

Report to Planning and Environment Committee

To: Chair and Members
Planning and Environment Committee

From: George Kotsifas P. Eng.,
Deputy City Manager, Planning and Economic Development

Subject: 1140 Sunningdale Road East
Public Participation Meeting

Date: January 10, 2022

Recommendation

That, on the recommendation of the Director, Planning and Development, the following actions be taken with respect to the application of 2839069 Ontario Inc. c/o Royal Premier Homes relating to the property located at 1140 Sunningdale Road East:

- (a) the proposed by-law attached hereto as Appendix "A" **BE INTRODUCED** at the Municipal Council meeting on January 25, 2022 to amend Zoning By-law No. Z.-1, in conformity with the Official Plan for the City of London (1989), to change the zoning of the subject property **FROM** a Convenience Commercial Special Provision (CC(14)) Zone, **TO** a compound Convenience Commercial Special Provision/Residential R8 Special Provision Bonus (CC4(_)/R8-4(_))•H16•B(_)) Zone;

The Bonus Zone shall be implemented through one or more agreements to facilitate the development of a mixed-use apartment building, with a maximum density of 100 units per hectare, in general conformity with the Site Plan, Elevations and Renderings attached as Schedule "1" to the amending by-law, and provides for the following:

- 1) Exceptional Site and Building Design
 - i. A building placement that is street-oriented and which reinforces the existing window-street context along Sunningdale Road East to provide for continuity of the built street-wall.
 - ii. The provision of a pedestrian walkway across the front of the subject lands that functions as a continuation of the city sidewalk located west of the subject lands on the north side of Pleasantview Drive, and connecting to the city sidewalk located east of the subject lands on the north side of Sunningdale Road East.
 - iii. The provision of yard depths along all edges of the proposed development to accommodate a landscaped buffer able to support tree growth and screen the proposed development from adjacent residential uses.
 - iv. The provision of enhanced landscaping along Sunningdale Road East to screen any surface parking areas located in the front yard from the city-owned boulevard.
 - v. A well pronounced, street-oriented principal building entrance for residential uses
 - vi. A well pronounced, street-oriented unit entrance for commercial uses with large expanses of clear glazing, a wrap around canopy and signage.
 - vii. Individual ground-floor residential unit access and private individual courtyards on the street-facing (south) elevation.
 - viii. Inset balconies to screen views from the proposed development to the existing single detached dwellings to the west.

- ix. A high-level of articulation and architectural detailing on the street-facing front facade for visual interest.
 - 2) A minimum of 80% of the required parking spaces provided underground.
 - 3) A minimum of 5% of the required parking spaces fitted with electric vehicle charging stations
 - 4) Provision of Affordable Housing
 - i. A total of two (2) 1-bedroom units will be provided for affordable housing.
 - ii. Rents not exceeding 80% of the Average Market Rent for the London Census Metropolitan Area as determined by the Canadian Mortgage and Housing Corporation at the time of building occupancy.
 - iii. The duration of affordability set at 50 years from the point of initial occupancy.
 - iv. The proponent is to enter into a Tenant Placement Agreement with the Corporation of the City of London to align the affordable units with priority populations.
- (b) IT **BEING NOTED** that the following site plan matter(s) was (were) raised during the application review process to be addressed through the Site Plan Approval process:
- 1) The noise recommendations and warning clauses contained in the Environmental Noise Assessment Report – 1140 Sunningdale Road East prepared by Strik Baldinelli Moniz Ltd. dated May 2021 assessing predicted noise levels resulting from road traffic volumes (Sunningdale Road East) on the proposed development be considered by the Site Plan Approval Authority for inclusion in any Site Plan and Development Agreement.
- (c) pursuant to Section 34(17) of the *Planning Act*, as determined by the Municipal Council, no further notice **BE GIVEN** in respect of the proposed by-law as the recommended zoning implements the site concept submitted with the application.

Executive Summary

Summary of Request

The Applicant, 2839069 Ontario Inc. c/o Royal Premier Homes, has requested a change to the zoning of the subject lands from a Convenience Commercial Special Provision (CC(14)) Zone, to a compound Convenience Commercial Special Provision/Residential R8 Special Provision Bonus (CC4(_)/R8-4(_)-H16-B(_)) Zone to permit and facilitate the development of a 4-storey mixed-use apartment building with convenience commercial uses on the ground floor.

The existing permitted convenience commercial uses would continue to be permitted restricted to locations within apartment buildings and all without a drive-through facility. The existing permitted convenience commercial uses include: Florist Shops; Convenience Service Establishments; Convenience Stores; Financial Institutions; and Personal Service Establishments.

Requested special provisions would provide a maximum commercial gross floor area and reduced commercial parking rate. Requested special provisions would provide an increased minimum front, rear and west interior side yard depth and reduced minimum east interior side yard depth in response to the site-specific context. Requested special provisions would provide a minimum yard depth along all lot lines for any underground parking ramps to ensure ramps are located away from adjacent properties. A height symbol would be site-specifically applied to permit a maximum building height above the standard maximum permitted by the requested zone. A bonus zone would be site-specifically applied to permit a maximum density above the standard maximum

permitted by the requested zone, in return for facilities, services and matters in the public interest, including affordable housing.

Purpose and Effect of Recommended Action

The purpose and effect of the recommended action is to permit and facilitate the development of the subject lands for a 4-storey mixed-use apartment building consisting of 42-residential dwelling units (including 2-affordable housing units) and a commercial gross floor area of 250m² (2,691.0 ft²).

Rationale of Recommended Action

1. The recommended amendment is consistent with the Provincial Policy Statement, 2020, as it will contribute to the mix of residential types and housing options (including affordable housing) available to address diverse housing needs; is a compact form of development that will use land, infrastructure, and public service facilities efficiently; and provides for infill and residential intensification at an appropriate location identified and supported by municipal policy directions.
2. The recommended amendment conforms to the in-force policies of The London Plan that contemplate low-rise apartment buildings as a primary permitted use on lands identified as Neighbourhoods and located on major streets. The proposed convenience commercial use will be scaled appropriately for the in-force policies that aim to achieve an appropriate range of commercial uses, including retail, service, and office uses, within the Neighbourhoods Place Type. The proposed development will provide for residential intensification in a form that can minimize and mitigate the impacts of the development on adjacent properties thereby being sensitive, compatible and a good fit with its context.
3. The recommended amendment conforms to the in-force policies of the 1989 Official Plan that contemplates low-rise apartment buildings as primary permitted uses and convenience commercial uses as secondary permitted uses on lands identified as Multi-Family, Medium Density Residential on major streets. Convenience commercial uses are contemplated as stand-alone uses or on the ground floor of apartment buildings. The proposed development will provide for convenience commercial uses that are appropriately sized and neighbourhood-oriented serving the needs of the surrounding residents.
4. The proposed development is eligible for bonus zoning under the bonus zoning criteria in the 1989 Official Plan and will secure public benefit and site and building design elements that are commensurate to the additional building density.
5. The use of bonus zoning will secure two (2) affordable housing units within the proposed development in support of Municipal Council's commitment to the Housing Stability Action Plan, Strategic Area of Focus 2: Create More Housing Stock to meet current and future needs for affordable housing.
6. The use of bonus zoning will secure electric vehicle charging stations for residents in support Municipal Council's commitment to minimizing and mitigating climate change.

Linkage to the Corporate Strategic Plan

Building a Sustainable City – London's growth and development is well planned and sustainable over the long term.

Climate Emergency

On April 23, 2019, Municipal Council declared a Climate Emergency. Through this declaration the Corporation of the City of London (the "City") is committed to reducing and mitigating climate change by encouraging sustainable and resilient development and directing intensification and growth to appropriate locations. This includes the efficient use of urban land and infrastructure, support for active modes of transportation and transit, and directing development away from natural hazards to minimize and mitigate risk.

Analysis

1.0 Background Information

1.1 Previous Reports Related to this Matter

None.

1.2 Property Description

The subject lands are known municipally as 1140 Sunningdale Road East and are located on the north side of Sunningdale Road East, about 900 metres east of Adelaide Street North, in the northeast quadrant of the city. Together with the lands to north, the subject lands contain a grouping of non-residential buildings that have been used as a Florist Shop (Springhill Flowers) since approximately 1985 (see Figure 1).

The subject lands are regular and rectangular in shape and are relatively flat. The lot area is approximately 0.45 hectares (1.11 acres) in size, the lot frontage is approximately 75.3 metres (247.0 feet), and the lot depth is approximately 60.2 metres (197.5 feet). It appears that trees were removed from the subject lands between 2015 and 2018 and the subject lands are now relatively clear of vegetation.

The immediate surrounding land uses include planned and existing single detached dwellings to the north and to the west; a converted (3-unit) dwelling to the east; and on the south side of Sunningdale Road East a mix of land uses consisting of a secondary school and single detached and cluster townhouse dwellings.

Figure 1 – Subject Lands Google 3D



1.3 Current Planning Information (see more detail in Appendix D)

- The London Plan Place Type – Neighbourhoods (frontage Civic Boulevard)
- Official Plan Designation – Multi-Family, Medium Density Residential
- Existing Zoning – Convenience Commercial Special Provision (CC(14)) Zone

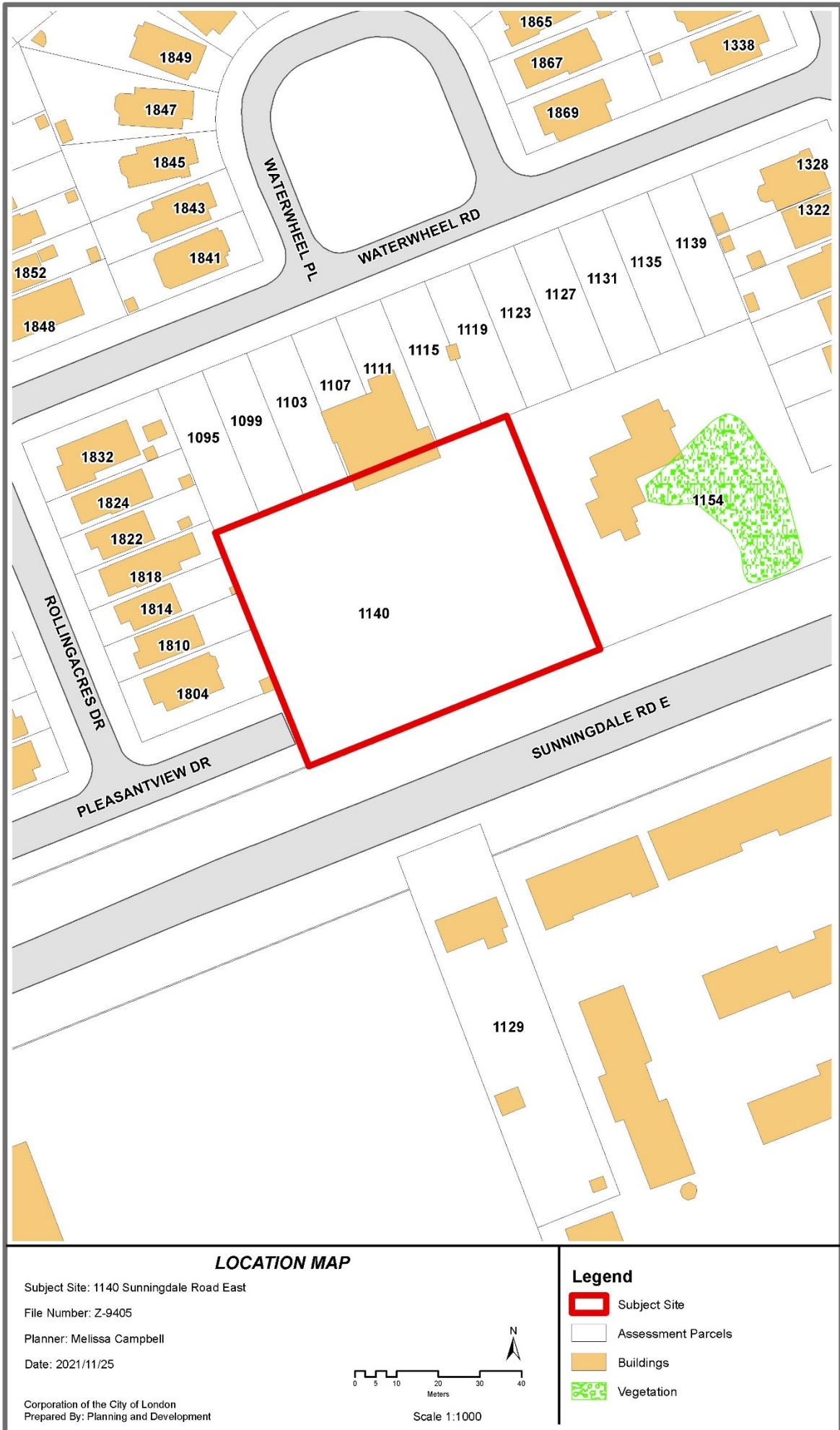
1.4 Site Characteristics

- Current Land Use – Florist Shop
- Frontage – 75.3 metres (247.0 feet)
- Depth – 60.2 metres (197.5 feet)
- Area – 0.45 hectares (1.11 acres)
- Shape – Regular

1.5 Surrounding Land Uses

- North – Vacant Lots and Single Detached Dwellings
- East – Converted Dwelling (3-units) and Single Detached Dwellings
- South – Secondary School, Single Detached Dwellings, and Cluster Townhouse Dwellings
- West – Single Detached Dwellings

1.6 Location Map



1.7 Planning History

The subject lands have been involved in several planning and development applications.

The surrounding lands located north, east and west of the subject lands (excluding 1154 Sunningdale Road East) have developed through various phases of a Plan of Subdivision (City of London File No. 39T-99515):

- The lands to the northwest developed as Phase 1, that was registered in 2002 (Plan 33M451);
- The lands to the northeast developed as Phase 2, that was registered in 2004 (Plan 33M484); and
- The lands to the north developed as Phase 3, that was registered in 2006 (Plan 33M540).

In 2002, a request was made to modify the Plan of Subdivision (Phase 2) to include the rear (north) portion of 1140 Sunningdale Road East to provide for a normalized street network, eliminating temporary dead ends and providing for the completion of Street 'H' that became known as Waterwheel Road.

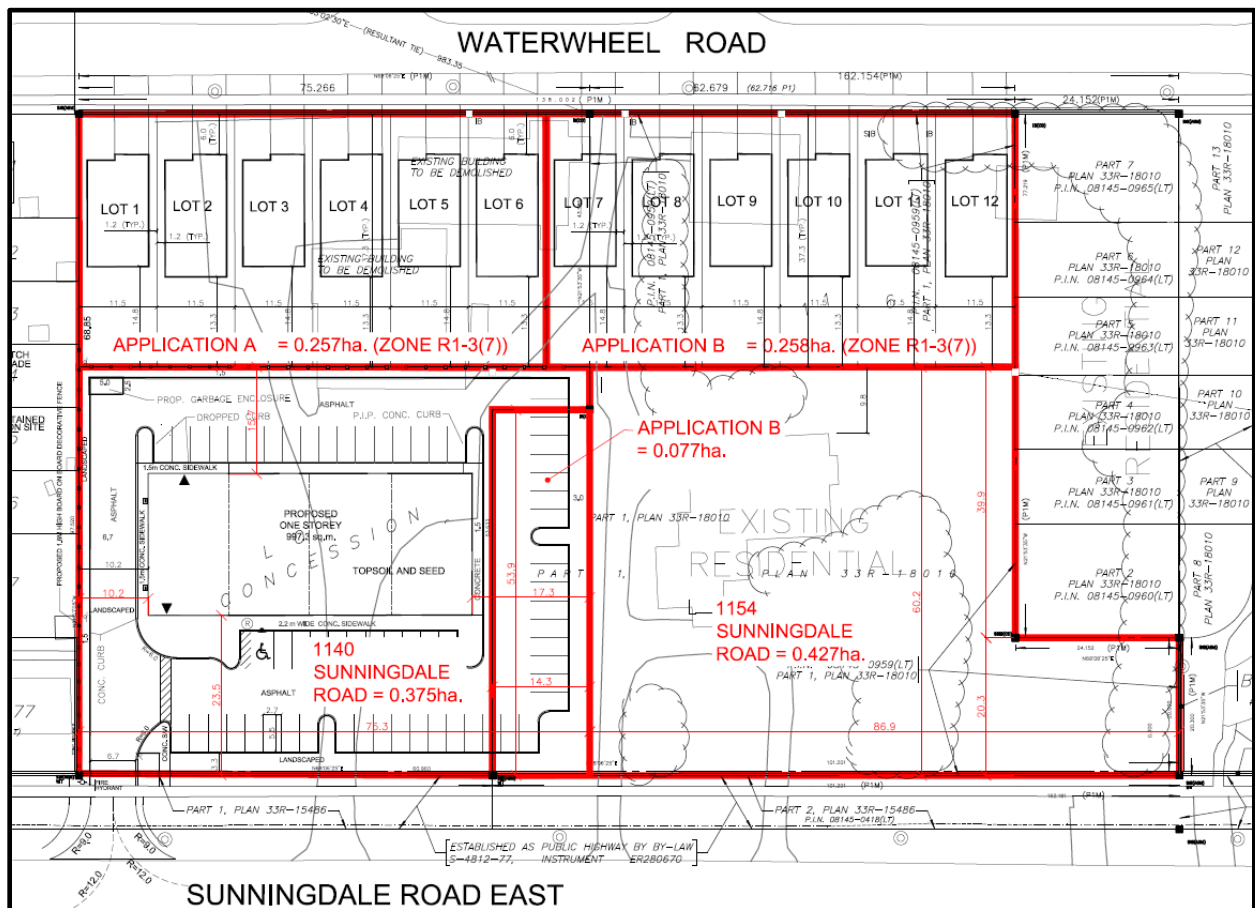
In 2015, a Site Plan Application (City of London File No. SP 15-023249) requested approval for a 1-storey building for a Florist Shop (Springhill Flowers) on the subject lands that complied with the existing Convenience Commercial Special Provision (CC(1)) Zone. The conceptual site plan submitted with the application showed the removal of the existing grouping of non-residential buildings on the rear (north) portion of the subject lands and a proposed 1-storey building on the front (south) portion of the subject lands. The proposed 1-storey building consisted of one (1) unit and a gross floor area of approximately 248m² (2,669 ft²), with the potential for a total of four (4) units through future development phases. The Site Plan Application was approved by the Site Plan Approval Authority but was not implemented by the landowner.

In 2017, two (2) Consent Applications (City of London File No. B.034/17 and B.035/17) requested to sever and create a total of twelve (12) lots on the rear (north) portion of 1140 and 1154 Sunningdale Road East and adjust the easterly lot line between 1140 and 1154 Sunningdale Road East. The London Consent Authority issued provisional consent approvals subject to conditions.

In the same year, a Zoning By-law Amendment Application (City of London File No. Z-8805) requested to change the zoning of 1140 Sunningdale Road East from a Convenience Commercial Special Provision (CC(1)) Zone to a Residential R1 Special Provision (R1-3(7)) Zone and a Convenience Commercial Special Provision (CC(14)) Zone; and to change the zoning of 1154 Sunningdale Road East from an Urban Reserve Special Provision (UR1(1)) Zone to a Residential R1 Special Provision (R1-3(7)) Zone and a Urban Reserve Special Provision (UR1(1)) Zone. The purpose and effect of the Zoning By-law Amendment was to permit and to facilitate the proposed lot creation, lot adjustment and redevelopment of the subject lands for convenience commercial uses.

The conceptual site plan submitted with the Zoning By-law Amendment Application showed the removal of the existing grouping of non-residential buildings on the north (rear) portion of the subject lands and twelve (12) lots for future single detached dwellings fronting onto the south side of Waterwheel Road. The conceptual site plan showed a proposed 1-storey building with 4-commercial units and a gross floor area of approximately 997m² (10,731.6 ft²) on the front (south) portion of the subject lands (see Figure 2). Municipal Council approved the Zoning By-law Amendment and directed that the London Consent Authority be advised that Municipal Council does not support conditions of consent that would require a road allowance be provided to connect the east and west-legs of Pleasantview Drive based on the public comments received through the Zoning By-law Amendment Application. Development of the 1-storey commercial building could be implemented through the prior 2015 Site Plan Approval.

Figure 2 – Conceptual Site Plan submitted with 2017 Zoning By-law Amendment Application (City of London File No. Z-8805)



The provisional consent approvals lapsed in 2019 due to the failure of the applicant to satisfy all conditions before the lapse date identified in the decision of the London Consent Authority. As is common in these situations the applicant submitted two (2) new Consent Applications (City of London File No. B.022/19 and B.023/19) for the same purpose as the prior Consent Applications. The London Consent Authority issued provisional consent approvals subject to conditions, and all conditions were fulfilled within the lapse period and final consent approvals provided.

In 2021, a subsequent Consent Application (City of London File No. B.009/21) requested to sever and convey a remnant portion of 1140 Sunningdale Road East to one of the lots that was severed and created from 1154 Sunningdale Road East. The subsequent Consent Application was required due to inconsistencies between the consent sketch submitted with the 2019 Consent Application (B.022/19) and the final reference plan that was prepared by an Ontario Land Surveyor. The London Consent Authority issued provisional consent approval subject to conditions, and all conditions were fulfilled within the lapse date and final consent approval provided.

Shortly after the 2021 Consent Application, the ownership of the subject lands transferred to 2839069 Ontario Inc. c/o Royal Premier Homes (the "Applicant"). In July 2021, Siv-ik Planning and Design Inc. (the "Applicant's Agent") submitted the current Zoning By-law Amendment Application (City of London File No. Z-9405) to permit and facilitate the development of the subject lands for a 4-storey mixed-use apartment building.

2.0 Discussion and Considerations

2.1 Development Proposal

The Applicant has proposed to demolish the existing grouping of non-residential buildings and develop the subject lands for a 4-storey mixed-use apartment building (see Figures 3 and 4). The proposed mixed-use apartment building will contain 42-residential dwelling units and a commercial gross floor area of 250 m² (2,691 ft²). The proposed convenience commercial space may include Springhill Flowers which is the current use on the site. 64 underground parking spaces are proposed for the residential

use and would be accessed via an underground parking ramp located on the west side of the subject lands. 10 surface parking spaces are proposed for the convenience commercial uses and are located between the front of the proposed building and Sunningdale Road East. The vehicular driveway access to Sunningdale Road East is proposed on the west side of the subject lands and is generally aligned with an existing driveway access for Mother Teresa Catholic Secondary School on the south side of Sunningdale Road East. The proposal does not contemplate the completion of the window-street network across the front of the subject lands to connect the west-leg of Pleasantview Drive to the east-leg of Pleasantview Drive. Only pedestrian access is proposed to connect from the west-leg of Pleasantview Drive across the front of the subject lands.

