

Report to Planning and Environment Committee

To: Chair and Members
Planning & Environment Committee

From: George Kotsifas, P. Eng.
Managing Director, Development & Compliance Services and
Chief Building Official

Subject: 800, 805 & 810 Chelton Road
Application for Zoning By-law Amendment

Public Participation Meeting on: October 7, 2019

Recommendation

That, on the recommendation of the Director, Development Services based on the application of The Ironstone Building Company Inc. relating to the lands located at 800, 805 and 810 Chelton Road, the proposed by-law attached hereto as Appendix 'A' **BE INTRODUCED** at the Municipal Council meeting to be held on October 15, 2019 to amend Zoning By-law No. Z.-1, in conformity with the Official Plan, to change the zoning of the subject lands **FROM** a Community Shopping Area CSA3 Zone, Associated Shopping Area Commercial (ASA1/ASA2/ASA3) Zone, and a Holding Residential R6/R9 (h-54•R6-5/R9-3•H20) Zone **TO** a Residential R5 (R5-7) Zone, a Residential R5 Special Provision R5-7() Zone, a Residential R9 Special Provision Bonus (R9-7()•H16•B-_) Zone, and an Open Space OS5 Zone.

IT BEING NOTED THAT the proposed Bonus Zone will be implemented through the required development agreement(s) to facilitate the development of low to mid-rise apartment buildings up to a maximum of five (5) storeys and six (6) storeys which substantially implements the vision and principles of the Chelton Road Master Plan for 800, 805 and 810 Chelton Road, and site concept plans attached to the amending by-law in exchange for the following facilities, services and matters:

- i. Enhanced building and site design features including an active street edge along Bradley Avenue, Chelton Road, and Meadowgate Boulevard;
- ii. Dedication of public open space (OS5) lands;
- iii. Measures to enhance the Natural Heritage System including substantial buffer planting with native vegetation to protect adjacent natural features;
- iv. Large quantities of secure bicycle parking and cycling infrastructure including indoor and outdoor storage facilities, and a multi-use pathway internal to the site providing connections to surrounding public sidewalks and pedestrian trails; and,
- v. Substantial tree planting of large caliper trees to be planted along the street frontage and outdoor amenity areas.

Executive Summary

Purpose and the Effect of Recommended Action

The purpose and effect of the recommended action is to amend the Zoning By-law to allow residential dwellings including townhouses and back-to-back townhouse units up to three storeys in height; and apartment buildings ranging in height from four (4) to six (6) storeys.

Rationale of Recommended Action

1. The recommended zoning amendment is consistent with the *Provincial Policy Statement (PPS), 2014*, as it promotes efficient development and land use patterns; accommodates an appropriate range and mix of land uses, housing types, and densities to meet projected needs of current and future residents; and minimizes land consumption and servicing costs.
2. The recommended zoning amendment conforms to the in-force policies of The London Plan, including but not limited to the Neighbourhoods Place Type, Our Strategy, City Building and Design, Our Tools, and all other applicable London Plan policies.
3. The recommended zoning amendment permits a use, form and intensity of residential development that conforms to the in-force policies of the (1989) Official Plan, including but not limited to the Community Commercial Node and Multi-Family, High Density Residential designations.
4. The recommended zoning amendment will allow for an increase in building height through a Bonus Zone which requires that developments implement the vision and principles of the Chelton Road Master Plan, and site concept plans attached to the amending by-law. The recommended Bonus Zone provides for an increased height in return for a series of bonusable features, matters and contributions that benefit the public.
5. The subject development blocks are of a size and shape suitable to accommodate the proposal. The recommended zoning amendment provides appropriate regulations to control the use and intensity of the building and ensure a well-designed development with appropriate mitigation measures.
6. The proposed uses, form, and intensity are considered appropriate and compatible with existing residential development in the surrounding neighbourhood.

Analysis

1.0 Site at a Glance

1.1 Property Description

The subject lands are comprised of vacant blocks within a registered plan of subdivision (Block 150, 151 and 152 Registered Plan 33M-756). The site topography is relatively flat and devoid of vegetation, with the exception of trees inside the northerly limit of Blocks 151 and 152 proposed to be retained and zoned as Open Space.

1.2 Current Planning Information (see more detail in Appendix D)

- The London Plan Place Types – Shopping Area, Neighbourhoods and Green Space
- (1989) Official Plan Designations – Community Commercial Node, Multi-family, High Density Residential and Open Space
- Zoning:
 - Community Shopping Area CSA3
 - Associated Shopping Area Commercial (ASA1/ASA2/ASA3)
 - Holding Residential R6/R9 (h-54•R6-5/R9-3•H20)

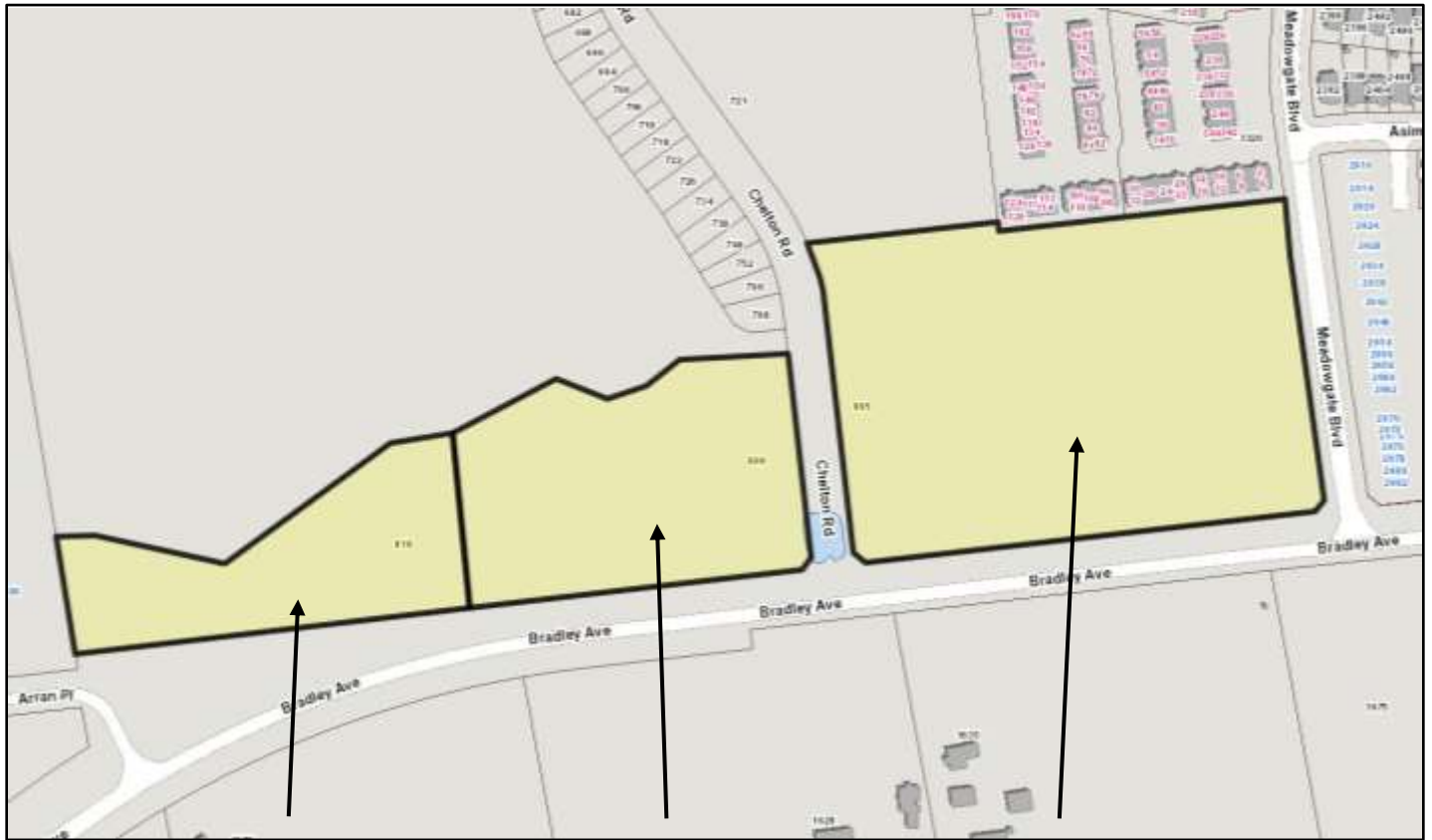
1.3 Site Characteristics

	Current Land Use	Frontage (approx.)	Depth (approx.)	Area	Shape
Block 150	Vacant	160 metres	250 metres	4.15 hectares	Regular
Block 151	Vacant	108 metres	176 metres	1.97 hectares	Irregular
Block 152	Vacant	209 metres	92 metres	1.34 hectares	Irregular

1.4 Surrounding Land Uses

- North – townhouses, future medium density residential, and open space lands (Provincially Significant Wetland)
- East – future residential (single detached and street townhouse dwellings)
- South – farm and cultivated fields
- West – cultivated fields and open space lands

Site Location Map

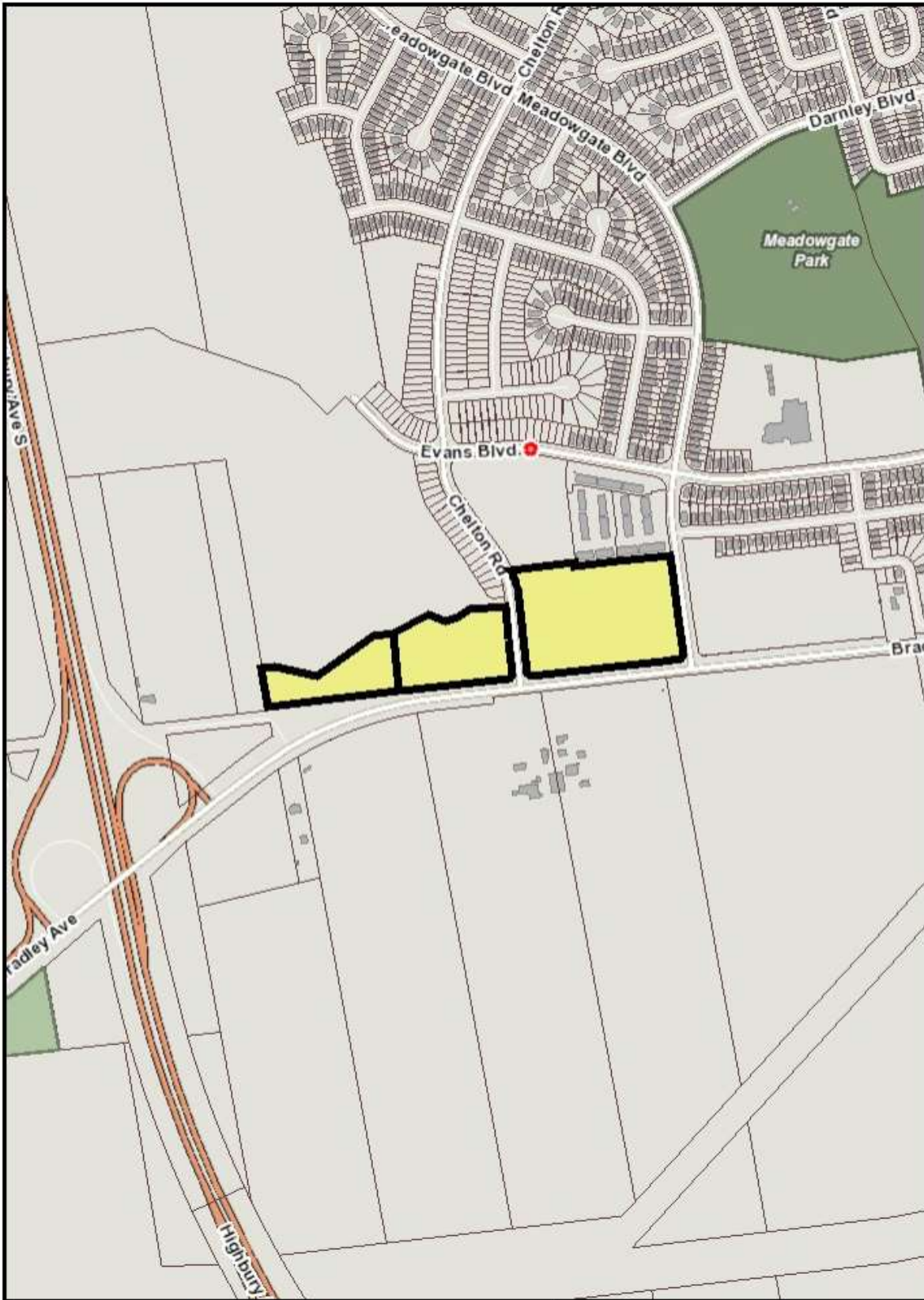


**810 Chelton Road
(Block 152)**

**800 Chelton Road
(Block 151)**

**805 Chelton Road
(Block 150)**

1.5 Location Map



Location Map

Subject Property: 800 Chelton Road
Applicant: **The Ironstone Building Co. Inc.**
File Number: Z-9089
Created By: Larry Mottram
Date: 7/2/2019
Scale: 1:8000

Legend

-  Subject Property
-  Parks
-  Assessment Parcels
-  Buildings
-  Address Numbers



