

Report to Planning and Environment Committee

To: Chair and Members
Planning & Environment Committee
From: Scott Mathers, MPA, P. Eng.,
Deputy City Manager, Planning and Economic Development
Subject: 910 Gainsborough Road
Public Participation Meeting on: March 28, 2022

Recommendation

That, on the recommendation of the Director, Planning and Development, with respect to the application of Crown Homes London relating to the property located at 910 Gainsborough Road:

- (a) The proposed by-law attached hereto as Appendix "A" **BE INTRODUCED** at the Municipal Council meeting on April 12, 2022 to amend Zoning By-law No. Z.-1, in conformity with the Official Plan, to change the zoning of the subject property **FROM** a Residential R1 (R1-11) Zone, **TO** a Residential R6 Special Provision (R6-5(_)) Zone.
- (b) **IT BEING NOTED** that the following Site Plan matters have been raised through the application review process for consideration by the Site Plan Approval Authority:
 - i) Engineering
 - a. Ensure stormwater runoff and drainage is contained on site
 - ii) Urban Design/Landscaping
 - a. Consider appropriate measures to further mitigate privacy impacts on abutting townhouse units to the east, including the provision of transom windows and additional plantings
 - b. Enhanced design of Unit 1 to establish a pedestrian-friendly streetscape
 - c. Include a walkway that extends across the front of the units and connects to the public sidewalk on Gainsborough Road
 - d. Locate any surface parking area away from Gainsborough Road and provide additional screening and landscaping
 - e. Include all requirements of the Site Plan Control By-Law in the site design, in particular as it relates to parking (landscape islands, parking setbacks) and garbage pick-up (location)
 - f. Take into consideration any existing significant mature trees on the site and along property boundaries

Executive Summary

Summary of Request

The applicant is requesting to rezone 910 Gainsborough Road (the "subject lands") to permit a two-storey semi-detached dwelling and five-unit, two-storey townhouse.

Purpose and the Effect of Recommended Action

The purpose and effect of the recommended Zoning By-law amendment is to permit a multi-unit residential development including a two-storey semi-detached dwelling and a two-storey, five-unit townhouse with a total density of approximately 25.2 units per hectare. Additional Special Provisions are requested to permit a reduced east and west interior side yard depth of 3.8 metres and 5.3 metres, respectively.

Rationale of Recommended Action

1. The requested amendment is consistent with the policies of the Provincial Policy Statement, 2020 that encourage efficient development and land use patterns.
2. The recommended amendment conforms to the in-force policies of The London Plan, including but not limited to the Key Directions and Neighbourhood Place Type policies;
3. The recommended amendment conforms to the in-force policies of the 1989 Official Plan, including but not limited to the Low Density Residential designation;
4. The requested amendment conforms to the Residential Intensification policies of The London Plan and the 1989 Official Plan which direct intensification to ensure that character and compatibility with the surrounding neighbourhood is maintained.
5. The subject lands represent an appropriate location for Residential Intensification, within the Built-Area Boundary, along a higher-order street at the periphery of an existing neighbourhood.
6. The recommended amendment would permit development at an intensity that is appropriate for the site and the surrounding neighbourhood.
7. The recommended amendment facilitates the development of a vacant, underutilized site within the Built-Area Boundary with an appropriate form of infill development.

Analysis

1.0 Background Information

1.1 Property Description

The subject lands are located on the south side Gainsborough Road, approximately 550 metres east of Hyde Park Road in the Hyde Park Planning District. Surrounding land uses include single-storey and two-storey townhouses to the immediate east and west, respectively, a parking lot to the immediate south, and a place of worship to the north.

Currently, the subject lands are vacant with an existing paved driveway located along the westerly lot line (see figure 3, below). The site was previously occupied by a one storey, single-detached dwelling that was demolished sometime in 2021 (see figure 1, below). The subject lands are generally flat in topography and contain several mature trees and existing vegetation growing along the interior and rear property lines. The southwest corner of the site is encumbered by an existing servicing easement for 466 Sophia Crescent (stormwater sewer).



Figure 1. View of the Subject Lands and Previously Existing Single Detached Dwelling (Demolished), facing South from Gainsborough Road (June, 2021)



Figure 2. View of the Subject Lands, facing Southwest from Gainsborough Road (December, 2021)



Figure 3. View of the Subject Lands, facing South from Gainsborough Road (December, 2021)

1.2 Current Planning Information

- Official Plan Designation – Multi-Family, Medium Density Residential (MFMDR)
- The London Plan Place Type – Neighbourhoods
- Existing Zoning – Residential R1 (R1-11)

1.3 Site Characteristics

- Current Land Use – Vacant (previously single detached dwelling)
- Frontage – 45.7 metres
- Depth – ~61.0 metres
- Area – 2,878.1 square metres (0.29 hectares)
- Shape – Rectangular

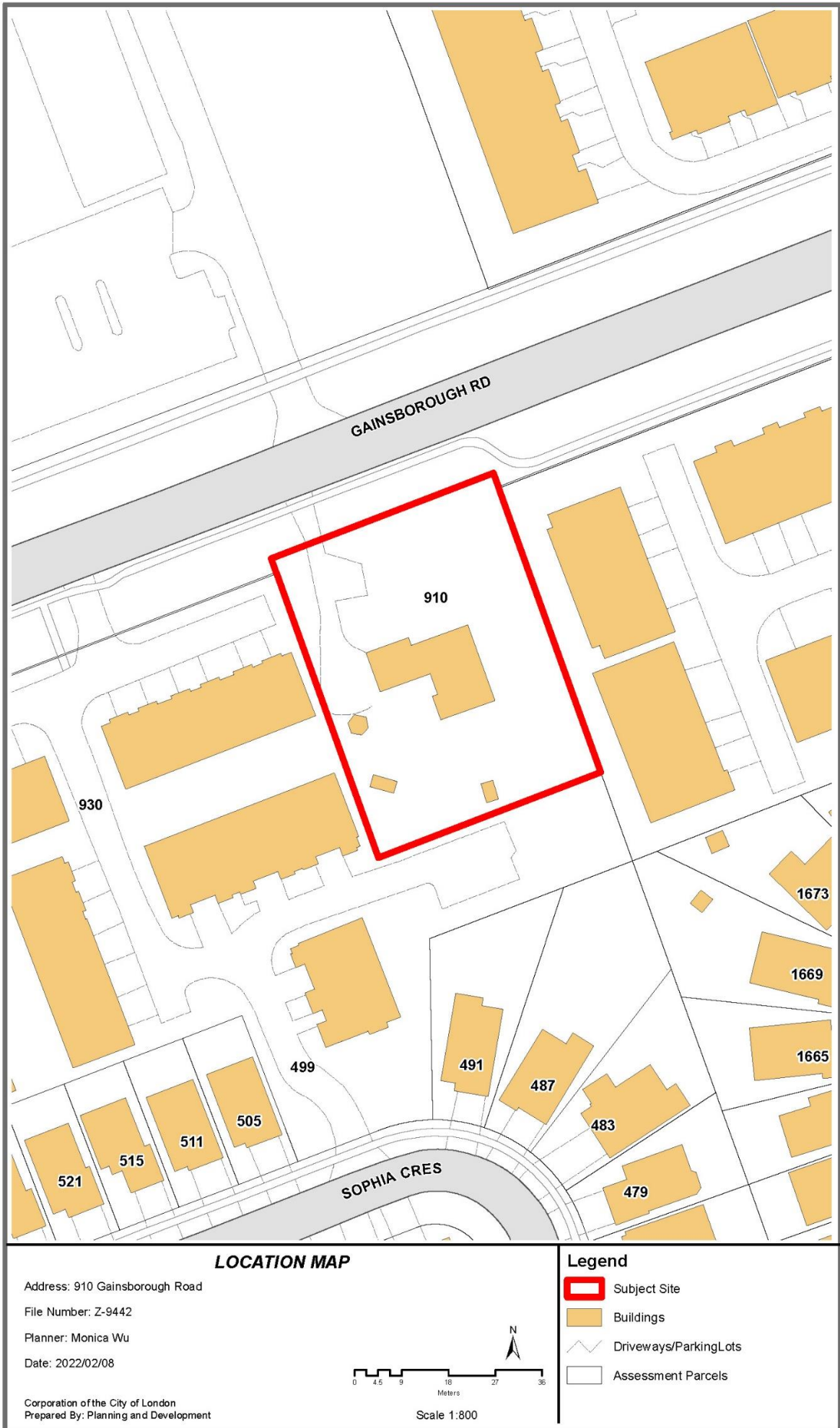
1.4 Surrounding Land Uses

- North – Place of worship
- West – Townhouses
- East – Townhouses
- South – Parking area, single detached dwellings

1.5 Intensification

The proposed development represents intensification within the Built-Area Boundary through the addition of seven (7) new residential units. The site is located outside of the Primary Transit Area (PTA).

1.6 Location Map



2.0 Description of Proposal

2.1 Development Proposal

The proposed multi-residential development includes a two-storey semi-detached dwelling and a two-storey townhouse comprising a total of seven units. As shown on the applicant's site concept plan (see figure 5, below), the semi-detached dwelling is proposed to be located perpendicular to Gainsborough Road, facing west, and the five-unit townhouse is proposed along the rear of the site, facing north.