Figure 3 – Conceptual Site Plan submitted with current Zoning By-law Amendment Application (City of London File No. Z-9405)

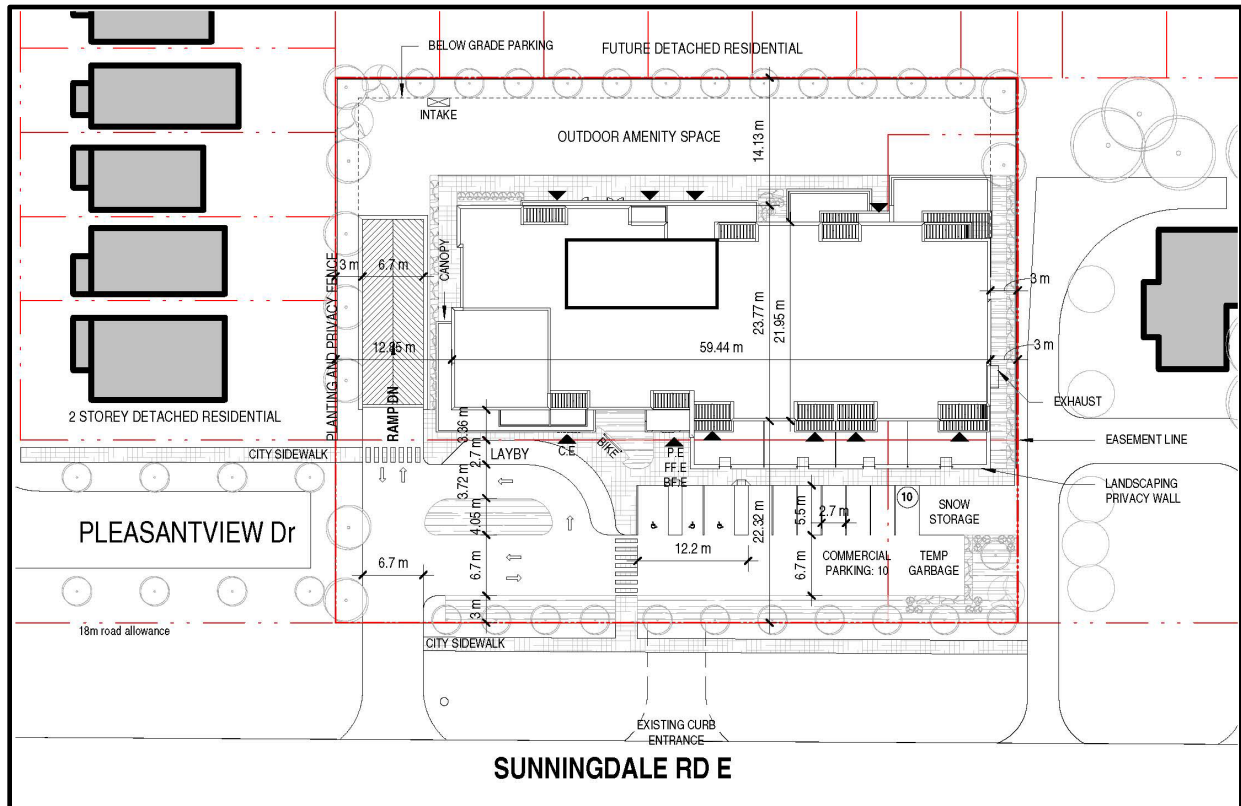
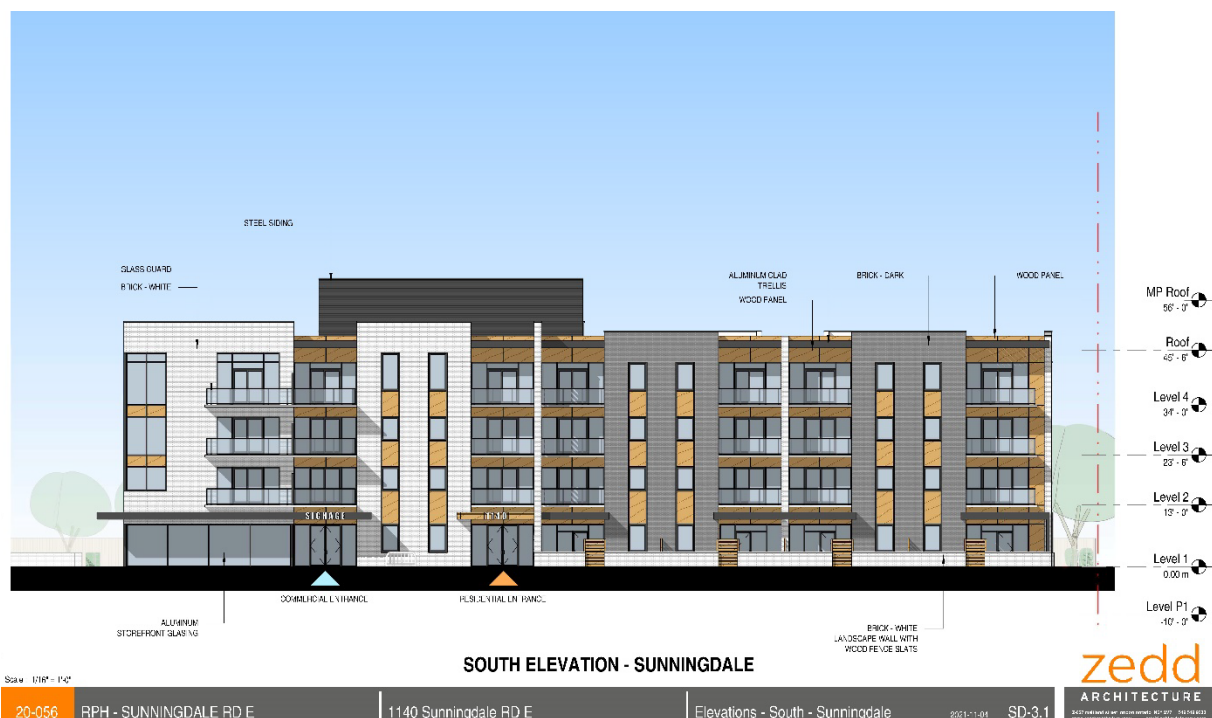
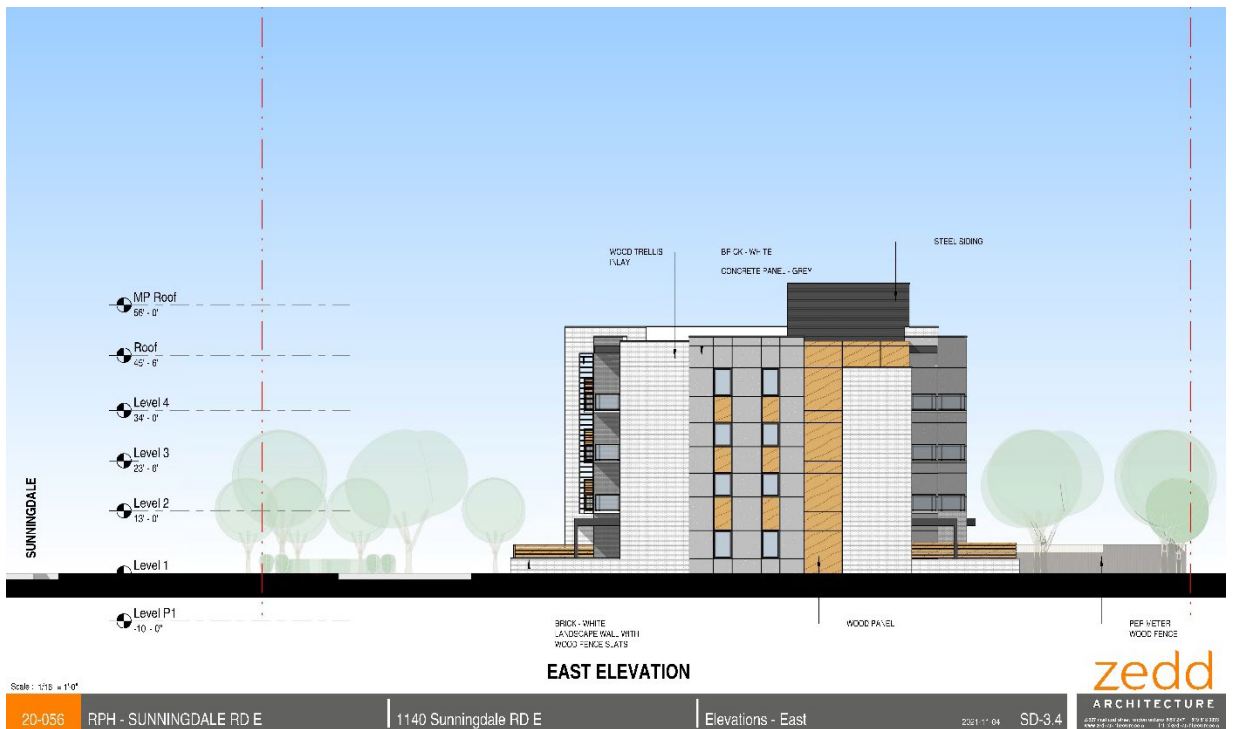


Figure 4– Elevations submitted with current Zoning By-law Amendment Application (City of London File No. Z-9405)





2.2 Requested Amendment

The Applicant's initial request was to change the zoning of the subject lands from a Convenience Commercial Special Provision (CC(14)) Zone to a Residential R8 Special Provision Bonus (R8-4()•B()) Zone to permit and facilitate the development of a 4-storey mixed-use apartment building with convenience commercial uses on the ground floor. Special provisions would add existing permitted convenience commercial uses (i.e. Florist Shops, Convenience Service Establishments, Convenience Stores, Financial Institutions and Personal Service Establishments all without a drive-through facility) as additional permitted uses to the R8-4 Zone.

The R8-4 Zone provides for and regulates development in the form of low-rise apartment buildings but does not make mention of convenience commercial uses in the description of the general purpose and intent of the zone (Zoning By-law No. Z.-1, Section 29.1). Planning and Development staff suggested that based on the structure of the Z.-1 Zoning By-law, a compound zone be considered and a CC4 Zone that provides for convenience commercial uses restricted to locations within apartment buildings be added to the requested amendment (Zoning By-law No. Z.-1, Section 12.1).

The Applicant subsequently modified their request to reflect a compound Convenience Commercial Special Provision/Residential R8 Special Provision Bonus Zone (CC4()/R8-4()•H16•B()) Zone. Other modifications in consultation with Planning and Development staff included increasing the requested east interior side yard depth to provide sufficient space for tree plantings, adding a minimum yard depth to all lot lines for any underground parking ramps to ensure ramps are located away from adjacent properties, and adding a height symbol in addition to the special provision for an increase maximum building height. Noting that for heights over 13.0 metres (42.7 feet) the R8 Zone variations require that height be applied site-specifically to the zoning maps.

The requested special provisions to the CC4 Zone would permit and regulate the following:

- Additional Permitted Uses to include Florist Shops restricted to a location within an apartment building and without a drive-through facility. (It being noted that all other existing permitted convenience commercial uses are standard permitted uses of the CC4 Zone and don't need to be recognized through special provisions, they are Convenience Service Establishments, Convenience Stores, Financial Institutions and Personal Service Establishments all restricted to a location within an apartment building and without a drive-through facility.)
- A maximum gross floor area of 250m² (2,691ft²) for all permitted commercial uses
- A reduced minimum parking rate of 1 space/25 m² for all permitted commercial uses; whereas the most onerous minimum parking rate amongst the existing permitted convenience commercial uses is 1 space/10m² for Personal Service Establishments. (It being noted that the requested minimum parking rate would require 10 spaces for the maximum allowable gross floor area for all permitted commercial uses.)

The requested special provisions to the R8-4 Zone would permit and regulate the following:

- An increased minimum front yard depth of 22.0 metres (72.2 feet) as measure from the front lot line existing on the date of passing this by-law; whereas, a minimum front yard depth of 8.0 metres (26.3 feet) is required based on the minimum front yard depth standard rate and the proposed maximum height of 16.0 metres (52.5 feet). (It being noted that the requested wording for the minimum front yard depth would ensure that the depth as specified would continue to comply with the Zoning By-law should the limit of the Sunningdale Road East ultimate road allowance be increased in the future).

- A reduced east interior side yard depth of 3.0 metres (9.8 feet); whereas a minimum interior side yard depth of 6.0 metres (19.7 feet) is required based on the minimum interior side yard depth standard rate and the proposed maximum height of 16.0 metres (52.5 feet). (It being noted that the initial request was 2.3 metres (7.5 feet).)
- An increased minimum rear and west interior side yard depth expressed as a rate of 1.0 metre (3.2 feet) per 1.0 metre (3.2 feet) of main building height or fraction thereof above 3.0 metres (9.8 feet), but in no case less than 7.5 metres (24.6 feet); whereas the minimum rear and interior side yard depth standard rate is 1.2 metres (3.9 feet) per 3.0 metres (9.8 feet) of main building height or fraction thereof above 3.0 metres (9.8 feet), but in no case less than 4.5 metres (14.8 feet). (It being noted that the increased minimum rear and west interior side yard depth and standard minimum rear and west interior side yard depth expressed as absolute numbers would be 13.0 metres (42.7 feet) and 6.0 metres (19.7 metres) respectively based on the proposed maximum height of 16.0 metres (52.5 feet).)
- For underground parking ramps, a minimum 3.0 metre (9.8 feet) yard depth to all lot lines.
- An increased maximum height of 16.0 metres (52.5 feet) or 4-storeys, whichever is less; whereas a maximum height of 13.0 metres (42.7 feet) is permitted by the R8 Zone variations.

The requested bonus zone would permit an increased maximum density of 100 units per hectare (uph) in return for facilities, services and matters in the public interest; whereas a maximum density of 75 uph is permitted.

2.3 Policy Context

2.3.1 Provincial Policy Statement, 2020

The Provincial Policy Statement (“PPS”), 2020 provides policy direction on matters of provincial interest related to land use planning and development.

The PPS encourages healthy, livable and safe communities which are sustained by promoting efficient land use patterns and development which supports the financial well-being of the Province and municipalities (PPS, Policy 1.1.1 a)). Healthy, livable and safe communities are sustained by an appropriate range and mix of residential types (including affordable and market-based housing), employment, institutional, recreation and open space and other uses to meet long-term needs (PPS, Policy 1.1.1 b)). The PPS promotes intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs (PPS, Policy 1.1.1 e)).

The PPS encourages settlement areas to be the focus of growth and development (PPS, Policy 1.1.3.1). Land use patterns within settlement areas shall be based on densities and a mix of land uses which efficiently use land and resources, are appropriate for infrastructure and public service facilities and support active transportation and are transit-supportive (PPS, Policy 1.1.3.2). Within settlement areas, planning authorities are to identify appropriate locations to accommodate intensification and redevelopment and transit-supportive development (PPS, Policy 1.1.3.3). Appropriate development standards will facilitate intensification, redevelopment and compact form while maintaining appropriate levels of public health and safety (PPS, Policy 1.1.3.4).

The PPS also promotes an appropriate range and mix of housing types and densities to meet projected market-based and affordable housing needs of current and future residents (PPS, Policy 1.4.3). The PPS directs that development standards be established for residential intensification and redevelopment and for new residential development which minimizes the cost of housing and facilitates a compact form while maintaining appropriate levels of public health and safety (Policy 1.4.3 f)). The PPS also identifies that long term economic prosperity should be supported by encouraging residential uses to respond to dynamic market-based needs and provide necessary housing supply and range of housing options; and by optimizing the long-term

availability and use of land, resources, infrastructure and public service facilities (PPS, Policy 1.7.1 b) and c)).

In accordance with Section 3 of the Planning Act, R.S.O. 1990 c. P.13 (Planning Act), all planning decisions “*shall be consistent with*” the PPS.

2.3.2 *The London Plan*

The London Plan is the new Official Plan for the City of London (Council adopted, approved by the Ministry with modifications, and the majority of which is in force and effect). The London Plan policies under appeal to the Ontario Land Tribunal (Appeal PL170100) and not in force and effect are indicated with an asterisk throughout this report. The London Plan policies under appeal are included in this report for informative purposes indicating the intent of Municipal Council but are not determinative for the purposes of this Zoning By-law Amendment application.

The London Plan (and the earlier 1989 Official Plan) contain policies that guide the use and development of land within the city and are consistent with the policy direction set out in the PPS. All lands in the city are assigned a place type (or land use designation in the 1989 Official Plan) and the policies associated with a place type (or designation) provide for a general range of uses, form and intensity of development that may be contemplated.

The subject lands are located within the Neighbourhoods Place Type on *Map 1 – Place Types with frontage on a Civic Boulevard (Sunningdale Road East) on Map 3 – Street Classifications in The London Plan. The range of permitted uses and the intensity of development contemplated in the Neighbourhoods Place Types varies depending upon the street classification onto which the property has frontage (The London Plan, Policies 789_6. and 919_2.).

Within the Neighbourhoods Place Type with frontage on a Civic Boulevard, a broad range of residential uses are contemplated including, but not limited to, single detached, semi-detached, duplex and converted dwellings, triplexes, fourplexes, townhouses, stacked townhouses and low-rise apartments (The London Plan, Table 10 - Range of Permitted Uses in Neighbourhoods Place Type). Mixed-use buildings are contemplated at the intersection of two or more major streets and must include residential uses and may also include appropriately-sized retail, service and office uses on the ground floor to service the surrounding neighbourhoods (The London Plan, Policy 925_, Table 10 - Range of Permitted Uses in Neighbourhoods Place Type and *Table 12 – Retail, Service and Office Floor Area Permitted in Neighbourhood Place Type).

With respect to intensity of development, The London Plan provides direction on minimum and maximum building height but does not provide general direction on density within the Neighbourhoods Place Type. Within the Neighbourhoods Place Type with frontage on a Civic Boulevard, the range of building heights contemplated include a minimum height of 2-storeys and a maximum height of 4-storeys, and up to 6-storeys through Bonus Zoning (The London Plan, *Table 11 - Range of Permitted Heights in Neighbourhoods Place Type). The London Plan also contemplates gross floor area maximums for retail, service, and office use in the Neighbourhoods Place Type in mixed-use buildings or stand-alone, conditional on the classification of the intersecting streets (The London Plan, Policy *935_2. and *Table 12 – Retail, Service and Office Floor Area Permitted in Neighbourhoods Place Type).

To achieve the vision and key directions of The London Plan, residential intensification within existing neighbourhoods is encouraged to provide opportunities for aging in place, diversity of built form, affordability, vibrancy, and the effective use of land in neighbourhoods (The London Plan Policy, 937_). The London Plan supports all forms of intensification, including infill development, with the understanding that intensification should be appropriately located, compatible, and fit well within receiving neighbourhood (The London Plan Policies 80_4., 83_, 939_ 5., 940_).

2.3.3 *1989 Official Plan*

The 1989 Official Plan is still in force and effect. The subject lands are located within the Multi-Family, Medium Density Residential (“MFMDR”) designation on Schedule A – Land Use in the 1989 Official Plan. The MFMDR designation contemplates multi-unit

residential development having a low-rise profile and densities that exceed those found in Low Density Residential areas (1989 Official Plan, Section 3.3). The MFMDR designation will provide for greater variety and choice in housing at locations that have desirable attributes, but may not be appropriate as Multi-Family, High Density Residential areas (1989 Official Plan, Section 3.3). Low-rise apartment buildings are contemplated as a primary permitted use within the MFMDR designation (1989 Official Plan, Section 3.3.1). Existing convenience commercial uses are contemplated as secondary permitted uses within the MFMDR designation and new convenience commercial uses are also permitted by Official Plan and Zoning By-law Amendments (1989 Official Plan, Section 3.3.1 ii)).

The scale of development within the MFMDR designation usually will not exceed 4-storeys in height and 75 uph in density (1989 Official Plan, Section 3.3.3 i) and ii)). Exceptions to the usual density limit, up to 100 uph, can be contemplated where developments qualify under the bonusing criteria in the 1989 Official Plan, however the height limit of 4-storeys will remain in effect (1989 Official Plan, Section 3.3.3 ii)). The Planning Impact Analysis (“PIA”) criteria in the 1989 Official Plan, are to be used to evaluate the appropriateness of density bonusing and identify ways to reduce any adverse impacts on surrounding land uses (1989 Official Plan, Sections 3.3.3 ii) and 3.7).

Convenience commercial uses within residential designations are intended to be neighbourhood-oriented and function at a neighbourhood-scale (The 1989 Official Plan, Section 3.6.5 i)). The intensity of convenience commercial uses will be specified in the Zoning By-law and will be at a scale compatible with surrounding land uses (The 1989 Official Plan, Section 3.6.5 iv)).

2.4 Community Engagement (see more detail in Appendix B)

Notice of Application was published in the Public Notices and Bidding Opportunities section of The Londoner on September 16, 2021 and sent to property owners in the surrounding area on September 15, 2021.

The notice advised of a possible amendment to the Z.-1 Zoning By-law to change the zoning from a Convenience Commercial Special Provision (CC(14)) Zone to a Residential R8 Special Provision Bonus (R8-4()•B()) Zone. Special provisions would add existing permitted convenience commercial uses (i.e. Florist Shops, Convenience Service Establishments, Convenience Stores, Financial Institutions and Personal Service Establishments all without a drive-through facility) as additional permitted to the R8-4 Zone. Special provisions would permit an increased maximum building height; an increased minimum front, rear and west interior side yard depth; a reduced east interior side yard depth, a maximum gross floor area for all permitted commercial uses, and a reduced minimum parking rate for all permitted commercial uses. A bonus zone would permit an increased maximum density in return for facilities, services and matters in the public interest.

The Applicant would later modify their request to reflect a compound Convenience Commercial Special Provision/Residential R8 Special Provision Bonus Zone (CC4()/R8-4()•H16•B()) Zone in consultation with Planning and Development staff. Special provisions were also modified resulting in an increase in the east interior side yard depth to allow sufficient space for tree plantings and adding a minimum yard depth to all lot lines for any underground parking ramps to ensure ramps are located away from adjacent properties. A height symbol was added in addition to the special provision for an increased maximum building height. Noting that for heights over 13.0 metres (42.7 feet) the R8 Zone variations require that height be applied site-specifically to zoning maps.