2.0 Description of Proposal

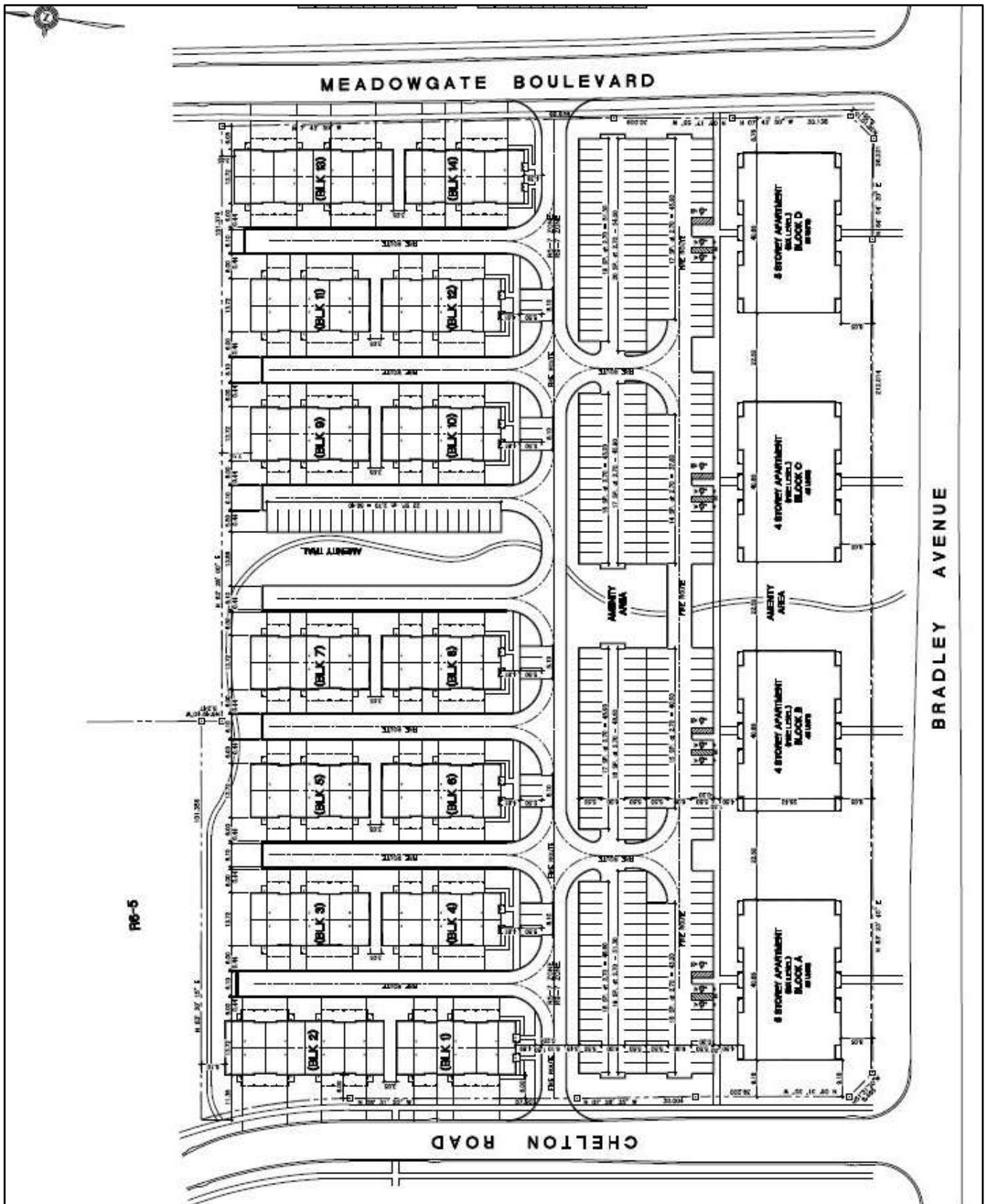
2.1 Development Proposal

805 Chelton Road – Four apartment buildings (two 4-storey and two 5-storey buildings) oriented to Bradley Avenue on the southerly portion of the site (200 units total); and 3-storey, back-to-back cluster townhouses on the northerly portion of the site (88 units total).

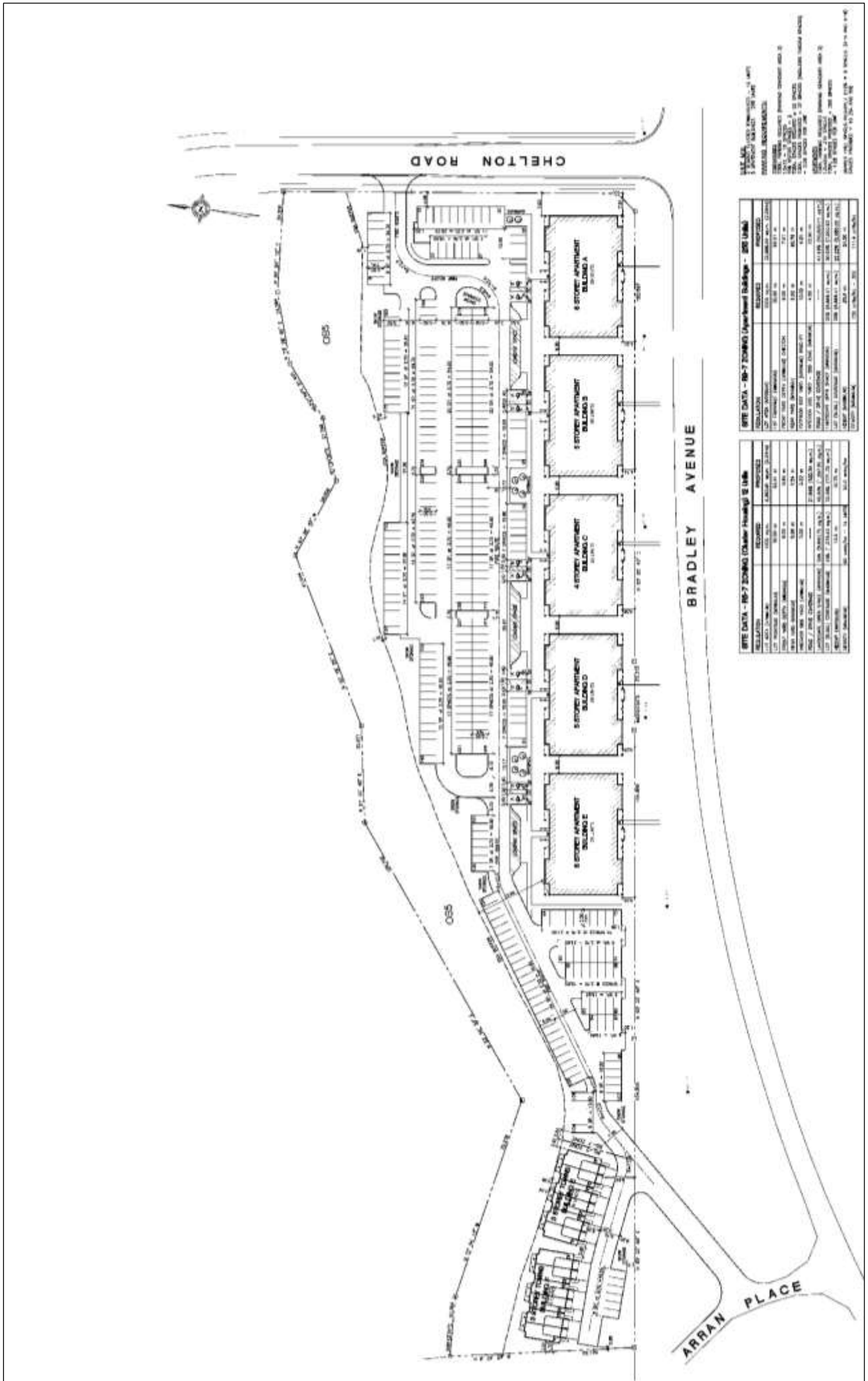
800 - 810 Chelton Road – Five apartment buildings (one 4-storey, two 5-storey and two 6-storey buildings) oriented to Bradley Avenue on the southerly portion of the site (255 units total); and cluster townhouses on the westerly portion of the site (12 units total).

2.2 Site Concept Plans

805 Chelton Road



800 - 810 Chelton Road



SCALE:
 1:1000 (AS SHOWN)

REGULATIONS:

- 1. ALL BUILDINGS SHALL BE SET BACK FROM THE FRONT BOUNDARY BY A MINIMUM OF 10 METRES.
- 2. ALL BUILDINGS SHALL BE SET BACK FROM THE SIDE BOUNDARIES BY A MINIMUM OF 5 METRES.
- 3. ALL BUILDINGS SHALL BE SET BACK FROM THE REAR BOUNDARY BY A MINIMUM OF 5 METRES.
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- 7. ALL BUILDINGS SHALL BE SET BACK FROM THE ADJACENT WATER BOUNDARY BY A MINIMUM OF 10 METRES.
- 8. ALL BUILDINGS SHALL BE SET BACK FROM THE ADJACENT AIRPORT BOUNDARY BY A MINIMUM OF 10 METRES.
- 9. ALL BUILDINGS SHALL BE SET BACK FROM THE ADJACENT INDUSTRIAL BOUNDARY BY A MINIMUM OF 10 METRES.
- 10. ALL BUILDINGS SHALL BE SET BACK FROM THE ADJACENT RESIDENTIAL BOUNDARY BY A MINIMUM OF 10 METRES.

SITE DATA - (80-7 ZONING) (Other Housing) 2 Units

REGULATION	REQUIREMENTS	PROPOSED
1. ALL BUILDINGS SHALL BE SET BACK FROM THE FRONT BOUNDARY BY A MINIMUM OF 10 METRES.	10.0 M	10.0 M
2. ALL BUILDINGS SHALL BE SET BACK FROM THE SIDE BOUNDARIES BY A MINIMUM OF 5 METRES.	5.0 M	5.0 M
3. ALL BUILDINGS SHALL BE SET BACK FROM THE REAR BOUNDARY BY A MINIMUM OF 5 METRES.	5.0 M	5.0 M
4. ALL BUILDINGS SHALL BE SET BACK FROM THE CORNER BOUNDARIES BY A MINIMUM OF 10 METRES.	10.0 M	10.0 M
5. ALL BUILDINGS SHALL BE SET BACK FROM THE ADJACENT ROAD BOUNDARY BY A MINIMUM OF 10 METRES.	10.0 M	10.0 M
6. ALL BUILDINGS SHALL BE SET BACK FROM THE ADJACENT RAILWAY BOUNDARY BY A MINIMUM OF 10 METRES.	10.0 M	10.0 M
7. ALL BUILDINGS SHALL BE SET BACK FROM THE ADJACENT WATER BOUNDARY BY A MINIMUM OF 10 METRES.	10.0 M	10.0 M
8. ALL BUILDINGS SHALL BE SET BACK FROM THE ADJACENT AIRPORT BOUNDARY BY A MINIMUM OF 10 METRES.	10.0 M	10.0 M
9. ALL BUILDINGS SHALL BE SET BACK FROM THE ADJACENT INDUSTRIAL BOUNDARY BY A MINIMUM OF 10 METRES.	10.0 M	10.0 M
10. ALL BUILDINGS SHALL BE SET BACK FROM THE ADJACENT RESIDENTIAL BOUNDARY BY A MINIMUM OF 10 METRES.	10.0 M	10.0 M

SITE DATA - (80-7 ZONING) (Apartment Building) - 200 Units

REGULATION	REQUIREMENTS	PROPOSED
1. ALL BUILDINGS SHALL BE SET BACK FROM THE FRONT BOUNDARY BY A MINIMUM OF 10 METRES.	10.0 M	10.0 M
2. ALL BUILDINGS SHALL BE SET BACK FROM THE SIDE BOUNDARIES BY A MINIMUM OF 5 METRES.	5.0 M	5.0 M
3. ALL BUILDINGS SHALL BE SET BACK FROM THE REAR BOUNDARY BY A MINIMUM OF 5 METRES.	5.0 M	5.0 M
4. ALL BUILDINGS SHALL BE SET BACK FROM THE CORNER BOUNDARIES BY A MINIMUM OF 10 METRES.	10.0 M	10.0 M
5. ALL BUILDINGS SHALL BE SET BACK FROM THE ADJACENT ROAD BOUNDARY BY A MINIMUM OF 10 METRES.	10.0 M	10.0 M
6. ALL BUILDINGS SHALL BE SET BACK FROM THE ADJACENT RAILWAY BOUNDARY BY A MINIMUM OF 10 METRES.	10.0 M	10.0 M
7. ALL BUILDINGS SHALL BE SET BACK FROM THE ADJACENT WATER BOUNDARY BY A MINIMUM OF 10 METRES.	10.0 M	10.0 M
8. ALL BUILDINGS SHALL BE SET BACK FROM THE ADJACENT AIRPORT BOUNDARY BY A MINIMUM OF 10 METRES.	10.0 M	10.0 M
9. ALL BUILDINGS SHALL BE SET BACK FROM THE ADJACENT INDUSTRIAL BOUNDARY BY A MINIMUM OF 10 METRES.	10.0 M	10.0 M
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2.2 Conceptual Building Renderings



3 storey townhouse units



3 storey, back-to-back townhouse units



5 storey apartment building



6 storey apartment building

3.0 Relevant Background

3.1 Planning History

On January 9, 2017 the City of London Approval Authority granted draft-approval to a revised draft plan of subdivision representing Phase 13 of the Summerside subdivision located on the north side of Bradley Avenue, east of Highbury Avenue South. The draft plan phase included 196 single detached lots, one (1) residential block, one (1) hydro easement block, two (2) medium density residential blocks, one (1) multi-family residential block, two (2) commercial blocks, one (1) open space block, one (1) walkway block, and several 0.3 m reserve blocks; all served by Bradley Avenue, the extensions of Evans Boulevard, Chelton Road, and Meadowgate Boulevard, and two (2) new streets shown as Irish Moss Road and Chelton Court.