Access to the site is provided off of Gainsborough Road by a two-way driveway and a 1.5m-wide sidewalk located on the westerly portion of the site. Three visitor parking spaces, including one barrier-free parking space, are proposed near the entrance of the site. Each individual unit contains a private, single-vehicle garage accessed by a private driveway, for a total of 14 parking spaces. Private amenity areas are proposed at the rear of each unit. The proposed development will retain a number of boundary trees located along the east property line, as well as the three existing trees located next to the visitor parking (see figure 9, below).

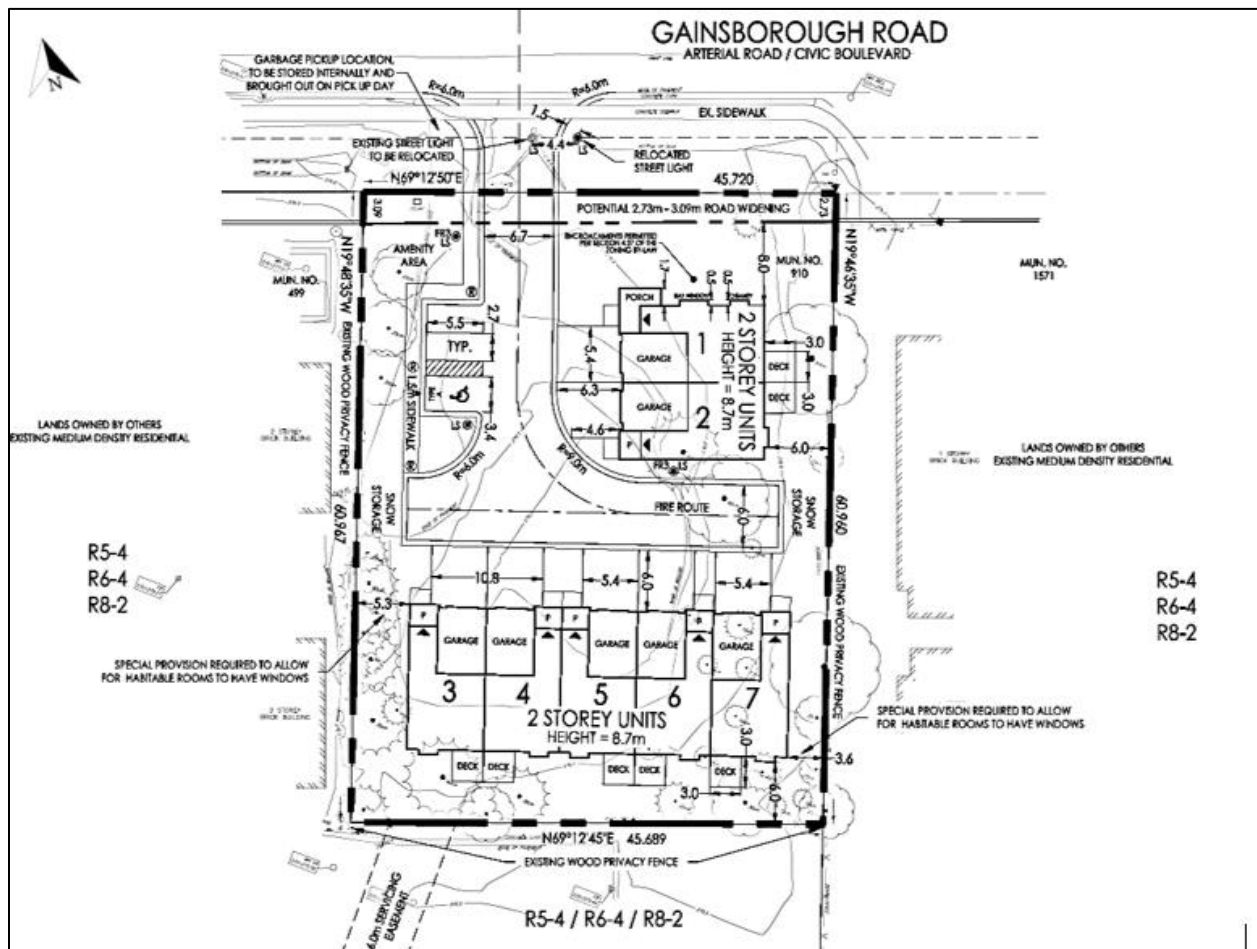


Figure 5. Site Concept Plan



Figure 6. Rendering of the Proposed Development, facing Northeast from Gainsborough Road

3.0 Relevant Background

3.1 Planning History

No previous planning applications.

3.2 Requested Amendment

The applicant is requesting an amendment to Zoning By-law Z.-1 to change the zoning of the subject lands from a Residential R1 (R1-11) Zone, which permits the use of the subject lands for one single-detached dwelling, to a Residential R6 Special Provision (R6-5 (_)) Zone to permit a two-storey semi-detached dwelling and a two-storey townhouse with 5 units for a total density of approximately 25.2 units per hectares. Special provisions are requested to permit a reduced east and west interior yard setback.

3.3 Community Engagement (see more detail in Appendix A)

Staff received six (6) comments during the public consultation period, which will be addressed under Section 4 of this report. The comments can be summarized as follows:

- Privacy impacts on the neighbouring properties to the east as a result of a reduced east interior side yard setback;
- Concerns about stormwater run-off towards the neighbouring properties to the east;
- The proposed height and density are too intense for the site.

3.4 Policy Context (see more detail in Appendix B)

Provincial Policy Statement, 2020

The *Provincial Policy Statement, 2020* (PPS) provides policy direction on matters of provincial interest related to land use planning and development. In accordance with Section 3 of the Planning Act, all planning decisions “shall be consistent with” the PPS.

The PPS encourages healthy, livable and safe communities which are sustained by accommodating an appropriate range and mix of residential, employment and institutional uses to meet long-term needs (Section 1.1.1 b)). The PPS also directs planning authorities to identify appropriate locations and promote opportunities for transit-

supportive development and accommodating a range of housing options through residential intensification (Section 1.1.3.3).

The London Plan

The London Plan is the new Official Plan for the City of London (Council adopted, approved by the Ministry with modifications, and the majority of which is in force and effect). The London Plan policies and maps under appeal to the Local Planning Appeals Tribunal (Appeal PL170100) are not in force and effect and are indicated with an asterisk throughout this report. The London Plan policies under appeal are included in this report for informative purposes indicating the intent of City Council, but are not determinative for the purposes of this planning application.

The London Plan provides Key Directions (54_) that must be considered to help the City effectively achieve its vision. These directions give focus and a clear path that will lead to the transformation of London that has been collectively envisioned for 2035. Relevant Key Directions are outlined below.

Key Direction #5: The London Plan provides direction to build a mixed-use compact city by:

- Planning to achieve a compact, contiguous pattern of growth – looking “inward and upward”;
- Planning for infill and intensification of various types and forms to take advantage of existing services and facilities and to reduce our need to grow outward; and,
- Ensure a mix of housing types within our neighbourhoods so that they are complete and support aging in place (Directions 1, 2, 4 and 5).

Key Direction #7: The London Plan also provides direction to build strong, healthy and attractive neighbourhoods for everyone by:

- Integrating affordable forms of housing in all neighbourhoods (Direction 10).

The subject lands are located within the Neighbourhoods Place Type on *Map 1 – Place Types of The London Plan, with frontage on a Civic Boulevard (Gainsborough Road) as identified on Map 3 – Street Classifications. The Neighbourhoods Place Type contemplates a broad range of residential uses at this location including, but not limited to single-detached, semi-detached, duplex and converted dwellings, triplexes, fourplexes, townhouses, stacked townhouses and low-rise apartments (Table 10 – Range of Permitted Uses). With respect to intensity, a minimum height of 2-storeys up to a maximum height of 4-storeys (or 6-storeys through bonusing) is permitted (Table 11 – Range of Permitted Heights).

In addition to the above-noted policies, consideration has been given to the general policies of the Our Strategy, Our City, City Building and Design sections of The London Plan.

Official Plan (1989)

The subject lands are designated Multi-Family, Medium Density Residential (MFMDR) in accordance with Schedule ‘A’ of the 1989 Official Plan. The MFMDR designation primarily permits multiple-attached dwellings, such as row houses or cluster houses, and low-rise apartments buildings. These areas may also be developed for single detached, semi-detached and duplex dwellings (3.3.1). Development shall have a low-rise form and a site coverage and density that could serve as a transition between low-density residential areas and more intensive forms of commercial, industrial, or high-density residential development, and will generally not exceed four-storeys in height (3.3.3 i)). Medium density development will not exceed an approximate net density of 75 units per hectare (3.3.3. ii)).

4.0 Key Issues and Considerations

4.1 – Issue and Consideration # 1: Use & Intensity

Provincial Policy Statement, 2020 (PPS)

The PPS directs settlement areas to be the focus of intensification and redevelopment (1.1.2). To achieve this, appropriate land use patterns within settlement areas are to be established by providing appropriate densities and mix of land uses that efficiently use land and resources, and the surrounding infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed (1.1.3.2). As well, the PPS encourages an appropriate affordable and market-based range and mix of residential types, including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons to meet long-term needs (1.1.1b)).

The proposed development is considered residential intensification through the development of an underutilized site and the creation of new residential units. The mixed-residential development comprising a semi-detached dwelling and five-unit townhouse represents a more compact, higher density form of development than the previously existing use and will contribute to providing choice and diversity in housing options for current and future residents. The recommended amendment will result in an efficient, cost-effective development pattern within an existing settlement area and no new roads or infrastructure are required to service the site.