Notice of Revised Application is not required as possible consideration of a compound zone and additional special provisions were advertised in the Notice of Application and in some instances as modified, special provisions are brought closer to compliance with the standard zone regulations and the magnitude of change from what was initially advertised is minor.

The Applicant’s agent, Siv-ik Planning and Design Inc., hosted two Community Information Meeting about the proposed development. The first meeting was held on May 27, 2021, prior to the submission of the Zoning By-law Amendment (“ZBA”)

Application to the City. The second meeting was held on September 30, 2021 and followed the City's advertisement of the ZBA Application. City staff attended the second meeting, and both meetings were conducted virtually due to COVID.

Two (2) written replies were received from the public as part of the community engagement process. No telephone calls were received.

The concerns expressed included the change from the prior proposal for a "commercial plaza" or "strip mall" to the current proposal for an apartment building; the proposed apartment building being too intense; security, noise, and traffic constraints and congestion associated with an increasingly populated area; and traffic from the proposed development being directed through the local streets internal to the neighbourhood. The concerns expressed in the written replies were echoed in the verbal comments heard at the second Community Information Meeting. Responses to the public concerns are provided throughout this report and a summary is provided in the Planning Impact Assessment in Appendix C.

3.0 Financial Impact/Considerations

There are no financial impacts for the City that are expected to result from the proposed development.

4.0 Key Issues and Considerations

4.1. Land Use Compatibility

Through an analysis of use, intensity and form, Planning and Development staff have considered the compatibility and appropriateness of the requested ZBA and development proposal taking into account the policies that guide the use and development of land in the city and having regard for character of the receiving neighbourhood.

Use

Consistent with the PPS, the proposed mixed-use apartment building will add to the range and mix of residential types and housing options (including affordable and market-based housing) available within the receiving neighbourhood to address diverse housing needs over the long-term (PPS, Policy 1.1.1 b)). On the north side of Sunningdale Road East the housing options consist of single detached dwellings on individual lots and cluster, single detached dwellings. The development of the subject lands is an opportunity to provide for infill development and residential intensification along a major street (Sunningdale Road East) at the periphery of the receiving neighbourhood. The PPS directs that planning authorities identify appropriate location to accommodate housing options through intensification and redevelopment (PPS Policy 1.1.3.3).

The London Plan directs that mixed-use buildings and commercial uses will be permitted at appropriate locations with the Neighbourhoods Place Type to meet the daily needs of neighbourhood residents (The London Plan, Policy 918_5.). Within the Neighbourhoods Place Type the range of uses that may be permitted on a property (as well as the intensity of development) is related to the classification of the street onto which the property has frontage (The London Plan, Policies 789_6. and 919_2.). The major street classifications contemplate a broader range of permitted uses and if a property is located at the intersection of two major street classifications the range of permitted uses may broaden further (The London, Policy 919_3. and 4.). The London Plan aims to achieve an appropriate range of commercial uses, including retail, service, and office uses, within the Neighbourhoods Place Type (The London Plan, Policy 924_). In particular, retail, service, and office uses are to be appropriately sized, and service uses are to be neighbourhood-oriented (The London Plan, Policies 925_ and 926_).

The subject lands are located within the Neighbourhoods Place Type on *Map 1 – Place Types and have frontage on a Civic Boulevard (Sunningdale Road East) on Map 2 – Street Classifications in The London Plan. Low-rise apartments are contemplated as a primary permitted use within the Neighbourhoods Place Type fronting onto a Civic Boulevard and on the subject lands (The London Plan, Table 10 – Range of Permitted

Uses in Neighbourhoods Place Type). The London Plan directs mixed-use buildings and commercial uses to the intersection of two major streets as a secondary permitted use that is conditional on the classification of intersecting streets in the Neighbourhoods Place Type (The London Plan, Table 10 – Range of Permitted Uses in Neighbourhoods Place Type and *Table 12 – Retail, Service and Office Floor Area Permitted in Neighbourhoods Place Type). The subject lands are not located at the intersection of two major street and the proposed mixed-use apartment building, and in particular the proposed convenience commercial uses, may in the future be a non-conforming use to The London Plan. At this time, lands identified as Neighbourhoods Place Type on *Map 1 – Place Types are subject to appeal, and the Neighbourhoods policies are informative, but not determinative for the purpose of this application.

Intensification within the Neighbourhoods Place Type is encouraged and is key to realizing The London Plan's vision for aging in place, diversity of built form and vibrancy, affordability, and effective use of land (The London Plan, Policy 937_). Intensification should add value to the planned and existing character, quality, and sustainability of neighbourhoods (The London Plan, Policy 937_). Residential intensification means the development of a property at a higher residential density than currently exists and The London Plan identifies variety of opportunities for intensification ranging from light, discreet forms of intensification to more visible and obvious forms of intensification (The London Plan, Policy 939_). Infill development is a form of residential intensification. It is an important strategy of The London Plan to provide for all forms of intensification while ensuring they are appropriately located, compatible and fit well within the receiving neighbourhood (The London Plan, Policy 940_).

The subject lands are located in the MFMDR designation on Schedule A – Land Use in the 1989 Official Plan. The use of residential designations in the 1989 Official Plan is guided by general objectives that support the provision and distribution of choice of dwelling types, promote residential development that makes efficient use of land, encourage infill development in residential areas where existing land uses are not adversely affected and where development can make efficient use of services and facilities, and support the provision of services and amenities that enhance the quality of residential areas (The 1989 Official Plan, Section 3.1.1 i)-iv), vi), viii) and x)).

The MFMDR designation is intended to support the development of low-rise, multi-unit residential developments at locations where there is access to services and amenities and where the quality of residential areas will be enhanced (1989 Official Plan, Section 3.1.3 i)). Low-rise apartments are contemplated as a primary permitted use within the MFMDR designation and on the subject lands (1989 Official Plan, Section 3.3.1).

Existing convenience commercial uses are contemplated as secondary permitted uses within the MFMDR designation and new convenience commercial uses are also permitted by Official Plan and Zoning By-law Amendments (1989 Official Plan, Section 3.3.1 ii)). Convenience commercial uses within residential designations are intended to be neighbourhood-oriented in function, providing services to the surrounding residential area and the incidental traveling public (The 1989 Official Plan, Section 3.6.5 i)). Convenience commercial uses permitted in residential designations may include such uses as Variety Stores, Financial Institutions, Personal Service Establishments, Medical/Dental Offices, Small-scale Offices, Restaurants, Studios, and Florist Shops etc.; and these convenience commercial uses are permitted on the ground floor of an apartment building (Section 3.6.5 ii) (a)). The 1989 Official Plan contemplates convenience commercial uses in residential designations on major streets where there will not be adverse impacts on the traffic-carrying capacity of the streets (The 1989 Official Plan, Section 3.6.5 iii)).

Existing convenience commercial uses recognized by the 1989 Official Plan are shown on "Appendix 1 – Convenience Commercial and Service Stations" for locational reference but Appendix 1 does not form part of the Official Plan; or existing convenience commercial uses are described in the list of "Locations of Convenience Commercial and Service Station Uses" in the 1989 Official Plan (1989 Official Plan, Sections 3.6.5 ii) c) and 3.6.5 vi)). The subject lands are neither shown on Appendix 1 nor listed in the 1989 Official Plan as the site of existing convenience commercial uses. However, the subject lands were zoned CC(1) for convenience commercial uses, which existed prior to the

adoption of the 1989 Official Plan. The current existing convenience commercial zoning is regarded as legal non-conforming to the 1989 Official Plan.

In the opinion of Planning and Development staff the proposed mixed-use development of the subject lands conforms to London Plan's vision for aging in place, diversity of built form and vibrancy, affordability, and effective use of land. The proposed mixed-use development of the subject lands also conforms to the general objectives for residential designations in the 1989 Official Plan for the efficient use of land and the provision of services and amenities that enhance the quality of residential areas. The proposed development will effectively and efficiently use land, mixing residential and commercial uses to share in, and optimize, the use of the land. The proposed multi-unit residential development will expand the residential types and housing options available within the neighbourhood to meet diverse needs and contribute to a more dynamic and vibrant neighbourhood. Single detached dwellings are the prevailing residential type in the neighbourhood and dwelling units within apartment buildings are typically more affordable than a single detached dwelling. It is important to note that two dwelling units that meet the City's definition of affordable housing will be provided. The proposed commercial gross floor area with a mixed-use apartment building will deliver services in support of neighbourhood residents helping meet their daily needs and enhancing the quality of life within the neighbourhood.

The proposed residential and convenience commercial uses are contemplated as permitted uses the MFMDR designation separately and where convenience commercial uses are located on the ground floor of an apartment building. With respect to land use compatibility and appropriate locations for multi-unit residential development and convenience commercial within residential areas in the 1989 Official Plan, the subject lands meet the location criteria and are located on a major street (Sunningdale Road East) where safe and efficient pedestrian, cycling, vehicular and public transit access can be provided (See Section 4.3 for more discussion on Transportation Considerations).

The proposed development conforms to the policies in The London Plan and the 1989 Official Plan that provide direction on appropriate locations for infill development and residential intensification by locating the infill development at the periphery of the receiving neighbourhood, on a major street and away from the adjacent sensitive single detached dwellings. Enhanced rear and west interior side yard depths that exceed the standard minimum rear and interior side yard depth required by the R8-4 Zone are planned between the proposed mixed-use apartment building and adjacent single detached dwellings. These enhanced yard depths are shown on the conceptual site plan and will minimize shadowing and overlook impacts. All yard depths requested and shown are sufficient to support landscaping/screening and specifically, tree growth, to mitigate noise, odour, visual or other nuisances. Subsequently, the proposed infill development and residential intensification will not adversely affect the function nor the amenity of the adjacent single detached dwellings.

Intensity

Consistent with the PPS, the subject lands will be developed at a higher intensity of development (100 uph) than the average level of intensity in the receiving neighbourhood (~20 uph) to efficiently use land, infrastructure and public service facilities afforded to the area, and support transit and affordability.

The proposed multi-unit, mixed-use apartment building is inherently more efficient in its use of land, infrastructure and public service facilities than the neighbourhood's prevailing single detached dwellings. The subject lands are within walking distance of the Stoney Creek Community Centre, Mother Teresa Catholic Secondary School, Stoney Creek Public School and the commercial node at the intersection of Adelaide Street North and Sunningdale Road East.

Major streets such as Sunningdale Road East can serve as significant routes for transit and the proposed multi-unit, mixed-use apartment building will provide a more suitable density for transit along Sunningdale Road East than single-detached dwellings.

The London Plan does not manage intensity of development by providing general direction on density for Neighbourhoods, instead The London Plan provides direction on minimum and maximum building height. Like permitted uses, the intensity of

development contemplated is related to the classification of the street onto which the property has frontage (The London Plan, Policies 789_6. and 919_2.). For the subject lands, identified as Neighbourhood Place Type in The London Plan with frontage on a Civic Boulevard, the standard minimum and maximum building heights contemplated are 2-storeys and 4-storeys respectively (The London Plan, Policy *935_1. and *Table 11- Range of Permitted Heights in Neighbourhoods Place Type).

The London Plan also contemplates gross floor area maximums for retail, service, and office uses where permitted in the Neighbourhoods Place Type (The London Plan, Policy *935_2. and *Table 12 - Retail, Service and Office Floor Area Permitted in Neighbourhoods Place Type). The gross floor area maximums range between 200m² (2,152.8ft²) and 2,000m² (21,527.8ft²) conditional on the classification of the intersecting streets (The London Plan, *Table 12 - Retail, Service and Office Floor Area Permitted in Neighbourhoods Place Type). Urban design considerations for residential intensification in Neighbourhoods in The London Plan, directs that the intensity of development will be appropriate for the size of the lot and able to accommodate various necessary site functions (The London Plan, Policy 953_3).

The intensity of development within the MFMDR will usually not exceed 4-storeys in height and 75 uph in density (1989 Official Plan, Section 3.3.3 i) and ii)). Exceptions to the usual density limit can be contemplated, up to 100 uph through the bonusing criteria in the Official Plan, but the height limit of 4-storeys remains (1989 Official Plan, Section 3.3.3 i) ii)). The 1989 Official Plan directs that the intensity of convenience commercial uses will be specified in the Zoning By-law and will be at a scale compatible with surrounding land uses (The 1989 Official Plan, Section 3.6.5 iv)).

The proposed 4-storey (~16.0 metre) building conforms to the maximum building height contemplated for the subject lands identified as Neighbourhoods Place Type in The London Plan and identified as MFMDR designation in the 1989 Official Plan. The density attributed to the proposed residential dwelling units and commercial gross floor area is equivalent to 100 uph which exceeds the usual density limit for MFMDR in the 1989 Official Plan. As noted in the policies above, the proposed density can be achieved through the bonus zoning provisions in the 1989 Official Plan (See Section 4.2 for more discussion on Bonus Zoning).

To ensure that the proposed convenience commercial uses are appropriately sized and neighbourhood-oriented, the requested amendment includes a special provision to regulate the amount of commercial gross floor area. The proposed maximum gross floor area of 250m² (2,691.0 ft²) for all permitted commercial uses is within the lower-end of the intensity range for retail, service and office floor area contemplated in Neighbourhood Place Types (albeit at intersecting major streets) and is less than the maximum gross floor area of 300m² (3,229.2 ft²) that applies to individual uses permitted by the existing convenience commercial zoning of the subject lands (Zoning By-law No. Z.-1, Section 29.3 1)). In the opinion of Planning and Development staff, the proposed maximum gross floor area of 250m² (2,691.ft²) for all permitted commercial uses will ensure that the convenience commercial uses are neighbourhood-oriented and at a neighbourhood scale, and will not allow for large, intensive uses that should be located in commercial areas.

The intensity of the proposed development conforms to the urban design considerations for residential intensification in Neighbourhoods in The London Plan and is generally compatible with surrounding land uses through the continuation of a low-rise form and moderate-site coverage. Enhanced rear and west interior side yard depths between the proposed development and adjacent single detached dwellings ensure the quality of the neighbourhood is maintained. The proposed development is appropriately sized for the site and can provide for the necessary site functions such as parking, loading, garbage and snow storage, and outdoor amenity space. Although refinement to the location of garbage and snow storage may be required through any Site Plan Approval process as noted by comments received from Site Plan staff and the Urban Design Peer Review Panel (UDPRP).

Form

Consistent with the PPS the requested amendment will facilitate infill development and residential intensification in a compact urban form (PPS Policy 1.1.3.4). Notable

characteristics of the compact urban form include a contiguous development pattern with adjacent developed lands, concentrated residential and non-residential density on a single site, and a high degree of connectivity to pedestrian sidewalk and vehicular street networks in the area.

Non-residential uses may be permitted within the Neighbourhoods Place Types when the proposed form of development can fit well within the existing and planned context (The London Plan, Policy 936_3.). All planning and development applications, and residential intensification proposals, will conform to the City Design policies in The London Plan (The London Plan, Policies 936_1., and 953_). Intensification shall be sensitive to, compatible with, and fit within the neighbourhood context; and from a form-based perspective compatibility and fit is evaluated based on site layout, building and main entrance orientation, building line and setback from the street, and height and massing transitions with adjacent development (The London Plan, Policies 953_, 1953_2.).

The objectives for MFMDR development in the 1989 Official Plan include well-designed and visually attractive forms (1989 Official Plan, Section 3.1.3 ii)). Development within the MFMDR designation is directed to have a low-rise form and site-coverage that can serve as a transition from low intensity development to more intensive forms of development (1989 Official Plan, Section 3.3.3).

The proposed development has been evaluated from a form-based perspective and found to be compatible and a good fit with the neighbourhood context based on the following considerations.

With respect to site layout, the vehicular driveway access to Sunningdale Road East is proposed on the west side of the subject lands and is generally aligned with an existing driveway access on the south side of Sunningdale Road East. The location of the ramp to the underground parking is also located on the west side of the subject lands to provide more separation between the proposed building and the existing adjacent single detached dwellings to the west. The requested amendment includes a minimum yard depth to all lot lines of 3.0 metres (9.8 feet) for the underground parking ramp to ensure sufficient space is provided for landscaping/screening to minimize and mitigate any adverse impacts from the underground parking ramp on the adjacent dwellings. Boundary fencing is a matter for any subsequent Site Plan Approval process and is regulated by the City's Site Plan Control By-law C.P. -1455-541 and Fence By-law PS-6-21003. Most of the required on-site parking is proposed to be located underground and where a modest amount of surface parking is proposed for convenience commercial use, it is proposed in the front yard and away from the planned and existing single detached dwellings to the north and to the west. Pedestrian walkways are proposed across the front of the subject lands and will connect the subject lands to the city sidewalks internal to local streets completing the pedestrian walkway network in the area.

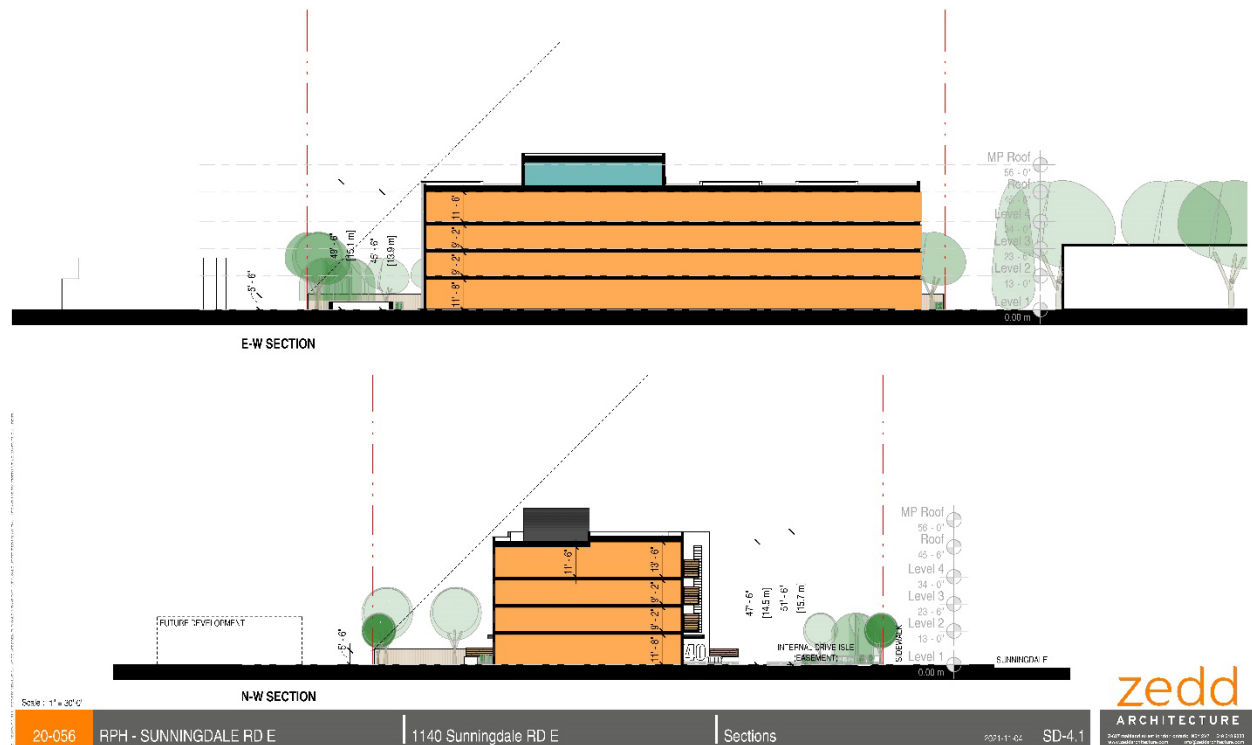
With respect to building and main entrance orientation, the proposed building has been oriented towards Sunningdale Road East and away from abutting properties and the internal portion of the neighbourhood. The street-facing elevation includes the principal building entrance for residential uses and for the convenience commercial uses to animate the Sunningdale Road East streetscape and focus public interactions away from abutting properties. To minimize and mitigate overlook from the proposed development on the existing single detached dwellings, balconies are proposed only on the north (rear) and south (front/street-facing) elevations and are inset to screen views to the existing single detached dwellings to the west.

With respect to building line and setback from the street, the Plan of Subdivision that surrounds the subject lands established a series of window-streets adjacent to Sunningdale Road East. Although, the proposed development will not complete the window-street network based on past public input and Municipal Council direction, the placement and setback of the proposed building will mimic the window-street network visually and continue the built-edge condition or "built street-wall" along Sunningdale Road East.

Lastly, with respect to height and massing transitions with adjacent development, the proposed development has been designed to be sensitive to the abutting properties to

the north and to the west. These properties are planned, or have relatively recently been developed, for single detached dwellings and are unlikely to redevelop in the near future while the property to the east (1154 Sunningdale Road East) has infill potential. All components of the proposed building have been designed to be contained within a 45-degree angular plane measured from 3.0 metres above grade at the rear (north) and west lot lines to provide a gradual transition in height over the distance or depth of the subject lands' rear (north) and west interior side yards (See Figure 5). This gradual transition in height over the yard depth will minimize and mitigate shadow impacts on the adjacent properties. As noted above, the rear (north) and west interior side yard depths exceed the standard minimum yard depth required by the requested R8-4 Zone.

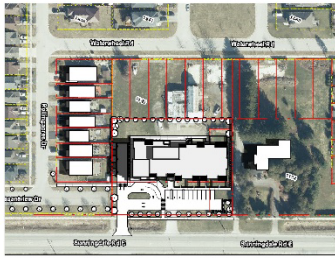
Figures 5 –Sections showing angular plane submitted with current Zoning By-law Amendment Application (City of London File No. Z-9405)



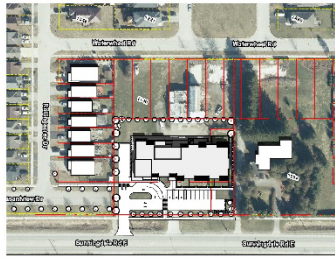
A sun/shadow study was submitted in support of the application using industry-standard modeling practices to illustrate how the sun moves across the proposed development and the resulting shadow impacts for adjacent properties. Apart from the winter solstice when shadows are at their largest and longest, the shadows associated with the proposed development are contained on-site most of the day (see Figure 6). The shadows during the winter solstice are a function of the north-south orientation of the impacted properties and the subject lands to one another and the depth provided by the properties along major streets oriented east-west.