The lands which are the subject of this application were identified as Community Shopping Area, Associated Shopping Area, and Medium Density Residential blocks in the original draft plan approved in July of 2006. This draft plan was granted several extensions to the lapse date over a period of time. In August of 2015, Drewlo Holdings Inc. acquired the remaining undeveloped lands within the Summerside subdivision previously owned by Jackson Land Corp. and Jackson Summerside Land Corp. They subsequently came forward with a request for red-line revisions to the portions of the Summerside draft plan representing Phase 13. Final approval for part of the Phase 13 draft plan, including the two commercial blocks (Blocks 150 and 151) and the multi-family block (Block 152) was given in November 20, 2018, and the plan was registered on December 5, 2018 as Registered Plan No. 33M-756.

3.2 Requested Amendment

Amendment to Zoning By-law No. Z.-1 to change the zoning from a Community Shopping Area (CSA3) Zone, an Associated Shopping Area Commercial (ASA1/ASA2/ASA3) Zone, and a holding Residential R6/R9 (h-54 R6-5/R9-3 H20) Zone to a Residential R5 (R5-7) Zone to permit cluster townhouses and cluster stacked townhouses up to 60 units per hectare and maximum height of 12 metres (north half Block 150); a Residential R5 Special Provision (R5-7()) Zone with special provisions to permit an interior side yard depth of 2.0 metres minimum, and 0.0 metre yard setback adjacent to an OS5 Zone (west portion of Block 152); a Residential R9 Special Provision (R9-7()) Zone to permit apartment buildings, senior citizens apartment buildings, and continuum-of-care facilities up to 150 units per hectare and maximum height of 25.0 metres, together with special provisions to permit a front yard depth of 6.0 metres minimum, exterior side yard depth of 3.0 metres

minimum, and 0.0 metre yard setback adjacent an OS5 Zone (south half Block 150, Block 151, and portion of Block 152); and to an Open Space (OS5) Zone to permit conservation lands, conservation works, passive recreation uses which include hiking trails and multi-use pathways, and managed woodlots (northerly perimeter Blocks 151 & 152). Bonus zoning for additional building height exceeding four (4) storeys up to six (6) storeys is also requested.

3.3 Community Engagement (see more detail in Appendix B)

Comments/concerns received from the community are summarized as follows:

- There are no shopping or restaurant establishments in the immediate area so the current zoning seems the most appropriate.
- High density residential will result in greater traffic volumes at peak times on an already congested, two-lane Bradley Avenue.

3.4 Policy Context (see more detail in Appendix C)

Provincial Policy Statement, 2014

The proposal must be consistent with the Provincial Policy Statement (PPS) policies and objectives aimed at:

1. Building Strong Healthy Communities;
2. Wise Use and Management of Resources; and,
3. Protecting Public Health and Safety.

The PPS contains policies regarding the importance of promoting efficient development and land use patterns, accommodating an appropriate range and mix of land uses, housing types, and densities to meet projected needs of current and future residents, and minimizing land consumption and servicing costs (Sections 1.1 and 1.4). The policies for Settlement Areas require that land use patterns be based on densities and mix of uses that efficiently use land and resources; are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available; minimize negative impacts on the environment; promote energy efficiency; support active transportation; and are transit supportive where transit is planned, exists or may be developed (Section 1.1.3.2).

The policies for Public Spaces, Recreation, Parks, Trails and Open Space promote healthy and active communities by planning public streets, spaces and facilities to be safe, meet the needs of pedestrians, foster social interaction and facilitate active transportation and community connectivity (Section 1.5.1). Planning Authorities shall also support energy conservation and efficiency through land use and development patterns which, among other matters, promotes design and orientation which maximizes opportunities for renewable energy systems (Section 1.8.1). Natural features and areas shall be protected for the long term (Section 2.1.1).

The London Plan

The London Plan is the new Official Plan for the City of London (Council adopted, approved by the Ministry with modifications, and the majority of which is in force and effect). The London Plan policies under appeal to the *Local Planning Appeals Tribunal* (Appeal PL170100) and not in force and effect are indicated with an asterisk* throughout this report. The London Plan policies under appeal are included in this report for informative purposes indicating the intent of City Council, but are not determinative for the purposes of this planning application.

Map 1 – Place Types* shows Block 150 as being within a Shopping Area Place Type and Blocks 151 and 152 within the Neighbourhoods, and partially within the Green Space Place Types. The Shopping Area Place Type permits a broad range of retail, service, office, entertainment, recreational, educational, institutional, and residential uses. The Neighbourhoods Place Type permits a range of residential uses including single detached and semi-detached dwellings, triplexes, fourplexes, townhouses, stacked townhouses, low-rise apartments, mixed use buildings, and small-scale community facilities. The

Green Space Place Type allows a range of open space uses including natural heritage features and natural resources, public parks, cemeteries, golf courses, recreational and community facilities. Bradley Avenue is classified on Map 3 – Street Classifications* as Urban Thoroughfare, and Meadowgate Boulevard and Chelton Road are classified as Neighbourhood Connectors. An excerpt from The London Plan Map 1 – Place Types* is found at Appendix D. It should be noted that the High Density Residential Overlay (from 1989 Official Plan)* applies to Block 152. The HDR overlay allows residential development to a maximum height of 12 storeys and density up to 150 units per hectare.

(1989) Official Plan

Blocks 150 and 151 are predominately designated Community Commercial Node, and Block 152 is predominantly designated Multi-family, High Density Residential on Schedule A – Land Use Map. In addition, the Open Space designation applies to a portion of land in the northwest corner of Block 152 and along the northern perimeter of Block 151. The Community Commercial Node permits a range of commercial and retail uses including food stores, pharmacies, convenience commercial uses, personal services, restaurants, financial institutions, limited automotive services, and various office uses. Multi-family, high density residential uses and community facilities may also be permitted in this designation through a zoning by-law amendment application, site plan application and consideration of design features which provide for the proper integration of the two uses.

The Multi-family, High Density Residential designation permits low-rise and high-rise apartment buildings, apartment hotels, multiple-attached dwellings, emergency care facilities, nursing home, and rest homes as the main uses. The Open Space designation permits such uses as public and private parks, cemeteries, golf courses, hazard lands, flood plain lands and components of the Natural Heritage System. An excerpt from Land Use Schedule 'A' is found at Appendix D.

4.0 Key Issues and Considerations

4.1 Issue and Consideration # 1 – Is the recommended zoning appropriate from the perspective of use, intensity and form?

Use

The recommended zoning results in more low to medium density residential housing types (3-storey townhouses and back-to-back townhouses, and low to mid-rise apartment buildings) within a predominantly low density residential community composed of single detached homes, stacked townhouses and street townhouses. The proposed zoning change maintains an appropriate range and mix of land uses that are compatible with the surrounding neighbourhood. The mix of uses in the area is characterized by a transition from suburban development north of Bradley Avenue to active agricultural uses to the south. The lands to the south are zoned Urban Reserve (UR6) and are intended over the long term for light industrial uses. Future planning must take into consideration compatibility concerns and potential nuisance impacts on existing residential uses and developments which are approved or underway.

Intensity

The site concept plans (attached) proposes a row of two 4-storey and two 5-storey apartment buildings on the southerly half of Block 150 fronting on to Bradley Avenue, consisting of a total of 200 apartment units. Rows of back-to-back townhouse dwellings are shown on the north half having a total of 88 units. The overall density (apartment and townhouse units combined) is approximately 70 units per hectare. The 5-storey buildings are shown at each end of the block oriented to the intersections of Bradley Avenue with Chelton Road and Meadowgate Boulevard, with the two 4-storey buildings in the middle. The arrangement of all building forms on the site addresses the principles of compatibility and building height in the Chelton Road Master Plan (also attached). The concept site plan demonstrates a transition in height and density with taller and denser buildings located towards Bradley Avenue, and shorter, less dense townhouses on the north half

of the block, in proximity to the lower density and low-rise profile of existing and planned single detached and townhouse dwellings to the north and east.

Similarly, the site concept plan for Blocks 151 and 152 proposes a row of one 4-storey, two 5-storey, and two 6-storey buildings oriented to Bradley Avenue, with a total of 255 apartment units. A cluster of 12 townhouse units are shown at the westerly end of the development block. The overall density (apartment and townhouse units combined) is approximately 105 units per hectare. The building arrangement for this block shown in the Chelton Road Master Plan has been revised following consultation meetings with City staff, and the apartment buildings have now been shifted closer to Bradley Avenue, as well as a parking structure has been removed from the site concept plan. The apartment buildings have been aligned in a row with 6-storey buildings at each end, decreasing to 5-storeys and a 4-storeys in the middle. This provides variation to the building profile along the streetscape while also concentrating building height and mass towards the intersection of Chelton Road and Bradley Avenue.

This zoning application included a request for height increases through bonusing. Building heights may be permitted to exceed four storeys in order to facilitate the development of apartment buildings up to a maximum of five (5) storeys and six (6) storeys through Type 2 Bonus zoning. The Bonus (B-) Zone will be implemented through the site plan(s) and development agreement(s); and substantial implementation of the vision and principles of the Chelton Road Master Plan, and site concept plans attached to the amending by-law. Benefits for bonusing include enhanced building and site design features adhering to the Chelton Road Master Plan; dedication of additional open space lands within the proposed OS5 zone; substantial buffer planting with native vegetation to protect adjacent natural features (various types of buffer enhancements will be explored through the detailed site planning process); bicycle parking and cycling infrastructure, and a multi-use pathway internal to the site providing connections to surrounding public sidewalks and multi-use trails; and the planting of large caliper trees along the street frontage and outdoor amenity areas.

The Type 2 Bonus Zoning criteria were reviewed and the planning merits and enhanced design elements outlined in the Chelton Road Master Plan, Urban Design Brief and Planning Justification Report have been considered. The applicant has demonstrated that the resulting intensity and form of development is appropriate for the development context, commensurate with the public benefit derived from the project enhancements, and represents good planning.

Form

The proposed development includes mid-rise apartments and low-rise townhouses. Apartment buildings will be positioned to frame the street intersections, and include pedestrian entrances from the public sidewalks. Townhouses will also be front-facing to the surrounding public streets. Various architectural techniques are proposed to break up the massing of the apartment blocks, including building recesses, changes in building materials/colours, and incorporation of varying window sizes. Building massing should be designed to create a comfortable pedestrian environment which will be further enhanced through the provision of private amenity space including a connected walkway/trail system. The Chelton Road Master Plan document which accompanied the application submission establishes the key design principles and provides specific direction to guide future built form.

4.2 Issue and Consideration # 2 – Issues raised from the community engagement.

- *There are no shopping or restaurant establishments in the immediate area so the current zoning seems the most appropriate.*

There is a significant amount of leasable commercial/retail space and vacant commercially zoned lands in close proximity to this location, primarily along Commissioners Road East, which is available to serve the needs of current and future residents. The southeast quadrant of Commissioners Road East and Highbury Avenue is

zoned Community Shopping Area (CSA5) with large amounts of vacant retail floor space. Commercial services such as food stores, drug stores, banks, restaurants, and a public library exist further to the west along Commissioners Road East, west of Highbury Ave. This area of the City is seeing more residential development activity which will help to build up the population base, generate demand and attract more local stores and services to the area. Staff have had discussions with the owners of the subject lands who have advised that there is little interest at this location for community shopping or commercial development. Opportunities for commercial uses are being maintained through the O.P. designation and Shopping Area Place Type should demand shift in the future.

- High density residential will result in greater traffic volumes at peak times on an already congested, two-lane Bradley Avenue.

Bradley Avenue is an Arterial Road and carries approximately 6500 vehicles per day (AADT). The City's Transportation Planning and Design Division were circulated on the application and did not indicate any concerns regarding the traffic carrying capacity of Bradley Avenue. Transportation staff did report that construction of a left turn lane on Bradley Avenue will be required to provide access from Arran Place. The alignment of the proposed access to Arran Place needs to be revised to comply with the City's Access Management Guidelines. Detailed comments regarding access location, design, and external road works will be made through the site plan process. Construction of left turn and right turn lanes on Bradley Avenue at Chelton Road and at Meadowgate Boulevard were previously required to satisfy conditions of approval for this subdivision phase.

5.0 Conclusion

The recommended zoning amendments are appropriate and consistent with the Provincial Policy Statement, conform to The London Plan and the (1989) Official Plan. The proposed uses and recommended zoning are considered appropriate and compatible with existing residential development in the surrounding neighbourhood.