The London Plan and Official Plan (1989)

The subject lands are located in the Neighbourhoods Place Type along a Civic Boulevard (Gainsborough Road) in The London Plan (*Map 1 – Place Types and Map 3 – Street Classifications). A broad range of residential uses are contemplated at this location, including semi-detached dwellings and townhouses (Table 10 – Range of Permitted Uses). The London Plan uses height as a measure of intensity in the Neighbourhoods Place Type. A minimum height of two-storeys and a maximum height four-storeys (up to six-storeys with bonusing) is contemplated where a property has frontage on a Civic Boulevard (*Table 11 – Range of Permitted Heights).

The London Plan encourages intensification where appropriately located and provided in a way that is sensitive to and a good fit within existing neighbourhoods (83; 937; 953_1). The intensity of development must be appropriate to the neighbourhood context as it relates to height, massing, setbacks etc. (953_2), as well as appropriate for the size of the lot, and accommodate such things as adequate parking in appropriate locations, landscaped open space, outdoor residential amenity area etc. (953_3).

Within the 1989 Official Plan, the subject lands are designated MFMDR which permits multiple-unit residential developments having a low-rise profile (3.3). The primary permitted uses include multiple-attached dwellings, such as row houses, and low-rise apartments, however these areas may also be developed for low-density forms of development, including single detached and semi-detached dwellings (3.3.1.).

Development within areas designated MFMDR shall take into account surrounding land uses in terms of height, scale and setbacks and shall not adversely impact the amenities and character of the surrounding area (3.3.2.i)). Medium density development will not exceed a net density of 75 units per hectare and shall be no more than four-storeys in height (3.3.3.i); 3.3.3.ii)).

The proposed development, being a two-storey semi-detached dwelling and a five-unit, two-storey townhouse, is contemplated in both Official Plans as it relates to the use and intensity of the site. The proposed building height of two-storeys meets the minimum height requirements in The London Plan; at the same time, the proposed density of 25.2

units per hectares is well below the maximum permitted density of 75 units per hectare in the 1989 Official Plan.

With respect to the appropriateness of the proposed intensity, the site design provides for all required parking spaces and sufficient private outdoor amenity space and landscaped open space. Generally, reductions in parking and landscaped open space, as well as increases in height, density, and lot coverage often serve as indicators of possible over-intensification. With the exception of the requested reductions in the east and west interior side yard setbacks, no additional special provisions are required to facilitate the proposed development, indicating that the site is of sufficient size to support the proposed intensity and site design.

Issues relating to the compatibility and fit within the surrounding neighbourhood will be addressed in the following section.

4.2 – Issue and Consideration #2: Reduced Interior Side Yard Setback

The requested amendment includes a reduced east and west interior side yard setback of 3.6 metres and 5.3 metres, respectively, whereas a minimum setback of 6.0 metres is required for the Residential R6 Zone variation when the wall of a unit contains windows to habitable rooms. Due to the existing sanitary servicing easement that runs north-south along the westerly property line, a reduced west interior side yard setback is required to provide for sufficient space from the easement. It is noted that relief from east and west interior side yard setback is only required for the end units of the proposed townhouse and not the semi-detached dwelling, which will maintain the minimum 6.0 metres yard setback.

West Interior Side Yard Setback

As shown on the applicant's site concept plan, the proposed townhouse will directly abut the interior side yard of the townhouses on the neighbouring property to the west (499 Sophia Crescent). The height of the proposed two-storey townhouse units is similar to that of the existing two-storey townhouses on 499 Sophia Crescent, alleviating concerns with respect to shadowing and overlook. Further, the end unit located on the west abutting property does not contain any windows or openings along the portion of the wall facing the subject lands (see figure 7, below). As such, Planning staff are of the opinion that a reduction of 0.7 metres in the required west interior side yard setback is minor and will not result in adverse impacts.



Figure 7. View of 499 Sophia Crescent – Unit 30, Facing East Towards the Subject Lands (2021)

East Interior Side Yard Setback: Privacy Impacts

Through the circulation of the application, multiple concerns were raised by the

neighbouring property owners to the east that the reduced east interior side yard setback of 3.6 metres would negatively impact the privacy and amenity of their properties. In this instance, the end unit of the proposed townhouse row will directly abut the rear yard of two townhouse units on 1571 Coronation Drive, being units 37 and 39.

Currently, there is an existing board-on-board privacy fence located along the property boundary (see figure 8, below). To further mitigate potential privacy impacts, the applicant is proposing to retain several existing boundary trees located along the east property line to screen the proposed end townhouse unit from the east abutting townhouses, including two mature trees located adjacent to units 37 and 39 (see figure 11, below). Moreover, windows facing the adjacent properties will be transom-style windows to further maintain the privacy of both future tenants and existing adjacent homeowners (see figure 10, below) As noted in the applicant's Planning and Design Report, the windows at the end of walls of the proposed townhouse row are intended to let in additional light but are not necessarily intended to serve as the primary windows for habitable rooms. City staff may require the applicant to provide additional mitigation measures at the time of Site Plan Approval to ensure that impacts on the east abutting townhouses are further minimized, such as additional plantings along the east property line and architectural treatments.

Based on the above analysis, Planning staff are of the opinion that a reduced 3.6m east interior side yard setback is appropriate and will not result in significant impacts on the abutting properties to the east.



Figure 8. View of Subject Lands and East Abutting Townhouses, facing Northeast from Gainsborough Road