Figure 6 – Shadow Study submitted with current Zoning By-law Amendment Application (City of London File No. Z-9405)

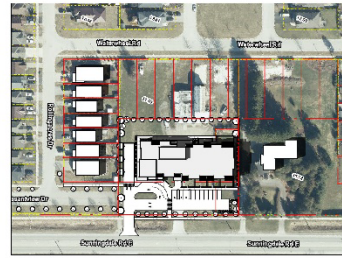




JUNE 21 - 9AM

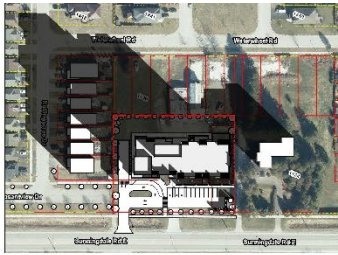


JUNE 21 - 12PM



JUNE 21 - 3PM

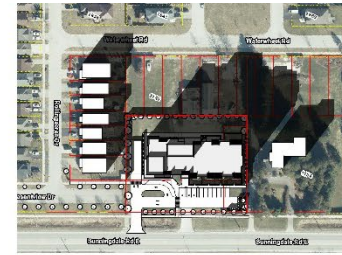
Scale: 1" = 100'
 20-056 RPH - SUNNINGDALE RD E | 1140 Sunningdale RD E | Shadow - June | 2021-06-03 | SD-7.2 | zedd ARCHITECTURE



DECEMBER 21 - 9AM



DECEMBER 21 - 12PM



DECEMBER 21 - 3PM

Scale: 1" = 100'
 20-056 RPH - SUNNINGDALE RD E | 1140 Sunningdale RD E | Shadow - December | 2021-06-03 | SD-7.3 | zedd ARCHITECTURE

The applicant is proposing a density bonus in return for high-quality building and site design. Urban Design staff and the UDPRP commended the site and building design for the following features: a successful site layout that appears to make the most of the planning context; an articulated building that architecturally defines public and private function/space as distinct from one another, and effective use of signage and canopies.

4.2. Bonus Zoning

In accordance with the Planning Act and the Bonus Zoning provisions in the Our Tools section of The London Plan and Section 19.4.4 in the 1989 Official Plan, Municipal Council may authorize increases in building heights and densities **above the limits otherwise permitted in the Zoning By-law** in return for the provision of certain public facilities, services or matters (The London Plan, Policy *1638_; 1989 Official Plan, Section 19.4.4). Bonus Zoning is implemented through one or more agreements with the City that are registered on title to the subject lands and secure public benefit and elements of the development that are commensurate to the additional building height and/or density.

Type 1 Bonus Zoning in The London Plan can be utilized to ensure that design features required to mitigate the impacts of additional height and density are provided where the proposed bonus zone will allow for a height and density that is **within the standard maximum heights or densities allowed within the applicable place type** (The London Plan, Policies *1643_1., *1645_, *1646_1., *1647_). The proposed development conforms to the standard maximum height of 4-storeys permitted within the Neighbourhoods Place Type with frontage on a Civic Boulevard. As such, the application of Type 1 Bonus Zoning in The London Plan is appropriate to secure design features to minimize and mitigate the impact of development.

In addition to the consideration of Type 1 Bonus Zoning in The London Plan, the Bonus Zoning provisions of the 1989 Official Plan, can be used to obtain design features that support the City's urban design principles as well as support the provision of affordable housing, support the provision of underground parking, and support innovative and

environmentally sensitive development (The 1989 Official Plan, Section 19.4.4 ii) (a), (c), and (h)).

Design Features

The requested increase in density above the standard maximum density of 75 uph permitted in the R8-4 Zone is proposed to be tied to the conceptual site plan, elevations and renderings submitted in support of the application which illustrate the following notable design features intended to minimize and mitigate the impact of the proposed development:

- A building placement that is street-oriented and which also reinforces the existing window-street context along Sunningdale Road East and provides for continuity of the existing built street-wall.
- The provision of a pedestrian walkway across the front of the subject lands that functions as a continuation of the city sidewalk that is located west of the subject lands on the north side of Pleasantview Drive and connecting to the city sidewalk located on the north side of Sunningdale Road East.
- The provision of sufficient yard depths along all edges of the proposed development to accommodate a landscaped buffer able to support tree growth and screen the proposed development from adjacent residential uses.
- The provision of sufficient space adjacent to Sunningdale Road East to accommodate enhanced landscaping to screen surface parking area(s) located in the front yard from the city-owned boulevard.
- A well pronounced, street-oriented principal building entrance for residential uses
- A well pronounced, street-oriented unit entrance for convenience commercial uses with large expanses of clear glazing, a wrap around canopy and signage.
- Individual ground-floor residential unit access and private individual courtyards on the street-facing (south) elevation.
- Inset balconies to minimize and mitigate overlook for existing single detached dwellings to the west and their associated rear yard amenity space.
- A high-level of articulation and architectural detailing on the street-facing front facade for visual interest.

As is common practice for the City, the conceptual site plan, elevations, and renderings would be appended to an amending by-law for Bonus Zoning and would effectively “locking in” the design features described and illustrated in return for increased density.

Affordable Housing

Dwelling units in apartment buildings are typically more affordable than the neighbourhood’s prevailing single detached dwelling units. The proposed multi-unit, mixed-use apartment building will diversify unit size, (offering 1 and 2-bedroom units), and possibly diversify tenure (ownership or rental) in the neighbourhood to support affordability in the neighbourhood and housing options for all types of households including aging in place. Moreover, the addition of the proposed units to the housing supply may also free-up other more affordable units elsewhere in support of Municipal Council’s commitment to the Housing Stability Action Plan, Strategic Area of Focus 2: Create More Housing Stock.

Through discussions with the Housing Development Corporation (“HDC”), London, the Applicant has agreed to dedicate two (2) one-bedroom residential dwelling units to affordable housing in return for Municipal Council authorizing an increase in maximum density from 75 uph to 100 uph. Based on the lot area of subject lands the increase in density would yield eleven (11) additional residential dwelling unit. The two (2) residential dwelling units agreed to be dedicated to affordable housing, are equal to 18% of the eleven (11) additional residential dwelling units

Through an agreement registered on title, affordability will be defined as not exceeding 80% of the Canada Mortgage and Housing Corporation (“CMHC”) Average Market Rent

("AMR") for one-bedroom units for the London Census Metropolitan Area at the time of building occupancy, and the duration of the affordable period will be set at 50-years calculated from initial occupancy of each unit. The Applicant will also be required to enter into a Tenant Placement Agreement with the City to align bonused affordable housing units with priority populations. (See HDC letter of understanding dated November 4, 2021 in Appendix B.)

The provision of affordable housing in return for increased density will contribute to the more than 300 affordable housing units identified by the City's Housing Stability Action Plan to be developed each year across the city to meet current and future needs for affordable housing.

Underground Parking

Most of the required parking is proposed to be located underground with no surface parking areas proposed adjacent to the planned and existing single detached dwellings to the north and to the west, to minimize and mitigate potential conflict and safeguard compatibility. The requested bonus provisions would recognize a minimum of 80% of the required parking be located underground. As shown on the site concept plan, 64-underground parking spaces, or approximately 86% of the required parking spaces, are proposed to serve the residential use and 10-surface parking spaces are proposed to serve the convenience commercial use.

The provision of underground parking is a more efficient use of the subject allowing for a consolidated outdoor amenity space to be located north of the proposed building. With respect to the Climate Emergency and ways to reduce and mitigate climate change through environmental design, this amenity space is an opportunity for soft landscaping that has a cooling effect, whilst surface parking areas contribute to the heat island effect.

Environmentally Sensitive Development

Further to the City's commitments to reducing and mitigating climate change, a minimum of 5% of the required parking spaces are proposed to be fitted with electric vehicle (EV) charging station, which is the equivalent of four stations. These EV charging stations will make charging points readily accessible to residents to encourage and support a shift to zero-emission vehicles to reduce air emissions that contribute to climate change.

Planning and Development staff are satisfied that the public benefit and elements of the development to be secured through Bonus Zoning are commensurate to the requested increase in density. Should the Bonus Zoning not be implemented, it is important to note that the Applicant has requested special provisions to the underlying R8-4 Zone to provide for increased minimum rear and west interior side yard depths that exceed the minimum standard requirements. These enhanced requirements will ensure that any future development for apartment buildings will provide increased yard depths to minimize and mitigate the impact of development on planned and existing single detached dwellings to the north and to the west.

4.3. Transportation Considerations

Through public engagement concerns were expressed about the proposed development contributing traffic constraints and congestion in an increasingly populated area and traffic being directed through the existing neighbourhood to the subject lands.

Complete Street Design, Function and Capacity

Sunningdale Road East is classified as a Civic Boulevard on Map 2 – Street Classifications in The London Plan and is classified as an Arterial on Schedule C – Transportation Corridors in the 1989 Official Plan. Civic Boulevards and Arterials are higher-order street classifications intended to move medium to high volumes of traffic, with priority given to pedestrian, cycling and transit movements as the streets are upgraded over-time to a complete urban cross-section.

At present, the cross-section along Sunningdale Road East, between Adelaide Street North and North Wenige Drive, consists of two-through lanes and intermittent turning lanes. The cross section is not fully urbanized and contains open ditches separating the

roadway from the city sidewalks. Intersections are controlled by stop signs, except for the major intersections of Adelaide Street North and Sunningdale Road East and South Wenige Drive and Sunningdale Road East that are fully signalized.

With respect to anticipated timing for street upgrades proximate to the subject lands, the 2021 Development Charges Background Study identifies the following planned construction. However, construction is subject to potential changes as strategic priorities and municipal budgets change.

- Sunningdale Road (from Adelaide Street North, west to Bluebell Road): upgrade from two to four-through lanes anticipated 2025
- Sunningdale Road (from South Wengie Drive, east to Highbury Road North): upgrade to existing two-through lanes anticipated 2028
- +Adelaide Street North (from Fanshawe Road East, north to Sunningdale Road East): upgrade from two to four-through lanes anticipated 2029
- +Adelaide Street North and Sunningdale Road East intersection – upgrade anticipated 2029

+ symbol indicates projects that may be reviewed and incorporate into the upcoming Mobility Master Plan.

In the review of this ZBA Application, no comments were received from Transportation Planning and Design staff to suggest that the proposed development has the potential to adversely impact the designed function or capacity of Sunningdale Road East or other major streets in the area.

Consistent with past Municipal Council direction on the matter, the current ZBA Application does not contemplate completion of the window-street network across the front of the subject lands to connect the east and west-legs of Pleasantview Drive for vehicular traffic. That said, the proposed building placement on the subject lands does not preclude the ability to complete the window-street network in the future and respects the need for a servicing easement (through Site Plan Approval) to extend municipal servicing from the west-leg of Pleasantview Drive to the subject lands' east lot line in favour of the abutting property at 1154 Sunningdale Road East for any development needs in the future.

It is important to note that Transportation Planning and Design staff still consider the completion of the window-street network to be desirable and that the temporary turning circle at the terminus of the east-leg of Pleasantview Drive does not meet current design standards.

The conceptual site plan shows direct vehicular driveway access from the proposed development to Sunningdale Road East, with no proposed vehicular driveway access to the local streets internal to the neighbourhood. As such, the proposed development is not anticipated to add new automobile traffic to those local streets.

Unlike the window-street network, the completion of the pedestrian walkway network is proposed across the front of the subject lands and will function as a continuation of the city sidewalk located west of the subject lands on the north side of Pleasantview Drive, and connecting to the city sidewalk located east of the subject lands on the north side of Sunningdale Road East.

This pedestrian connection supports the neighbourhood-orientation of the convenience commercial uses proposed for the subject lands, whereby residents can access the site through active modes of transportation for their day-to-day needs. Planning and Development staff heard public concern at the Applicant-led Community Information Meeting held in September 2021, that the proposed pedestrian connection may result in visitors and patrons to the proposed development parking on local streets internal to the neighbourhood (e.g. Pleasantview Drive and/or Rollingacres Drive). However, given that the route through the neighbourhood to the subject land is circuitous and inefficient, adverse parking impacts are not expected to result from the proposed development and the pedestrian connection to the internal neighbourhood.

Reduced Parking Rate for Commercial Uses

The on-site parking supply and requested reduction in required parking for proposed convenience commercial uses was reviewed by Transportation Planning and Design staff. The proposed development will provide 64 underground parking spaces to serve the residential use. The 64 underground parking spaces include a surplus of 11 parking spaces above the minimum parking requirement for the number of residential dwelling units. The 64 underground parking spaces are equivalent to a parking rate of 1.5 spaces/unit; whereas the minimum parking rate requirement is 1.25 spaces/unit in the Zoning By-law (Zoning By-law No. Z.-1, Section 4.19).

The Applicant has requested a reduced minimum parking rate for all permitted commercial uses of 1 space/25 m² (269.1 ft²), or the equivalent of 10 surface parking spaces based on the maximum gross floor area of 250 m² (2691.0 ft²) for all permitted commercial uses. In comparison, the requested commercial use with the most onerous minimum parking rate is a Personal Service Establishment and has a minimum parking rate of 1 space/10 m² (107.6 ft²) (Zoning By-law No. Z.-1, Section 4.19) resulting in a total parking requirement of 25 spaces based on the proposed gross floor area.

Transportation Planning and Design staff requested a Parking Reduction Study be provided by the Applicant to justify and demonstrate that the requested parking reduction for convenience commercial uses would not create adverse impacts. Transportation Planning and Design staff have accepted the findings and recommendations of the Parking Reduction Study prepared by Strik, Baldinelli, Moniz and dated December 7, 2021 and do not have any objections to the requested reduced parking rate for convenience commercial uses. The study found that the requested reduced parking rate is within the value range of the parking requirements sampled from other comparable municipalities, and in some instances other municipalities have lower requirements than the requested reduced rate.

The requested reduced minimum parking rate conforms to the policies in The London Plan that zoning will establish parking standards ensuring that excessive amounts of parking are not required (The London Plan, Policy 271_). The 1989 Official Plan directs that provision of parking shall be adequate for the land uses the parking supports and developed to a standard that promotes compatibility with adjacent land uses (1989 Official Plan, Section 18.2.12). Again, the convenience commercial uses proposed for the subject lands are to be small-scale and neighbourhood-oriented to support the day-to-day needs of residents and should encourage and promote access by way of active modes of transportation reducing automobile parking demands.

Conclusion

The recommended amendment is consistent with the Provincial Policy Statement, 2020. It will contribute to the mix of residential types and housing options (including affordable housing) available to meet diverse housing needs; it will facilitate the development of a compact urban form that will use land, infrastructure, and public service facilities efficiently; and it will provide for infill development and residential intensification on an underutilized site at an appropriate location.

The recommended amendment conforms to the in-force policies of The London Plan for lands located within the Neighbourhoods Place Type that contemplates low-rise apartment buildings on major streets. The proposed convenience commercial use will be scaled appropriately for the in-force policies that aim to achieve an appropriate range of commercial uses, including retail, service, and office uses, within the Neighbourhoods Place Type. The recommended amendment will provide for infill development and residential intensification in a form that can minimize and mitigate the impacts of the development on adjacent properties thereby being sensitive, compatible and a good fit with its neighbourhood context.

The recommended amendment conforms to the in-force policies of 1989 Official Plan for lands located in the Multi-Family, Medium Density Residential designation that contemplates the proposed mixed-use apartment building and convenience commercial uses on major streets and at an appropriate intensity to support neighbourhood residents in their daily needs.

The proposed development is eligible for bonus zoning under the bonus zoning criteria in the 1989 Official Plan and will secure public benefit and design elements that are commensurate to the additional building density, including affordable housing.

Prepared by: **Melissa Campbell, MCIP, RPP**
Senior Planner, Long Range Planning and Research

Reviewed by: **Mike Corby, MCIP, RPP**
Manager, Planning Implementation

Recommended by: **Gregg Barrett, AICP**
Director, Planning and Development

Submitted by: **George Kotsifas, P. Eng**
**Deputy City Manager, Planning and Economic
Development**

Note: The opinions contained herein are offered by a person or persons qualified to provide expert opinion. Further detail with respect to qualifications be obtained from Planning and Development.

Appendix A

Bill No. (number to be inserted by Clerk's Office)
2022

By-law No. Z.-1-22_____

A by-law to amend By-law No. Z.-1 to
rezone an area of land located at 1140
Sunningdale Road East

WHEREAS 2839069 Ontario Inc. c/o Royal Premier Homes has applied to rezone an area of land located at 1140 Sunningdale Road East, as shown on the map attached to this by-law, as set out below;

AND WHEREAS this rezoning conforms to the Official Plan;

THEREFORE the Municipal Council of The Corporation of the City of London enacts as follows:

1) Schedule "A" to By-law No. Z.-1 is amended by changing the zoning applicable the lands located at 1140 Sunningdale Road East, as shown on the attached map comprising part of Key Map No. A103, from a Convenience Commercial Special Provision (CC(14)) Zone to a compound Convenience Commercial Special Provision/Residential R8 Special Provision Bonus Zone (CC4(_)/R8-4(_))•H16•B(_)) Zone.

2) Section Number 29.4 of the Convenience Commercial (CC) Zone is amended by adding the following Special Provision:

CC4(_)	1140 Sunningdale Road East	
a)	Additional Permitted Use:	
i)	Florist Shop, restricted to a location within an apartment building and without a drive-through facility	
b)	Regulations:	
i)	Gross Floor Area for all permitted commercial uses (maximum)	250 square metres (2,691 square feet)
ii)	Parking for all permitted commercial uses (minimum)	1 space/25 square metres (269 square feet)

3) Section Number 12.4 of the Residential R8 Zone is amended by adding the following Special Provision:

R8-4(_)	1140 Sunningdale Road East	
a)	Regulations:	
i)	Front Yard Depth (minimum)	22.0 metres (72.2 feet) as measured from the front lot line existing on the date of passing this by-law
ii)	Interior Side Yard Depth (East) (minimum)	3.0 metres (9.8 feet)

iii)	Interior Side Yard Depth (West) (minimum)	1.0 metre (3.2 feet) per 1.0 metre (3.2 feet) of main building height or fraction thereof above 3.0 metres (9.8 feet), but in no case less than 7.5 metres (24.6 feet)
iv)	Rear Yard Depth (minimum)	1.0 metre (3.2 feet) per 1.0 metre (3.2 feet) of main building height or fraction thereof above 3.0 metres (9.8 feet), but in no case less than 7.5 metres (24.6 feet)
v)	Location of Underground Parking Ramp (minimum)	3.0 metres (9.8 feet) to all lot lines.
vi)	Height (maximum)	16.0 metres (52.5 feet) or 4-storeys, whichever is less.

- 4) Section Number 4.3 (Bonus Zones) of the General Provisions is amended by adding the following Site-Specific Bonus Provision:

4.3() B() 1140 Sunningdale Road East

The bonus zone shall be implemented through a mixed-use apartment building with a maximum density of 100 units per hectare, in general conformity with the Site Plan, Elevations, and Renderings attached as Schedule "1" to the amending by-law; and provides for the following:

- 1) Exceptional Site and Building Design
 - i. A building placement that is street-oriented and which reinforces the existing window-street context along Sunningdale Road East to provide for continuity of the built street-wall.
 - ii. The provision of a pedestrian walkway across the front of the subject lands that functions as a continuation of the city sidewalk located west of the subject lands on the north side of Pleasantview Drive, and connecting to the city sidewalk located east of the subject lands on the north side of Sunningdale Road East.
 - iii. The provision of yard depths along all edges of the proposed development to accommodate a landscaped buffer able to support tree growth and screen the proposed development from adjacent residential uses.
 - iv. The provision of enhanced landscaping along Sunningdale Road East to screen any surface parking areas located in the front yard from the city-owned boulevard.
 - v. A well pronounced, street-oriented principal building entrance for residential uses
 - vi. A well pronounced, street-oriented unit entrance for commercial uses with large expanses of clear glazing, a wrap around canopy and signage.

- vii. Individual ground-floor residential unit access and private individual courtyards on the street-facing (south) elevation.
 - viii. Inset balconies to screen views to the existing single detached dwellings to the west.
 - ix. A high-level of articulation and architectural detailing on the street-facing front facade for visual interest.
- 2) A minimum of 80% of the required parking spaces provided underground.
 - 3) A minimum of 5% of the required parking spaces fitted with electric vehicle charging stations
 - 4) Provision of Affordable Housing
 - i. A total of two (2) 1-bedroom units will be provided for affordable housing.
 - ii. Rents not exceeding 80% of the Average Market Rent for the London Census Metropolitan Area as determined by the Canadian Mortgage and Housing Corporation at the time of building occupancy.
 - iii. The duration of affordability set at 50 years from the point of initial occupancy.
 - iv. The proponent is to enter into a Tenant Placement Agreement with the Corporation of the City of London to align the affordable units with priority populations.

The following special regulations apply within the bonus zone upon the execution and registration of the required development agreement(s):

- | | | |
|----|---|-------------------------|
| a) | Regulations: | |
| | vii) Density:
(maximum) | 100 units per hectare |
| | viii) Interior Side Yard
Depth (West)
(minimum) | 12.5 metres (41.0 feet) |
| | ix) Rear Yard Depth
(minimum) | 13.5 metres (44.2 feet) |

The inclusion in this By-law of imperial measure along with metric measure is for the purpose of convenience only and the metric measure governs in case of any discrepancy between the two measures.

This By-law shall come into force and be deemed to come into force in accordance with Section 34 of the *Planning Act, R.S.O. 1990, c. P13*, either upon the date of the passage of this by-law or as otherwise provided by the said section.