Prepared by:	Larry Mottram, MCIP, RPP Senior Planner, Development Planning
Recommended by:	Paul Yeoman, RPP, PLE Director, Development Services
Submitted by:	George Kotsifas, P. Eng. Managing Director, Development and Compliance Services and Chief Building Official
<p>Note: The opinions contained herein are offered by a person or persons qualified to provide expert opinion. Further detail with respect to qualifications can be obtained from Development Services.</p>	

CC: Matt Feldberg, Manager, Development Services (Subdivisions)
 Lou Pompillii, Manager, Development Planning
 Ismail Abushehada, Manager, Development Engineering

Appendix A

Appendix "A"

Bill No. (number to be inserted by
Clerk's Office)
(2019)

By-law No. Z.-1-19_____

A by-law to amend By-law No. Z.-1 to
rezone an area of land located at 800,
805 and 810 Chelton Road.

WHEREAS The Ironstone Building Company Inc. has applied to rezone an area of land located at 800, 805 and 810 Chelton Road, as shown on the map attached to this by-law, as set out below;

AND WHEREAS this rezoning conforms to the Official Plan;

THEREFORE the Municipal Council of The Corporation of the City of London enacts as follows:

- 1) Schedule "A" to By-law No. Z.-1 is amended by changing the zoning applicable to lands located at 800, 805 and 810 Chelton Road, as shown on the attached map, from a Community Shopping Area CSA3 Zone, an Associated Shopping Area Commercial (ASA1/ASA2/ASA3) Zone, and a Holding Residential R6/R9 (h-54•R6-5/R9-3•H20) Zone **TO** a Residential R5 (R5-7) Zone, a Residential R5 Special Provision (R5-7()) Zone, a Residential R9 Special Provision Bonus (R9-7()•H16•B-) Zone, and an Open Space OS5 Zone.
- 2) Section Number 4.3 of the General Provisions is amended by adding the following Site Specific Bonus Provision:

B-__ 800, 805 and 810 Chelton Road

The Bonus Zone shall be implemented through the required development agreement(s) to facilitate the development of low to mid-rise apartment buildings up to a maximum of five (5) storeys and six (6) storeys which substantially implements the vision and principles of the Chelton Road Master Plan for 800, 805 and 810 Chelton Road attached as Schedule "1", and site concept plans attached as Schedule "2" to the amending by-law; and,

- i. Enhanced building and site design features including an active street edge along Bradley Avenue, Chelton Road, and Meadowgate Boulevard;
- ii. Dedication of public open space (OS5) lands;
- iii. Measures to enhance the Natural Heritage System including substantial buffer planting with native vegetation to protect adjacent natural features;
- iv. Large quantities of secure bicycle parking and cycling infrastructure including indoor and outdoor storage facilities, and a multi-use pathway internal to the site providing connections to surrounding public sidewalks and pedestrian trails; and,
- v. Substantial tree planting of large caliper trees to be planted along the street frontage and outdoor amenity areas.

The following special regulations apply within the bonus zone upon the execution and registration of the required development agreement(s):

a) Regulations:

805 Chelton Road

- | | | |
|----|---------------------------------------|-------------------------------|
| i) | Height of Buildings A and D (Maximum) | Five (5) storeys
18 metres |
|----|---------------------------------------|-------------------------------|

800 – 810 Chelton Road

- | | | |
|-----|---------------------------------------|-------------------------------|
| i) | Height of Buildings A and E (Maximum) | Six (6) storeys
22 metres |
| ii) | Height of Buildings B and D (Maximum) | Five (5) storeys
18 metres |

3) Section Number 9.4 of the Residential R5 Zone is amended by adding the following special provisions:

R5-7()

a) Regulations:

- | | | |
|-----|---|------------|
| i) | Interior Side Yard Yard Setback (Minimum) | 2.0 metres |
| ii) | Setback from an Open Space (OS5) Zone (Minimum) | 3.0 metres |

4) Section Number 13.4 of the Residential R9 Zone is amended by adding the following special provisions:

R9-7()

a) Regulations:

- | | | |
|------|---|-------------------------------|
| i) | Front Yard Setback (Minimum)
(Maximum) | 3.0 metres
6.0 metres |
| ii) | Exterior Side Yard Setback (Minimum)
(Maximum) | 3.0 metres
6.0 metres |
| iii) | Height (Maximum) | Four (4) storeys
16 metres |

File: Z-9089
Planner: L. Mottram

This By-law shall come into force and be deemed to come into force in accordance with Section 34 of the *Planning Act, R.S.O. 1990, c. P13*, either upon the date of the passage of this by-law or as otherwise provided by the said section.

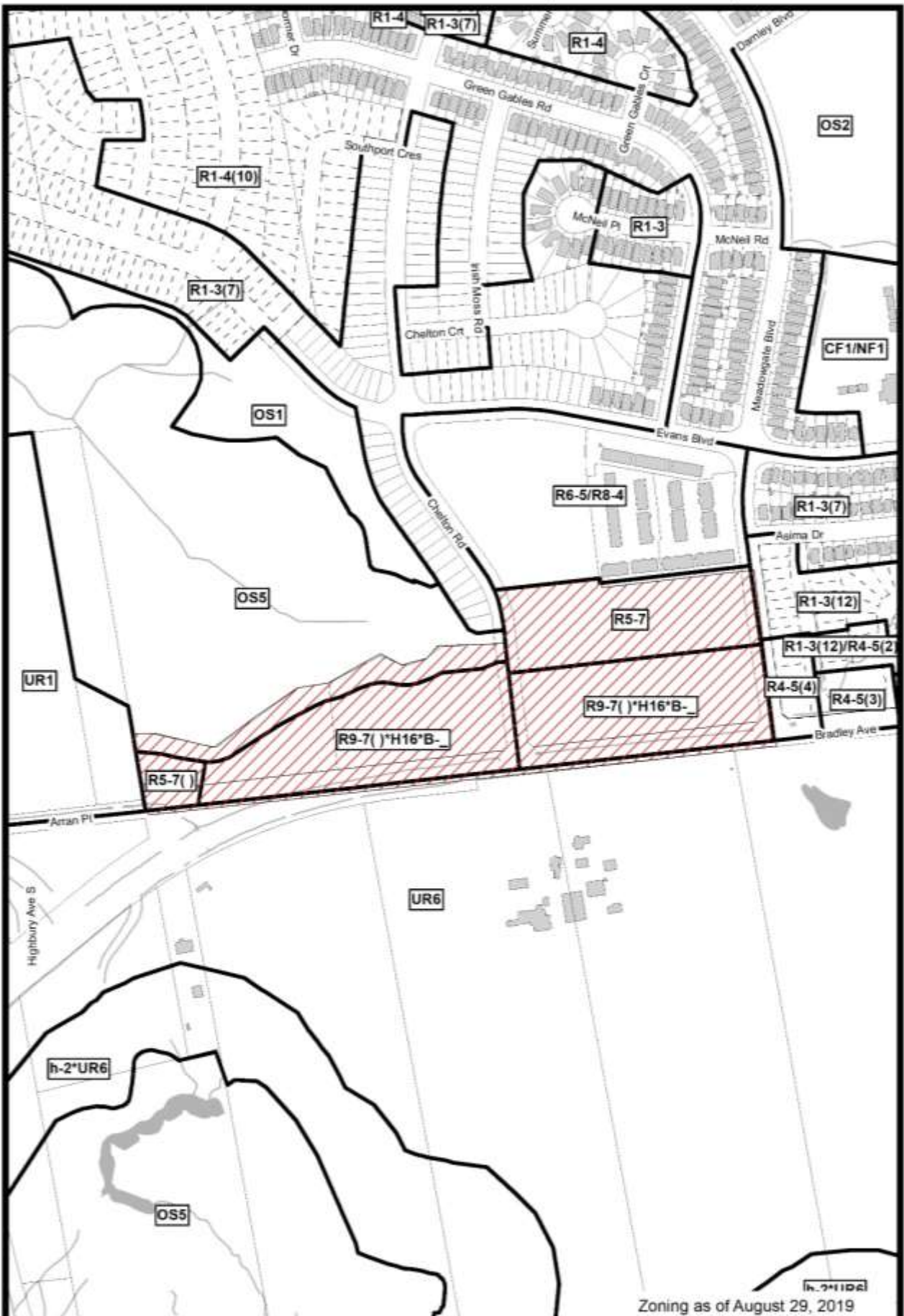
PASSED in Open Council on October 15, 2019

Ed Holder
Mayor


Catharine Saunders
City Clerk

First Reading – October 15, 2019
Second Reading – October 15, 2019
Third Reading – October 15, 2019

AMENDMENT TO SCHEDULE "A" (BY-LAW NO. Z.-1)




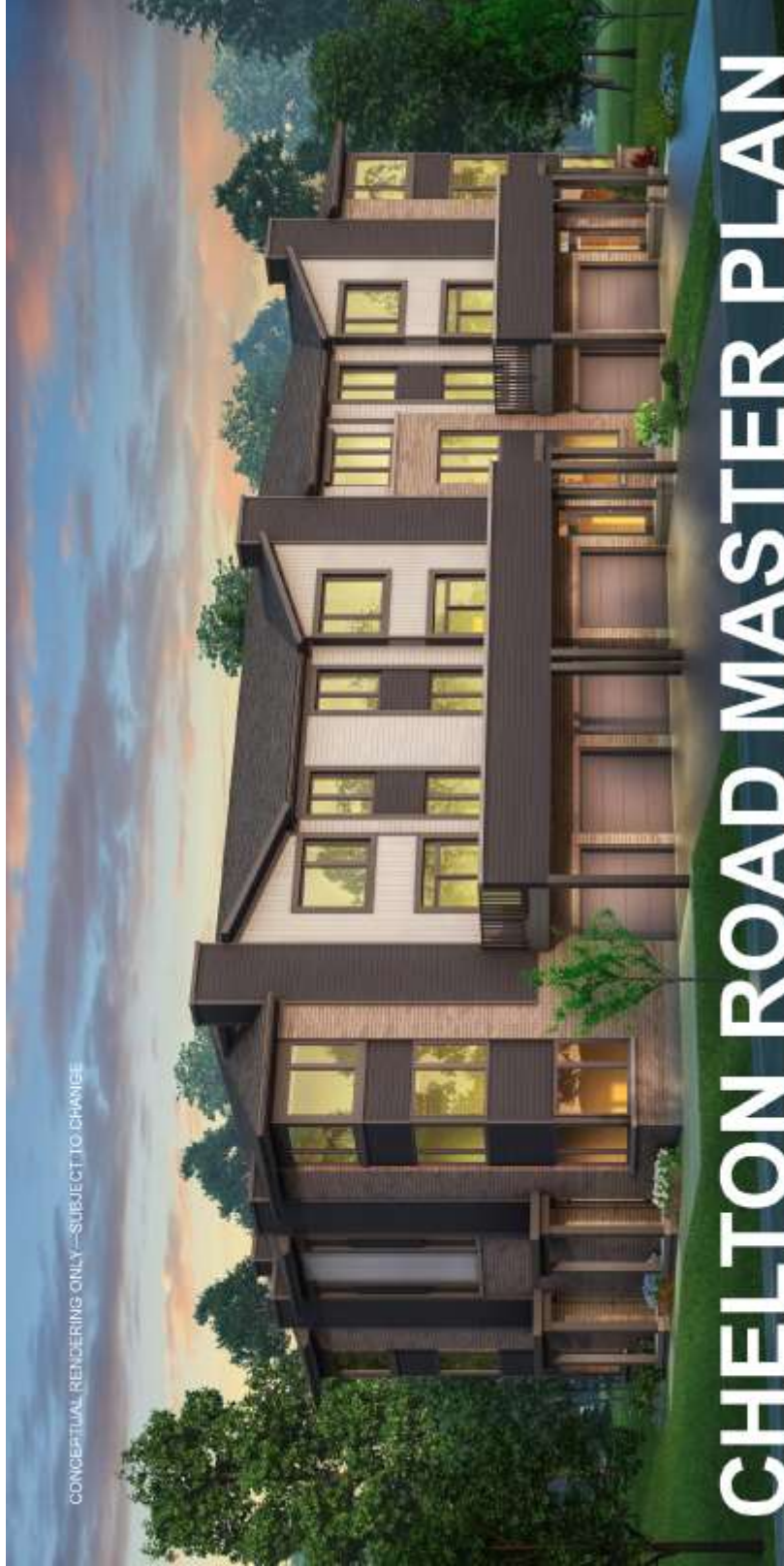
File Number: Z-9089
Planner: LM
Date Prepared: 2019/09/23
Technician: DM
By-Law No: Z.-1-

SUBJECT SITE 

1:5,000

0 25 50 100 150 200 Meters





CONCEPTUAL RENDERING ONLY—SUBJECT TO CHANGE

800, 805, & 810 CHELTON ROAD, LONDON
(39T-92020-E, BLOCKS 150, 151 & 152)

THE IRONSTONE BUILDING COMPANY INC.
FEBRUARY 2019 | FILE NO. 1861A





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2.0 VISION AND DESIGN PRINCIPLES	3
3.0 CONCEPTUAL MASTER PLAN	5
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6.0 COMPATIBILITY AND BUILDING HEIGHTS	10
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8.0 LANDSCAPE CONSIDERATIONS	14
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1 INTRODUCTION

MacNaughton Hermesen Britton Clarkson Planning Limited (MHBC Planning) has been retained by the Ironstone Building Company Inc. to seek approvals for an Official Plan Amendment and a Zoning By-law Amendment to permit the development of lands municipally known as 800, 805 and 810 Chelton Road, London. The properties (referred to herein as the 'subject lands') are located in southeast London, just east of the Bradley Avenue and Highbury Avenue North interchange. The subject lands will have access to the future extension of Chelton Road, with Chelton Road dividing the east parcel from the west parcel.

