082 Ontario Inc. have applied to rezone an area of land located at 3510-3524 Colonel Talbot Road, as shown on the map attached to this by-law, as set out below;

AND WHEREAS this rezoning conforms to the Official Plan;

THEREFORE the Municipal Council of The Corporation of the City of London enacts as follows:

- 1) Schedule "A" to By-law No. Z.-1 is amended by changing the zoning applicable to lands located at 3510-3524 Colonel Talbot Road, as shown on the attached map comprising part of Key Map No. A112, from an Urban Reserve (UR4) Zone **TO** a Holding Residential R8 Special Provision Bonus (h-18*R8-4(_)) Zone.
- 2) Section Number 12.4 of the Residential R8 (R8-4) Zone is amended by adding the following Special Provision:
 -) R8-4(_) 3510 & 3524 Colonel Talbot Road
 - a) Additional permitted uses, limited to the first floor
 - i) Bake shop
 - ii) Commercial recreation establishment
 - iii) Convenience store
 - iv) Food stores
 - v) Office, business
 - vi) Office, service
 - vii) Office, professional
 - viii) Personal service establishments
 - ix) Pharmacy
 - x) Retail store
 - b) Regulations
 - i) Front and Exterior Side Yard Depth 1.0 metres (3.3 feet) (Minimum)
 - ii) Front and Exterior Side Yard Depth 3.0 metres (9.8 feet) (Maximum)

- | | | |
|-------|-------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------|
| iii) | Height
(Maximum) | the lesser of 15.0 metres,
or 4 storeys |
| iv) | Density | 87 units per hectare |
| v) | Gross Floor Area
for Additional Permitted Uses
(Maximum) | 435.0 square metres
(4682.3 square feet) |
| vi) | Parking
(Minimum) | 60 spaces |
| vii) | Notwithstanding the Site Plan Control By-law, setback for the parking area from south property line shall be 3.0 metres (9.8 feet). | |
| viii) | The primary entrance of commercial units shall be oriented to adjacent streets. | |

The inclusion in this By-law of imperial measure along with metric measure is for the purpose of convenience only and the metric measure governs in case of any discrepancy between the two measures.

This By-law shall come into force and be deemed to come into force in accordance with Section 34 of the *Planning Act, R.S.O. 1990, c. P13*, either upon the date of the passage of this by-law or as otherwise provided by the said section.

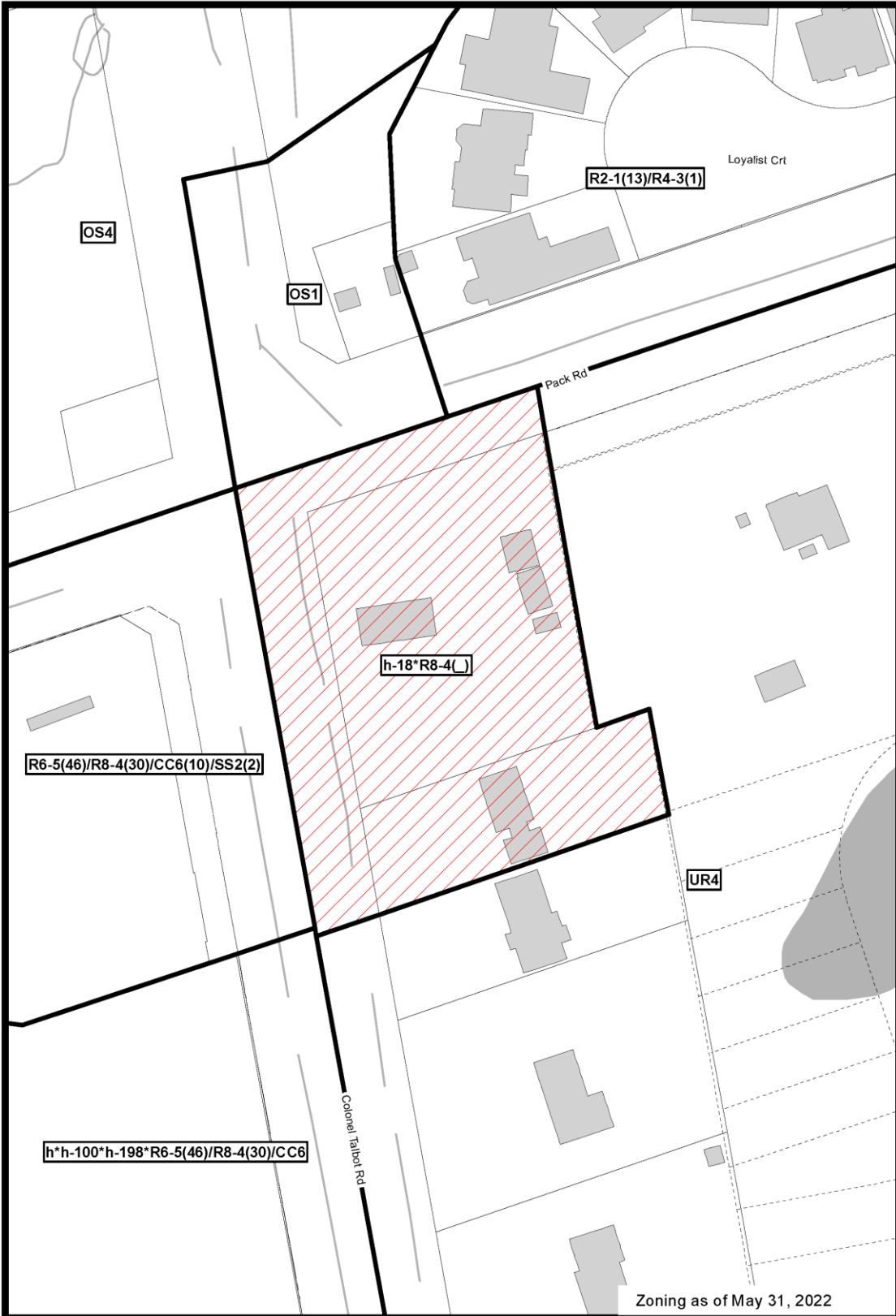
PASSED in Open Council on August 2, 2022.