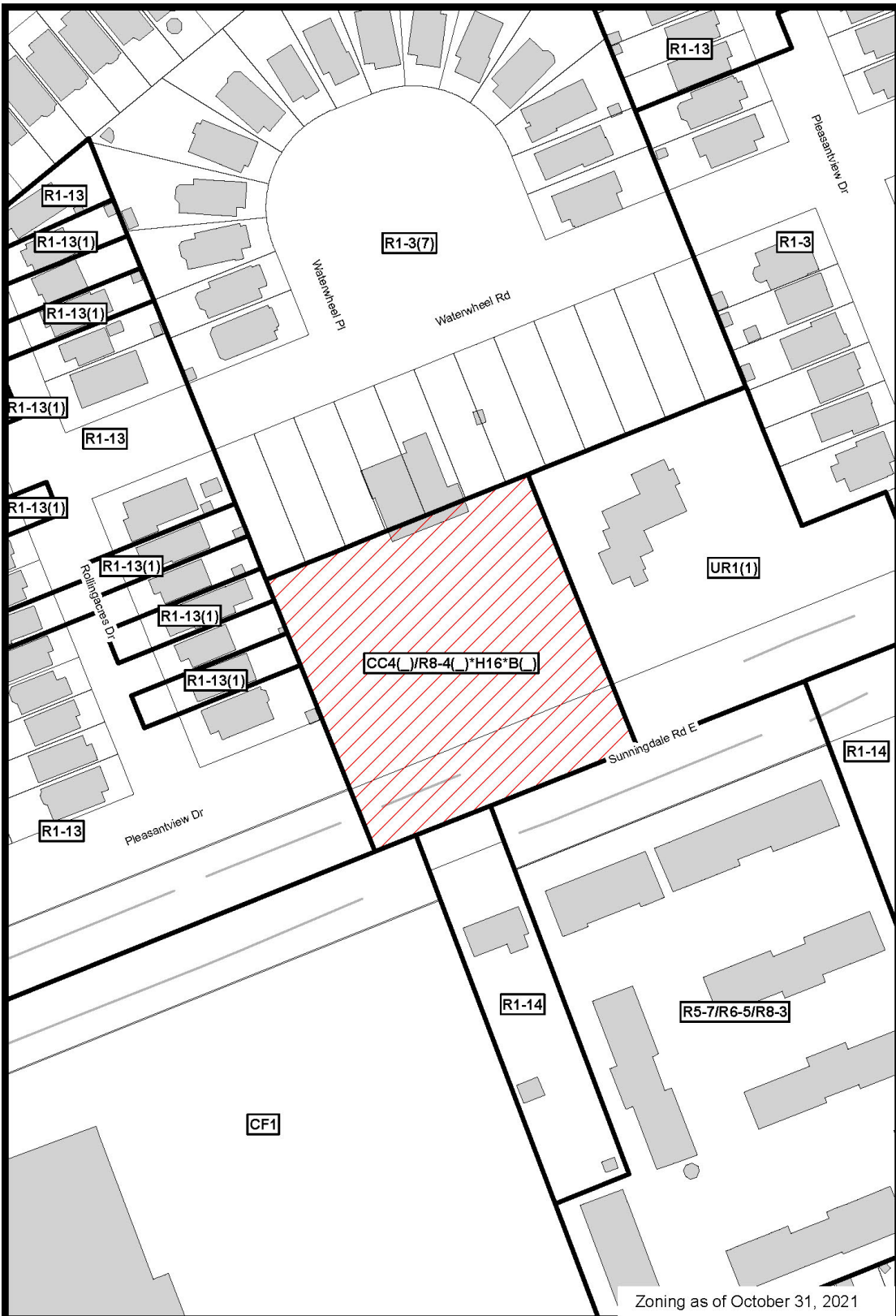
PASSED in Open Council on January 25, 2022.

Ed Holder
Mayor

Catharine Saunders
City Clerk

First Reading – January 25, 2022
Second Reading – January 25, 2022
Third Reading – January 25, 2022

AMENDMENT TO SCHEDULE "A" (BY-LAW NO. Z-1)



Zoning as of October 31, 2021

File Number: Z-9405
 Planner: MC
 Date Prepared: 2021/11/25
 Technician: RC
 By-Law No: Z-1-

SUBJECT SITE 

1:1,250

0 5 10 20 30 40 Meters 



Schedule "1"

ALSO FROM LUTUM INC HOLD THE DRAWING, THE COPY (PH) AND COPY (S) OF THE DESIGN AND ALL THE DETAILS OF THE BUILDING AS SHOWN ON THIS DRAWING SHALL BE KEPT FOR THE USE OF THE ARCHITECT AND NOT BE REPRODUCED OR USED FOR ANY OTHER PROJECT, SOLD OR BE LOANED AS PART OF A SALE OF PROPERTY WITHOUT THE WRITTEN CONSENT OF ZEDD ARCHITECTURE INC.

2021-11-04 10:28:55

20-056

RPH - SUNNINGDALE RD E

1140 Sunningdale RD E

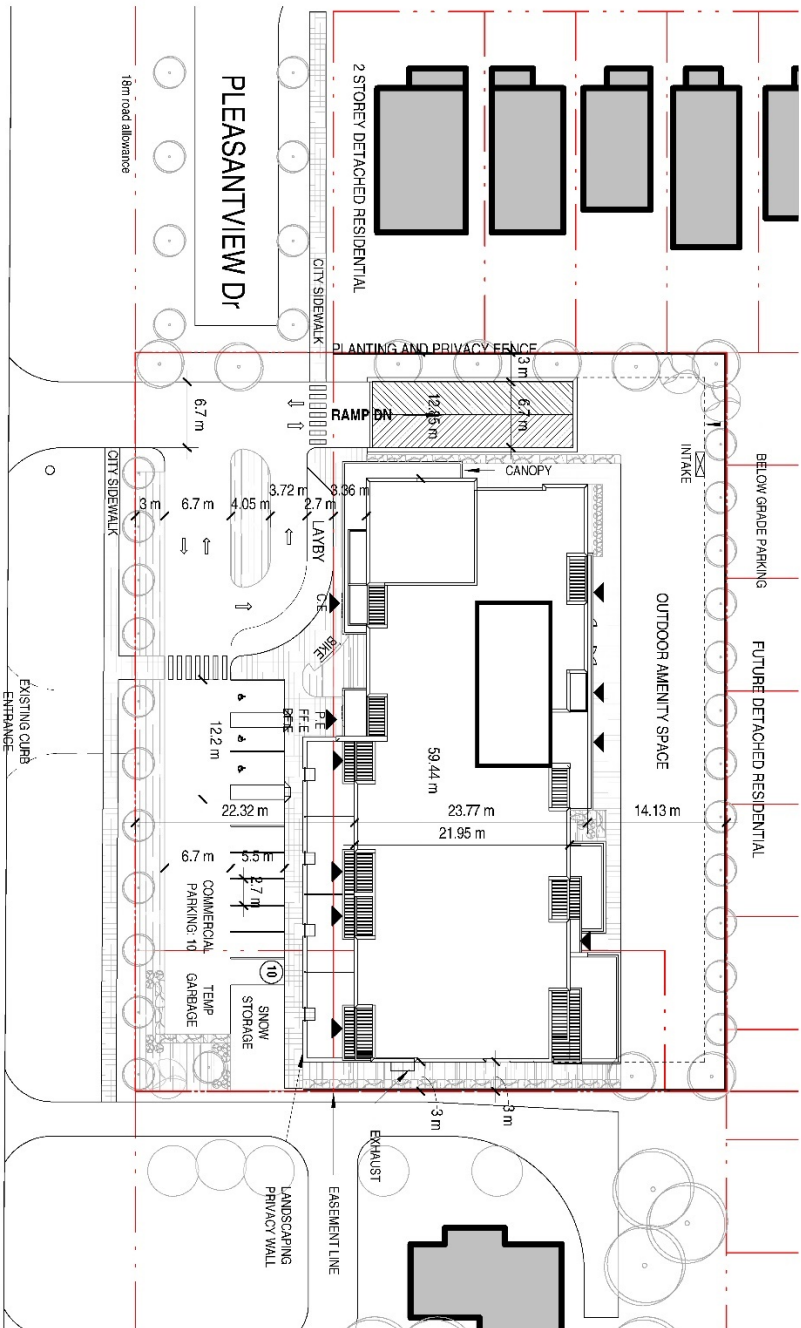
Site Plan

2021-11-04

SD-1.1

Zedd ARCHITECTURE

Scale: 1" = 40'-0"



Site Development Stats

1140 Sunningdale RD E, London, Ontario

R8-4 ZONE

ITEM	EXISTING	PROPOSED
1 ZONE - PROPOSED BUILDING PERMITTED USES	Apartment Buildings	Mixed-use Building*
2 LOT AREA (MINIMUM)	1000sqm (min)	4,534sqm
3 LOT FRONTAGE (MINIMUM)	30m (min)	75.28m
4 FRONT YARD SETBACK (MINIMUM)	6m+1m per 10m above first 3m (7.11m)	22.32m
5 EXTERIOR SIDEYARD SETBACK (MINIMUM)	n/a	n/a
6 REAR YARD DEPTH (MINIMUM)	1.2m per 3m above first 3m (4.46m)	14.13m
7 INTERIOR SIDEYARD DEPTH (MINIMUM)	1.2m per 3m above first 3m (4.46m)	12.85m (west) 3m (east)*
8 LANDSCAPE OPEN SPACE (% MINIMUM)	30%	2,243sqm (49%)
9 COVERAGE (% MINIMUM)	40%	1,225sqm (27%)
10 HEIGHT (M/MAXIMUM)	13m	15.7m*
11 DENSITY (UPH MAXIMUM)	75 uph	42 units 250sqm commercial 100 uph*
12 COMMERCIAL ALLOWABLE (SQM MAXIMUM)	n/a	250sqm*
13 COMMERCIAL PARKING	17	10*
14 RESIDENTIAL PARKING	11.25 per unit (53 req)	64
15 RESIDENTIAL BIKE PARKING	1.0.75 per unit	32 spaces

* - REQUIRES SPECIAL PROVISION

NOTE:

- BIKE PARKING LOCATED BELOW GRADE
- GARAGE WILL BE STORED IN GARAGE ROOM, AND BROUGHT OUTSIDE TO TEMPORARY HOLDING AREA ON PICKUP DAYS

STATS:

- BUILDING AREA: 13,185 sqft (1,225 sqm)
- GROSS FLOOR AREA: 49,185 sqft (4,565sqm)
- TOTAL UNITS: 42 UNITS
- TOTAL COMMERCIAL: 250 SQM
- TOTAL RESIDENTIAL: 84 RESIDENTIAL
- TOTAL PARKING: 10 COMMERCIAL
- 74 TOTAL

Zedd ARCHITECTURE

Zedd ARCHITECTURE
2021-11-04 10:28:55

ZEDD ARCHITECTURE INC. 1115 BOWLING GREEN COURT, SUITE 100, WASHINGTON, DC 20004
THIS DOCUMENT IS THE PROPERTY OF ZEDD ARCHITECTURE INC. AND IS NOT TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, WITHOUT THE WRITTEN CONSENT OF ZEDD ARCHITECTURE INC.



ALUMINUM STOREFRONT GLASSING

COMMERCIAL ENTRANCE

RESIDENTIAL ENTRANCE

BRICK - WHITE LANDSCAPE WALL WITH WOOD FENCE SLATS

GLASS GUARD
BRICK - WHITE

STEEL SIDING

ALUMINUM CLAD
TRELLIS
WOOD PANEL

BRICK - DARK

WOOD PANEL

- MP Roof 56'-0"
- Roof 45'-6"
- Level 4 34'-0"
- Level 3 23'-6"
- Level 2 13'-0"
- Level 1 0.00 m
- Level P1 -10'-0"

SOUTH ELEVATION - SUNNINGDALE

Scale : 1/16" = 1'-0"
20-056

RPH - SUNNINGDALE RD E

1140 Sunningdale RD E

Elevations - South - Sunningdale

2021-11-04

SD-3.1



Zedd ARCHITECTURE
1115 BOWLING GREEN COURT, SUITE 100, WASHINGTON, DC 20004
202-462-1111



STEEL SIDING
CONCRETE PANEL - GRAY

BRICK - WHITE

WOOD TRELLIS
IN FRONT

WOOD PANEL

ALUMINUM
STOREFRONT GLASSING

BRICK - WHITE
LANDSCAPE WALL WITH
WOOD FENCE SLATS

WEST ELEVATION

- MP Roof 55'-0"
- Roof 48'-6"
- Level 4 34'-0"
- Level 3 23'-6"
- Level 2 -3'-0"
- Level 1 0'-0"
- Level P1 -3'-0"

SUNNINGDALE

Scale: 1/8" = 1'-0"

20-0056

RPH - SUNNINGDALE RD E

1140 Sunningdale RD E

Elevations - West

2021.11.01

SD-3-2

zedd
ARCHITECTURE

ZEDD ARCHITECTURE
2000 UNIVERSITY AVENUE, SUITE 200
ANN ARBOR, MI 48106

ZEDD ARCHITECTURE INC. IS PROVIDING THE SERVICES OF ARCHITECTURE AND DESIGN SERVICES AS AN EXCLUSIVE PROPERTY AND AS A "NOT SUBJECT TO ANY OTHER FORM OF CONTRACT OR AGREEMENT" AND IS NOT PROVIDING ANY OTHER SERVICES OR PRODUCTS WITHOUT THE WRITTEN CONSENT OF ZEDD ARCHITECTURE INC.

Scale: 1/16" = 1'-0"

20-056

RPH - SUNNINGDALE RD E

1140 Sunningdale Rd E

Elevations - North

2021-11-04

SD-3.3

ZEDD ARCHITECTURE
ARCHITECTURE

zedd
ARCHITECTURE



NORTH ELEVATION



MIP Roof
58'-0"

Roof
45'-6"

Level 4
34'-0"

Level 3
29'-6"

Level 2
19'-0"

Level 1

Level P1
-16'-0"

WOOD SLATS
RILAY

BRICK - W-HU
CONCRETE PANEL - GREY

STEEL SLATS

BRICK - WHITE
LANDSCAPE WALL WITH
WOOD FENCE SLATS

WOOD PANEL

FENESTER
WOOD FRAME

SUNNINGDALE

EAST ELEVATION

Scale: 1/16" = 1'-0"

20-056

RPH - SUNNINGDALE RDE

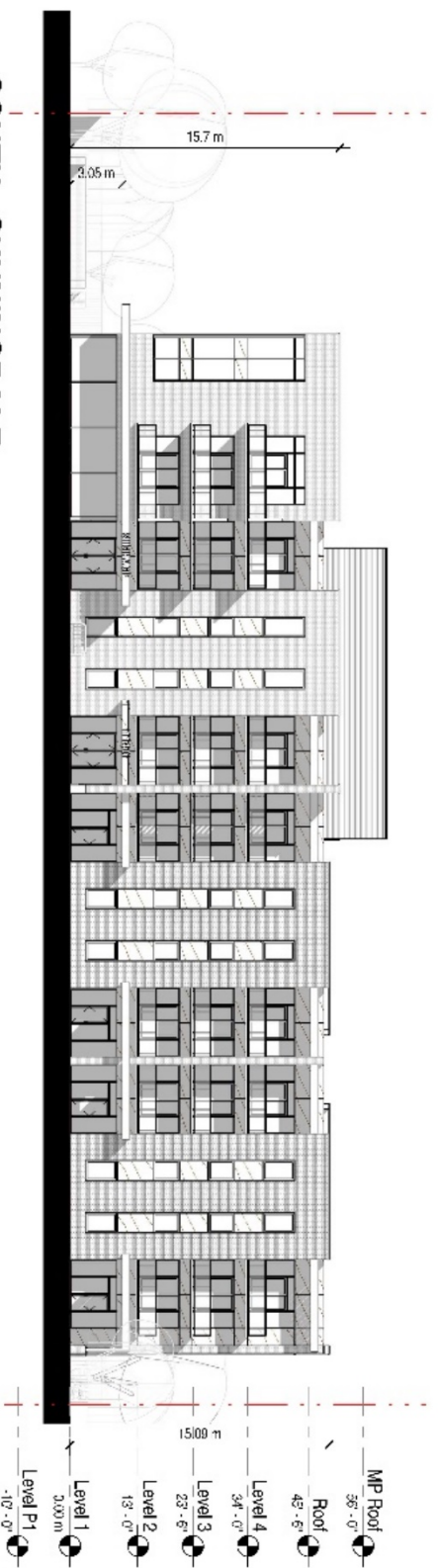
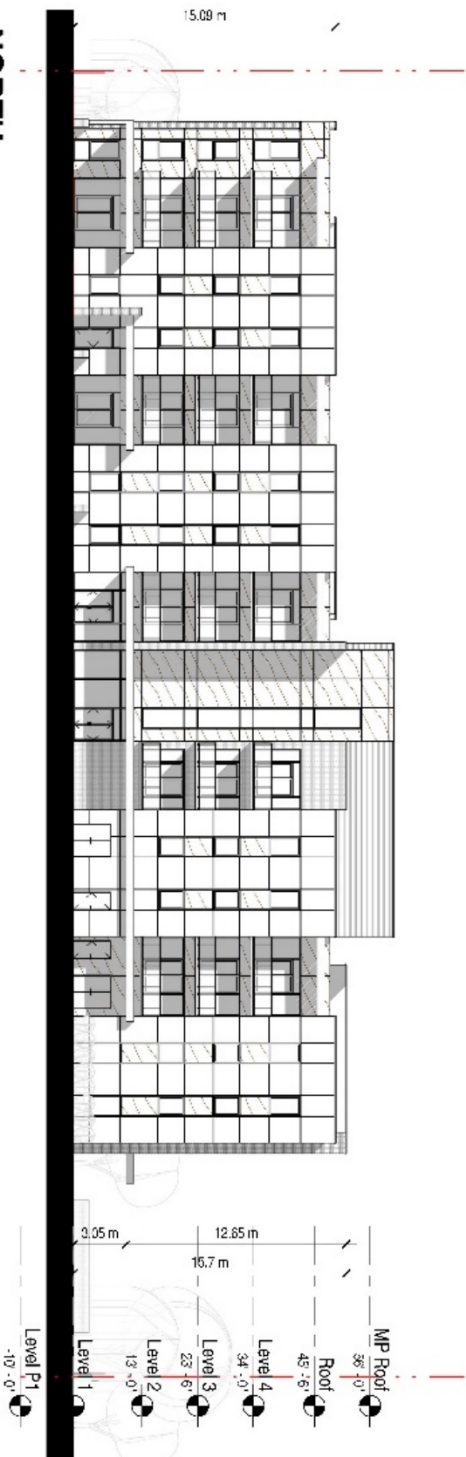
1140 Sunningdale RDE

Elevations - East

2021-11-01

SD-3.4

zedd
ARCHITECTURE



SOUTH - SUNNINGDALE

NORTH

Scale: 3/8" = 1'-0"

20-056

RPH - SUNNINGDALE RD E

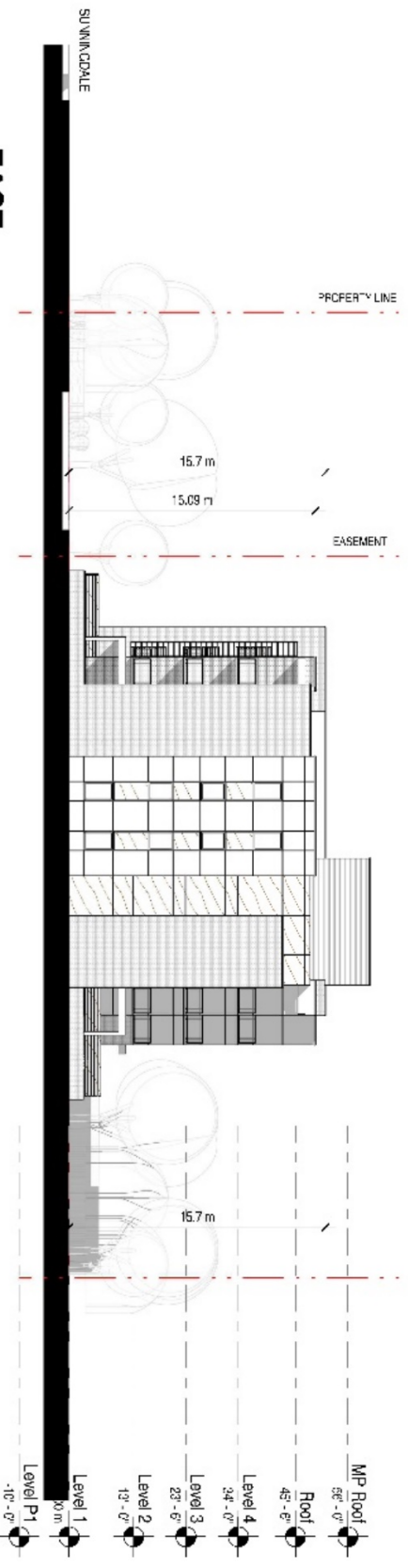
1140 Sunningdale RD E

Elevations BW

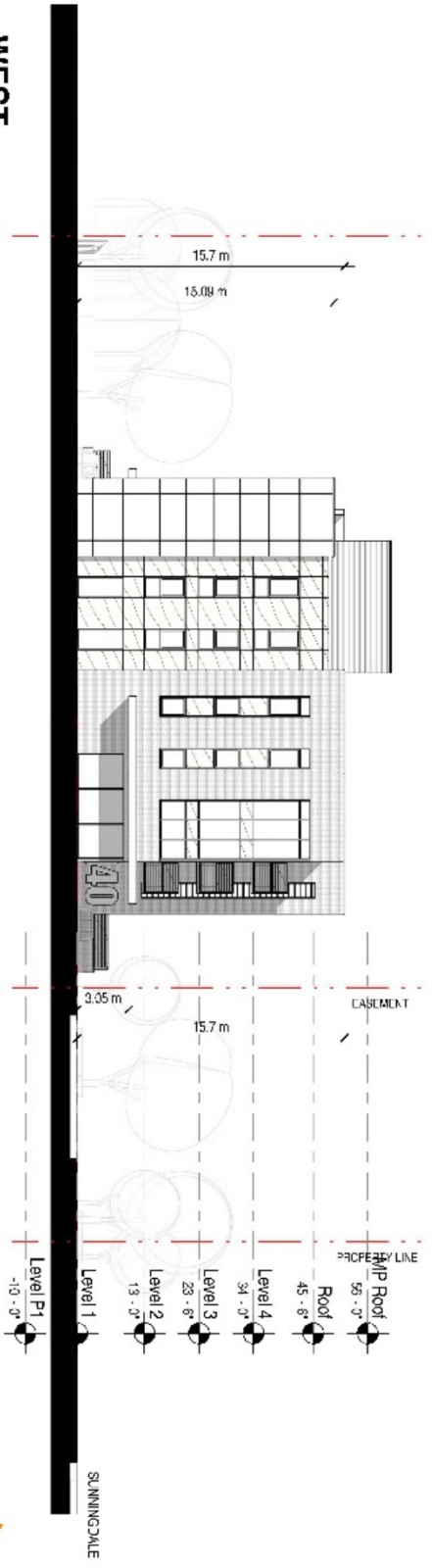
2021-1-01

SD-3.5

zedd
ARCHITECTURE
2477 MIDLAND AVENUE SUITE 102 ST. CATHARINES
ONTARIO L7M 1V7 CANADA TEL: 905.709.1171

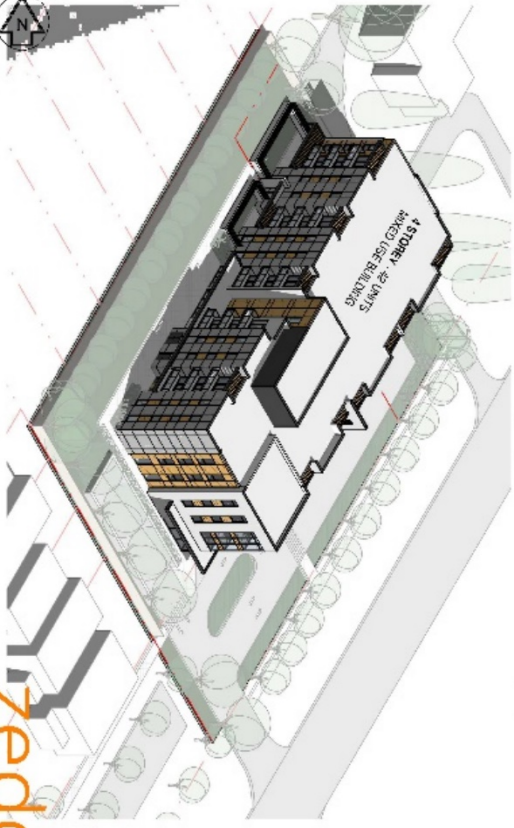
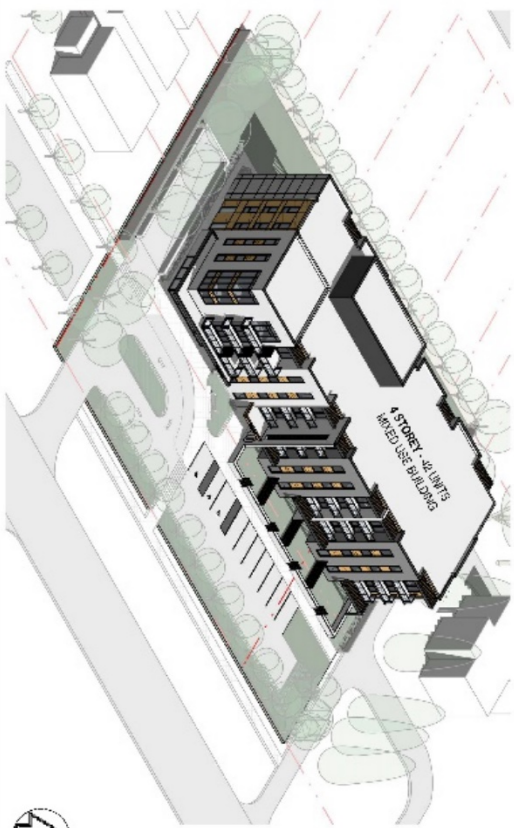


EAST



WEST

Scale: 3/8" = 1'-0"



zedd
ARCHITECTURE

2027 BROADWAY, SUITE 200, NEW YORK, NY 10013
347.362.1111

SD-5.1

ISO

1140 Sunningdale RDE

RPH - SUNNINGDALE RDE

20-056

Scale:
1" = 10'-0"



Appendix B – Community Engagement

Community Engagement

Public Liaison:

- On September 15th, 2021, Notice of Application was sent to **143** property owners in the surrounding area. Notice of Application was also published in the “Public Notices and Bidding Opportunities” section of “The Londoner” on September 16th, 2021. A “Planning Application” sign was also posted on the site.