The purpose of this Master Plan is to establish key design principles and outline more specific design direction to guide the future development of the site plan. Among other matters, this Master Plan provides guidance as it relates to: site design; building placement; built form and compatibility.

THE POLICY FRAMEWORK AND CITY VISION

The development of the subject lands is subject to policies in both the City of London Official Plan and the in-effect policies of the London Plan. The London Plan establishes a general vision for the City as a whole to create an **exciting, exceptional and connected London by 2035**. This vision, along with general City Design and Urban Design Policies have been considered in the preparation of this Master Plan.

OUR APPROACH

In response to the City's vision, MHBC, on behalf of the Ironstone Building Company Inc. have prepared this Master Plan to illustrate how the proposed development should develop, while having regard for the design policy framework set out in the City's Official Plan and the London Plan.

Design staff at MHBC have prepared an Urban Design Dictionary to help visually explain 32 of the most common Urban Design terms. Illustrations from this Dictionary are found throughout this document to illustrate key concepts that are being addressed in the various sections of this Master Plan. A copy of the complete design dictionary is included at the end of this document.

We look forward to discussing this Master Plan with you in further detail.

Yours truly,

MHBC



Andrea Sinclair, MUDS, BES, MCIP, RPP
Associate and Urban Designer

LOCATION MAP

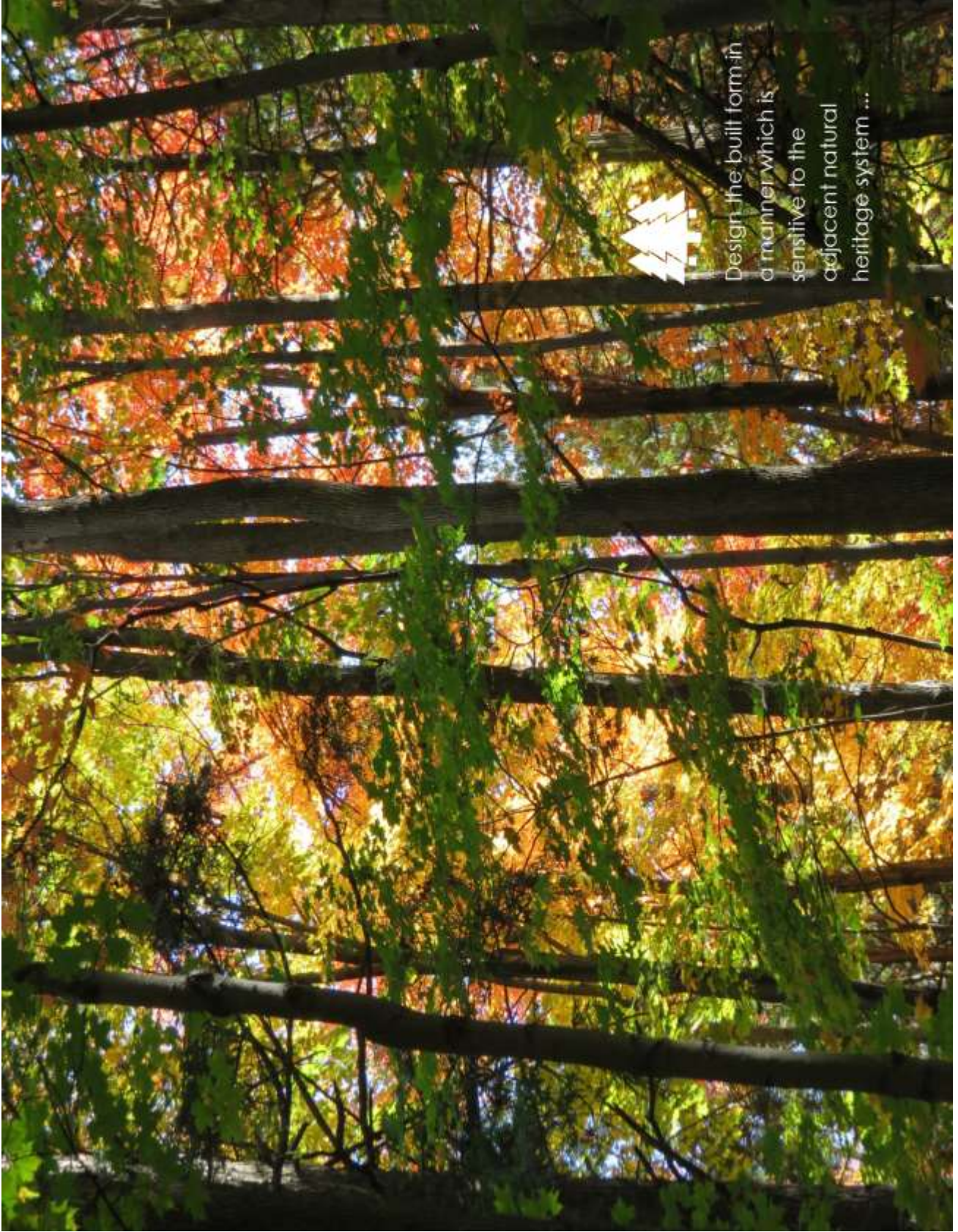


2

DESIGN PRINCIPLES

This Master Plan establishes a number of design principles to guide future development within the Chelton Road area. These principles will be used to guide the future site plan application. For the purpose of this document the design principles have been grouped into four main themes:

 <p>WALKABILITY</p>	 <p>AMENITY AREAS & OPEN SPACE</p>	 <p>CULTURAL & NATURAL HERITAGE</p>	 <p>VARIETY & PLACEMAKING</p>
<p>Develop a community that is walkable and supportive of future transit.</p> <p>High and medium density uses are appropriate along collector roads and potential future transit routes to encourage greater transit use.</p> <p>Develop a well-connected network and hierarchy of streets, paths and trails that enhance connectivity and safely accommodate various modes of active transportation.</p>	<p>Carefully design amenity spaces to enhance the identity of the area and create gathering points and spaces to promote pedestrian interaction.</p> <p>Establish an open space network of integrated and connected private/public spaces, including parks, trails and sidewalks.</p> <p>Establish a range of active and passive amenity and recreation spaces that will serve all age groups and physical abilities.</p>	<p>Design the built form in a manner which is sensitive to the adjacent natural heritage system and mitigates impacts on natural features, functions, and linkages through buffers, development setbacks and other established practices.</p> <p>Promote development which respects and reflects the physical and cultural identity and the heritage attributes of the area.</p>	<p>Provide a diverse mix of land uses and unit types to support a vibrant diverse neighbourhood. Ensure that the proposed development includes a range of housing types which will enhance the overall variety of the area.</p> <p>Plan for context appropriate height and massing with highest density buildings along Bradley Avenue.</p>



Design the built form in
a manner which is
sensitive to the
adjacent natural
heritage system ...

3 CONCEPTUAL MASTERPLAN

- Several buildings ranging from 3-6 storeys in height.
- Townhouse development directed to areas adjacent planned or existing lower rise residential (i.e. the north of the east parcel).
- Low and mid-rise apartments proposed to provide a strong street edge along Bradley Avenue.
- Parking areas located internal to the sites where possible. Consideration of a parking deck on the west parcel to reduce surface parking area.
- Private amenity areas proposed and connected by trails / walkways and the internal sidewalk system.
- Apartment buildings oriented to achieve passive solar benefits.
- Intersection of Bradley and Chelton well defined by building mass.
- Transition of heights with highest building heights generally located furthest from low-rise residential to the north.
- Protection of the natural heritage feature and incorporation of buffers.





CONCEPTUAL MASTER PLAN

4

SITE DESIGN

The following provides general direction related to key site design elements.

Locate buildings to frame intersections, in particular the intersection of Bradley Avenue and Chelton Road.

Design a **pedestrian oriented** community by designing outdoor common amenity spaces that are well connected via walkways, trails and/or sidewalks.

Use signage and landscape elements to delineate site entrances and to improve pedestrian **wayfinding** throughout the site.

Maintain and protect the adjacent natural feature and incorporate an appropriate buffer into the site design.

Locate any future parking decks on the west parcel, away from the public street.

Screen any parking areas visible from surrounding public street with landscaping, or where appropriate, fencing.

Support **sustainability** by orienting buildings for passive solar benefits where possible.

Locate lower density buildings, such as townhomes where adjacent to existing or proposed low-rise building forms.

Provide multiple pedestrian access points to the surrounding public road and public sidewalk system.

Incorporate landscape islands into large surface parking areas to visually soften these areas.

Where possible screen loading, garbage facilities and other service functions from the street and public view.

Explore opportunities for the City owned residual land along Bradley Avenue. A Community Garden would support City policy that promotes opportunities for community gardens on surplus lands where it is safe and suitable.



PEDESTRIAN-ORIENTED
An environment designed to ensure pedestrian safety and comfort for all ages and abilities



WAYFINDING
Design elements that help people to navigate through an area (e.g. signs, spatial markers)



SUSTAINABILITY
Developing with the goal of maintaining natural resources and reducing human impact on ecosystems



PEDESTRIAN CONNECTIVITY

5

BUILDING SITING

The following direction related to building siting shall be considered in the development of future site plan concepts for the subject lands.

BRADLEY AVENUE :

Locate buildings along Bradley Avenue to frame the street edge and to encourage an active pedestrian environment.

Orient buildings so the long edge of the building runs parallel to Bradley Avenue in order to create a **street wall**.

Provide multiple pedestrian connections to connect the proposed buildings to future sidewalks along Bradley Avenue.

Where possible locate buildings in a manner which maximizes passive solar orientation.



STREETWALL

The consistent edge formed by buildings fronting on a street

CHELTON & MEADOWGATE:

Provide multiple building entrances along Chelton Road and Meadowgate Boulevard to activate these streetscapes.

Orient buildings to ensure a defined street edge along both Chelton Road and Meadowgate Boulevard.

Provide multiple pedestrian connections from the surrounding public sidewalk to the proposed development.

Allow for a **transition of building height**, with greater heights directed to the south.



HEIGHT TRANSITION

The gradual change in height between buildings within a community

INTERNAL SITING:

For the east parcel, locate buildings around a central amenity area. Design buildings in accordance with the 'eyes on the park' concept.

For the west parcel consider the integration of a parking deck to minimize surface parking.

Step building heights down towards the natural heritage feature to maximize views of the feature.

Locate buildings to encourage pedestrian **circulation** between built form elements.



CIRCULATION

The movement patterns of people and vehicles through a site or community

6

COMPATIBILITY AND BUILDING HEIGHTS

The following direction related to compatibility and building heights shall be considered in the development of future site plan concepts for the subject lands.

A range of building heights are encouraged across the Master Plan area to ensure variety and to create visual interest.

The proposed concept generally transitions height and density with taller denser buildings located towards Bradley Avenue, and shorter, less dense buildings at the north end, in proximity to planned single detached and townhouse dwellings on surrounding lands.

Overall building heights should range from 2-6 storeys. Along Bradley the height should transition downward from the Bradley/Chelton intersection (with highest heights at the intersection).

Buildings with the greatest height (6 storeys) should be located on the west parcel to take advantage of views to the natural feature. The natural feature also helps to visually buffer taller buildings from low rise buildings located further north.

In laying out future site plans for the development, care should be taken to avoid creating front to back relationships

between planned residential to the north and the Master Plan Area. To support a reasonable level of privacy and compatibility, the front doors of new units/buildings should avoid facing onto the rear yards of existing homes.

Where possible parking areas should be located internal to the site and directed away from planned development to the north.

Larger buildings should incorporate a number of massing techniques to reduce the appearance of bulk.

The intensity of the proposed development shall be appropriate for the size of the lot such that it can accommodate such things as driveways, adequate parking in appropriate locations, landscaped open space, outdoor residential amenity area, adequate buffering and setbacks, and garbage storage areas.

The diagram on the following page illustrates potential building heights.





POTENTIAL BUILDING HEIGHTS

7

BUILT FORM

The following direction related to **built form** shall be considered in the development of a future site plan and building elevations.

The mass of proposed buildings, in particular low and mid-rise apartments should be broken up using a number of **massing** techniques including: projections and recessions; changes in building materials/ colours; and the incorporation of varying window sizes.

The massing of all proposed buildings should be designed to create a comfortable pedestrian environment, which will be further enhanced through the provision of private amenity space, including a connected walkway/trail system.

Building designs and architectural elements that add variety to rooflines are encouraged. All building façades should be **articulated**, with particular attention to building elevations visible from the surrounding **public realm**.

Articulation of larger buildings should be designed with elements that create a **rhythm** along the streetscape.

High quality materials including a large amount of glass will be incorporated into the building facades, resulting in an attractive modern design. Repetition of lines and windows through both vertical and horizontal articulations and setbacks will be used to further break up building mass.

Outdoor living spaces of individual units should be provided in the form of patios, porches or balconies.

Building corners that are highly visible from the public realm (such as the intersection of Chelton and Bradley) should have a high degree of architectural detail.

Design the northern townhouses (east parcel) to be compatible in massing and architectural style with the planned townhouses on the abutting property to the north.