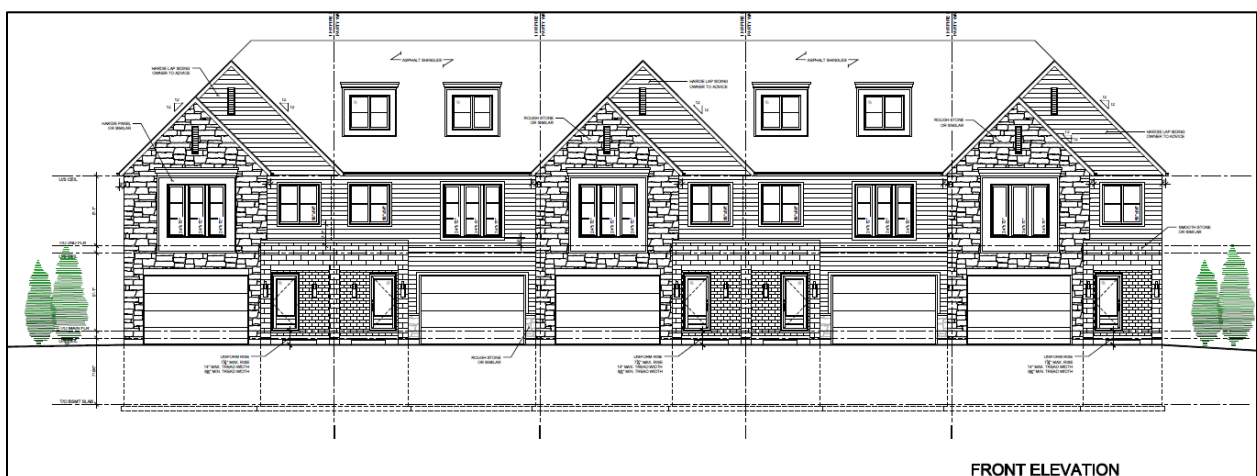


Figure 9. Front (North) Elevation of the Proposed Townhouses

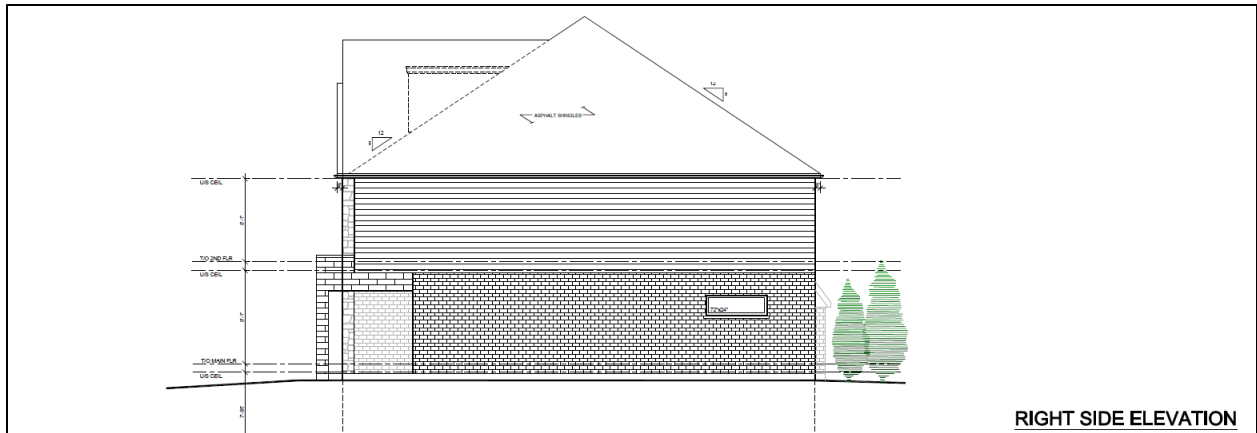


Figure 10. Right Side (East) Elevation of the Proposed End Townhouse Unit

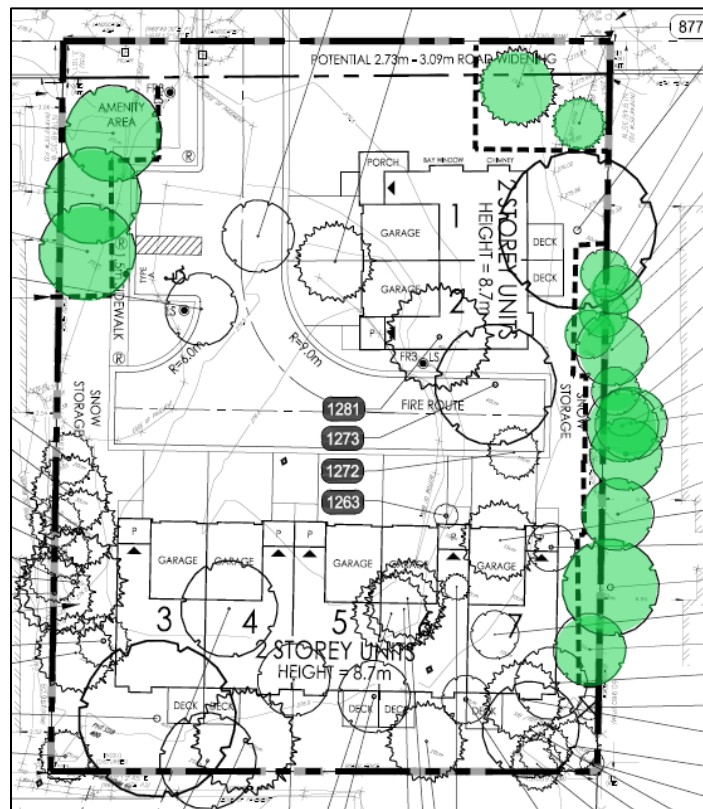


Figure 11. Existing Trees on the Subject Lands (Retained Trees shown in Green)

East Interior Side Yard Setback: Stormwater Runoff

In addition to privacy impacts, concerns were raised about stormwater runoff and drainage impacts on the abutting townhouse units to the east resulting from the proposed development.

Per the City of London’s Design Specifications & Requirements Manual (DSRM), all multi-family block drainage is to be self-contained (DSRM, 9.4.1). Through the Site Plan Approval process, the owner will be required to provide a lot grading plan for stormwater flows and major overland flows on site demonstrating that all stormwater flows will be self-contained on site, up to the 100 year event and safely convey the 250 year storm event. Approval will not be granted for the development until such time that all grading standards can be met.

4.3 – Issue and Consideration #3: Form

Provincial Policy Statement, 2020

The PPS is supportive of development standards which facilitate intensification, redevelopment and compact form (1.1.3.4). The PPS also identifies that long term economic prosperity should be supported by encouraging a sense of place by promoting a well-designed built form, and by conserving features that help define

character (1.7.1 e)).

Consistent with the PPS, the proposed intensification and redevelopment of the subject lands will optimize the use of land and public investment in infrastructure within a developed area of the City and would contribute to achieving more efficient forms of growth. The proposed two-storey semi-detached dwelling and cluster townhouses represent a more compact form of development than the current undeveloped state of the site and the single-detached dwelling that previously existed. Further analysis of the built form will be provided below.

The London Plan and (1989) Official Plan

The London Plan encourages compact forms of development as a means of planning and managing for growth (7_ ; 66_) and encourages growing “inward and upward” to achieve compact forms of development (59_ 2; 79_). The London Plan accommodates opportunities for infill and intensification of various types and forms that take advantage of existing services and facilities (59_ 4).

Within the Neighbourhoods Place Type, compatibility and fit will be evaluated from a form-based perspective through consideration of the following: site layout in the context of the surrounding neighbourhood; building and main entrance orientation; building line and setback from the street; height transitions with adjacent development; and massing appropriate to the scale of the surrounding neighbourhood (953_ 2. a.-f.). The City Design and Our Tools sections of The London Plan contain additional considerations for the evaluation of all planning and development applications as it relates to the built form (1578; 194).

Development within areas designated Multi-Family, Medium Density Residential shall have a low-rise form and a site coverage and density that could serve as a transition between low density residential areas and more intensive forms of commercial, industrial, or high-density residential development, and shall generally not exceed four storeys. Applications for residential intensification are also to be evaluated on the basis of Section 3.7 – Planning Impact Analysis (3.3.3 ii).

The proposed two-story semi-detached dwelling and two-storey townhouse are generally consistent with the existing built form and housing types observed along Gainsborough Road, which include one to two-storey townhouse clusters and single detached dwellings. As noted in the applicant’s Planning and Design report, the proposed orientation and layout of the buildings is intentional. The semi-detached dwelling will maintain an inward-facing façade to mirror the townhouse development to the immediate east (see figures 10 and 11, below). A front yard setback of 8.0 metres is proposed from the required road widening, which is generally consistent with the existing setback of the east abutting townhouse development, thereby maintaining a continuous street wall. Additional contemporary architectural elements are incorporated in the building design to create visual interest along the streetscape and to mimic the design of the adjacent townhouses to the west. The end-unit abutting Gainsborough Road (unit 1) will be designed with a strong side façade with large windows and wrap-around deck to maintain a more continuous connection to the streetscape (see figure 10, below). Main entrances for each of the units will contain a bricked, covered porch, and bay windows, bump outs, and feature materials of differing colours that will provide a pedestrian-scale environment along the street.



Figure 10. Rendering of the Proposed Development, Facing South on Gainsborough Road

Finally, surface parking is provided near the front of the site to align with the access road of the adjacent development to the west (see figure 12, below). Landscaping is to be provided between the adjacent development to the west and the proposed surface parking to allow for natural screening between these uses (see figure 9).



Figure 11. View of the Townhouse Development to the Immediate East of the Subject Lands (1571 Coronation Drive), Facing South on Gainsborough Road (June 2021)



Figure 12. View of the Townhouse Development to the Immediate West of the Subject Lands (499 Sophia Crescent), Facing South on Gainsborough Road (June 2021)

Comments provided by Urban Design and Site Plan staff highlighted various considerations for an improved site layout and building design, as summarized below:

- Consider reconfiguring the site layout to locate the semi-detached dwelling and townhouses parallel to Gainsborough Road, with street-oriented units and garages accessed by a rear lane;
- Alternatively, enhance the design of 'Unit 1' so that it is oriented towards Gainsborough Road, including the principal entrance, a wrap-around porch, and

the same number of windows, articulation, materials, etc. that would typically be found on a front elevation;

- Include a walkway that extends across the front of the units and connects to the public sidewalk on Gainsborough Road;
- Locate any surface parking area away from Gainsborough Road and provide additional screening and landscaping;
- The proposal should take into consideration any existing significant mature trees on the site and along property boundaries.

City staff will continue to work with the applicant at the Site Plan Approval stage to incorporate appropriate building and site design features in the final approved drawings and development agreement. Overall, Planning staff are of the opinion that the proposed mixed-residential development is appropriate and sensitive to the abutting lands and provides a form of compatibility that aligns with the design, density, height, and scale of the adjacent land uses and surrounding neighbourhood.

5.0 Conclusion

The requested amendment to permit a two-storey, seven-unit mixed residential development is consistent with the Provincial Policy Statement, 2020 and conforms to the in-force policies of The London Plan, including but not limited to the Key Directions and the Neighbourhoods Place Type. Further, the recommended amendment is in conformity with the in-force policies of the 1989 Official Plan, including but not limited to the Multi-family, Medium Density Residential designation. The recommended amendment will facilitate the development of an underutilized vacant site located within the Built-Area Boundary with a land use, intensity, and form that is appropriate for the site and compatible with the surrounding context.

Prepared by: **Monica Wu, MCIP, RPP
Planner II**

Reviewed by: **Mike Corby, MCIP, RPP
Manager, Planning Implementation**

Recommended by: **Gregg Barrett, AICP
Director, Planning and Development**

Submitted by: **Scott Mathers, MPA, P. Eng.
Deputy City Manager, Planning and Economic
Development**

Appendix A

Bill No. (number to be inserted by Clerk's Office)
2022

By-law No. Z.-1-22_____

A by-law to amend By-law No. Z.-1 to rezone an area of land located at 910 Gainsborough Road.

WHEREAS Crown Homes London has applied to rezone an area of land located at 910 Gainsborough Road, as shown on the map attached to this by-law, as set out below;

AND WHEREAS this rezoning conforms to the Official Plan;

THEREFORE the Municipal Council of The Corporation of the City of London enacts as follows:

- 1) Schedule "A" to By-law No. Z.-1 is amended by changing the zoning applicable to lands located at 910 Gainsborough Road, as shown on the attached map comprising part of Key Map No. A101, from a Residential R1 (R1-11) Zone to a Residential R6 Special Provision (R6-5(□)) Zone.
- 2) Section 10.4 of the Residential R6 (R6-5) Zone is amended by adding the following Special Provision:

R6-5(□)	910 Gainsborough Road	
a)	Regulations	
i)	East interior side yard depth for walls with windows to habitable rooms (minimum)	3.6 metres (11.8 feet)
ii)	West interior side yard depth for walls with windows to habitable rooms (minimum)	5.3 metres (17.4 feet)

The inclusion in this By-law of imperial measure along with metric measure is for the purpose of convenience only and the metric measure governs in case of any discrepancy between the two measures.