The were no telephone replies, and **two (2)** written replies received.

- On December 22nd, 2021, Notice of Public Meeting was sent to **143** property owners in the surrounding area. Notice of Public Meeting was also published in the “Public Notices and Bidding Opportunities” section of “The Londoner” on December 23rd, 2021 and advised of modifications to the application.

Nature of Liaison:

The Notice of Application advised of a possible amendment to Zoning By-law Z.-1 to change the zoning from a Convenience Commercial Special Provision (CC(14)) Zone to a Residential R8 Special Provision Bonus (R8-4()•B()) Zone to permit and facilitate the development of a mixed-use building consisting of 42-residential dwelling units and a commercial gross floor area of 250m² (2,691.0 ft²). The notice advised of special provisions to the R8-4 Zone to permit Florist Shops and all existing permitted convenience commercial uses (Convenience Service Establishments, Convenience Stores, Financial Institutions and Personal Service Establishments) all without a drive-through facility as additional permitted uses. Additional special provisions would regulate:

- an increased maximum building height of 16.0 metres (52.5 feet); whereas a maximum of 13.0 metres (42.7 feet) is permitted;
- an increased minimum front yard depth of 22.0 metres (72.2 feet) measured from the front lot line as existing on the date of passing the site-specific by-law; whereas a minimum of 6.0 metres (19.7 feet) plus 1.0 metre (3.3 feet) per 10.0 metres (32.8 feet) of main building height or fraction thereof above 3.0 metres (9.8 feet) is required and equal to 8.0 metres (26.3 feet) based on the proposed maximum building height of 16.0 metres.
- a reduced minimum east interior side yard depth of 2.3 metres (7.5 feet); whereas a minimum interior side yard depth of 1.2 metres (3.9 feet) per 3.0 metres (9.8 feet) of main building height or faction thereof above 3.0 metres (9.8 feet), but in no case less than 4.5 metres (14.8 feet) is required and equal to 6.0 metres (19.7 feet) based on the proposed maximum building height of 16.0 metres (52.5 feet).
- an increased minimum west interior side yard depth and rear yard depth of 1.0 metre (3.3 feet) per 1.0 metre (3.3 feet) of main building height or fraction thereof above 3.0 metres (9.8feet), but in no case less than 7.5 metres (24.6 feet), and equal to 13.0 metre (42.7 feet) based on the proposed maximum building height of 16.0 metres (52.5 feet); whereas a minimum interior side yard depth and rear yard depth of 1.2 metres (3.9 feet) per 3.0 metres (9.8 feet) of main building height or faction thereof above 3.0 metres (9.8 feet), but in no case less than 4.5 metres (14.8 feet)) is required and equal to 6.0 metres (19.7 feet) based on the proposed maximum building height of 16.0 metres (52.5 feet).
- a reduced minimum number of required parking spaces for all permitted commercial uses to permit a minimum of 10 parking spaces (1 space/25 m² (269.1 ft²)); whereas a minimum of 25 parking spaces is required (1 space/10 m² (107.6 ft²)) for Personal Service Establishments.
- a maximum gross floor area of 250m² (2,691.0 ft²) for all permitted commercial uses.

The notice advised of a bonus zone to permit an increased maximum density of 100 uph in return for eligible facilities, services and matters outlined in Section 19.4.4 of the

1989 Official Plan; whereas 75 uph are permitted. The noticed advised that the City may also consider a compound zone and additional special provisions.

The Notice of Public Meeting advised of the modification of the application to change the zoning from a Convenience Commercial Special Provision (CC(14)) Zone to a compound Convenience Commercial Special Provision/Residential R8 Special Provision Bonus Zone (CC4(_)/R8-4(_)**•**H16**•**B(_)) Zone. The notice advised of modified special provisions to regulate:

- a reduced minimum east interior side yard depth of 3.0 metres (9.8 feet); whereas a minimum interior side yard depth of 1.2 metres (3.9 feet) per 3.0 metres (9.8 feet) of main building height or fraction thereof above 3.0 metres (9.8 feet), but in no case less than 4.5 metres (14.8 feet) is required and equal to 6.0 metres (19.7 feet) based on the proposed maximum building height of 16.0 metres (52.5 feet).
- a minimum yard depth of 3.0 metres (9.8 feet) to all lot lines for any underground parking garage ramp.

The Notice of Public Meeting advised that the special provision for an increased maximum building height of 16.0 metres (52.5 feet) would be replaced with a height symbol. It being noted that noting that for heights over 13.0 metres (42.7 feet) the R8 Zone variations require that height be applied site-specifically to zoning maps.

Public Responses:

A summary of the various comments received include concern for:

- The change from the prior proposal for a “commercial plaza” or “strip mall” to the current proposal for an apartment building;
- The proposed apartment building being too intense;
- Security, noise, traffic constraints and congestion associated with an increasingly populated area; and
- Traffic from the proposed development being directed through the local streets internal to the neighbourhood.

Responses to Public Liaison Letter and Publication in “The Londoner”

Telephone	Written
None.	COMMERFORD, SHARON 1124 PLEASANTVIEW DR LONDON ON N5X 4K3
	CORRY, CAROL 1108 PLEASANTVIEW DR LONDON ON N5X 4K3

From:

Sent: September 27, 2021 11:39 AM

To: Campbell, Melissa <mecampbe@london.ca>

Cc: Cassidy, Maureen <mcassidy@london.ca>; Holder, Ed <edholder@london.ca>

Subject: [EXTERNAL] Notice of Planning Application at 1140 Sunningdale Rd East

Melissa Campbell

I am an 11 year resident at 1124 Pleasantview Dr in north London. I am contacting you re: the Notice of Planning Application at 1140 Sunningdale Rd East, that I recently received in the mail.

I attended a meeting several years ago now at the Stoneycreek Library, hosted by our City Councillor Maureen Cassidy. The proposal for this site was presented by a different gentleman from the City Planning Department. This new proposal is NOT even close to the original proposal presented to us that evening. The original proposal called for demolition of the present flower shop, replaced by a 4 unit strip mall, one of those units housing the flower shop business. Also, 12 additional housing lots would be offered at the back of this property, along Waterwheel Rd. That was all! Imagine my

shock at learning that now it would be 4 storeys high, with 42 residential units included!! No residential units were ever mentioned in the original proposal!

We have enough high density housing being built in this corner of the city. Look at all the development happening on the NW corner of Adelaide St and Sunningdale Rd. Before that, it was apartments and condos on the SW corner of this same intersection. We also have increased traffic flows from the new commercial businesses on the SE corner of this same intersection. There is constant traffic going through Tim Horton's as well.

All this construction has increased traffic immensely in our small Forest Hill neighbourhood already. The road infrastructure on Sunningdale was never widened to 4 lanes, to accommodate all this increased traffic. (I lived in North London 40 years ago when a Sunningdale Rd Ring Road was proposed, but nothing ever came of that!) North-South and East-West Traffic is already brutal in this city on the major streets!

I see the increased use of Sunningdale Road with the increased traffic early in the morning and later in the day!

We really feel this issue needs to be debated some more.

Thank you.

Sharon and Mark Hofner (Commerford)
1124 Pleasantview Dr
London ON
N5X 4K3

From: Carol C

Sent: September 29, 2021 8:37 AM

To: Campbell, Melissa <mecampbe@london.ca>

Cc: Cassidy, Maureen <mcassidy@london.ca>

Subject: [EXTERNAL] 1140 Sunningdale Road

Dear Melanie,

My name is Carol Corry. I have been a resident at 1108 Pleasantview Drive for over 18 years.

We have watched this community grow and blossom into a lovely, busy neighbourhood.

We have also been involved in the process to develop 1140 Sunningdale Rd.

Approximately 4 years ago we had to petition so that the city didn't change the names of a couple of our streets at the beginning of the Springhill Flowers development. With a lot of time, energy and neighbourhood concern the city council agreed that changing the names of our street and another street in our community would be ridiculous.

Neighbours then went to a development information session held at the local library and we as a community were ok with the idea of a commercial plaza going there with access via Sunningdale Rd.

This has all changed and I am writing this email to you on behalf of my neighbours on Pleasantview Drive to let you know that we do not want a busy residential apartment unit added to 1140 Sunningdale Rd.

We have many concerns including security, traffic and noise all related to an increased densely populated area. There is already a lot of residential construction going on in this area.

Sunningdale has become a very busy street and increased traffic would hinder the already congested flow especially due to Mother Teresa High School and during morning and evening rush hour.

The residents of a Pleasantview Drive are also deeply concerned that traffic from this new build will be directed through our neighbourhood.

Please let our voices be heard. We have spent our time, taxes and put our hearts into making this community a great one. Please consider our issues and realize that this plan shapes the future of our neighbourhood.

Thank you for your time.

Carol Corry

Agency/Departmental Responses:

September 17, 2021: Parks Planning and Design

Parks Planning and Design staff have reviewed the submitted notice of application and offer the following comments:

- Parkland dedication is required in the form of cash in lieu, pursuant to By-law CP-9 and will be finalized at the time of site plan approval.

Craig Smith, MCIP, RPP

Senior Planner
Parks Planning and Design
City of London

September 17, 2021: London Hydro Engineering

Servicing the above proposal should present no foreseeable problems. Any new and/or relocation of existing infrastructure will be at the applicant's expense, maintaining safe clearances from L.H. infrastructure is mandatory. A blanket easement will be required.

Note: Transformation lead times are minimum 16 weeks. Contact Engineering Dept. to confirm requirements & availability.

London Hydro has no objection to this proposal or possible official plan and/or zoning amendment. However, London Hydro will require a blanket easement.

Hans Schreff

Manager- Developer & Operations Support,
Engineering & Operations Administration Dept.
519-661-5800 ext. 5014

September 21, 2021: Water Engineering

From: Chromczak, David <dchromcz@London.ca>

Sent: September 21, 2021 11:34 AM

To: Campbell, Melissa <mecampbe@london.ca>

Cc: Lambert, Brent <blambert@london.ca>

Subject: RE: Z-9405 - Notice of Planning Application for Zoning By-Law Amendment - 1140 Sunningdale Road East - 2839069 Ontario Inc. c/o Royal Premier Homes (WARD 5)

Water Engineering comments for Zoning amendment for 1140 Sunningdale Rd E – Water is available from the 150mm PVC watermain on Pleasantview Drive. The applicant shall confirm there will be adequate domestic and fire flow supply for the proposed development.

Thanks

Dave Chromczak

Technologist II
Water Engineering Division
City of London

September 27, 2021: Transportation Planning and Design

From: Chamorro, Juan <jchamorr@london.ca>

Sent: September 27, 2021 12:13 PM

To: Di Losa, Paul <pdilosa@london.ca>; Lambert, Brent <blambert@london.ca>

Cc: Grady, Sarah <sgrady@london.ca>; Harpal, Dhaval <dharpal@london.ca>

Subject: Z-9405 - Notice of Planning Application for Zoning By-Law Amendment - 1140 Sunningdale Road East

Good afternoon,

Please find below Transportations comments regarding the Zoning By-Law Amendment for 1140 Sunningdale Road East.

- Right-of-way dedication of 18.0 m from the centre line be required along Sunningdale Road East.

- A parking reduction study will be required.
- Detailed comments regarding access design and location will be made through the site plan process.

Rgs,

Juan C. Chamorro, CET

Senior Transportation Technologist
 Transportation Planning & Design
 City of London

October 7, 2021: Environment and Engineering Services

The City of London's Environmental and Engineering Services Department offers the following comments with respect to the aforementioned Zoning By-Law amendment application:

General:

- An easement is to be established and servicing is to be extended from Pleasantview Drive to the east property line of the subject site. The easement is to be in favour of 1154 Sunningdale for their future development needs.

Transportation:

- Right-of-way dedication of 18.0 m from the centre line be required along Sunningdale Road East.
- A parking reduction study will be required.
- Detailed comments regarding access design and location will be made through the site plan process.

Water:

- Water for the development site is available from the 150mm PVC on Pleasantview Dr.

Wastewater:

- The municipal sanitary for the south half of the property is to the 200mm municipal sanitary sewer at Pleasantview Drive. The lands are part of accepted (Forest Hill Subdivision) sanitary area plan.

Stormwater:

- As per as-constructed drawing 25953, the south portion of the site at C=0.50 is tributary to the existing 375 mm storm sewer on the west end of Pleasantview Drive.
- As per as-constructed drawing 25953, the parcel to the east of this Site (1154 Sunningdale Road East) is also tributary to the existing 375 mm storm sewer on the west end of Pleasantview Drive. Therefore, this site shall be provide a private easement to 1154 Sunningdale Road East for the purpose of private services.
- Changes in the land use of the south portion of this site from residential to commercial will trigger the need to comply with the approved City Standard Design Requirements for Permanent Private Stormwater System (PPS), including LIDs.
- Since the "C" value for the proposed commercial use is expected to be higher than 0.50, the applicant's consulting engineer is to include in the Storm/Drainage Servicing Report rationale and calculations of the on-site SWM controls to the satisfaction of the City Engineer. On-site SWM controls design should include, but not be limited to required storage volume calculations, flow restrictor sizing, etc.
- If the number of parking spaces exceed 29, the owner shall be required to have a consulting Professional Engineer addressing the water quality to the standards of the Ministry of the Environment, Conservation and Parks (MECP) and to the

satisfaction of the City Engineer. Applicable options could include, but not be limited to the use of oil/grit separators or any suitable infiltration/filtration LID solutions.

- The subject lands are located in the Stoney Creek Subwatershed. The Owner shall provide a Storm/Drainage Servicing Report demonstrating compliance with the SWM criteria and environmental targets identified in the Stoney Creek Subwatershed Study that may include but not be limited to, quantity/quality control (80% TSS), erosion, stream morphology, etc.
- This site plan may be eligible to qualify for a Stormwater Rate Reduction (up to 50% reduction) as outlined in Section 6.5.2.1 of the Design Specifications and Requirements manual. Interested applicants can find more information and an application form at the following: <http://www.london.ca/residents/Water/water-bill/Pages/Water-and-Wastewater-Rates.aspx>.
- Any proposed LID solutions should be supported by a Geotechnical Report and/or a Hydrogeological Assessment report prepared with a focus on the type(s) of soil present at the Site, measured infiltration rate, hydraulic conductivity (under field saturated conditions), and seasonal high groundwater elevation. Please note that the installation of monitoring wells and data loggers may be required to properly evaluate seasonal groundwater fluctuations. The report(s) should include geotechnical and hydrogeological recommendations of any preferred/suitable LID solution. All LID proposals are to be in accordance with Section 6 Stormwater Management of the Design Specifications & Requirements manual.
- An Operations and Maintenance manual should be provided as a separate report/manual identifying any implemented/constructed LIDs. For examples of such report contents please refer to the following website: <https://cvc.ca/low-impact-development/lid-maintenance-monitoring/>.

If you have any questions, please feel free to contact Richard Roobroeck at (519) 661-2500 ext. 4952.

October 7, 2021: Urban Design

From: Varughese, Prasanth <pvarughese@london.ca>
Sent: October 7, 2021 4:48 PM
To: Campbell, Melissa <mecampbe@london.ca>
Cc: O'Hagan, Britt <bohagan@london.ca>
Subject: UD Comments: Z9405: 1140 Sunningdale Road East.

Hi Melissa,

Please find below UD Comments for ZBA Application related to **1140 Sunningdale Road East** for your review.

- The applicant is commended for a site and building design that incorporates the following features; a low-rise mixed use built form with active uses along ground floor; ground floor residential units with front courtyards and porches with connections to a common walkway that lead to the city sidewalk; a well-designed and articulated building massing with recesses and projections(e.g., in form of balconies), variation in materials(e.g., brick, steel, aluminum trellis and wood panels) and colour; and clearly distinguished primary entrances from commercial and ground floor residential units; provides for a storefront commercial façade; enhanced North, West and East elevations with increased glazing and articulation; clearly articulated top floor with integrated with lift, stair well and mechanical room; and appropriately sized and located amenity space.
- Ensure adequate setbacks to provide efficient landscape treatment and buffer from North and West property boundaries to mitigate the noise and privacy impacts generated from the outdoor amenity space (towards North) and underground parking entrances towards west of the proposed building.
- Improve the street interface between the proposed building and Sunningdale Road frontage through efficient landscaping along the frontage for screening and

alternative parking and driveway surface treatment (unit pavers or similar materials) to minimize the visual impacts of proposed surface parking and driveway between the building and the ROW.

Please let me know if you have any comments,

Best Regards,

Prasanth C. Varughese, AICP

Urban Designer

Community Planning, Urban Design & Heritage

Planning & Development

City of London

October 14, 2021: Urban Design

From: Varughese, Prasanth <pvarughese@london.ca>

Sent: October 14, 2021 11:01 AM

To: Campbell, Melissa <mecampbe@london.ca>

Subject: RE: UD Comments: Z9405: 1140 Sunningdale Road East.

Hi Melissa,

Good Morning,

Thank you for reaching out to clarify the comments.

For 2nd point with regard to the setbacks, I was concerned with the setbacks between the underground parking and the Western property line, but 3m setback and appropriate landscape treatment within that width should be sufficient to offset any noise and privacy impact.

For 3rd point, We would like to lock-in landscape screening for the exposed surface parking between building and the street through the bonus zone. Surface treatment at this location may not be necessary.

Please let me know if you need further clarifications.

Thanks,

Prasanth C. Varughese, AICP

Urban Designer

Community Planning, Urban Design & Heritage

Planning & Development

City of London

October 19, 2021: Ecology

From: McNiven, Lisa <lmcniven@London.ca>

Sent: October 19, 2021 10:04 AM

To: Campbell, Melissa <mecampbe@london.ca>

Subject: RE: Z-9405 - 1140 Sunningdale Road East - Comments from Site Plan

Good Morning Melissa

Sorry for the delay in getting back to you.

A 3 meter setback is sufficient to support tree growth to provide screening to adjacent private residences. It would be ideal to also request soil amelioration in the area adjacent to parking structure. The soil will become compacted and contaminated during construction of underground structure. To ensure a health growth of trees the contaminated soils to be removed and replaced with growing medium up to 0.6m and undisturbed native soils striated.

Enhanced landscaping to be provided along Sunningdale street frontage to provide screening of parking to sidewalk and ROW in the 3m setback.

Lisa McNiven, MLA OALA CSLA

[She/her]

Landscape Architect

Long Range Planning, Research and Ecology

Planning & Development

City of London

October 19, 2021: Site Plan

From: Vivian, Melanie <mvivian@london.ca>

Sent: October 19, 2021 11:25 AM

To: Campbell, Melissa <mecampbe@london.ca>; McNiven, Lisa <lmcniven@London.ca>

Subject: RE: Z-9405 - 1140 Sunningdale Road East - Comments from Site Plan

Hi Melissa,

I've had a chance to review the submitted concept site plan and the notice. From a Zoning standpoint it looks like all required special provisions were captured accordingly.

For the site design, I have the following comments:

1. Dimension the barrier-free stalls to ensure zoning compliance (if these are not the correct size, it will result in changes to the parking)
2. Dimension the sidewalk width

There were some comments relating to the garbage location and it's functionality in terms of:

- a) The egress of parking stall 10 when the garbage bins are out and;
- b) There is no turn-around location for the trucks.

Is there the opportunity to provide moloks/earth bins in this location instead?

The snow storage is not fully functional in terms of location as it is impacted by the garbage bins on collection day and/or parking stall 10. Through SPC there was a comment relating to exploring opportunities to have the snow storage removed from the site. Is this something that is being considered? If so, there may be more space for moloks/earth bins, landscaping or even garbage collection turnarounds.