BUILT FORM
The physical shape of developments including buildings and structures



MASSING
The effect of modifying the height and bulk of the form of a building or group of buildings



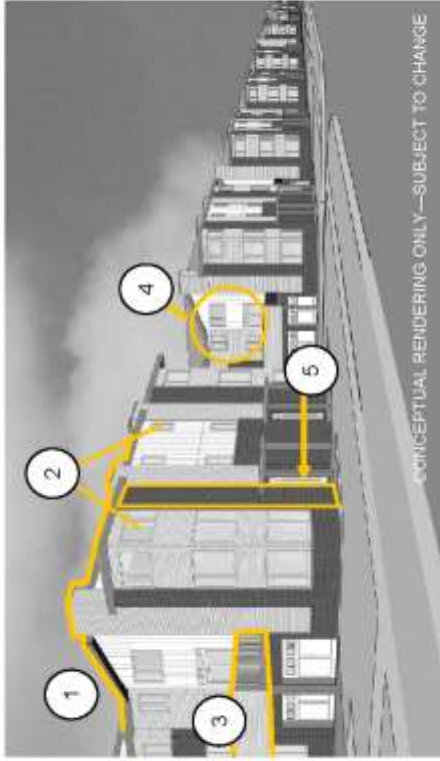
ARTICULATION
The layout or pattern of building elements (e.g. windows, porches) that defines space and affects the facade



PUBLIC REALM
Public spaces between buildings including boulevards and parks where pedestrian activities occur



RHYTHM AND PATTERN
The repetition of elements such as materials, details, styles and shapes that provide visual interest



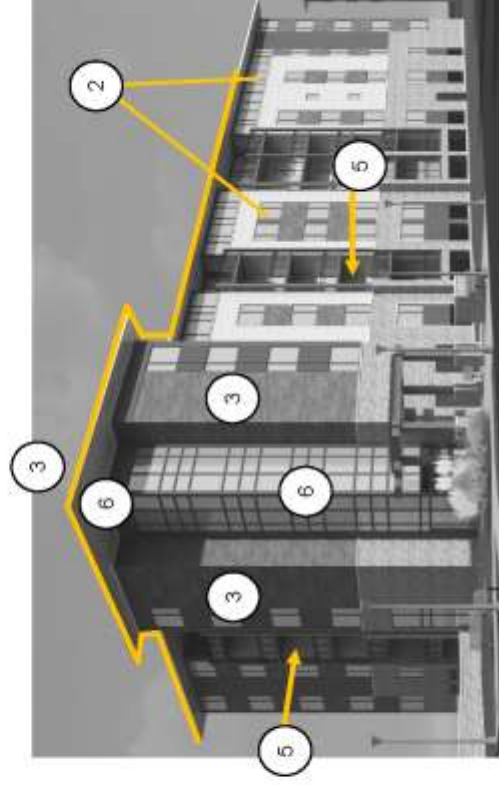
MASSING TECHNIQUES

1. VARIATION IN ROOFLINE.
2. VARYING WINDOW SIZES.
3. PROJECTION.
4. CHANGES IN BUILDING MATERIALS/COLOURS
5. RECESSION
6. CORNER ARTICULATION



"A diversity of materials should be used in the design of buildings to visually break up massing, reduce visual bulk and add interest to the building design. Materials should be selected for their scale, texture, quality, durability, and consistency within their context."

THE LONDON PLAN



CONCEPTUAL RENDERING ONLY—SUBJECT TO CHANGE

8

LANDSCAPE CONSIDERATIONS

Detailed landscape plans will be required as part of a future site plan application. The following provides general guidance for landscape priority areas:

Provide high quality landscape materials that, when combined with the built form, contributes positively to the community **character**.

Create defined street edges through a combination of building mass and landscaping.

Any parking areas abutting public streets should be visually screened with landscape materials.

Provide multiple pedestrian connections from the subject lands to the surrounding public sidewalk system. Consider **desire lines** when determining final locations for pedestrian walkways/trails.

Provide private amenity spaces connected by trails/walkways.

Provide visual interest along Bradley Avenue through landscape features designed to provide a visually attractive sightline into the subject lands.

Explore the potential for community gardens in the City owned parcel south of the Master Plan area. Provide pedestrian connections from the western parcel to the potential community garden site.

Future landscape plans will consider the use of drought and salt tolerant species and trees will be planted to provide shade opportunities. This will contribute to the overall **sustainability** of the project.

Future landscape plans should also consider Crime Prevention Through Environmental Design principles, including using landscaping to clearly demarcate private from public space; ensuring that areas are well lit; avoiding landscape materials that decrease visibility; and by designing sites and landscaping in a manner which avoids entrapment areas.



CHARACTER

The look and feel of an area, including activities that occur there



DESIRE LINE

Shortest or most easily navigated route marked by the erosion of the ground caused by human traffic



SUSTAINABILITY

Developing with the goal of maintaining natural resources and reducing human impact on ecosystems



LANDSCAPE PRIORITIES



8

HERITAGE CONSIDERATIONS

The subject lands are not designated under the Ontario Heritage Act, nor are there any designated properties adjacent to the subject lands. Notwithstanding, a Heritage Impact Statement has been completed given the adjacency of the subject lands to 1629 & 1635 Bradley, which are listed on the municipal register (City of London's Inventory of Heritage) as non-designated properties.

The HIA concluded that the proposed development is consistent with Provincial Policy and Section 13.2.3.1 of the City of London Official Plan. There are no lands that are contiguous, or that are directly opposite (separated only by a laneway or municipal road) that are protected under Parts IV, V or VI of the Ontario Heritage Act. 1629 & 1635 Bradley Avenue are not protected under the Ontario Heritage Act, therefore are not considered protected heritage properties as per the PPS. Notwithstanding, removal of these properties is not contemplated as a result of the proposed development. There are no design restraints to the subject lands as a result of heritage considerations. Further no specific design direction is recommended as a result of the Heritage analysis.

10

CONCLUSION

The proposed Master Plan for the subject lands is appropriate and represents good planning and urban design for the following reasons:

- The proposed development is consistent with the Provincial Policy Statement and supports intensification and a range and mix of housing types.
- The proposal conforms to the relevant policies of The London Plan and the City of London Official Plan.
- The proposal has been designed around design principles that were established to ensure a high quality built environment.
- The proposed development appropriately integrates open space, and assists in the completion of a neighbourhood trail.
- The proposed development results in a mix of housing types.
- The proposed development will result in increased densities and will help to support future transit routes.
- The Maser Plan provides sufficient design direction for future site plans, while allowing some flexibility in site layout, building footprints and building orientation.

THE BELOW IMAGES ARE INTENDED TO PROVIDE A GENERAL IDEA OF THE PROPOSED BUILT FORM FOR THE SITE. BUILDING ELEVATIONS AND EXTERIOR DESIGN WILL BE FORMALIZED THROUGH A FUTURE SITE PLAN PROCESS. GENERAL DIRECTION RELATED TO BUILT FORM CONTAINED IN THIS MASTER PLAN WILL INFORM FINAL BUILDING ELEVATIONS.

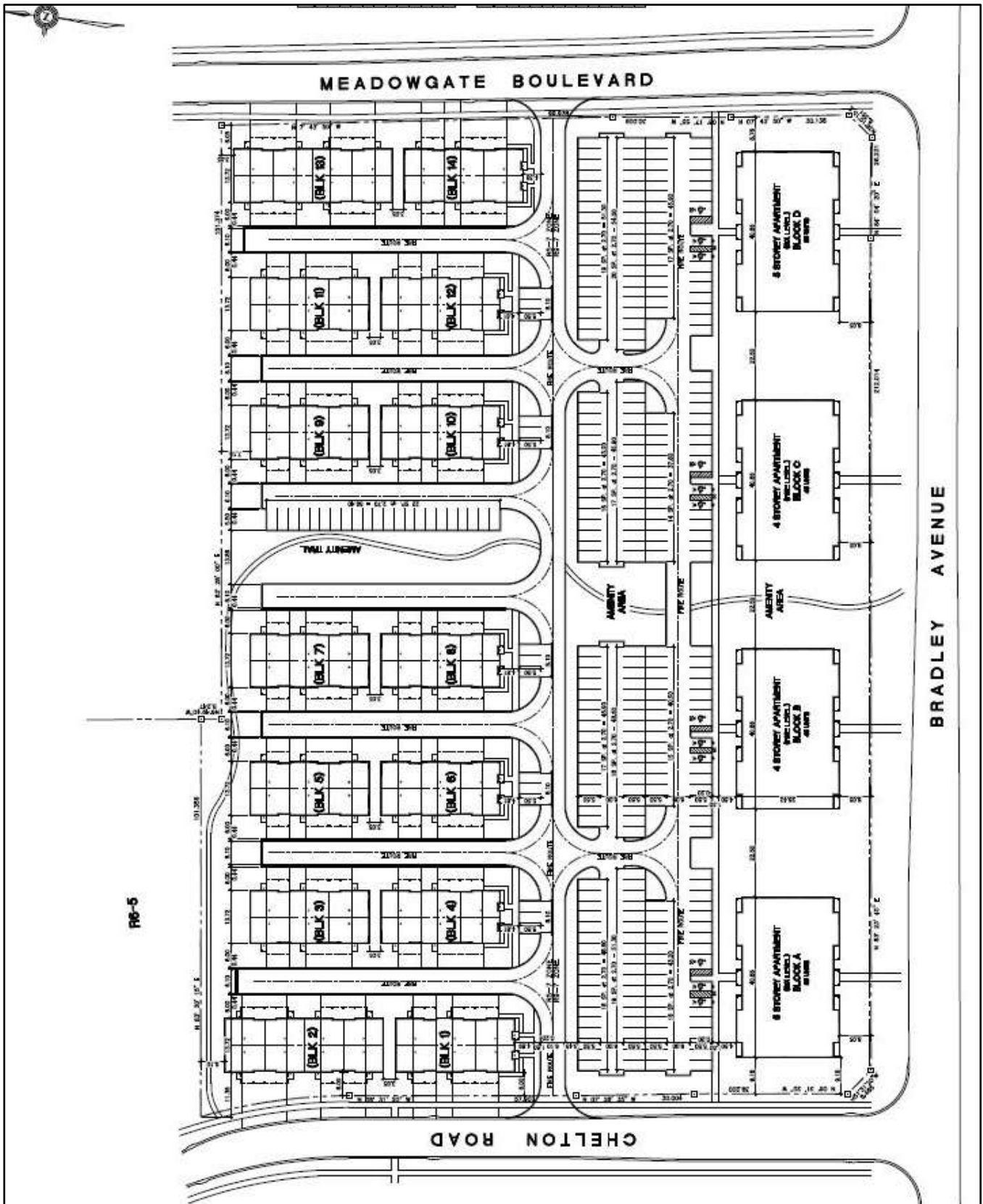


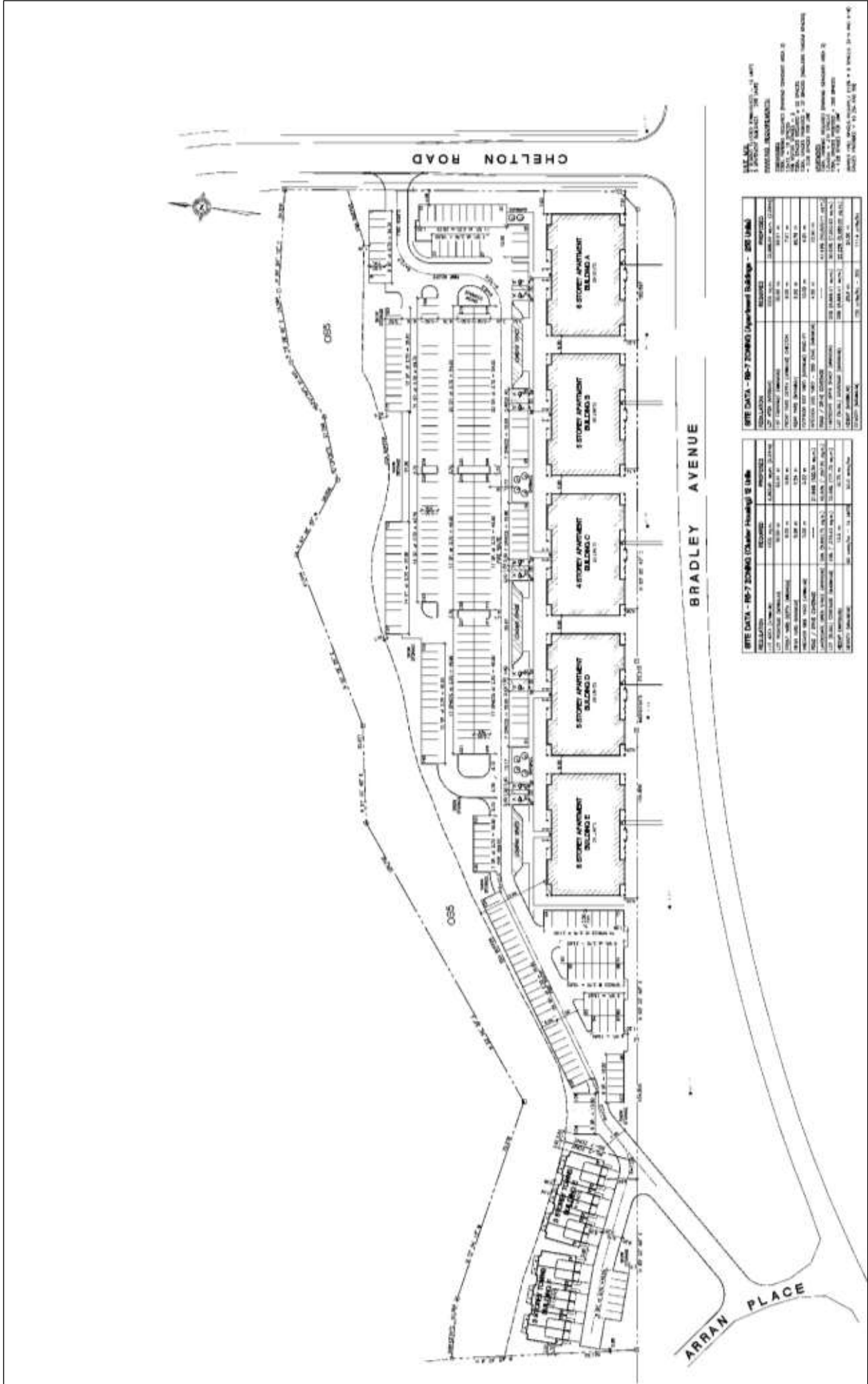
CHELTON ROAD | MASTER PLAN

Implementation of this Master Plan will ensure a high quality, well designed development featuring a range of building types and unit sizes; high quality landscape and amenity areas; and a well connected pedestrian network.