This By-law shall come into force and be deemed to come into force in accordance with Section 34 of the *Planning Act, R.S.O. 1990, c. P13*, either upon the date of the passage of this by-law or as otherwise provided by the said section.

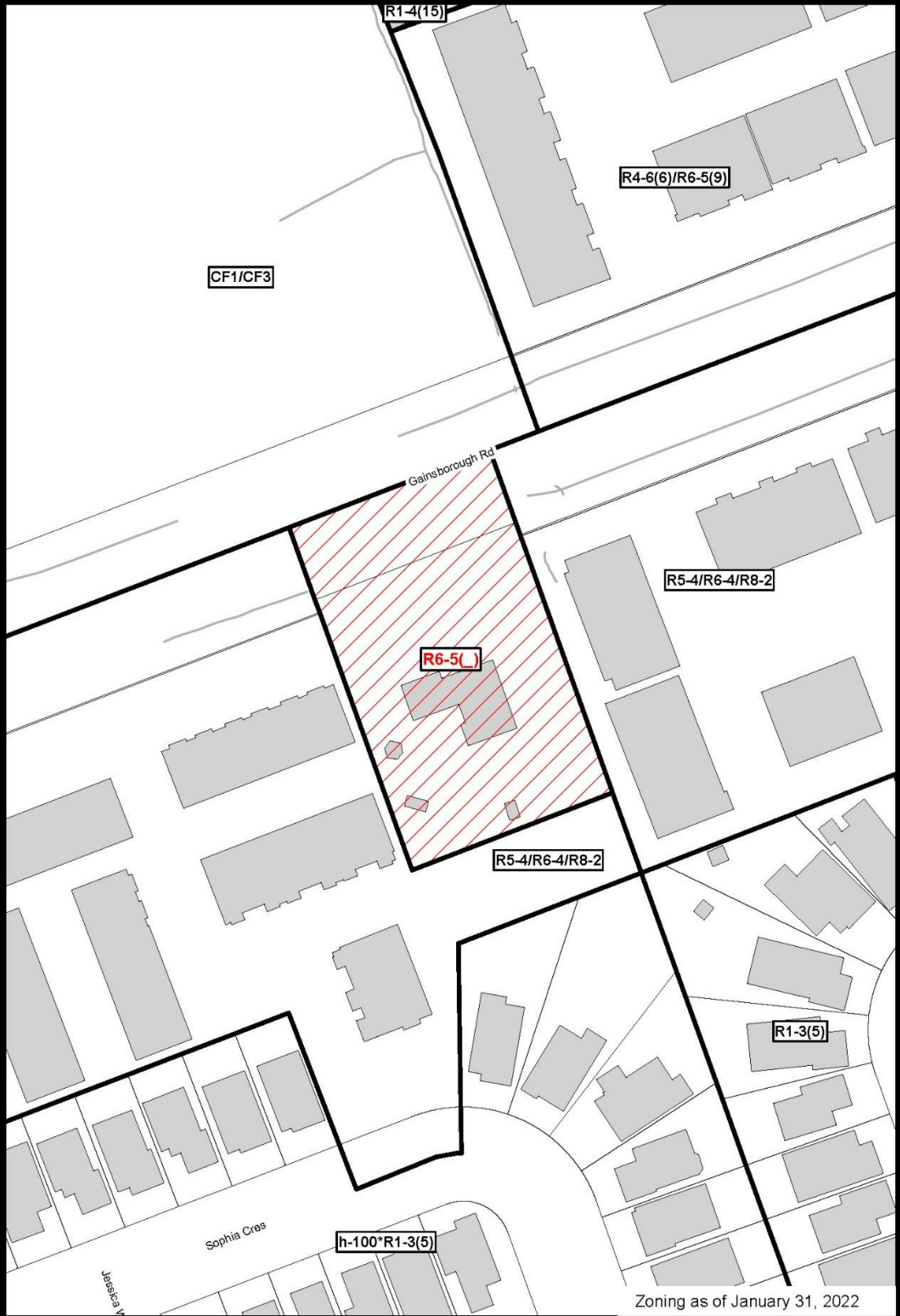
PASSED in Open Council on April 12, 2022.

Ed Holder
Mayor



Michael Schulthess
City Clerk

Second Reading – April 12, 2022
Third Reading – April 12, 2022

AMENDMENT TO SCHEDULE "A" (BY-LAW NO. Z.-1)



Zoning as of January 31, 2022

<p>File Number: Z-9442 Planner: MW Date Prepared: 2022/02/09 Technician: rc By-Law No: Z.-1-</p>	<p>SUBJECT SITE </p> <p>1:1,000</p> <p>0 5 10 20 30 40 Meters</p> 
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Appendix B – Public Engagement

Community Engagement

Public liaison: On December 10, 2021, Notice of Application was sent to all property owners with 120 m of the property. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on December 9, 2021. A “Planning Application” sign was also posted on the site.

Nature of Liaison: The purpose and effect of this zoning change is to permit a 8, 2-storey stacked back-to-back townhouse dwellings. Possible change to Zoning By-law Z.-1 **FROM** a Residential R1 (R1-9) Zone **TO** a Residential R8 Special Provision (R8-4 (_)) Zone with a 0.0 minimum front yard setback and 8 parking spaces whereas 12 parking spaces are required.

Responses: Six (6) comments were received (see below).

We would like to express our concerns about building two story units proposed by Crown Homes London. When we moved in at 1571 Coronation Dr. We asked our agent would the ranch behind us remain a single-family home, there were no plans at that time and a limited number owns the property. If 2 story units are built it will block our view of the sunsets. And the two-unit deck will look into our back deck. Therefor our privacy is gone. Wondering if building one-story units would help our problem some. Thanks for your consideration in this matter.

Gerald and Linda Perry
1571 Coronation Dr. Unit 45

I am writing in regards to the requested zoning change and special provision by Crown Homes London on 910 Gainsborough Rd.

I am very much against the proposals. First of all this company purchased a well cared for home with a lovely yard and pool and allowed it to become vacant, rundown and eventually filled with mold to the point it had too be destroyed. To me it was a dishonest and careless disregard of property.

When I purchased my condo approximately 10 years ago I took great care in choosing my retirement home, considering where it was located. Our living room faces the back of the property overlooking a portion of 910 Gainsborough and at that time, felt that the home located there was a permanent structure especially as it was zoned single family. What could go wrong? Crown Homes purchasing this property and allowing it to be destroyed! Is this what corporations can do and the neighborhood having no recourse?

I am particularly opposed to the special provision request to reduce the side yard depth to 3.6 meters. There is a reason for these zoning bylaws. As I mentioned above my living room window faces the back and as I currently look at trees, bushes and greenery etc I am NOT willing to have to look right inside a neighbour's side window, so close that neither home owner will have privacy.

I feel the existing home owners need to be protected with the least amount of disruption as possible under these circumstances.

Thank you

Pam and Jerry Golab
1571 Coronation Dr unit 37

I am writing to express my opposition to the zoning change and special provision requested by Crown homes London on 910 Gainsborough Rd.

I bought this specific unit for my retirement home with the knowledge that a residential house behind me would provide a quiet and private atmosphere. Crown Homes subsequently purchased this beautiful residential house and had it demolished in preparation for this proposal, prior to receiving any zoning change. This seems like the behaviour of a corporation who would rather ask forgiveness than approval for their plans.

To add insult to injury, they are trying to cram more units onto this property than is possible. The request of a special provision to permit a reduced east side yard depth of only 3.6 m is insulting and offensive. If anything, they should be proposing to double the minimum side yard allotment to try and offset some of the inconvenience/detriment to the current residents of our complex.

In my opinion, this proposal shows a blatant disregard for existing community members. I think the city should send a clear message to the builders, that city of London residents come before their profits, by rejecting this proposal.

Sincerely,
Nancy Hickling
1571 Coronation Dr, unit 35

I am writing in regards to the requested zoning change and special provision by Crown Homes on 910 Gainsborough Road.

I have only been living here for 1 year, I bought my house in Nov 2020 and the house on the property was abandoned and in disrepair. I was absolutely amazed that anyone could let a beautiful piece of property and a beautiful house fall into such disrepair. It was heartbreaking. My bedroom, office and kitchen look directly onto the property and if the zoning is changed I will be have to keep my bedroom curtains closed all the time and will not have any privacy in my ensuite bathroom. I will have to shower in the dark, get dressed in the dark and if I do open the blinds I will look directly into someone's back deck and kitchen.

I work shift work and do midnight shifts. I am able to sleep because there is quiet but if that goes away who knows what kind of noise I will need to put up with. To even consider allowing this indicates a complete lack of respect for the current homeowners. I believe we should have some form of protection given the property taxes we currently pay. If you are reducing the amount of property footage we will have are you planning to reduce our taxes ? I doubt it.

Kathy Chadwick
1571-Coronation Drive, Unit 39

Middlesex Standard Condominium Corporation Number 800 (1571 Coronation Drive) is in receipt of file : Z-9442 regarding a Zoning By-law Amendment for 910 Gainsborough Rd.

This development will be located to the west side of MSCC 800 and will create drainage problems. Currently, the condo corporation is addressing their problem by having the west side of the property re-graded and adding enhanced drainage at considerable cost. The new property will add to this problem if the grading and drainage slopes in any way towards 1571 Coronation Drive.