Ed Holder
Mayor

Michael Schulthess
City Clerk

First Reading – August 2, 2022
Second Reading – August 2, 2022
Third Reading – August 2, 2022

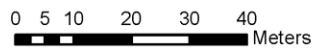
AMENDMENT TO SCHEDULE "A" (BY-LAW NO. Z.-1)



File Number: Z-9491
Planner: NP
Date Prepared: 2022/06/13
Technician: RC
By-Law No: Z.-1-

SUBJECT SITE 

1:1,000



Appendix C – Public Engagement

Community Engagement

Notice of Application (April 13, 2022):

On April 14, 2022, Notice of Application was sent to 22 property owners and tenants in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on April 14, 2022. “Planning Application” signs were also posted on the site.

Replies were received from, or on behalf of 1 household.

Nature of Liaison: The purpose and effect of this zoning change is to permit a mixed-use (commercial/office and residential) apartment building with a height of 4 storeys, 37 residential units, and 430 square metres of commercial/office on the first floor. Possible change to Zoning By-law Z.-1 **FROM** an Urban Reserve (UR4) Zone **TO** a Residential R8 Special Provision (R8-4(_)) Zone. Permitted Uses would include: apartment buildings; handicapped person’s apartment buildings; lodging house class 2; stacked townhousing; senior citizen apartment buildings; emergency care establishments; continuum-of-care facilities. The proposed special provisions would permit additional permitted uses such as: bake shop; commercial recreation establishment; convenience stores; food stores; office – business, service and professional; personal service establishments; pharmacy; retail store; restaurant; a reduced minimum front and exterior side yard depth of 0m whereas 8m is required; a maximum height of 15m whereas 13m maximum is required; a density of 87 units per hectare, whereas 75 units per hectare maximum is permitted; 63 parking spaces maximum whereas 75 parking spaces are required; a parking area setback of 0m from the ultimate road allowance; a parking area setback of 0.8m from the internal property line and non-residential uses shall be located on the ground floor and shall occupy no more than 435m² of total gross floor area. The City may also consider additional considerations such as a different base zone, the use of holding provisions, and/or additional special provisions.

Responses: A summary of the various comments received include the following:

Concern for:

- Privacy
- Construction impacts
- Traffic
- Noise
- Impact on property values
- Environmental impacts
- Grading concerns/stormwater

Responses to Public Liaison Letter and Publication in “The Londoner”

Telephone	Written
	Scott Miller 3534 Colonel Talbot Road
	Sapphire Miller 3534 Colonel Talbot Road

Scott Miller
3534 Colonel Talbot Road

I am requesting any additional information on the progress or status of planning application Z-9491 for 3510- 3524 Colonel Talbot Rd.

Please also provide information on when a public hearing on this application is available and when an appeals process can begin.

As a concerned citizen of the proposed area this project will negatively impact the area and the continued enjoyment of those who use it.

I am writing to express opposition to the application for land use change at 3510 Colonel Talbot Rd. This development is causing undue hardships to my family and the use of my property as well as financial difficulties from decreasing property value.

I am opposed this development as it will deliver excessive noise to the surrounding area. As a family with two young children who nap during the day as well as someone who works from home this will be difficult to deal with and something we were not expecting when we moved to the property.

Additionally the dust and debris created by a construction project of this scale will be excessive and not tolerated by my elderly grandmother with respiratory problems who will be living very close to this development.

Also I am opposed to the indicated traffic plan. One while during construction the excessive number of vehicles and heavy trucks will make it very difficult to access my adjoining property. After construction finishes there is to be a no left turn sign off Colonel Talbot Rd. As my only property entrance is from Colonel Talbot Rd. This make it extremely difficult to access my property and places undue difficulty in having only access from Colonel Talbot north bound.

Under outside review this application and subsequent development has decreased my property value significantly. In one estimation from qualified agents I have seen a decrease of \$50,000 to \$80,000. This is not acceptable.

The plan also calls for the destruction of many trees on both properties. The large willow tree at the rear of the property is home to many birds as well as others who use it and the pond behind the property during migration. It's elimination as well as construction activities will negatively impact the ecosystem in the Lambeth area. Please consult with the migratory bird act before removal. Also the tree removal plan as two trees indicated for removal not on the applicant's property these can not come down.

I would also like to request additional information as the files provided for public review do not include critical information such as a grading plan. Grading and proper drainage in this area is sensitive and I fear the addition of a large building and associated hardscapes from parking lots sidewalks and others will negatively impact adjoining properties which do not have modern foundation drainage. Also I would like to know if it is the city or developers decision to raise the final grade of the property to street level or keep final grading at current levels below street level on Colonel Talbot Rd. If being raised this could even further dramatically effect the use to other properties.

I hope Council will agree that this development does not suit the area and will negatively impact the health and wellness of residents in neighboring communities. Please halt the development of this project.

Sapphire Miller
3534 Colonel Talbot Road

I am writing to express my concern regarding the application for rezoning on the Southeast corner of Colonel Talbot Road and Pack Road. I live in the property that will be adjacent to the proposed 4 storey apartment building. Allowing this rezoning and building is significantly impacting our property value as well as our quality of life. A large apartment building with mixed use commercial on the main floor will greatly reduce the privacy of my property. Once built, the balconies for the apartments will overlook my front yard and my back yard. It will also increase traffic, noise and congestion significantly in this area. I know that the demolition of the existing homes as

well as the construction of the apartment building will take months, if not years. There will be significant noise and debris from the building process that will impact the enjoyment of my home and yard. I have two small children who are likely to be disturbed by the noise.