Schedule 2





SCALE: 1:500 (AS SHOWN)
 DATE: 10/10/17
 PROJECT: RESIDENTIAL DEVELOPMENT
 SHEET: 1 OF 2
 DRAWN: [Name]
 CHECKED: [Name]
 APPROVED: [Name]

SITE DATA - R5-7 ZONING (Apartment Buildings - 6S Units)

REQUIREMENT	REQUIRED	PROPOSED
MINIMUM SETBACK	10.0 M	10.0 M
MINIMUM FRONT SETBACK	10.0 M	10.0 M
MINIMUM SIDE SETBACK	5.0 M	5.0 M
MINIMUM REAR SETBACK	5.0 M	5.0 M
MINIMUM FRONT YARD GREENING	2.0 M	2.0 M
MINIMUM SIDE YARD GREENING	1.0 M	1.0 M
MINIMUM REAR YARD GREENING	1.0 M	1.0 M
MINIMUM FRONT YARD TREE CANOPY	10.0 M	10.0 M
MINIMUM SIDE YARD TREE CANOPY	5.0 M	5.0 M
MINIMUM REAR YARD TREE CANOPY	5.0 M	5.0 M
MINIMUM FRONT YARD TREE CANOPY	10.0 M	10.0 M
MINIMUM SIDE YARD TREE CANOPY	5.0 M	5.0 M
MINIMUM REAR YARD TREE CANOPY	5.0 M	5.0 M
MINIMUM FRONT YARD TREE CANOPY	10.0 M	10.0 M
MINIMUM SIDE YARD TREE CANOPY	5.0 M	5.0 M
MINIMUM REAR YARD TREE CANOPY	5.0 M	5.0 M

SITE DATA - R5-7 ZONING (Apartment Buildings - 6S Units)

REQUIREMENT	REQUIRED	PROPOSED
MINIMUM SETBACK	10.0 M	10.0 M
MINIMUM FRONT SETBACK	10.0 M	10.0 M
MINIMUM SIDE SETBACK	5.0 M	5.0 M
MINIMUM REAR SETBACK	5.0 M	5.0 M
MINIMUM FRONT YARD GREENING	2.0 M	2.0 M
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MINIMUM FRONT YARD TREE CANOPY	10.0 M	10.0 M
MINIMUM SIDE YARD TREE CANOPY	5.0 M	5.0 M
MINIMUM REAR YARD TREE CANOPY	5.0 M	5.0 M

SITE DATA - R5-7 ZONING (Apartment Buildings - 6S Units)

REQUIREMENT	REQUIRED	PROPOSED
MINIMUM SETBACK	10.0 M	10.0 M
MINIMUM FRONT SETBACK	10.0 M	10.0 M
MINIMUM SIDE SETBACK	5.0 M	5.0 M
MINIMUM REAR SETBACK	5.0 M	5.0 M
MINIMUM FRONT YARD GREENING	2.0 M	2.0 M
MINIMUM SIDE YARD GREENING	1.0 M	1.0 M
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MINIMUM FRONT YARD TREE CANOPY	10.0 M	10.0 M
MINIMUM SIDE YARD TREE CANOPY	5.0 M	5.0 M
MINIMUM REAR YARD TREE CANOPY	5.0 M	5.0 M

SITE DATA - R5-7 ZONING (Apartment Buildings - 6S Units)

REQUIREMENT	REQUIRED	PROPOSED
MINIMUM SETBACK	10.0 M	10.0 M
MINIMUM FRONT SETBACK	10.0 M	10.0 M
MINIMUM SIDE SETBACK	5.0 M	5.0 M
MINIMUM REAR SETBACK	5.0 M	5.0 M
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MINIMUM REAR YARD TREE CANOPY	5.0 M	5.0 M

SITE DATA - R5-7 ZONING (Apartment Buildings - 6S Units)

REQUIREMENT	REQUIRED	PROPOSED
MINIMUM SETBACK	10.0 M	10.0 M
MINIMUM FRONT SETBACK	10.0 M	10.0 M
MINIMUM SIDE SETBACK	5.0 M	5.0 M
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MINIMUM SIDE YARD TREE CANOPY	5.0 M	5.0 M
MINIMUM REAR YARD TREE CANOPY	5.0 M	5.0 M

SITE DATA - R5-7 ZONING (Apartment Buildings - 6S Units)

REQUIREMENT	REQUIRED	PROPOSED
MINIMUM SETBACK	10.0 M	10.0 M
MINIMUM FRONT SETBACK	10.0 M	10.0 M
MINIMUM SIDE SETBACK	5.0 M	5.0 M
MINIMUM REAR SETBACK	5.0 M	5.0 M
MINIMUM FRONT YARD GREENING	2.0 M	2.0 M
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MINIMUM SIDE YARD TREE CANOPY	5.0 M	5.0 M
MINIMUM REAR YARD TREE CANOPY	5.0 M	5.0 M

Appendix B – Public Engagement

Community Engagement

Public liaison: On July 16, 2019, Notice of Application was sent to 120 property owners in the surrounding area. Four (4) Planning Application signs were erected on the site, and notice was also posted on the City of London’s website. Notice of Public Meeting was published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on September 19, 2019.

Responses: 2 replies received.

Nature of Liaison: Possible amendment to Zoning By-law Z.-1 to change the zoning from a Community Shopping Area CSA3 Zone, an Associated Shopping Area Commercial (ASA1/ASA2/ASA3) Zone, and a holding Residential R6/R9 (h-54 R6-5/R9-3 H20) Zone to a Residential R5 (R5-7) Zone to permit cluster townhouses and cluster stacked townhouses up to 60 units per hectare and maximum height of 12 metres; a Residential R5 Special Provision (R5-7()) Zone to permit cluster townhouses and cluster stacked townhouses up to 60 units per hectare and maximum height of 12 metres, together with special provisions to permit an interior side yard depth of 2.0 metres minimum, and 0.0 metre yard setback adjacent an OS5 Zone; a Residential R9 Special Provision (R9-7()) Zone to permit apartment buildings, lodging house class 2, senior citizens apartment buildings, handicapped persons apartment buildings, and continuum-of-care facilities up to 150 units per hectare and maximum height of 25.0 metres, together with special provisions to permit a front yard depth 6.0 metres minimum, exterior side yard depth 3.0 metres minimum, and 0.0 metre yard setback adjacent an OS5 Zone; and to an Open Space OS5 Zone to permit conservation lands, conservation works, passive recreation uses which include hiking trails and multi-use pathways, and managed woodlots.

Responses: A summary of the comments received include the following:

- No shopping or restaurants in the area so current zoning should be maintained.
- Higher traffic volumes on already congested 2-lane Bradley Avenue.

Response to Notice of Application and Publication in “The Londoner”

Telephone	Written
Michelle – Inquiry for further information. Caller didn’t leave a last name or address.	Ron Johnston – #234 - 3320 Meadowgate Boulevard

Hello...

I would like to **object** to the rezoning applications for 805 Chelton Road and 800 Chelton Road.

The current zoning for Commercial Shopping area and Associated Shopping area Commercial seems to be the most appropriate use for these properties since there are currently no shopping and/ or restaurant establishments in the immediate area.

To change the zoning to allow even more high density residential would be a big mistake in my opinion, since the two lane Bradley avenue is barely able to handle high traffic volume at peak times as it is. Adding hundreds or thousand more vehicles to the area would only compound the existing peak time congestion.

Thank you.

Ron Johnston

234 - 3320 Meadowgate Blvd

London N6M 0A7

Agency/Departmental Comments:

1. Upper Thames River Conservation Authority – August 22, 2019

The subject lands are regulated by the UTRCA and a Section 28 permit application will be required. The UTRCA has no objections to this application as the proposed zone boundaries appear to align with the development setback limits identified in the technical reports; however it should be noted that the UTRCA does not permit any development within the established buffer zones, including trails/pathways. The UTRCA will require further information through the City of London Site Plan Application and UTRCA Section 28 permit application processes. Please ensure these applications include the following:

- Detailed Site Plan drawing(s) identifying the extent of the proposed development in relation to development limit and buffers determined by the technical reports;
- Re-location of proposed trails/pathways entirely outside of the buffers established by the technical reports;
- Identification of snow storage areas that do not result in encroachment into the buffers established by the technical reports;
- Stormwater Management plan/report including identification and description of LID features should any be proposed;
- Grading Plan; and,
- Sediment and Erosion Control Plan, ensuring protection of the both the buffer areas established by the technical reports.

2. London Hydro – July 23, 2019

Servicing the above proposal should present no foreseeable problems. Any new and/or relocation of existing infrastructure will be at the applicant's expense. Above-grade transformation is required. Note: A blanket easement will be required. Transformation lead times are minimum 16 weeks. Contact Engineering Dept. to confirm requirements & availability.

London Hydro has no objection to this proposal or possible official plan and/or zoning amendment. However, London Hydro will require a blanket easement.

3. Ministry of Transportation – August 9, 2019

The proposed Master Plan specifies the building of several apartment/townhouse condos on the north side of Bradley Ave. Could you please provide a timeline as to when these dwellings are scheduled to be built. I understand the proposed plan is only in the preliminary stages, but any information would be greatly appreciated.

4. City of London - Environmental and Engineering Services – August 23, 2019

The applicant is required to submit a sanitary capacity analysis for the increased densities being sought. The limits of the capacity report should extend to the 600mm trunk sanitary on Cudmore Crescent, and should include a new design sheet showing these three blocks only at 230l/cap/day

The following items are to be considered during the site plan approval stage:

Transportation:

- Construction of a left turn lane on Bradley Avenue will be required to provide for access from Arran Place.
- Alignment of proposed access to Arran Place will need to be revised to comply with the City's Access Management Guidelines.

- Detailed comments regarding access location, design, and external works will be made through the site plan processes.

Water:

- The proposed westerly development parcel would require a looped water service connection due to the number of units.
- The looped servicing triggers the requirement for DCVAs. It is suggested that the applicant's engineer investigate alternative water servicing options to avoid the requirement of the DCVA.
- Ownership of the apartments and townhomes is unclear, but all independently owned parcels will require independent servicing in order to avoid the creation of a non-municipal regulated drinking water system. This includes future parcels created by the formation of a condo corp.
- Water is available via the 300mm PVC watermain on Chelton Road. It is noted that the parcel has frontage along Bradley Ave where a 400mm PVC watermain exists.

Stormwater:

- SWM servicing design of this site should be in accordance with Summerside Subdivision Phase 9 (Plan 33M-528) and associated Functional SWM Report. Changes in catchment area size or C value shown on as-con 20930 will trigger the need for on-site SWM controls. The design of on-site SWM controls shall include but not be limited to, required storage volume calculations, flow restrictor sizing, etc.
- The site is within the UTRCA regulated area and therefore approval/permit will be required.
- For the proposed 1102 parking spaces, the owner shall be required to have a consulting Professional Engineer addressing water quality to the standards of the MECP and to the satisfaction of the City Engineer. Applicable options could include, but not be limited to the use of oil/grit separators, catchbasin hoods, bioswales, etc. along with the required inspection/sampling maintenance hole.
- Any proposed LID implementation should be supported by a Geotechnical Report and/or hydrogeological investigations prepared with focus on the type of soil, its infiltration rate, hydraulic conductivity (under field saturated conditions), and seasonal high ground water elevation. The report(s) should include geotechnical and hydrogeological recommendations of any preferred/suitable LID solution along with rationale and conclusions about the following points:
 - Description of relevant site features, including topography and surface water drainage, regional overburden geology, regional hydrogeology, and proximity to nearby natural heritage features (e.g., stream, ponds, wetlands, woodlots, etc.).
 - Advancement of boreholes at the site, including the installation of a minimum of one monitoring well.
 - Infiltration measurements from areas within the Site using standards infiltration/percolation testing methods (e.g., Guelph Permeameter Test, Double-ring infiltrometer test, etc.).
 - Description of the measured relevant site hydrogeological information, including aquifer properties (e.g., hydraulic conductivity) and static groundwater levels.
 - Establishing seasonal fluctuations in water levels, including capturing a representative seasonal high elevation. Note that the use of borehole and/or test pit observations to establish both static water levels and potential seasonal fluctuations is not standard practice.
- Additional SWM related comments will be provided upon future review of this site.