Terry McDonald

Further to my communication on December 20th, Middlesex Standard Condominium Corporation would like to go in record regarding the grading and drainage for the proposed development at 910 Gainsborough Rd. After a review of the property it is

apparent that a retaining wall and underground drainage to be directed to the north would be required in order to eliminate any drainage towards the adjacent property to the east. (MSCC 800 – 1571 Coronation Dr.)

Terry McDonald

As a resident of 1571 Coronation Drive, I wish to comment on the proposed development for 910 Gainsborough Rd.

Initially, this property contained a single family one story home and over the past 6 years it was allowed to deteriorate to the point that it had to be torn down. I remember when Mr M Plumbo purchased the property to the west of it he told me personally that he was not going to purchase the house, as it was a good solid home and too good to tear down. This was just 6 years ago.

I understand the importance of infill, however, I feel that 7 units on this property is too dense. A single story home occupied this area and I believe a more appropriate number would be 5. or perhaps 6.

I am not in support of the proposed variances being made as this brings the units too close to the existing homes and will impose unreasonably on their privacy and light exposure. Two story units are being proposed and I believe the developer should propose one story units. These one story units would fit in more appropriately with the Coronation Dr. neighbours. There are many seniors and empty nesters looking for this type of housing and very little is available in our neighbourhood at this time.

In addition, I would like to suggest that the developer consider switching the two units proposed for the east side of the property to the west side, as this would have very little impact on the neighbours to the west. In the proposal, the units to the west are 2 story units and there are no windows, backyards or decks along this area. If built in the suggested manner, they will tower over our homes and there will be no privacy for them or us. The new units will also impact the amount of light and sun our properties would receive.

There is also a very large, healthy tree that could possibly be saved if my suggestion is considered. Aesthetically, this would contribute in a real way to the neighbourhood, as well as the property in question.

Thank you for taking time to look at these suggestions and I look forward to being more involved with this important process.

Regards,
Muriel Stilson
1571 Coronation Dr. Unit 43

Agency/Departmental Comments

Archaeology

This memo is to confirm that I have reviewed the following and find the report's (analysis, conclusions and recommendations) to be sufficient to fulfill the archaeological assessment requirements for (Z-9442):

- Lincoln Environmental Consulting Corp. *Stage 1-2 Archaeological Assessment of 910 Gainsborough Road* [...] Middlesex County, Ontario (PIF P344-0410-2020), May 2020.

Please be advised that heritage planning staff recognizes the conclusion of the report that states that "no archaeological resources were identified during the Stage 2

archeological assessment of the study area, and as such no further archaeological assessment of the property is recommended.” (p.2)

An Ontario Ministry of Heritage, Sport, Tourism, Culture Industries archaeological assessment compliance letter has also been received, dating June 2, 2020 (MHSTCI Project Information Form Number P344-0410-2020, MHSTCI File Number 0012519).

Archaeological conditions can be considered satisfied for this application.

Urban Design

- Submit a full set of dimensioned elevations for the proposed addition with materials and colours labelled. Further urban design comments may follow upon receipt of the elevations.
- Explore opportunities for joint access with the neighbouring property (499 Sophia Cres) to continue the window street towards the west and to the south.
- If a joint access is not possible, reconfigure the site layout to locate semi-detached/ townhouses parallel to Gainsborough Road, with street-oriented units and garages accessed by a rear lane.
- Alternatively, enhance the design of ‘Unit 1’ so that it is oriented towards Gainsborough Road, including the principal entrance, a wrap-around porch, and the same number of windows, articulation, materials, etc. that would typically be found on a front elevation. Include direct access from the unit entrance to the public sidewalk on Gainsborough Road.
- Include a walkway that extends across the front of the units and connects to the public sidewalk on Gainsborough Road.
 - Include a walkway that connects to the unit entrance (Unit 1) flanking Gainsborough Rd.
- Locate any surface parking area away from Gainsborough Road and behind the building frontage. Screen any surface parking exposed to a public street with enhanced landscaping.
- Locate all parking areas and drive aisles a minimum of 1.5 metres (3.0 metres if along a street) from the property line to allow space for landscaping.
- Include all requirements of the Site Plan Control By-Law in the site design, in particular as it relates to parking (landscape islands, parking setbacks) and garbage pick-up (location).
- The proposal should take into consideration any existing significant mature trees on the site and along property boundaries.

Landscaping

- I have reviewed the Arborist Report prepared by Stantec Consulting Ltd, October 21, 2021 based on a tree inventory conducted by Landon Black on May 3, 2021. The tree inventory and assessment included the trees located within the property boundary, and trees on adjacent lands that may be impacted by the development and proposed grading work.
- No rare or endangered species were inventoried.
- Tree protection fencing is proposed at the dripline of offsite trees which will protect their critical root zones. This is acceptable practice.
- No City trees are impacted by the development proposed.
- I have no comments to provide on this application.

Engineering

- Engineering has no further comment on the provided application material. [The applicant is to confirm] that an easement with 499 Sophia Crescent exists for the proposed sanitary routing.

Appendix B – Policy Context

1577_Evaluation Criteria for Planning and Development Applications	
Criteria – General Policy Conformity	Response
Consistency with the Provincial Policy Statement and in accordance with all applicable legislation	The proposal is consistent with the Provincial Policy Statement as it provides for efficient development and land use patterns and for an appropriate range and mix of housing options and densities required to meet projected requirements of current and future residents of the regional market area. There are no significant natural, cultural heritage, or archaeological resources requiring protection and no natural or man-made hazards to be considered.
Conformity with the Our City, Our Strategy, City Building, and Environmental Policies of this Plan	The proposal provides for residential intensification within the Urban Growth Boundary and supports Key Directions related to the creation of a mixed-use compact City and strong, healthy and attractive neighbourhoods. The massing and scale of the proposed buildings can be appropriately integrated into the community through the application of the relevant City Design policies at the Site Plan Approval stage.
Conformity with the policies of the place type in which they are located	The proposed two-storey semi-detached and townhouse proposal provides for the use and intensity of development contemplated within the Neighbourhoods Place Type on a Civic Boulevard. Compatible intensification is encouraged in existing neighbourhoods. (937_)
Consideration of applicable guideline documents that apply to the subject lands	No additional guideline documents apply to the subject lands.
The availability of municipal services, in conformity with the Civic Infrastructure chapter of this Plan and the Growth Management/Growth Financing policies in the Our Tools part of this Plan	The site will be fully serviced by municipal water, sanitary and storm.
Criteria on Adjacent Lands	Response
Traffic and access management	Further consideration of traffic controls related to the driveway will occur at the Site Plan Approval stage.
Noise	The proposed development is not expected to generate any unacceptable noise impacts on surrounding properties. A noise study was not required for the Zoning By-law amendment application.
Parking on streets or adjacent properties	The proposal includes a slight excess of parking relative to the 1.5 spaces per unit normally required for townhouse development. It is not anticipated that overflow parking will be required on local streets.
Emissions generated by the use such as odour, dust or other airborne emissions	The proposed development will not generate noxious emissions.
Lighting	Lighting details will be addressed at the Site Plan Approval stage. It is a Site Plan standard that any lighting fixture is to minimize light spill onto abutting properties.

Garbage generated by the use	Garbage facilities should be screened; storage inside the building is a standard requirement for townhouse forms, with garbage to be placed outside on collection day.
Privacy	<p>As noted in the report, there is an existing board-on-board fence that surrounds the site and several existing boundary trees located along the east property line are to be retained to screen the proposed townhouses from the east abutting townhouse development. Transom-style windows are proposed to further maintain the privacy of future tenants and existing adjacent homeowners. Additional mitigation measures will be considered at the time of Site Plan Approval, such as additional plantings along the east property line and architectural treatments.</p> <p>Privacy impacts are not anticipated on the west abutting townhouses as a result of a 0.7m reduction in the west interior side yard depth.</p>
Shadowing	Acceptable shadowing impacts on the abutting properties to the east and west are anticipated as a result of the proposed development. The abutting townhouses to the east are further setback from the east lot line by approximately 4.8 to 5.0 metres, which is an appropriate distance.
Visual Impact	Landscaping, articulated building design, and architectural details and materials are to be implemented at the Site Plan Approval stage, with a focus on enhancing the design of Unit 1 and providing additional screening measures for the surface parking area to establish a positive visual presence and to maintain an active frontage along Gainsborough Road.
Loss of Views	There are no view corridors to significant features or landmarks to be affected by the proposed development.
Trees and canopy cover	The proposed development will result in the loss of some trees and canopy cover in order to achieve more compact forms of development within the built-up part of the City. A total of 18 trees are recommended for preservation. At the Site Plan stage, a complete landscape plan will be developed to provide for new tree planting and screening from adjacent land uses.
Cultural heritage resources	The subject lands are identified as having archaeological potential on the City's 2018 Archaeological Mapping. Based on the findings from the Stage 1-2 Archaeological Assessment prepared by Lincoln Environmental Consulting Corp, no archaeological resources were identified on the lands and all archaeological conditions can be considered satisfied for this application.
Natural heritage resources and features	Not applicable.
Natural resources	Not applicable.
Other relevant matters related to use and built for	Not applicable.