My husband and I are in the process of attempting to sell our home. We have had 12+ showings and every single potential buyer has stated that they do not want to place an offer due to the threat of the development next door. This is making it impossible to sell our home for the value that we should be able to get.

Is the city or the developer planning to compensate us as property owners for the loss of enjoyment, loss of value and mental exhaustion that this potential rezoning and development is already costing us?

I would like to be included in any public meetings, hearings or requests for input regarding this development. I wish to have my voice and concerns heard.

Departmental and Agency Comments

Urban Design (May 4, 2022)

- This site is located fully within the Neighbourhood Place Type in The London Plan [TLP] at the intersection of two Civic Boulevards and North Lambeth Neighbourhood of the Southwest Area Secondary Plan [SWASP]. TLP and SWASP contemplates the proposed form (Mixed-use medium density up to 6 storeys with bonus) on sites located within the Neighbourhoods Place Type along Civic Boulevards and as such the form and site design policies of the plan(s) apply:
- This proposal should take into consideration of the road widening allowance and ensure that any proposed sidewalk, entrances and canopies do not encroach.
- Relocate the transformer to a less prominent location away from the street frontage along Pack Road.
- To minimise visual impact of parking and make efficient use of land, relocate surface parking underground while allowing space for a greater amount of street-facing facades and appropriately sized amenity space [TLP 275_], [SWASP 20.5.3.9 iii g].
- Ensure parking areas visible from the street are screened using a combination of landscaping and low-rise masonry walls (max. 0.75m, including exposed footings) in order to reduce their visual impact along both streetscapes. [SWASP 20.5.3.9 iii g]
- Provide safe and effective pedestrian linkages to Colonel Talbot Road and Pack Road [SWASP 20.5.4.1 iii a].
 - Provide connections from the pedestrian walkway abutting the building with the road widening sidewalks and intersection for direct pedestrian access from the public sidewalk.
- Provide an appropriately sized and located common outdoor amenity area for the number of units proposed.
- Include all requirements of the Site Plan Control By-Law in the site design, in particular as it relates to parking (landscape islands, parking setbacks) and garbage collection/loading areas (location).
- The proposal should take into consideration any existing significant mature trees on the site and along property boundaries.
- Ensure that the proposed building/built form is oriented to both Colonel Talbot Road and Pack Road and establishes a pedestrian-oriented built edge with street oriented units. [SWASP 20.5.3.9 i a]
 - Ensure the building is appropriately scaled and located on the site to provide visual interest and enclose the street. (SWASP 20.5.3.9 iii a)
 - Extend the building façade along the perimeter of both Colonel Talbot Road and Pack Road to have a more efficient use of land and foster an enclosed pedestrian-oriented streetscape.

- Ensure that the proposed building has regard for its corner location. The massing/ articulation or other architectural features should emphasize the intersection [SWASP 20.5.3.9 iii c].
- Ensure development is designed in a main street format with buildings at the street edge with high proportions of vision glazing and principal entrances oriented to the street. (SWASP 20.5.3.9 iii b)
- Locate the principal residential building entrance (lobby) at the intersection of Colonel Talbot Road and Pack Road or an alternative location close to intersection along either of the public streets. Differentiate the residential lobby entrance from the commercial unit entrances with architectural features such as canopies, signage, lighting, increase in glazing, double doors, framing, materials, etc.
- Consider flexibility in ground floor space programming to incorporate mixed uses such as street oriented residential or/and commercial units to be able to respond to the market conditions while maintaining the street orientation.
- Commercial units proposed along Colonel Talbot Road should be designed with a human-scale rhythm and include a store-front design with high proportion of vision glass, appropriately-scaled signage, canopies and lighting, double doors, and an increased ground floor height.
 - Consider delineating commercial unit signage to further provide human scaled rhythm and a defined separation of space/uses.
- Ensure the top of the building is designed and distinguished through an articulated roof form, step-backs, cornices, material change and/or other architectural details and explore opportunities to screen/integrate the mechanical and elevator penthouses into an architecture of the building.

Urban Design Peer Review Panel (April 20, 2022) (see Appendix F)

Site Plan (June 13, 2022)

- Long-term bicycle parking is required. Short-term bicycle parking should be located further forward on the site to service the commercial uses and on a hard surface.
- Setback for parking need to be sufficient to allow for tree plantings, 3.0m is considered a minimum standard.
- Parking peninsulas should be included in accordance with the SPCBL with tree plantings both within the parking area and within the peninsulas already provided.
- The plan should anticipate the location of the future City sidewalk.
- As an apartment building a garbage room and put-out location should be provided.
- A communal amenity area for residents of the building located away from the City right-of-way is required

Parks Planning (May 3, 2022)

- Parkland dedication is required in the form of cash in lieu, pursuant to By-law CP-9 and will be finalized at the time of site plan approval.

Ecologist Planning (May 2, 2022)

- Natural Heritage Features on, or adjacent to the site have been identified on Map 5 of the London Plan or based on current aerial photo interpretation, including, but not limited to, Unevaluated Wetlands.

Upper Thames River Conservation Authority (April 26, 2022)

- Please be advised that the subject lands are not affected by any regulations (Ontario Regulation 157/06) made pursuant to Section 28 of the Conservation Authorities Act.
- Accordingly, the UTRCA has no objections to this application and a Section 28 permit application is not required.

Landscape Architect (Tree Prevention) (May 9, 2022)

- The proposed development poses some risk of injury to three CoL boulevard trees and will require the removal of one city tree to construct an access drive. All trees located on City of London Boulevards (including their root zones) are protected from any activities which may cause damage to them or cause them to be removed. To request the removal of the city tree and to cause injury to the roots of the 3 City Trees, contact Forestry Dispatcher at trees@london.ca with details of your request. Consent must be obtained from Forestry Operations prior to removals. The consent from Forestry Operations and proof of payment will need to be supplied to the City as part of the Site Plan Application process.
- The critical root zones of trees #24 and 28 will be encroached upon with the proposed tree protection fencing. Based on the trees' sizes, no excavation should occur within 3.6m of the trees' trunks. This would require the tree protection fencing to be moved slightly to the north from the property line shared with 3534 Colonel Talbot Rd.