Let me know if you want to chat further about this!

Thanks,

Melanie Vivian

Site Development Planner

Site Plans

Planning & Development

City of London

October 20, 2021: Urban Design Peer Review Panel

Urban Design Peer Review Panel Memo

To: Proponents

- Jerzy Smolarek, Partner, Urban Design, Siv-ik Planning & Design
- Max Sim, Lead Feasibility Planning, Zedd Architecture
- David Yuhasz, Partner & Senior Architect, Zedd Architecture

From: Urban Design Peer Review Panel (UDPRP)

- Tim Wickens, Architect
- Adrienne Hossfeld, Architect
- Kyle Poole, Landscape Architect
- Terence Lee, Landscape Architect

Regrets:

- Mike Davis, Planner
- Leo Lin, Architect

RE: Zoning By-law Amendment Application, 1140 Sunningdale Road East, October 20, 2021

- The panel commends the applicant for a clear and complete application, and detailed graphic site context analysis and planning rationale.

- The panel commends the applicant for a clear site strategy that appears to make the most of a difficult and unique planning context.
- The panel commends the applicant for an appropriately articulated building which attempts to architecturally define the publicly oriented program as distinct from the private.
- The panel commends the applicant's effective use of signage and canopies.
- The panel commends the provision of a 3m landscape strip and fencing along the West property line to buffer the proposed commercial use and amenity area from the existing residential rear yards.
- The front yard garbage staging location appears to be temporary/short term. The panel noted that if that is not the case, and it is used for long term storage, it is recommended it be screened with a solid enclosure constructed of opaque materials in keeping with the proposed project aesthetic.
- It was noted that the garbage staging location appears to be unreasonably remote from the interior waste management location and should be reconsidered for a successful project outcome.
- The panel commends the use of masonry walls and wood screens to delineate the boundary between private and public space, and strongly recommends that plant material be included as part of the design solution in future applications to soften transitions and aid in screening.
- The panel notes that the rear façade and building corners appear less resolved than the street façade, with some material treatments appearing inconsistently two-dimensional. A simplification more aligned with the street façade is recommended.
- The panel recommends the applicant explore design opportunities to connect the new building and site design to the heritage of the founding flower shop business, including the possibility of flipping the plan to link the proposed commercial function to the original house to remain on the adjacent site.
- The panel commends the applicant for providing a large amenity space, though expressed concerns that it's northern orientation and awkward proportion will require detailed and creative landscape solutions to resolve successfully. The panel recommends providing detailed solutions to these design opportunities in future applications.

Concluding comments:

This UDPRP review is based on City planning and urban design policy, the submitted brief, and the noted presentation. It is intended to inform the ongoing planning and design process. The development of this site as proposed appears to be an appropriate planning outcome. This project appears to be a creative solution to densifying a difficult and unique urban form. It is noted that this particular solution is unlikely to be successful outside of this specific context. Consider the panel's recommendations as noted above for future refinements to the project in the interest of enhanced experience of the public realm for current and future residents. The Panel looks forward to the proponent's response.

Sincerely on behalf of the UDPRP,
 Tim Wickens, UDPRP Acting Chair

October 28, 2021: Urban Design

From: Varughese, Prasanth <pvarughese@london.ca>
Sent: October 28, 2021 8:28 AM
To: Campbell, Melissa <mecampbe@london.ca>
Subject: RE: Z-9405 - 1140 Sunningdale Road East - Additional UD Questions

Hi Melissa,
 Good Morning,

1. With regard to the Interior Side-yard Setback requested on the east side. Yes, the setbacks are tight and will impact the development to the East property. But the impact is already known to the applicant as that property-1154 Sunningdale Road East belonged to the same owner.
2. December shadows are not a big concern as it tends to be longer and larger, the shadows from March and June are well within their site boundaries during majority of the time periods.

Please let me know if you need me to discuss before the meeting.

Best Regards,

Prasanth C. Varughese, AICP

Urban Designer

Community Planning, Urban Design & Heritage

Planning & Development

City of London

November 4, 2021: London Housing Development Corporation, London

November 4, 2021

TO: City of London Development Services (via e-mail only)

Attention: Mike Corby, Manager, Planning Implementation, Planning and Development
Melissa Campbell, Senior Planner, Long Range Planning and Research, Planning and Development

REGARDING: Bonusing for Affordable Housing

1140 Sunningdale Road East ("Subject Lands")

Background:

Housing Development Corporation, London (HDC) was engaged to work with 2425293 Ontario Inc. c/o Royal Premier Developments (the "Proponent") and provide a fair recommendation to the Director, City of London Development Services in response to the Zoning By-law Amendment application (City of London Planning File: Z-9405) proposal for height and density "bonusing" in exchange for the provision of affordable housing. The application is proposing a four-storey mixed-use building containing 42 residential units and 250 m² of non-residential gross floor area.

This letter reflects the recommendation of HDC and is provided with the concurrence of the Proponent.

RECOMMENDATION:

It is the recommendation of the HDC that the following elements constitute the affordable housing bonus zone:

1. **Two (2) one-bedroom residential units be dedicated to affordable rental housing in exchange for the granting of increased height and density.**
2. **"Affordability" for the purpose of an agreement be defined as rent not exceeding 80% of the Canada Mortgage and Housing Corporation (CMHC) Average Market Rent (AMR) for units where:**
 - i. AMR is defined at the one-bedroom rate for the London Census Metropolitan Area by CMHC at the time of building occupancy;
 - ii. the identified units will be mixed throughout and not otherwise identifiable within the building; and
 - iii. Rents for the affordable rental housing units shall only be increased to the allowable maximum, once per 12-month period in accordance to the *Residential Tenancy Act* or any successor legislation but not to exceed 80% of the CMHC AMR.
3. **The duration of the affordability period be set at 50 years** calculated from initial occupancy of each unit and for each month thereafter that the unit is occupied. At the conclusion of the agreement period, any sitting tenants within associated affordable unit shall retain security of tenure and rental rates until the end of their

tenancy. The rights of tenancy and affordability in the dedicated units shall not be allowed to be assigned or sublet during or after the agreement.

4. **The Proponent be required to enter a Tenant Placement Agreement (TPA) with the City of London.** This action aligns bonus units with priority populations vetted and referred to the Proponent or their agent by the City. The owner retains final tenant selection in accordance with the *Residential Tenancy Act*, subject to the established eligibility and compliance requirements.
5. **These conditions be secured through an agreement registered on title with associated compliance requirements and remedies.** This recommendation ensures the retained value of each affordable rental housing unit within the Bonus Zone for the 50-year affordability period. Compliance will be monitored in a similar fashion as is conducted with other agreements and shall include conditions related to default and remedy.

The Proponent's application proactively aligned their bonus interests to the City's affordable housing priorities and the associated discussions establishing the above recommendation were achieved with their concurrence.

Rationale for Affordable Housing Bonus:

Guiding Policy: The London Plan recognizes housing affordability as one of the City's principle planning challenges. It states that planning activities will provide for a mixture of dwelling types and integrated mixtures of housing affordability. The Plan identifies bonusing as a planning tool in support of the provision of affordable rental housing within planning and development proposals.

Location and Application Considerations: The Subject Lands are on located on the north side of Sunningdale Road East east of South Wenige Drive. The lands are proximate to a broad range of residential, community facility, institutional, open space and office uses. The lands are served by transit.

Alignment to Need: The locational attributes of the site align with factors used by HDC to advance affordable rental housing. The recommendations align with housing needs and priorities defined within the *Housing Stability for All Plan* and CMHC analytics related to housing stock, affordability rates, vacancy rates, rental rates, incomes, and other market conditions.

Conclusion:

The *Planning Act* provides municipalities the ability to advance public facilities, services or matters in exchange for additional height and density above existing zoning permissions. The ability to utilize this important tool as a mechanism to advance affordable rental housing aligns with a critical need in London, noting that London is currently ranked 5th in Canada for the highest percentage of households in "Core Housing Need" in major urban centres (CMHC, July 2018).

This recommendation recognizes Council's expressed interest to seek "...options for implementing and coordinating [planning] tools to be most effective..." to "...promote the development of affordable housing in London" (4.4/12/PEC, July 25, 2018).

Sincerely,

Brian Turcotte, Development Manager, HDC

c. Isabel da Rocha, Business and Program Manager, HDC

December 6, 2021: Subdivisions and Condominiums

From: Mottram, Larry <LMottram@London.ca>

Sent: December 6, 2021 8:27 AM

To: Campbell, Melissa <mecampbe@london.ca>

Subject: RE: Noise Report for Review - 1140 Sunningdale Road East (Z-9405)

Hi Melissa,

I have reviewed the Environmental Noise Assessment Report prepared by Strik Baldinelli Moniz Ltd. dated May 2021 for the above-noted development proposal and am satisfied that it meets the MECP requirements. The report assesses predicted noise levels resulting from road traffic volumes (Sunningdale Road East). Please ensure the

recommendations and specific Warning Clauses identified under Section 4 of the report are included within the Site Plan and Development Agreement for this site.

Should you have any questions or require further clarification, please let me know.

Thanks,

Larry Mottram, MCIP, RPP

Senior Planner - Subdivisions and Condominiums

Planning and Development

City of London

December 10, 2021: Transportation Planning and Design

From: Chamorro, Juan <jchamorr@london.ca>

Sent: December 10, 2021 12:15 PM

To: Campbell, Melissa <mecampbe@london.ca>

Cc: Grady, Sarah <sgrady@london.ca>

Subject: RE: Z-9405 - Parking Justification Brief

Melissa,

I reviewed the Parking Justification Report, and the proposed parking rate of 10 surface parking spaces at the front of the building for use by the commercial unit (equivalent to 1 parking space per 25sqm of commercial space) should be accepted based on other municipalities parking rates that are within those values (Table 4 of the parking brief-attached), regardless the current CoL parking rate is 1 per 10 sqm.

Let me know if you have further questions.

Rgs,

Juan C. Chamorro, CET

Senior Transportation Technologist

Transportation Planning & Design

City of London

Appendix C – Policy Context

The following policy and regulatory documents were considered in their entirety as part of the evaluation of this requested land use change. The most relevant policies, by-laws, and legislation are identified as follows:

Provincial Policy Statement, 2020 (PPS)

Policy 1.1.1 Building Strong Health Communities, Managing and Directing Land Use to Achieve Efficient and Resilient Development and Land Use Patterns

Policy 1.1.3.1 Building Strong Health Communities, Managing and Directing Land Use to Achieve Efficient and Resilient Development and Land Use Patterns, Settlement Areas

Policy 1.1.3.2 Building Strong Health Communities, Managing and Directing Land Use to Achieve Efficient and Resilient Development and Land Use Patterns, Settlement Areas

Policy 1.1.3.3 Building Strong Health Communities, Managing and Directing Land Use to Achieve Efficient and Resilient Development and Land Use Patterns, Settlement Areas

Policy 1.1.3.4 Building Strong Health Communities, Managing and Directing Land Use to Achieve Efficient and Resilient Development and Land Use Patterns, Settlement Areas

Policy 1.4.3 Building Strong Health Communities, Housing

Policy 1.7.1 Building Strong Health Communities, Long Term Economic Prosperity

1989 Official Plan

Section 3.1.1 i)-iv), vii) and x) Residential Land Use Designations, Objectives for Residential Land Use Designations, General Objectives for All Residential Designations

Section 3.1.3 i) and ii) Residential Land Use Designations, Objectives for Residential Land Use Designations, Multi-Family, Medium Density Residential Objectives

Section 3.3 Residential Land Use Designations, Multi-Family, Medium Density Residential

Section 3.3.1 Residential Land Use Designations, Multi-Family, Medium Density Residential, Permitted Uses

Section 3.3.1 ii) Residential Land Use Designations, Multi-Family, Medium Density Residential, Permitted Uses, Convenience Commercial and Service Stations

Section 3.3.3 Residential Land Use Designations, Multi-Family, Medium Density Residential, Scale of Development

Section 3.3.3 i) Residential Land Use Designations, Multi-Family, Medium Density Residential, Scale of Development, Height

Section 3.3.3 ii) Residential Land Use Designations, Multi-Family, Medium Density Residential, Scale of Development, Density

Section 3.6.5 i) Residential Land Use Designations, General Provisions for All Residential Land Use Designations, Convenience Commercial and Service Stations, Function

Section 3.6.5 ii) (a) and (c) Residential Land Use Designations, General Provisions for All Residential Land Use Designations, Convenience Commercial and Service Stations, Permitted Uses

Section 3.6.5 iii) Residential Land Use Designations, General Provisions for All Residential Land Use Designations, Convenience Commercial and Service Stations, Location

Section 3.6.5 iv) Residential Land Use Designations, General Provisions for All Residential Land Use Designations, Convenience Commercial and Service Stations, Scale of Development

Section 3.6.5 vi) Residential Land Use Designations, General Provisions for All Residential Land Use Designations, Convenience Commercial and Service Stations, Locations of Convenience Commercial and Service Station Uses

Section 3.7 Residential Land Use Designations, Planning Impact Analysis,

Section 3.7.2 Residential Land Use Designations, Planning Impact Analysis, Scope of Planning Impact Analysis

Section 3.7.3 Residential Land Use Designations, Planning Impact Analysis, Required Information

Section 18.2.12 Transportation Transportation Planning, Parking Policies

Section 19.4.4 Implementation, Zoning, Bonus Zoning

Section 19.4.4 ii) (a), (c), and (h) Implementation, Zoning, Bonus Zoning, Objectives

The London Plan

(Policies subject to Local Planning Appeals Tribunal, Appeal PL170100, indicated with asterisk.)

Policy 59_2., 4.-6. Our Strategy, Key Directions, Direction #7 Build a Mixed-use Compact City

Policy 61_ 1.-4. and 10. Our Strategy, Key Directions, Direction #7 Build Strong, Health and Attractive Neighbourhoods for Everyone.

Policy 80_4. Our City, City Structure Plan, The Growth Framework, Intensification

- Policy 83_ Our City, City Structure Plan, The Growth Framework, Intensification
- Policy 271 City Design, How Are We Going to Achieve This, Site Layout, Parking
- Policy 789_6. Place Type Policies, Urban Place Types, General Framework
- Policy 918_5. Place Type Policies, Urban Place Types, Neighbourhoods, Approach for Planning Neighbourhoods – Use, Intensity and Form
- Policy 919_2., 3. and 4. Place Type Policies, Urban Place Types, Neighbourhoods, Approach for Planning Neighbourhoods – Use, Intensity and Form
- Policy 924_ Place Type Policies, Urban Place Types, Neighbourhoods, Approach for Planning Neighbourhoods – Use, Intensity and Form, Permitted Uses
- Policy 925_ Place Type Policies, Urban Place Types, Neighbourhoods, Approach for Planning Neighbourhoods – Use, Intensity and Form, Permitted Uses
- Policy 926_ Place Type Policies, Urban Place Types, Neighbourhoods, Approach for Planning Neighbourhoods – Use, Intensity and Form, Permitted Uses
- *Policy 935_1. and 2. Place Type Policies, Urban Place Types, Neighbourhoods, Approach for Planning Neighbourhoods – Use, Intensity and Form, Intensity
- Policy 936_1. and 3. Place Type Policies, Urban Place Types, Neighbourhoods, Approach for Planning Neighbourhoods – Use, Intensity and Form, Form
- Policy 937_ Place Type Policies, Urban Place Types, Neighbourhoods, Residential Intensification in Neighbourhoods
- Policy 939_ Place Type Policies, Urban Place Types, Neighbourhoods, Forms of Residential Intensification
- Policy 940_ Place Type Policies, Urban Place Types, Neighbourhoods, Forms of Residential Intensification
- Policy 953_1., 2. and 3. Place Type Policies, Urban Place Types, Neighbourhoods, Residential Intensification in Neighbourhoods, Additional Urban Design Considerations for Residential Intensification
- *Policy 1638_ Our Tools, Planning and Development Controls, Bonus Zoning
- *Policy 1643_1. Our Tools, Planning and Development Controls, Bonus Zoning
- *Policy 1645_ Our Tools, Planning and Development Controls, Bonus Zoning, Type 1 Bonus Zoning
- *Policy 1646_1. Our Tools, Planning and Development Controls, Bonus Zoning, Type 1 Bonus Zoning
- *Policy 1647_ Our Tools, Planning and Development Controls, Bonus Zoning, Type 1 Bonus Zoning
- Table 10 Range of Permitted Uses in Neighbourhoods Place Type
- *Table 11 Range of Permitted Heights in Neighbourhood Place Type
- *Table 12 – Retail, Service and Office Floor Area Permitted in Neighbourhood Place Type

3.7 Planning Impact Analysis	
Criteria	Response
Compatibility of proposed uses with surrounding land uses, and the likely impact of the proposed development on present and future land uses in the area;	With respect to land use compatibility and appropriate locations for the proposed development, the subject lands are located on a major street and meet the locational criteria for low-rise apartment buildings and convenience commercial uses in residential areas as identified by the in-force Official Plan policies. The proposed development will provide for convenience commercial uses in a

residential area that are appropriately sized and neighbourhood-oriented for the needs of residents.

The proposed development has been evaluated from a form-based perspective and found to be compatible and a good fit with the neighbourhood context based on the following considerations site layout, building and main entrance orientation, building line and setback from the street, and height and massing transitions with adjacent development.

With respect to site layout, the vehicular driveway access to Sunningdale Road East is generally aligned with an existing driveway access on the south side of Sunningdale Road East. The location of the ramp to the underground parking is located on the west side of the subject lands to provide more separation between the proposed building and the existing adjacent single detached dwellings to the west. The requested amendment includes a minimum yard depth to all lot lines of 3.0 metres (9.8 feet) for the underground parking ramp to ensure sufficient space is provided for a landscape screen to minimize and mitigate any adverse impacts from the underground parking ramp on the adjacent dwellings. Most of the required on-site parking is proposed to be located underground and where surface parking is proposed it is located in the front yard and away from the planned and existing single detached dwellings to the north and to the west.

With respect to building and main entrance orientation, the proposed building has been oriented towards Sunningdale Road East and away from abutting properties and the internal portion of the neighbourhood. The street-facing elevation includes the principal building entrance for residential uses and the unit entrance for convenience commercial uses to animate the Sunningdale Road East streetscape and focus public interactions away from abutting properties. To minimize and mitigate overlook from the proposed development adversely impacting existing single detached dwellings, balconies are proposed only on the north (rear) and south (front/street-facing) elevations and are inset to screen views to the existing single detached dwellings to the west.

With respect to building line and setback from the street, the Plan of Subdivision

	<p>that surrounds the subject lands established a series of window-streets adjacent to Sunningdale Road East. The proposed development will not complete the window-street network based on past public input and Municipal Council direction. However, the placement and setback of the proposed building will mimic the window-street network visually and continue the built-edge condition or “built street-wall” along Sunningdale Road East.</p> <p>With respect to height and massing transitions to planned and existing single detached dwellings to the north and to the west, all components of the proposed building have been designed to be contained within a 45-degree angular plane to provide a gradual transition in height over the distance or depth of the subject lands’ rear (north) and west interior side yards. This gradual transition in height over the yard depth will minimize and mitigate shadow impacts on the adjacent properties. The rear (north) and west interior side yard depths exceed the standard minimum yard depth required by the requested R8-4 Zone variation.</p>
<p>The size and shape of the parcel of land on which a proposal is to be located, and the ability of the site to accommodate the intensity of the proposed use;</p>	<p>The size and shape of the subject lands appears generally able to accommodate the intensity of the proposed development. Detailed design at a future Site Plan Approval stage will refine the site elements.</p>
<p>The supply of vacant land in the area which is already designated and/or zoned for the proposed use;</p>	<p>There is no vacant land in the immediate area which is already designated and/or zoned for the proposed use.</p>
<p>The proximity of any proposal for medium or high density residential development to public open space and recreational facilities, community facilities, and transit services, and the adequacy of these facilities and services;</p>	<p>The subject lands are located within walking distance of the Stoney Creek Community Centre, Mother Teresa Catholic Secondary School, Stoney Creek Public School and the commercial node at the intersection of Adelaide Street North and Sunningdale Road East. Neighbourhood parks and natural heritage open space is also within walking distance of the subject lands.</p>
<p>The need for affordable housing in the area, and in the City as a whole, as determined by the policies of Chapter 12 – Housing;</p>	<p>Dwelling units in apartment buildings are intrinsically more affordable than the neighbourhood’s prevailing single detached dwelling units. The proposed multi-unit residential development will diversify unit size, (offering 1 and 2-bedroom units), and possibly diversify tenure (ownership or rental) in the neighbourhood to support affordability in the neighbourhood and housing options for all types of households including aging in place. Moreover, the addition of the</p>