3.7 Planning Impact Analysis	
Criteria	Response

<p>Compatibility of proposed uses with surrounding land uses, and the likely impact of the proposed development on present and future land uses in the area;</p>	<p>The proposed land use is a contemplated use in the Official Plan, similar to other uses in the area, and contributes to a variety of housing forms within the neighbourhood.</p>
<p>The size and shape of the parcel of land on which a proposal is to be located, and the ability of the site to accommodate the intensity of the proposed use;</p>	<p>The site concept achieves an intensity that allows for other on-site functions such as visitor and accessible parking, emergency services, and private and common amenity space.</p>
<p>The supply of vacant land in the area which is already designated and/or zoned for the proposed use;</p>	<p>There is no vacant land in the area which is already designated and/or zoned for the proposed use.</p>
<p>The proximity of any proposal for medium or high density residential development to public open space and recreational facilities, community facilities, and transit services, and the adequacy of these facilities and services;</p>	<p>The site is located within a 400 metres walking distance of public parks, a place of worship and an elementary school, and within 1000-1500 metres of commercial and service uses along Hyde Park Road and Fanshawe Park Road West. Bus service from the intersection of Gainsborough Road and Hyde Park Road, approximately 600-800 metres west of the site, provides public transit access to other parts of the City.</p>
<p>The need for affordable housing in the area, and in the City as a whole, as determined by the policies of Chapter 12 – Housing;</p>	<p>The proposal is not eligible to be considered for affordable housing as a bonus provision is not requested.</p>
<p>Compatibility of proposed uses with surrounding land uses, and the likely impact of the proposed development on present and future land uses in the area</p>	<p>The proposed land use is a contemplated use in the Official Plan, similar to other uses in the area, and contributes to a variety of housing forms within the neighbourhood.</p>
<p>The height, location and spacing of any buildings in the proposed development, and any potential impacts on surrounding land uses</p>	<p>The scale/height of the proposed two-storey semi-detached and townhouse development is appropriate at this location as the adjacent land uses include a two-storey townhouse development to the west of the site and a one-storey townhouse development to the east. Privacy impacts on the abutting townhouses to the east as a result of a reduced east interior side yard depth of 3.6 metres are intended to be mitigated by the retention of existing trees and transom windows, as noted in the report. No adverse impacts are anticipated on the west side of the property.</p>
<p>The extent to which the proposed development provides for the retention of any desirable vegetation or natural features that contribute to the visual character of the surrounding area</p>	<p>A total of 18 trees are recommended to be retained, with the majority located along the east lot line. Additional landscaping and screening opportunities will be considered at the Site Plan Approval stage that maintains, to the best extent possible, existing vegetation and introduces additional shrubs as needed to screen the proposed accessible and visitor parking and the overall development from adjacent properties.</p>
<p>The location of vehicular access points and their compliance with the City's road access policies and Site Plan Control By-law, and the likely impact of traffic generated by the proposal on City streets, on</p>	<p>Transportation Planning and Design was circulated on the planning application and development proposal and is satisfied that driveway location and design can be addressed at the Site Plan Approval stage. Gainsborough Road is an arterial road which serves high volumes of intra-urban traffic at moderate</p>

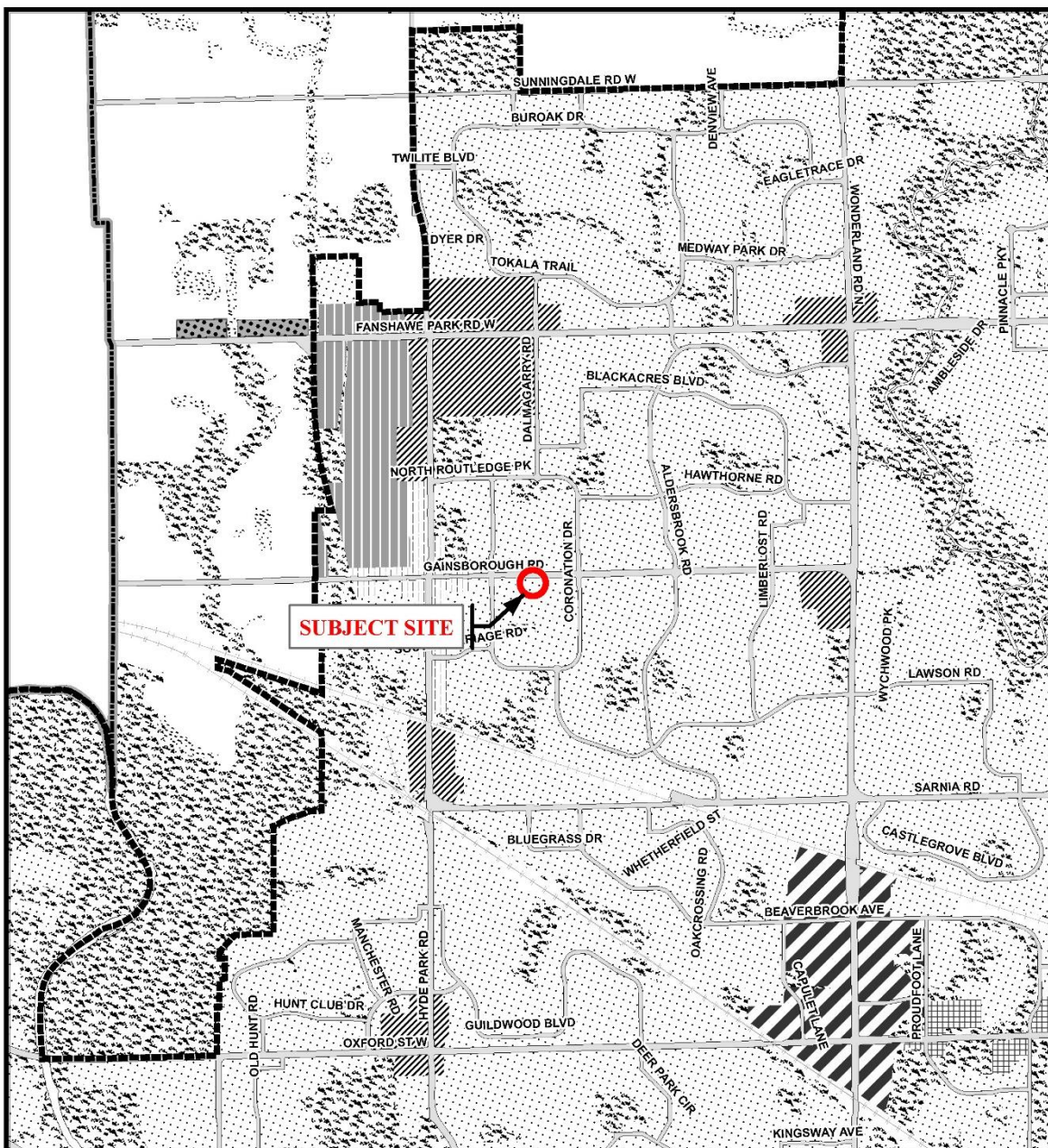
<p>pedestrian and vehicular safety, and on surrounding properties</p>	<p>speeds and has controlled or limited property access.</p>
<p>The exterior design in terms of the bulk, scale, and layout of buildings, and the integration of these uses with present and future land uses in the area</p>	<p>The orientation of the two-storey semi-detached block is intended to create a strong visual presence along Gainsborough Road and is in keeping with the building design of the adjacent development to the west while mirroring the development to the east. At the Site Plan stage, additional attention should be paid to the orientation and design of Unit 1 along Gainsborough Road, in accordance with comments provided by Urban Design staff.</p>
<p>The potential impact of the development on surrounding natural features and heritage resources</p>	<p>The subject lands are identified as having archaeological potential on the City's 2018 Archaeological Mapping. Based on the findings from the Stage 1-2 Archaeological Assessment prepared by Lincoln Environmental Consulting Corp, no archaeological resources were identified on the lands and all archaeological conditions can be considered satisfied for this application.</p>
<p>Constraints posed by the environment, including but not limited to locations where adverse effects from landfill sites, sewage treatment plants, methane gas, contaminated soils, noise, ground borne vibration and rail safety may limit development</p>	<p>Development is prohibited on top of the existing 6.0m wide sanitary sewer easement, which extends across the southwest corner of the site. The proposed west interior side yard setback of 5.6m ensures that sufficient space is provided between the townhouse building and existing easement.</p>
<p>Compliance of the proposed development with the provisions of the City's Official Plan, Zoning By-law, Site Plan Control By-law, and Sign Control By-law</p>	<p>The requested amendment is consistent with the in-force policies of the Official Plan. The majority of requirements of the Site Plan Control By-law have been considered through the design of the site, including provision of amenity space, drive aisle and driveway widths, sidewalk widths, and parking. The applicant is to identify areas for garbage storage and show parking setbacks and landscape islands through the Site Plan Approval process.</p>
<p>Measures planned by the applicant to mitigate any adverse impacts on surrounding land uses and streets which have been identified as part of the Planning Impact Analysis</p>	<p>As noted in the report, several existing boundary trees located along the east property line are to be retained to screen the proposed townhouses from the east abutting townhouse development. Transom-style windows are proposed to further maintain the privacy of future tenants and existing adjacent homeowners. Additional mitigation measures will be considered at the time of Site Plan Approval, such as additional plantings along the east property line and architectural treatments. As well, the owner will be required to provide a lot grading plan for stormwater flows and major overland flows on site demonstrating that all stormwater flows will be self-contained on site, up to the 100 year event and safely convey the 250 year storm event</p>

<p>Impacts of the proposed change on the transportation system, including transit</p>	<p>The residential intensification of the subject lands will have a negligible impact on the transportation system and provide a more transit-supportive form of development.</p>
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Appendix C – Relevant Background

Additional Maps

London Plan Designation



Legend

Downtown	Future Community Growth	Environmental Review
Transit Village	Heavy Industrial	Farmland
Shopping Area	Light Industrial	Rural Neighbourhood
Rapid Transit Corridor	Future Industrial Growth	Waste Management Resource Recovery Area
Urban Corridor	Commercial Industrial	Urban Growth Boundary
Main Street	Institutional	
Neighbourhood	Green Space	

This is an excerpt from the Planning Division's working consolidation of Map 1 - Place Types of the London Plan, with added notations.