Engineering (May 4, 2022)

The following items are to be considered during a future development application stage:

Sewers:

- If Planning, Council and DS are supportive SED fundamentally has no issue with the increase above allocated going to the 600 mm dia trunk. I would caution SP approval should not happen until after they have proper zoning in place. And once zoning is in place and you accept the SPA let me know the final population so SED can make appropriate updates and track it

Stormwater:

1. As per attached as-con 30178, the site (at C=0.50) is tributary to the existing 600mm storm sewer stub on Colonel Talbot Road. In order to service the proposed site the applicant will be required to extend the sewers on Colonel Talbot Road to the South limit of their site; these works shall be in accordance with City Standards.
2. Recognizing that there are no storm sewers currently established for the proposed site on Colonel Talbot Road, and that extending the sewers would be costly and unfair; per the Drainage By-Law, section 5.2, where no storm sewer is accessible the applicant shall provide a dry well or storm water retention system which is certified by a Professional Engineer to the satisfaction of the City Engineer.
3. Alternatively to the previous comment, if the adjacent subdivision 39T-16509 (3614, 3630 Colonel Talbot Rd and 6621 Pack Road – Phase 1) is developed prior to this site, servicing of the site may be coordinated through the engineering design of the proposed subdivision, ensuring that this site is considered as an external area as provided in Appendix G of the subdivision FPR.
4. The design and construction of SWM servicing works for the subject land shall be in accordance with:
 - a. The SWM criteria and targets for the Dingman Creek Subwatershed,
 - b. Any Municipal Class Environmental Assessment in the area,
 - c. The City Design Requirements for on-site SWM controls which may include but not be limited to quantity/quality and erosion controls, and
 - d. The City's Waste Discharge and Drainage By-Laws; the Ministry of the Environment Planning & Design Manual; as well as all applicable Acts, Policies, Guidelines, Standards and Requirements of all approval agencies.
5. The design of the SWM servicing work shall include but not be limited to such aspects as on-site SWM controls design, possible implementation of SWM Best Management Practices (e.g. Low impact Development "LID" features), grading and drainage design (minor, and major flows), storm drainage conveyance from external areas (including any associated easements), hydrological conditions, etc.

6. Any proposed LID solutions should be supported by a Geotechnical Report and/or a Hydrogeological Assessment report prepared with a focus on the type(s) of soil present at the Site, measured infiltration rate, hydraulic conductivity (under field saturated conditions), and seasonal high groundwater elevation. Please note that the installation of monitoring wells and data loggers may be required to properly evaluate seasonal groundwater fluctuations. The report(s) should include geotechnical and hydrogeological recommendations of any preferred/suitable LID solution. All LID proposals are to be in accordance with Section 6 Stormwater Management of the Design Specifications & Requirements manual.
7. If the number of proposed parking spaces exceeds 29, the owner shall be required to have a consulting Professional Engineer confirm how the water quality may be addressed to the standards of the Ministry of the Environment, Conservation and Parks (MECP) as practical with a target of 80% TSS removal to the satisfaction of the City Engineer. Applicable options could include, but not be limited to the use of oil/grit separators or any LID filtration/infiltration devices.
8. Additional comments may be provided upon future review of the site.
9. The site falls within the Dingman Subwatershed. The Dingman EA requires the control hierarchy for the 25 mm event to be considered in new development design. This approach and LID design is included in the Section 6 Stormwater Management of the Design Specifications & Requirements manual.
10. The Owner agrees to promote the implementation of SWM Best Management Practices (BMP's) within the plan, including Low Impact Development (LID) where possible, to the satisfaction of the City Engineer.
11. The owner is required to provide a lot grading plan for stormwater flows and major overland flows on site and ensure that stormwater flows are self-contained on site, up to the 100 year event and safely conveys up to the 250 year storm event, all to be designed by a Professional Engineer for review.
12. The Owner shall allow for conveyance of overland flows from external drainage areas that naturally drain by topography through the subject lands.
13. Stormwater run-off from the subject lands shall not cause any adverse effects to adjacent or downstream lands.
14. An erosion/sediment control plan that will identify all erosion and sediment control measures for the subject site shall be prepared to the specification and satisfaction of the City Engineer and shall be in accordance with City of London and MECP (formerly MOECC) standards and requirements. This plan is to include measures to be used during all phases of construction. These measures shall be identified in the Storm/Drainage Servicing Report. The Stormwater Engineering Division staff have reviewed the above noted application and have some additional comments beyond those previously provided (see attached):
15. Based on the Dingman Subwatershed study, the runoff control hierarchy for the 25mm event is to be achieved for sites within the Subwatershed. The consulting engineer is to ensure that any proposed option of LID solutions are to be in compliance with the LID screening Tools Section 6.5.2.2 Stormwater Management of the Design Specifications & Requirements Manual.
16. As per 9.4.1 of The Design Specifications & Requirements Manual (DSRM), all multi-family, commercial and institutional block drainage is to be self-contained. The owner is required to provide a lot grading plan for stormwater flows and major overland flows on site and ensure that stormwater flows are self-contained on site, up to the 100 year event and safely convey the 250 year storm event.

London Hydro (April 14, 2022)

- Servicing the above proposal should present no foreseeable problems. Any new and/or relocation of existing infrastructure will be at the applicant's expense, maintaining safe clearances from L.H. infrastructure is mandatory. Note: Transformation lead times are minimum 16 weeks. Contact the Engineering Dept. to confirm requirements & availability. London Hydro has no objection to this proposal or possible official plan and/or zoning amendment. Any new or relocation of the existing service will be at the expense of the owner.