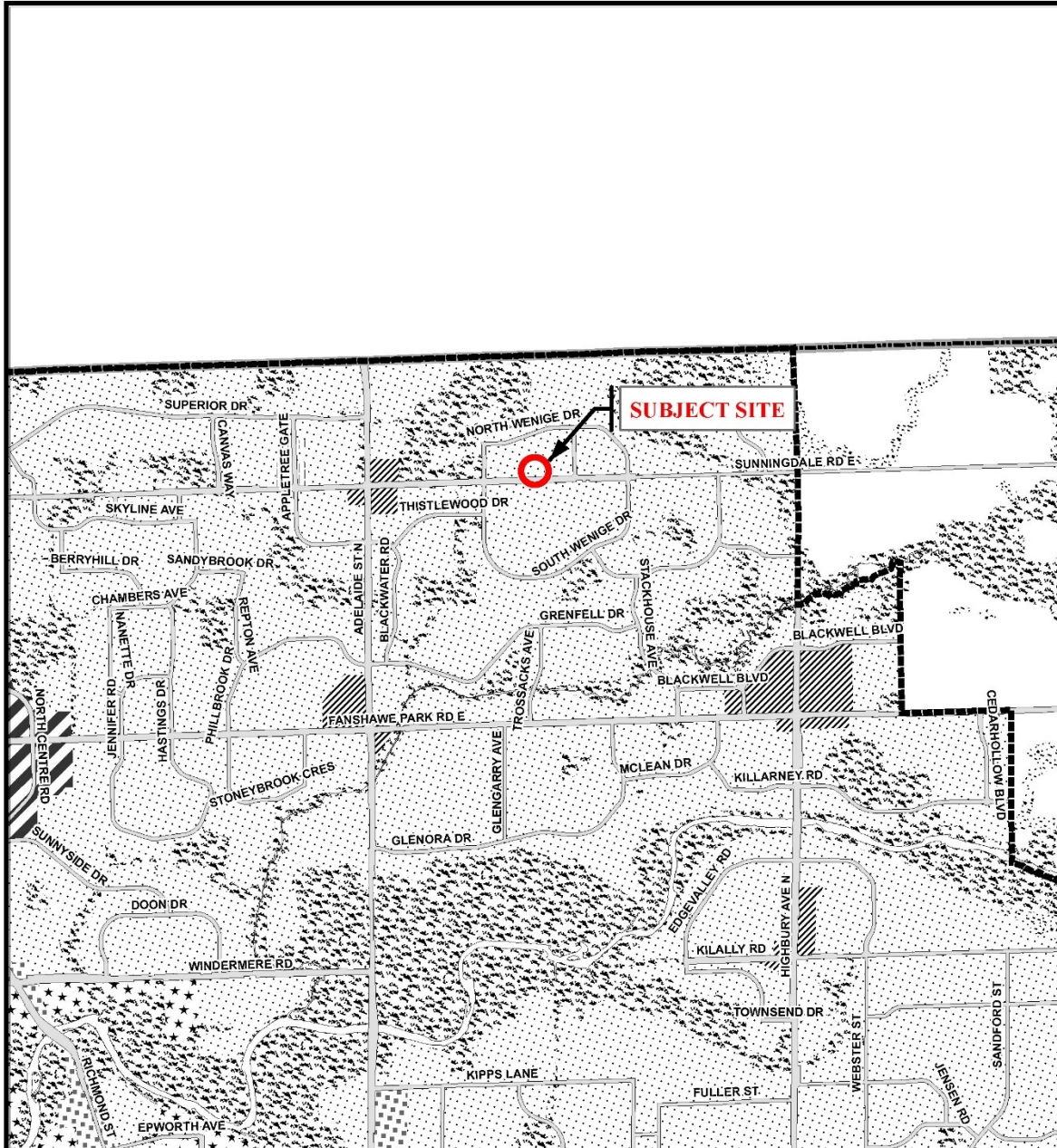
	<p>proposed units to the housing supply may also free-up other more affordable units elsewhere in support of Municipal Council's commitment to the Housing Stability Action Plan, Strategic Area of Focus 2: Create More Housing Stock. The use of bonus zoning will secure two (2) affordable housing units within the proposed development.</p>
<p>The height, location and spacing of any buildings in the proposed development, and any potential impacts on surrounding land uses;</p>	<p>The height and massing of the proposed 4-storey apartment building has been designed to be contained within a 45-degree angular plane measured from the rear (north) and west lot lines to mitigate the shadow impacts on the adjacent planned and existing single detached dwellings.</p> <p>The use of the angular plane provides for a gradual transition in height over the yard depths from the proposed 4-storey apartment building to the 2-storey existing single detached dwellings.</p> <p>Requested special provisions to the underlying R8-4 Zone will ensure that any apartment building proposal on the subject land will require an increased rear (north) and west interior side yard depth that is more onerous than the standard minimum requirement to minimize and mitigate the shadow and overlook impacts of the proposed development on the adjacent single detached dwellings.</p> <p>The location of the ramp to the underground parking is located on the west side of the subject lands to provide more separation between the proposed building and the existing adjacent single detached dwellings to the west.</p> <p>All yard depths will be sufficient to accommodate a landscape screen and support tree growth to minimize and mitigate loss of privacy for adjacent properties.</p>
<p>The extent to which the proposed development provides for the retention of any desirable vegetation or natural features that contribute to the visual character of the surrounding area;</p>	<p>It appears that trees were removed from the subject lands between 2015 and 2018 and the subject lands are now relatively clear of vegetation.</p> <p>Landscaping including and screening opportunities through tree planting will be considered at a future Site Plan Approval stage.</p>
<p>The location of vehicular access points and their compliance with the City's road access policies and Site Plan Control By-law, and the likely impact of traffic generated by the proposal on City streets, on pedestrian and vehicular safety, and</p>	<p>The subject lands have frontage on Sunningdale Road East, and the conceptual site plan shows direct vehicular driveway access to this street. There is no vehicular driveway access</p>

<p>on surrounding properties;</p>	<p>contemplated to the local streets internal to the neighbourhood.</p> <p>No comments were received from Transportation Planning and Design staff to suggest that the proposed development and/or location of vehicular driveway access has the potential to adversely impact the designed function or capacity of Sunningdale Road East or other major streets in the area.</p> <p>The proposed vehicular driveway access has been designed to generally aligned with an existing driveway access on the south side of Sunningdale Road East.</p>
<p>The exterior design in terms of the bulk, scale, and layout of buildings, and the integration of these uses with present and future land uses in the area;</p>	<p>Urban Design staff and the Urban Design Peer Review Panel commended the site and building design for the following features: a successful site layout that appears to make the most of the planning context; an articulated building that architecturally defines public and private function/space as distinct from one another, and effective use of signage and canopies.</p> <p>At the site plan stage, additional attention should be paid to the detailed design criteria to further urban design goals and provide screening and buffering adjacent to planned and existing single detached dwellings.</p>
<p>The potential impact of the development on surrounding natural features and heritage resources;</p>	<p>Not applicable.</p>
<p>Constraints posed by the environment, including but not limited to locations where adverse effects from landfill sites, sewage treatment plants, methane gas, contaminated soils, noise, ground borne vibration and rail safety may limit development;</p>	<p>Not applicable.</p>
<p>Compliance of the proposed development with the provisions of the City's Official Plan, Zoning By-law, Site Plan Control By-law, and Sign Control By-law;</p>	<p>The proposed 4-storey mixed use apartment building conforms to the in-force policies for lands identified as Neighbourhoods Place Type in The London Plan and Multi-Family Medium Density Residential in the 1989 Official Plan.</p> <p>The requirements of the Site Plan Control By-law will be considered through the detailed design of the site at a future Site Plan Approval stage to ensure functionality, including the provision of amenity space, drive aisle widths, sidewalk widths, garbage and snow storage, and long-term bicycle storage etc.</p>

<p>Measures planned by the applicant to mitigate any adverse impacts on surrounding land uses and streets which have been identified as part of the Planning Impact Analysis;</p>	<p>Enhanced yard depths, building height and massing transitions, and landscape screening in combination with privacy fencing are expected to mitigate minor adverse impacts on the surrounding land uses.</p>
<p>Impacts of the proposed change on the transportation system, including transit</p>	<p>The infill development and residential intensification of the subject lands will have a negligible impact on the transportation system and provide a more transit-supportive form and intensity of development abutting a major street (Sunningdale Road East).</p> <p>Major streets can serve as significant routes for transit.</p> <p>Sunningdale Road East is classified as a Civic Boulevard in The London Plan and an Arterial in the 1989 Official Plan. Civic Boulevards and Arterials are higher-order street classifications intended to move medium to high volumes of traffic, with priority given to pedestrian, cycling and transit movements as the streets are upgraded over-time to a complete urban street cross-section.</p>

Appendix D – Relevant Background

Additional Maps



Legend

Downtown	Future Community Growth	Environmental Review
Transit Village	Heavy Industrial	Farmland
Shopping Area	Light Industrial	Rural Neighbourhood
Rapid Transit Corridor	Future Industrial Growth	Waste Management Resource Recovery Area
Urban Corridor	Commercial Industrial	Urban Growth Boundary
Main Street	Institutional	
Neighbourhood	Green Space	

This is an excerpt from the Planning Division's working consolidation of Map 1 - Place Types of the London Plan, with added notations.

At the time of the printing of this map, the Rapid Transit EA is in progress. This map shows the Rapid Transit Corridors and Urban Corridors to recognize potential alignments. These Place Types will be modified to align with the results of the EA process for the final version of The London Plan.

CITY OF LONDON

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LONDON PLAN MAP 1 - PLACE TYPES -

PREPARED BY: Planning Services



Scale 1:30,000

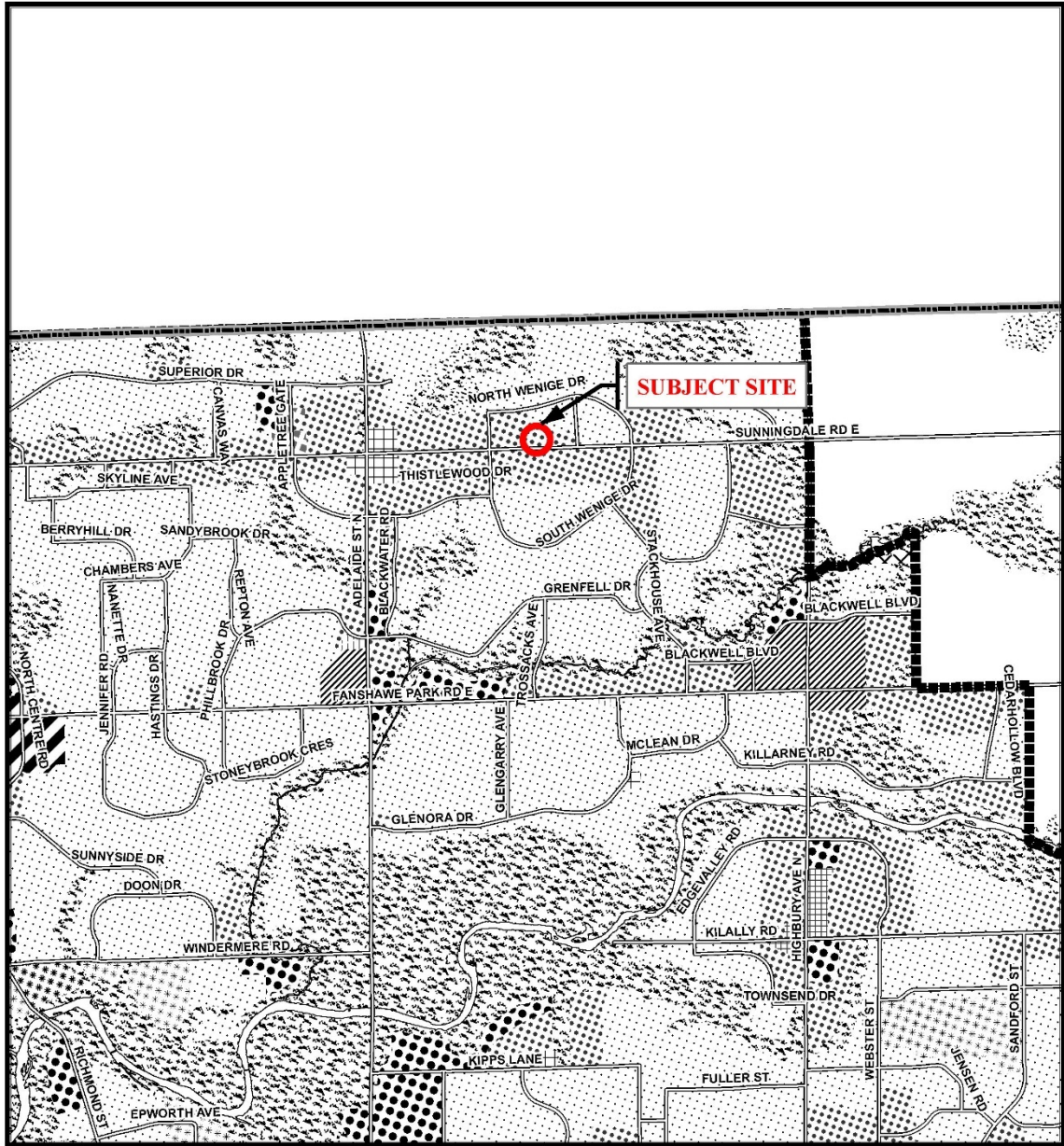


File Number: Z-9405

Planner: MC

Technician: RC

Date: November 25, 2021



Legend

- | | | |
|---|--|-------------------------|
| Downtown | Multi-Family, Medium Density Residential | Office Business Park |
| Wonderland Road Community Enterprise Corridor | Low Density Residential | General Industrial |
| Enclosed Regional Commercial Node | Office Area | Light Industrial |
| New Format Regional Commercial Node | Office/Residential | Commercial Industrial |
| Community Commercial Node | Regional Facility | Transitional Industrial |
| Neighbourhood Commercial Node | Community Facility | Rural Settlement |
| Main Street Commercial Corridor | Open Space | Environmental Review |
| Auto-Oriented Commercial Corridor | Urban Reserve - Community Growth | Agriculture |
| Multi-Family, High Density Residential | Urban Reserve - Industrial Growth | Urban Growth Boundary |

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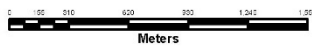
Planning Services /
Development Services

OFFICIAL PLAN SCHEDULE A
- LANDUSE -

PREPARED BY: Graphics and Information Services



Scale 1:30,000

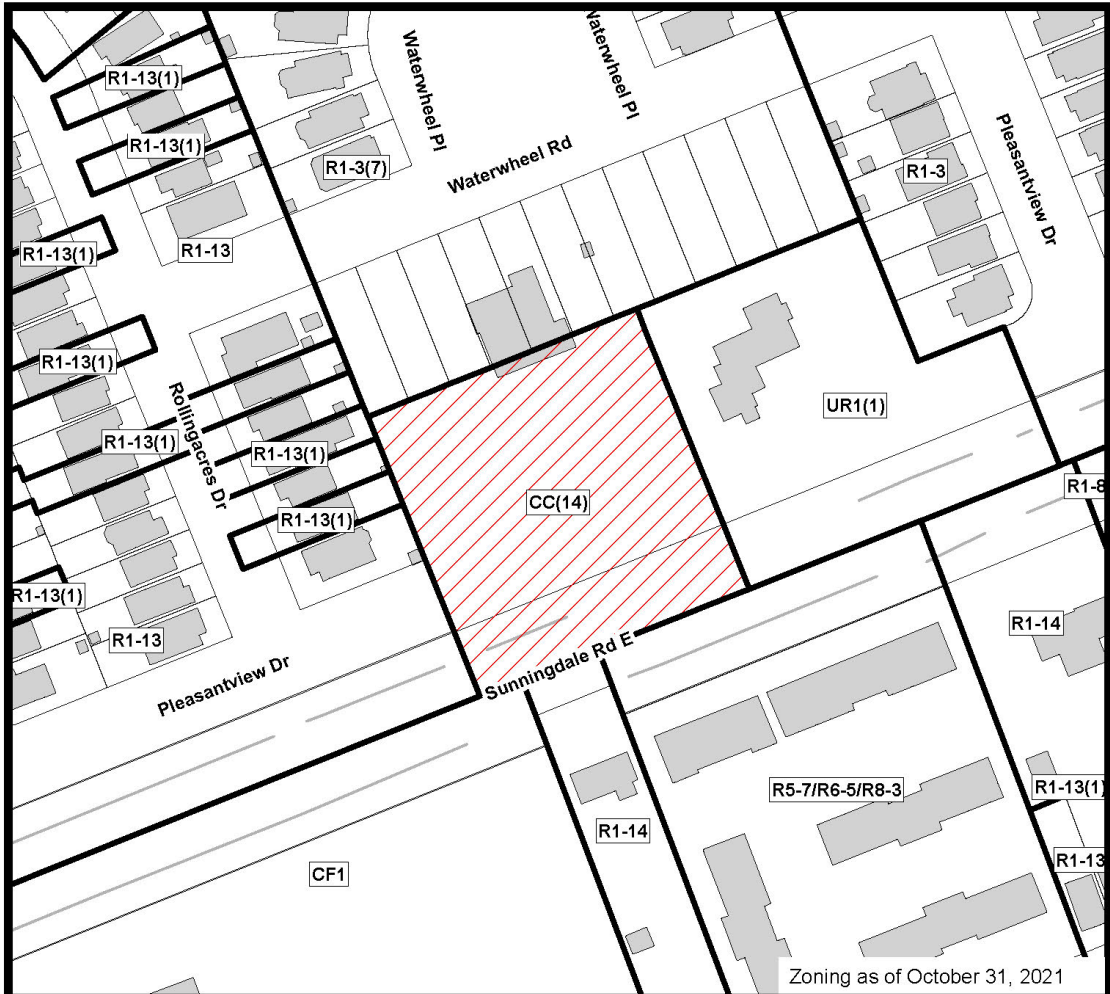


FILE NUMBER: Z-9405

PLANNER: MC

TECHNICIAN: RC

DATE: 2021/11/25



COUNCIL APPROVED ZONING FOR THE SUBJECT SITE:

1) **LEGEND FOR ZONING BY-LAW Z-1**

- R1 - SINGLE DETACHED DWELLINGS
- R2 - SINGLE AND TWO UNIT DWELLINGS
- R3 - SINGLE TO FOUR UNIT DWELLINGS
- R4 - STREET TOWNHOUSE
- R5 - CLUSTER TOWNHOUSE
- R6 - CLUSTER HOUSING ALL FORMS
- R7 - SENIOR'S HOUSING
- R8 - MEDIUM DENSITY/LOW RISE APTS.
- R9 - MEDIUM TO HIGH DENSITY APTS.
- R10 - HIGH DENSITY APARTMENTS
- R11 - LODGING HOUSE

- DA - DOWNTOWN AREA
- RSA - REGIONAL SHOPPING AREA
- CSA - COMMUNITY SHOPPING AREA
- NSA - NEIGHBOURHOOD SHOPPING AREA
- BDC - BUSINESS DISTRICT COMMERCIAL
- AC - ARTERIAL COMMERCIAL
- HS - HIGHWAY SERVICE COMMERCIAL
- RSC - RESTRICTED SERVICE COMMERCIAL
- CC - CONVENIENCE COMMERCIAL
- SS - AUTOMOBILE SERVICE STATION
- ASA - ASSOCIATED SHOPPING AREA COMMERCIAL

- OR - OFFICE/RESIDENTIAL
- OC - OFFICE CONVERSION
- RO - RESTRICTED OFFICE
- OF - OFFICE

- RF - REGIONAL FACILITY
- CF - COMMUNITY FACILITY
- NF - NEIGHBOURHOOD FACILITY
- HER - HERITAGE
- DC - DAY CARE

- OS - OPEN SPACE
- CR - COMMERCIAL RECREATION
- ER - ENVIRONMENTAL REVIEW

- OB - OFFICE BUSINESS PARK
- LI - LIGHT INDUSTRIAL
- GI - GENERAL INDUSTRIAL
- HI - HEAVY INDUSTRIAL
- EX - RESOURCE EXTRACTIVE
- UR - URBAN RESERVE

- AG - AGRICULTURAL
- AGC - AGRICULTURAL COMMERCIAL
- RRC - RURAL SETTLEMENT COMMERCIAL
- TGS - TEMPORARY GARDEN SUITE
- RT - RAIL TRANSPORTATION

- "h" - HOLDING SYMBOL
- "D" - DENSITY SYMBOL
- "H" - HEIGHT SYMBOL
- "B" - BONUS SYMBOL
- "T" - TEMPORARY USE SYMBOL

CITY OF LONDON

PLANNING SERVICES / DEVELOPMENT SERVICES

**ZONING
BY-LAW NO. Z-1
SCHEDULE A**



THIS MAP IS AN UNOFFICIAL EXTRACT FROM THE ZONING BY-LAW WITH ADDED NOTATIONS

FILE NO:

Z-9405

MC

MAP PREPARED:

2021/11/25

RC

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Meters

Appendix E – Applicant Response to UDPRP Comments

Urban Design Peer Review Panel Comments – Applicant Response

Address of Development Site: 1140 Sunningdale Road E

Date of Panel Meeting: 2021-10-20

Comment:

The panel commends the applicant for a clear and complete application, and detailed graphic site context analysis and planning rationale.

Applicant Response:

Acknowledged, thank you.

Comment:

The panel commends the applicant for a clear site strategy that appears to make the most of a difficult and unique planning context.

Applicant Response:

Acknowledged, thank you.

Comment:

The panel commends the applicant for an appropriately articulated building which attempts to architecturally define the publicly oriented program as distinct from the private.

Applicant Response:

Acknowledged, thank you.

Comment:

The panel commends the applicant's effective use of signage and canopies.

Applicant Response:

Acknowledged, thank you.

Comment:

The panel commends the provision of a 3m landscape strip and fencing along the West property line to buffer the proposed commercial use and amenity area from the existing residential rear yards.

Applicant Response:

Acknowledged, thank you.

Comment:

The front yard garbage staging location appears to be temporary/short term. The panel noted that if that is not the case, and it is used for long term storage, it is recommended it be screened with a solid enclosure constructed of opaque materials in keeping with the proposed project aesthetic.

Applicant Response:

The proposed garbage location in the front yard is a concrete pad that will be used only temporarily during garbage day pick-ups. Garbage in bins will be brought out only on garbage day and will be stored back indoors once pick-up is complete.

Comment:

It was noted that the garbage staging location appears to be unreasonably remote from the interior waste management location and should be reconsidered for a successful project outcome.

Applicant Response:

We will examine alternate locations for garbage pick-up, however we believe that this location would be the least impactful to the function and layout of the site. The building

manager would be responsible for moving the bins from the indoor garbage room to the outdoor garbage pick-up location.

Comment:

The panel commends the use of masonry walls and wood screens to delineate the boundary between private and public space, and strongly recommends that plant material be included as part of the design solution in future applications to soften transitions and aid in screening.

Applicant Response:

Acknowledged, thank you. Once we further develop our landscape plans, we will look to incorporate landscaping to soften the proposed screening walls.

Comment:

The panel notes that the rear façade and building corners appear less resolved than the street façade, with some material treatments appearing inconsistently two dimensional. A simplification more aligned with the street façade is recommended.

Applicant Response:

We are currently looking at the rear and side facades and may implement brick that would mimic the front (street facing) elevation.

Comment:

The panel recommends the applicant explore design opportunities to connect the new building and site design to the heritage of the founding flower shop business, including the possibility of flipping the plan to link the proposed commercial function to the original house to remain on the adjacent site.

Applicant Response:

We have explored the opportunity to locate the commercial use on the east end of the building and believe that the current location on the west side (next to the principle building entrance) allows for the dual functionality of turn around and the lay-by for both the commercial and residential uses. We did explore to locate both entrances and the layby at the east side of the site however due to the fact that the location of our vehicular entrance is locked-in place we would then have to have all drop-off/pickup/delivery traffic to cross the entire front parking lot.

Comment:

The panel commends the applicant for providing a large amenity space, though expressed concerns that it's northern orientation and awkward proportion will require detailed and creative landscape solutions to resolve successfully. The panel recommends providing detailed solutions to these design opportunities in future applications.

Applicant Response:

Acknowledged, thank you. We will work closely with the landscape architect selected for this project to come up with creative landscape solutions for the space.

Form Completed By: Jerzy Smolarek, Partner, Urban Design, Siv-ik Planning & Design