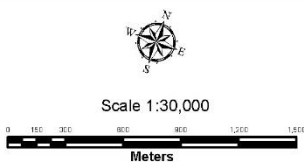
At the time of the printing of this map, the Rapid Transit EA is in progress. This map shows the Rapid Transit Corridors and Urban Corridors to recognize potential alignments. These Place Types will be modified to align with the results of the EA process for the final version of The London Plan.

CITY OF LONDON

Planning Services /
Development Services

LONDON PLAN MAP 1 - PLACE TYPES -

PREPARED BY: Planning Services



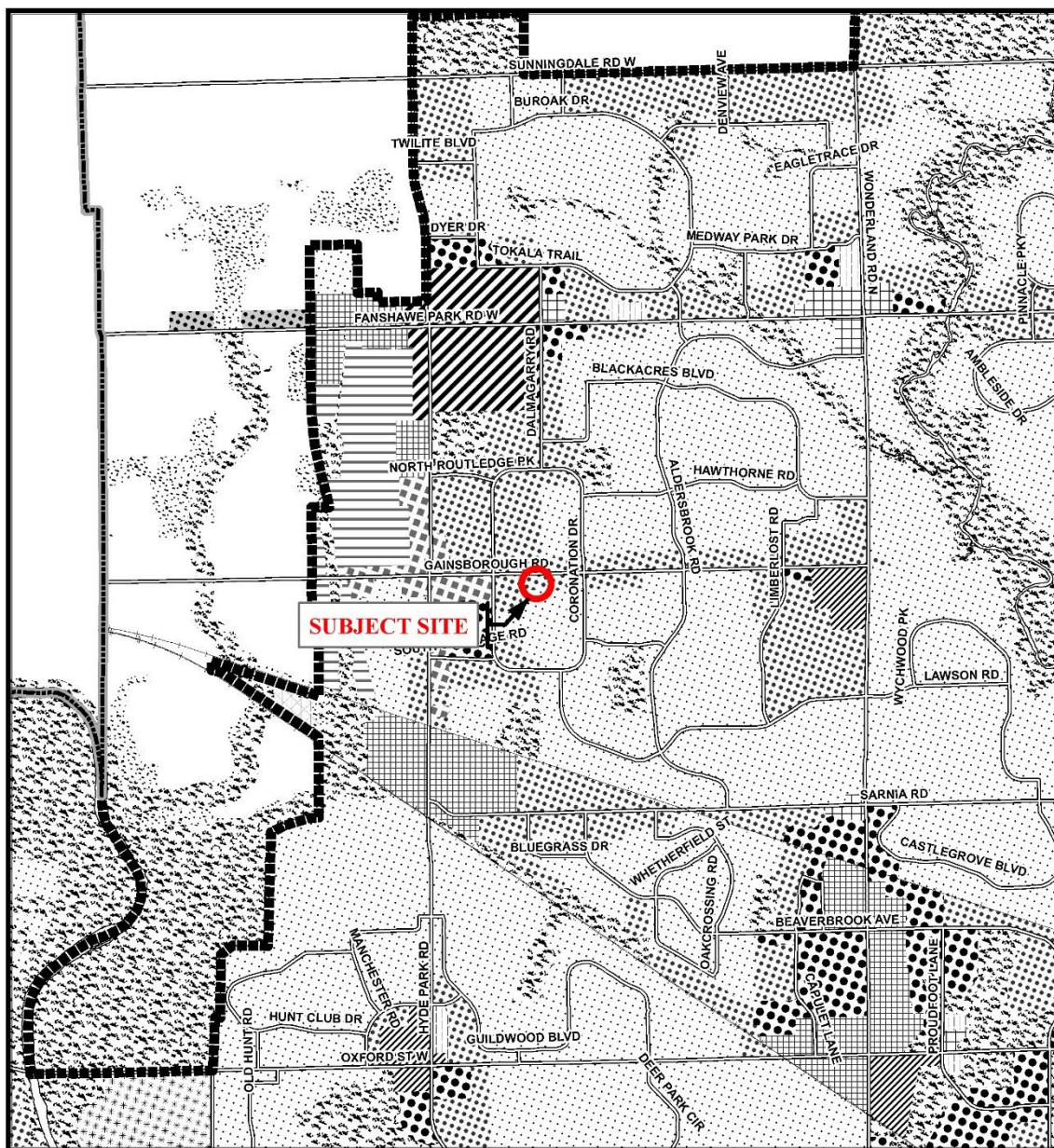
File Number: Z-9442

Planner: MW

Technician: RC

Date: February 9, 2022

Official Plan Designation



Legend		
Downtown	Multi-Family, Medium Density Residential	Office Business Park
Wonderland Road Community Enterprise Corridor	Low Density Residential	General Industrial
Enclosed Regional Commercial Node	Office Area	Light Industrial
New Format Regional Commercial Node	Office/Residential	Commercial Industrial
Community Commercial Node	Regional Facility	Transitional Industrial
Neighbourhood Commercial Node	Community Facility	Rural Settlement
Main Street Commercial Corridor	Open Space	Environmental Review
Auto-Oriented Commercial Corridor	Urban Reserve - Community Growth	Agriculture
Multi-Family, High Density Residential	Urban Reserve - Industrial Growth	Urban Growth Boundary

<p>CITY OF LONDON</p> <p>Planning Services / Development Services</p> <p>OFFICIAL PLAN SCHEDULE A - LANDUSE -</p> <p>PREPARED BY: Graphics and Information Services</p>	<p>Scale 1:30,000</p> <p>0 150 300 450 600 750 900 1,050 1,200</p> <p>Meters</p>	FILE NUMBER: Z-9442
		PLANNER: MW
		TECHNICIAN: RC
		DATE: 2022/02/09

Existing Zoning



COUNCIL APPROVED ZONING FOR THE SUBJECT SITE:

1) **LEGEND FOR ZONING BY-LAW Z-1**

- | | |
|--|---|
| <ul style="list-style-type: none"> R1 - SINGLE DETACHED DWELLINGS R2 - SINGLE AND TWO UNIT DWELLINGS R3 - SINGLE TO FOUR UNIT DWELLINGS R4 - STREET TOWNHOUSE R5 - CLUSTER TOWNHOUSE R6 - CLUSTER HOUSING ALL FORMS R7 - SENIOR'S HOUSING R8 - MEDIUM DENSITY/LOW RISE APTS. R9 - MEDIUM TO HIGH DENSITY APTS. R10 - HIGH DENSITY APARTMENTS R11 - LODGING HOUSE
 DA - DOWNTOWN AREA RSA - REGIONAL SHOPPING AREA CSA - COMMUNITY SHOPPING AREA NSA - NEIGHBOURHOOD SHOPPING AREA BDC - BUSINESS DISTRICT COMMERCIAL AC - ARTERIAL COMMERCIAL HS - HIGHWAY SERVICE COMMERCIAL RSC - RESTRICTED SERVICE COMMERCIAL CC - CONVENIENCE COMMERCIAL SS - AUTOMOBILE SERVICE STATION ASA - ASSOCIATED SHOPPING AREA COMMERCIAL
 OR - OFFICE/RESIDENTIAL OC - OFFICE CONVERSION RO - RESTRICTED OFFICE OF - OFFICE | <ul style="list-style-type: none"> RF - REGIONAL FACILITY CF - COMMUNITY FACILITY NF - NEIGHBOURHOOD FACILITY HER - HERITAGE DC - DAY CARE
 OS - OPEN SPACE CR - COMMERCIAL RECREATION ER - ENVIRONMENTAL REVIEW
 OB - OFFICE BUSINESS PARK LI - LIGHT INDUSTRIAL GI - GENERAL INDUSTRIAL HI - HEAVY INDUSTRIAL EX - RESOURCE EXTRACTIVE UR - URBAN RESERVE
 AG - AGRICULTURAL AGC - AGRICULTURAL COMMERCIAL RRC - RURAL SETTLEMENT COMMERCIAL TGS - TEMPORARY GARDEN SUITE RT - RAIL TRANSPORTATION
 "h" - HOLDING SYMBOL "D" - DENSITY SYMBOL "H" - HEIGHT SYMBOL "B" - BONUS SYMBOL "T" - TEMPORARY USE SYMBOL |
|--|---|

CITY OF LONDON

PLANNING SERVICES / DEVELOPMENT SERVICES

**ZONING
BY-LAW NO. Z-1
SCHEDULE A**



THIS MAP IS AN UNOFFICIAL EXTRACT FROM THE ZONING BY-LAW WITH ADDED NOTATIONS

FILE NO:

Z-9442

MW

MAP PREPARED:

2022/02/08

RC

1:1,500

0 5 10 20 30 40

Meters