Appendix D – Policy Context

3.7 Planning Impact Analysis	
Criteria	Response
Compatibility of proposed uses with surrounding land uses, and the likely impact of the proposed development on present and future land uses in the area;	The proposed land use is a contemplated use in the SWAP and Official Plan, and contributes to a variety of housing forms within the neighbourhood.
The size and shape of the parcel of land on which a proposal is to be located, and the ability of the site to accommodate the intensity of the proposed use;	The site concept achieves an intensity that allows for other on-site functions such as visitor and accessible parking, emergency services and landscaped open space.
The supply of vacant land in the area which is already designated and/or zoned for the proposed use;	There is no vacant land in the area which is already designated and/or zoned for the proposed use.
The proximity of any proposal for medium or high density residential development to public open space and recreational facilities, community facilities, and transit services, and the adequacy of these facilities and services;	The site is located close to shopping areas, offices, commercial and service uses, numerous parks both existing and planned, and future transit services.
The need for affordable housing in the area, and in the City as a whole, as determined by the policies of Chapter 12 – Housing;	The City is experiencing an affordable housing crisis. Although no affordable housing units are proposed through this development, the creation of alternative housing forms (apartment units) contributes to the overall mix of dwelling units and prices in the area.
The height, location and spacing of any buildings in the proposed development, and any potential impacts on surrounding land uses;	The scale/height of the proposed 4 storey apartment building is mitigated by the placement of the building toward the front of the property and the extensive setback of the building from the interior side yard of the property. Impacts on adjacent properties, such as overlook and light penetration, would be mitigated through a combination of yard depth, appropriate space for landscape screening, and photometric analysis/mitigation at the site plan approval stage.
The extent to which the proposed development provides for the retention of any desirable vegetation or natural features that contribute to the visual character of the surrounding area;	Landscaping and screening opportunities through vegetation will be considered at a future Site Plan Approval stage, including enhanced landscaping along the property edges. The proposed landscape plan will retain some trees on the site, while others will need to be removed.
The location of vehicular access points and their compliance with the City's road access policies and Site Plan Control By-law, and the likely impact of traffic generated by the proposal on City streets,	As noted in the Intensity analysis in this report, traffic impacts of this development will be negligible in relation to the anticipated function of the collector and arterial streets/Civic Boulevard.

<p>on pedestrian and vehicular safety, and on surrounding properties;</p>	
<p>The exterior design in terms of the bulk, scale, and layout of buildings, and the integration of these uses with present and future land uses in the area;</p>	<p>The applicant is commended for providing a built form that establishes a built edge along Colonel Talbot Road and Pack Road and provides for an active edge along Colonel Talbot Road with building and business entrances fronting the street. The placement of the building adjacent to the street provides for a significant setback from the property to the east and south, with enhanced landscaping along the property edges; includes a common outdoor amenity space and rooftop patio.</p>
<p>The potential impact of the development on surrounding natural features and heritage resources;</p>	<p>Not applicable.</p>
<p>Constraints posed by the environment, including but not limited to locations where adverse effects from landfill sites, sewage treatment plants, methane gas, contaminated soils, noise, ground borne vibration and rail safety may limit development;</p>	<p>Not applicable.</p>
<p>Compliance of the proposed development with the provisions of the City's Official Plan, Zoning By-law, Site Plan Control By-law, and Sign Control By-law;</p>	<p>The requested amendment is consistent with the in-force policies of the Official Plan. The requirements of the Site Plan Control By-law will be considered through the design of the site to ensure functionality, including provision of amenity space, drive aisle widths, sidewalk widths, garbage storage, and long-term bicycle storage through the site plan approval process.</p>
<p>Measures planned by the applicant to mitigate any adverse impacts on surrounding land uses and streets which have been identified as part of the Planning Impact Analysis;</p>	<p>Enhanced tree planting and landscaping in combination with privacy fencing and building massing treatments are expected to mitigate minor adverse impacts on the surrounding land uses.</p>
<p>Impacts of the proposed change on the transportation system, including transit</p>	<p>The residential intensification of the subject lands will have a negligible impact on the transportation system and provide a more transit-supportive form of development.</p>

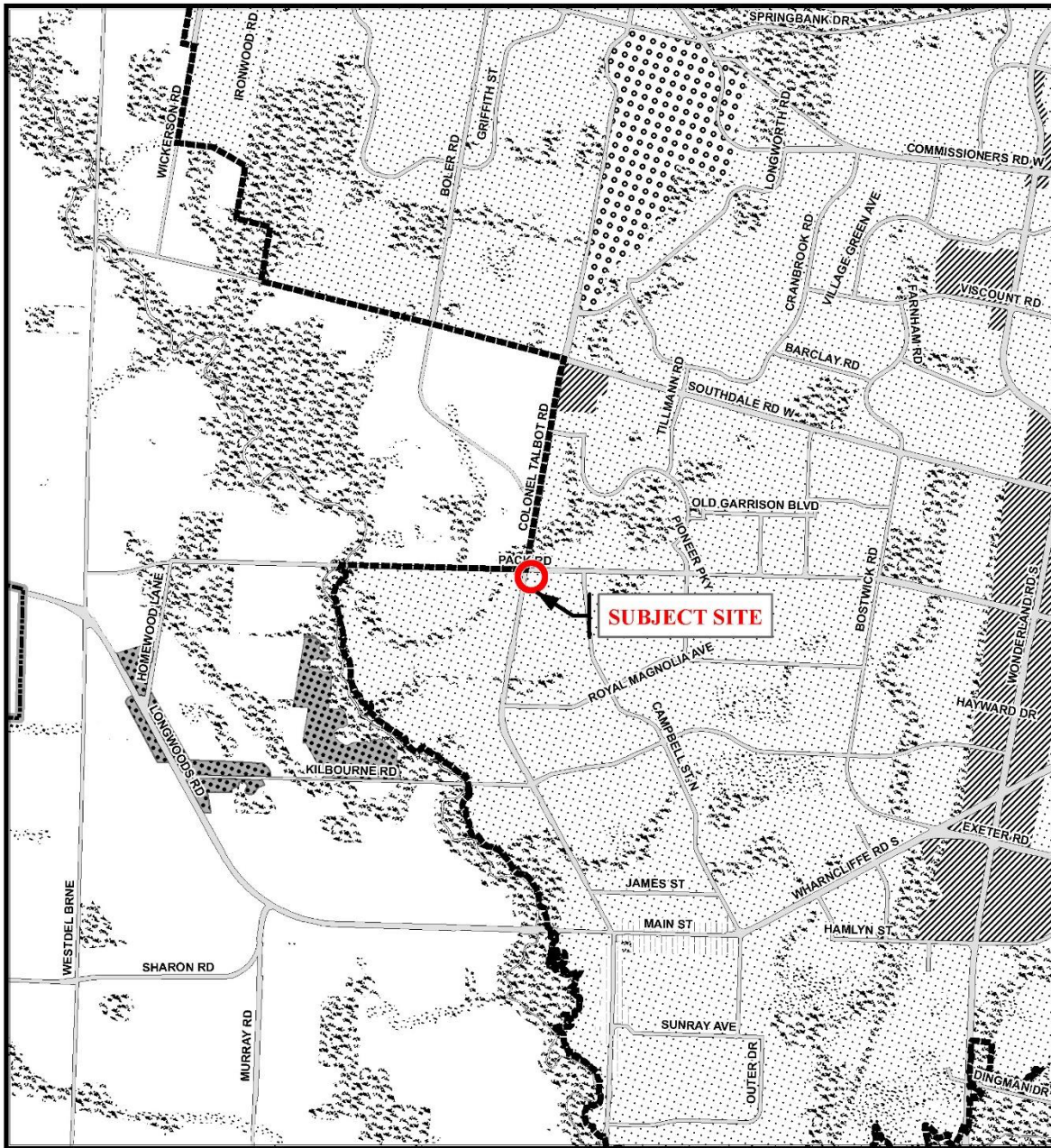
1577_ Evaluation Criteria for Planning and Development Applications	
Criteria – General Policy Conformity	Response
Consistency with the Provincial Policy Statement and in accordance with all applicable legislation.	The proposal is consistent with the Provincial Policy Statement as it provides for efficient development and land use patterns and for an appropriate range and mix of housing options and densities required to meet projected requirements of current and future residents of the regional market area. There are no significant natural or cultural heritage resources requiring protection and no natural or man-made hazards to be considered.
Conformity with the Our City, Our Strategy, City Building, and Environmental Policies of this Plan.	The proposal provides for residential intensification within the Urban Growth Boundary and supports Key Directions related to the creation of a mixed-use compact City and strong, healthy and attractive neighbourhoods. The massing and scale of the proposed building can be appropriately integrated into the community through the application of the relevant City Design policies at the site plan approval stage.
Conformity with the policies of the place type in which they are located.	The proposed 4 storey apartment building provides for the use and intensity of development contemplated within the Neighbourhoods Place Type at the intersection of two Civic Boulevards.
Consideration of applicable guideline documents that apply to the subject lands.	No additional guideline documents apply to the subject lands.
The availability of municipal services, in conformity with the Civic Infrastructure chapter of this Plan and the Growth Management/Growth Financing policies in the Our Tools part of this Plan.	The site will be fully serviced by municipal water, sanitary and storm sewers.
Criteria – Impacts on Adjacent Lands	
Traffic and access management	Further consideration of traffic controls related to the proposed driveways will occur at the site plan approval stage.
Noise	The proposed development is not expected to generate any unacceptable noise impacts on surrounding properties. A noise study was not required for the Zoning By-law amendment application but will be required at the site plan stage

	to address the mitigation of impacts of road noise on the new development.
Parking on streets or adjacent properties.	The proposal includes a slight reduction in the required parking spaces, however it is not anticipated that overflow parking will be required on local streets.
Emissions generated by the use such as odour, dust or other airborne emissions.	The proposed development will not generate noxious emissions.
Lighting	Lighting details will be addressed at this site plan approval stage. It is a site plan standard that any lighting fixture is to minimize light spill onto abutting properties.
Garbage generated by the use.	Garbage facilities should be screened, storage inside the building is a standard requirement for apartment forms, with garbage to be placed outside on collection day.
Privacy	The proposed development situates the proposed apartment building as far from abutting properties as possible. In addition to the spatial separation between the buildings and the lot lines, the provision of a combination of privacy fencing and enhanced landscaping to soften the property boundaries and provide screening to the neighbouring single detached lot will help screen views from the proposed building to neighbouring properties.
Shadowing	Minor shadowing may impact adjacent and nearby properties in the early morning or late afternoon, depending on the season.
Visual Impact	Enhanced landscaping, articulated building design, and architectural details and materials to be implemented through site plan are expected to have a positive visual impact on the area.
Loss of Views	There are no view corridors to significant features or landmarks to be affected by the proposed building.
Trees and canopy cover.	The development will result in the loss of some trees and canopy cover in order to achieve a more compact form of development. At the site plan stage, a complete landscape plan will be developed to provide for new tree

	planting and screening from adjacent land uses.
Cultural heritage resources.	Not applicable.
Natural heritage resources and features.	Not applicable.
Natural resources.	Not applicable.
Other relevant matters related to use and built form.	Not applicable.

Appendix E – Relevant Background

The London Plan



Legend

	Downtown		Future Community Growth		Environmental Review
	Transit Village		Heavy Industrial		Farmland
	Shopping Area		Light Industrial		Rural Neighbourhood
	Rapid Transit Corridor		Future Industrial Growth		Waste Management Resource Recovery Area
	Urban Corridor		Commercial Industrial		Urban Growth Boundary
	Main Street		Institutional		
	Neighbourhood		Green Space		

This is an excerpt from the Planning Division's working consolidation of Map 1 - Place Types of the London Plan, with added notations.

At the time of the printing of this map, the Rapid Transit EA is in progress. This map shows the Rapid Transit Corridors and Urban Corridors to recognize potential alignments. These Place Types will be modified to align with the results of the EA process for the final version of The London Plan.

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LONDON PLAN MAP 1 - PLACE TYPES -

PREPARED BY: Planning Services



Scale 1:30,000



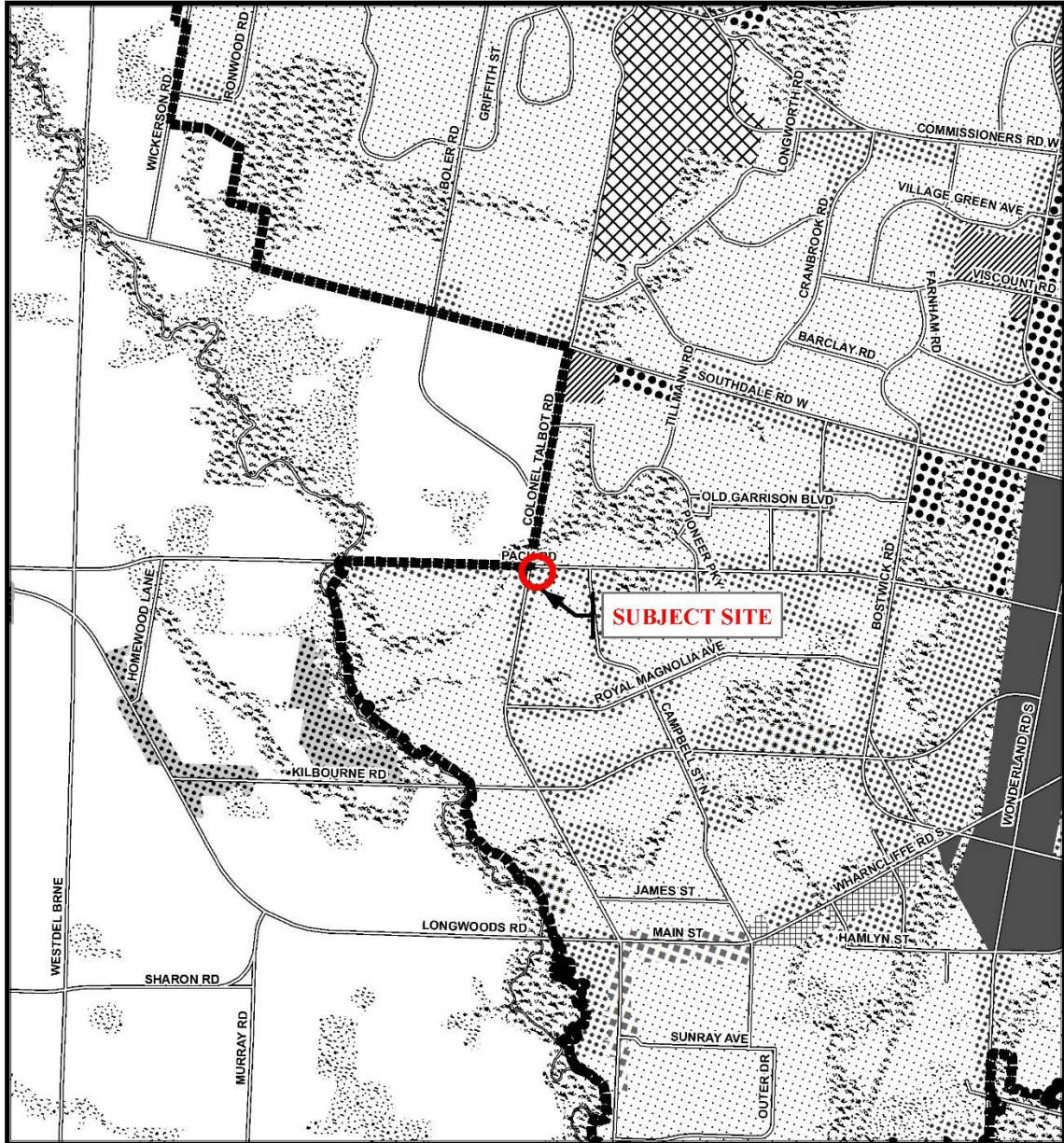
File Number: Z-9491

Planner: NP

Technician: RC

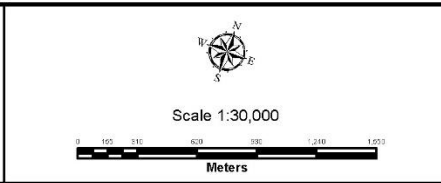
Date: May 20, 2022

1989 Official Plan – Schedule A – Land Use



Legend					
	Downtown		Multi-Family, Medium Density Residential		Office Business Park
	Wonderland Road Community Enterprise Corridor		Low Density Residential		General Industrial
	Enclosed Regional Commercial Node		Office Area		Light Industrial
	New Format Regional Commercial Node		Office/Residential		Commercial Industrial
	Community Commercial Node		Regional Facility		Transitional Industrial
	Neighbourhood Commercial Node		Community Facility		Rural Settlement
	Main Street Commercial Corridor		Open Space		Environmental Review
	Auto-Oriented Commercial Corridor		Urban Reserve - Community Growth		Agriculture
	Multi-Family, High Density Residential		Urban Reserve - Industrial Growth		Urban Growth Boundary

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 OFFICIAL PLAN SCHEDULE A
 - LAND USE -
 PREPARED BY: Graphics and Information Services



FILE NUMBER: Z-9491
 PLANNER: NP
 TECHNICIAN: RC
 DATE: 2022/05/20

Zoning By-law Z.-1 – Zoning Excerpt



COUNCIL APPROVED ZONING FOR THE SUBJECT SITE:

1) **LEGEND FOR ZONING BY-LAW Z-1**

- | | |
|-------------------------------------------|-----------------------------------|
| R1 - SINGLE DETACHED DWELLINGS | RF - REGIONAL FACILITY |
| R2 - SINGLE AND TWO UNIT DWELLINGS | CF - COMMUNITY FACILITY |
| R3 - SINGLE TO FOUR UNIT DWELLINGS | NF - NEIGHBOURHOOD FACILITY |
| R4 - STREET TOWNHOUSE | HER - HERITAGE |
| R5 - CLUSTER TOWNHOUSE | DC - DAY CARE |
| R6 - CLUSTER HOUSING ALL FORMS | |
| R7 - SENIOR'S HOUSING | OS - OPEN SPACE |
| R8 - MEDIUM DENSITY/LOW RISE APTS. | CR - COMMERCIAL RECREATION |
| R9 - MEDIUM TO HIGH DENSITY APTS. | ER - ENVIRONMENTAL REVIEW |
| R10 - HIGH DENSITY APARTMENTS | |
| R11 - LODGING HOUSE | OB - OFFICE BUSINESS PARK |
| | LI - LIGHT INDUSTRIAL |
| DA - DOWNTOWN AREA | GI - GENERAL INDUSTRIAL |
| RSA - REGIONAL SHOPPING AREA | HI - HEAVY INDUSTRIAL |
| CSA - COMMUNITY SHOPPING AREA | EX - RESOURCE EXTRACTIVE |
| NSA - NEIGHBOURHOOD SHOPPING AREA | UR - URBAN RESERVE |
| BDC - BUSINESS DISTRICT COMMERCIAL | |
| AC - ARTERIAL COMMERCIAL | AG - AGRICULTURAL |
| HS - HIGHWAY SERVICE COMMERCIAL | AGC - AGRICULTURAL COMMERCIAL |
| RSC - RESTRICTED SERVICE COMMERCIAL | RRC - RURAL SETTLEMENT COMMERCIAL |
| CC - CONVENIENCE COMMERCIAL | TGS - TEMPORARY GARDEN SUITE |
| SS - AUTOMOBILE SERVICE STATION | RT - RAIL TRANSPORTATION |
| ASA - ASSOCIATED SHOPPING AREA COMMERCIAL | |
| OR - OFFICE/RESIDENTIAL | "h" - HOLDING SYMBOL |
| OC - OFFICE CONVERSION | "D" - DENSITY SYMBOL |
| RO - RESTRICTED OFFICE | "H" - HEIGHT SYMBOL |
| OF - OFFICE | "B" - BONUS SYMBOL |
| | "T" - TEMPORARY USE SYMBOL |

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**ZONING
BY-LAW NO. Z.-1
SCHEDULE A**



THIS MAP IS AN UNOFFICIAL EXTRACT FROM THE ZONING BY-LAW WITH ADDED NOTATIONS

FILE NO:

Z-9491

NP

MAP PREPARED:

2022/05/20

RC

1:1,500

0 5 10 20 30 40
Meters

Appendix F – Applicant’s Reply to UDPRP Comments

Comment:
The Panel is generally in support of the rezoning application; however, encourage the applicant to return to the Panel once the development is at the detailed design and site plan submission stage.
Applicant Response:
Noted.

Comment:
The Panel notes that the internal pedestrian sidewalk is absent along the South façade of the building. The Panel suggests that the sidewalk continue along this façade, with adequate clearances for landscaping to create a less intimidating pedestrian experience.
Applicant Response:
The preliminary site plan has been revised to demonstrate and accommodate a sidewalk connection along the south side of the building.

Comment:
The Panel notes that the building sits on a highly visible ‘gateway site’, situated at the edge of the city. The building should respond to this landmark opportunity with a memorable and unique design.
Applicant Response:
We believe the building design provided fits within the current residential landscape but also contributes to the growing commercial development in this area. The below design attributes support our point of view. <ul style="list-style-type: none">• Strong connection to the property corner through a 4 storey masonry wall anchoring the building.• Strong connection to each street from the corner through bold projecting 4 storey forms, each with their own design styles, all the while anchored to a more traditional tri-part façade elevation treatment.• Colonel Talbot street side has a strong 4 storey series of projecting & recessed forms creating pleasant visual interest, light & shadows.• Both building sides have strong block form roof parapets that project/recess that provide visual interest and prevent a long monotonous façade.• Pack Road façade is designed more to address common materials found in the residential subdivision across the road to the north/east.• Façade masonry materials will be high caliber and noticeable.

Comment:
The Panel notes that the setback to the surface parking lot along the South property line is insufficient and would require the removal of existing trees and landscape long this edge. The Panel is concerned that this would create an inefficient privacy buffer between the residential property to the South, as well as create unfavourable wind conditions. Consider reducing the building footprint and shifting the building to the North to retain the existing South tree line and provide an adequate landscape buffer.

Applicant Response:

The Site Plan has been revised to extend the landscape strip along the south boundary limit to 3m. Further planting details may be reviewed at the Site Plan Control phase. The landscape strip is able to accommodate a privacy fence and planting, which would aid in reducing the perceived privacy and wind concerns.

Comment:

The Panel notes some inconsistencies between drawings and renderings in reference to the corner condition. The Panel recommends that additional attention be given to animate the corner, such as wrapped balconies, a differentiation in height, and/or relocating the residential/retail entrance.

Applicant Response:

At the Zoning stage, the preliminary building design is still being contemplated. Additional architectural details may be evaluated at the site plan design stage as further design elements are contemplated for the functionality of the site and building.