5. City of London – Ecologist Planner – September 19, 2019

- 1) The updated site plan on the airphoto has placed parking spaces inside of a native landscaped/naturalization area that was required to be implemented as per previous discussion (see Figure 6 and Recommendation #1). This is unacceptable and this parking is to be completely removed from the 10m additional setback area as per previous requirements and acceptance by the proponent.
- 2) The EIS is to clearly identify the need for a detailed native restoration plan for all buffer/naturalization areas (Recommendation #2 & #13)
- 3) Please change all references in the EIS from 'walkway' to pathway.
- 4) There is zero mention of the Snow Storage Areas located all along the buffer to the development. Given the large amount of parking backing onto the entire length of the features, addition details are needed for how this should be managed to protect the features and its functions. These areas should all be designed in a similar fashion to the area identified in Recommendation #1 that are in this case species tolerant of the conditions. There should not simply be manicured lawn. There should be a naturalization component to these areas and designed in a way so that the heavy salt laden flows from these areas will sheet back to the parking areas and not the PSW/ ESA. A discussions regarding these areas and additional recommendations in the EIS are needed to address this.
- 5) Identify that the pathway to be located as close to the development limits as possible to minimize infringement on the buffer.
- 6) Recommendation #7 is to identify that the erosion control measures are to be designed/installed to the City's satisfaction as well.
- 7) Given the expectation of a large amount of lighting that will be placed within the parking areas, a recommendation to require the be designed to not impact the adjacent natural communities (i.e directed downwards and away from the ESA/ PSW, no spillage into adjacent areas, bird friendly designs etc.).
- 8) Recommendation for a detailed monitoring plan is required.

I note that none of these should hold up the process, but they need to be fully addressed in the Final EIS.

Appendix C – Policy Context

The following policy and regulatory documents were considered in their entirety as part of the evaluation of this proposal. The most relevant policies, by-laws, and legislation are identified as follows:

Provincial Policy Statement, 2014

The proposal must be consistent with Provincial Policy Statement (PPS) policies and objectives aimed at:

1. Building Strong Healthy Communities;
2. Wise Use and Management of Resources; and,
3. Protecting Public Health and Safety.

The PPS contains polices regarding the importance of promoting efficient development and land use patterns, as well as accommodating an appropriate range and mix of land uses, housing types, and densities to meet projected needs of current and future residents; and minimizes land consumption and servicing costs (Sections 1.1 and 1.4). The application would result in more medium density residential housing types (townhouses and apartments) within a predominantly low density residential community composed of single detached homes, stacked townhouses and street townhouses. There is a significant amount of leasable commercial/retail space and vacant commercially zoned lands along Commissioners Road East available to serve the needs of current and future residents. Therefore, it is felt the proposed zoning change meets the policies by

maintaining an appropriate range and mix of land uses. This mix of land uses in the area is characterized by a transition from suburban development north of Bradley Avenue to active agricultural uses to the south. The long term intended use for these lands is light industrial, and future planning must take into consideration compatibility concerns and potential nuisance impacts on existing residential uses and development lands currently being built-out.

The policies for Settlement Areas require that land use patterns be based on densities and mix of uses that efficiently use land and resources; are appropriate for, and efficiently use, infrastructure and public service facilities which are planned or available; minimize negative impacts to air quality and climate change; promote energy efficiency; support active transportation; and are transit supportive where transit is planned, exists or may be developed (Section 1.1.3.2). These lands are immediately adjacent to existing and developing built-up areas to the north and east. Development will efficiently utilize services and infrastructure that was intended for future development of these lands, including wastewater and stormwater management facilities. The site is in close proximity to public parks, open spaces, schools, and community facilities. The proposed site layout integrates well with the surrounding neighbourhood streets and sidewalks to promote active transportation. The Summerside subdivision is also served by London Transit bus routes with transit stops located within walking distance just north of the subject lands along Meadowgate Boulevard and Darnly Boulevard.

Transportation policies promote a land use pattern, density and mix of uses that minimize the length and number of vehicle trips and support current and future use of transit and active transportation (Section 1.6.7.4). Planning Authorities shall also support energy conservation and efficiency through land use and development patterns which, among other matters, promotes design and orientation which maximizes opportunities for renewable energy systems (Section 1.8.1). The proposed development is supportive of transit service and is located in close proximity to existing and planned walking and cycling pathways. The close proximity to elementary schools and neighbourhood parks will help to encourage active transportation modes such as walking and cycling. The site concept plan will incorporate an internal pathway system that provides pedestrian connections to Chelton Road and the surrounding neighbourhood. Promoting energy efficiency through site planning and building design is achieved by the strong north-south orientation of the proposed townhouse and apartment blocks optimizing their exposure to passive solar energy capture.

Natural heritage features and functions shall be protected for the long term (Section 2.1.1). The adjacent wetland and woodland to the north of Block 151 and 152 will be protected by an open space buffer (within the proposed OS5 Zone) as recommended in the scoped Environmental Impact Study undertaken in conjunction with the subject application. Further revisions to the site plan will be required including removal of a row of parking spaces that are shown within a portion of the 10 metre buffer. Information on the locations for snow storage areas, and a detailed re-naturalization plan will also be required as part of the Site Plan Approval process. The proposed development is outside of any natural hazards and there are no known human-made hazards. The proposed development is not impacted by a flooding hazard; however, the subject lands are identified within an area of interference surrounding a wetland and within the regulation limits of the UTRCA who have indicated that a Section 28 Permit will be required. Cultural heritage and archaeological resources have previously been dealt with through the subdivision approval process. Based on our review, Development Services staff are satisfied that the recommended zoning by-law amendment is consistent with the Provincial Policy Statement.

The London Plan

The Our Strategy, City Building and Design, Place Types, and Our Tools policies in the London Plan have been reviewed and consideration given to how the proposed zoning by-law amendment contributes to achieving those policy objectives, including the following specific policies:

Our Strategy

Key Direction #5 – Build a mixed-use compact city

- 4. Plan for infill and intensification of various types and forms to take advantage of existing services and facilities and to reduce our need to grow outward.*
- 5. Ensure a mix of housing types within our neighbourhoods so that they are complete and support aging in place.*

Key Direction #6 – Place a new emphasis on creating attractive mobility choices

- 1. Create active mobility choices such as walking, cycling, and transit to support safe, affordable, and healthy communities.*
- 6. Dependent upon context, require, promote, and encourage transit-oriented development forms.*

Key Direction #7 – Build strong, healthy and attractive neighbourhoods for everyone

- 1. Plan for healthy neighbourhoods that promote active living, provide healthy housing options, offer social connectedness, afford safe environments, and supply well distributed health services.*
- 2. Design complete neighbourhoods by meeting the needs of people of all ages, incomes and abilities, allowing for aging in place and accessibility to amenities, facilities and services.*
- 3. Implement “placemaking” by promoting neighbourhood design that creates safe, diverse, walkable, healthy, and connected communities, creating a sense of place and character.*

Key Direction #8 – Making wise planning decisions

- 9. Ensure new development is a good fit within the context of an existing neighbourhood.*

City Building and Design Policies

*197_ The built form will be designed to have a sense of place and character consistent with the planned vision of the place type, by using such things as topography, street patterns, lotting patterns, streetscapes, public spaces, landscapes, site layout, buildings, materials and cultural heritage. **

The Chelton Road Master Plan document which accompanied the application submission establishes the key design principles and provides specific direction to guide future built form. Considerations of the built form include applying techniques such as projections and recessions, a variety of building materials/colours, and incorporation of varying window sizes to break up the massing of low and mid-rise apartment blocks. Building massing should be designed to create a comfortable pedestrian environment which will be further enhanced through the provision of private amenity space including a connected walkway/trail system. Architectural elements that add variety to rooflines are encouraged. Building corners that are highly visible from the public realm (such as the intersections of Chelton Road, Meadowgate Boulevard, and Bradley Avenue) should have a high degree of architectural detail. Townhouse designs on the northerly half of Block 150 are to be compatible in massing and architectural style with the planned townhouses on the abutting lands to the north.

216_ Street networks, block orientation, lot sizes, and building orientation should be designed to take advantage of passive solar energy while

*ensuring that active mobility and other design criteria of this chapter are satisfied.**

*217_ Neighbourhood street networks and block sizes will be designed to ensure connectivity and support active mobility including cycling, walking, blading, boarding and transit. Infrastructure and amenities to support these modes of mobility will be incorporated.**

The streets, blocks, site layout, and building placement all work together to create a strong north-south orientation optimizing exposure to passive solar energy capture. An internal pedestrian and cycling connection to the surrounding street network encouraging active mobility choices is to be provided, as illustrated on the site concept plans.

256_ Buildings should be sited so that they maintain and reinforce the prevailing street wall or street line of existing buildings. Where a streetscape has not been built out, buildings should be sited with regard for the planned street wall or street line.

*259_ Buildings should be sited with minimal setbacks from public rights-of-way and public spaces to create a street wall/edge and establish a sense of enclosure and comfortable pedestrian environment.**

The Chelton Road Master Plan and the proposed site concept plans show apartment buildings oriented so that the long edge of the building runs parallel with Bradley Avenue in order to create a street wall. The recommended zoning includes minimum and maximum building setback regulations to reinforce this street wall/edge spatial relationship to the public right-of-way, and establishes a sense of enclosure with the public realm.

268_ Sites shall be designed to provide a direct, comfortable and safe connection from the principle building entrance to the public sidewalk.

The objective for townhouses is to provide multiple building entrances along Chelton Road and Meadowgate Boulevard to activate these streetscapes, orient buildings to ensure a defined building edge along both Chelton Road and Meadowgate Boulevard, provide multiple pedestrian connections from the public sidewalk, and allow for a transition of building height as you proceed south towards Bradley Avenue.

*272_ The impact of parking facilities on the public realm will be minimized by strategically locating and screening these parking areas. Surface parking should be located in the rear yard or interior side yard.**

During the site planning consultation City staff recommended reducing the exterior yard setbacks in order to locate the apartment buildings closer to Chelton Road and Meadowgate Boulevard in line with or ahead of the proposed parking areas adjacent to these street frontages. Other suggestions included providing a combination of low masonry walls (max.0.75 metres) and landscaping along Chelton Road and Meadowgate Boulevard where parking is visible to the street in order to screen this function from the street.

495_ Providing accessible and affordable housing options for all Londoners is an important element of building a prosperous city. Quality housing is a necessary component of a city that people want to live and invest in. Housing choice is influenced by location, type, size, tenure, and accessibility. Affordability and housing options are provided by establishing variety in these factors.

Multiple-unit residential forms of development as proposed contributes to affordable housing options by bringing more diversity and choice, and by building up the inventory of townhouse and apartment units in this area of the City.

Place Type Policies

Map 1 – Place Types* shows Block 150 as being within a Shopping Area Place Type and Blocks 151 and 152 within the Neighbourhoods, and partially within the Green Space Place Types.

877(1) A broad range of retail, service, office, entertainment recreational, educational, institutional, and residential uses may be permitted within the Shopping Area Place Type.

878(1) It is the intent of this Plan to allow for the more intense and efficient use of Shopping Area sites through redevelopment, expansion, and the introduction of residential development.

Residential uses may be permitted in accordance with the Place Type policies as they apply to Block 150. The requested zoning amendment to facilitate future residential development of the subject lands as proposed is consistent with the intent of The London Plan.

878(2) Buildings within the Shopping Area Place Type will not exceed four storeys in height. Type 2 Bonus Zoning beyond this limit, up to six storeys, may be permitted in conformity with the Our Tools policies of this Plan.

878(4) Development within the Shopping Area Place Type will be sensitive to adjacent land uses and employ such methods as transitioning building heights and providing sufficient buffers to ensure compatibility.

The proponents are requesting an increase in height through Type 2 Bonus Zoning over Block 150 of up to five (5) storeys. The site concept plans attached propose a row of two 4-storey and two 5-storey apartment buildings oriented to Bradley Avenue. The five (5) storey buildings are shown at each end of the block oriented to the intersections of Bradley Avenue with Chelton Road and Meadowgate Boulevard. This addresses the principles of compatibility and building height in the Chelton Road Master Plan. The concept site plan demonstrates a transition in height and density with taller and denser buildings located towards Bradley Avenue, and shorter, less dense buildings at the north end, in proximity to the lower density and low-rise profile of existing and planned single detached and townhouses dwellings on surrounding lands.

The Neighbourhoods Place Type - Table 10* as it applies to Blocks 151 and 152 permits a range of residential uses including single detached and semi-detached dwellings, triplexes, fourplexes, townhouses, stacked townhouses, low-rise apartments, mixed use buildings, and small-scale community facilities.

*935(3) Zoning will be applied to ensure an intensity of development that is appropriate to the neighbourhood context, utilizing regulations for such things as height, density, gross floor area, coverage, frontage, minimum parking, setback, and landscaped open space.**

*936(2) New neighbourhoods, or parts thereof, should be designed to avoid rear lotting and to avoid noise walls that are required to protect amenity areas as defined by provincial guidelines. The Our Tools part of this Plan includes noise wall policies that provide greater detail.**

The recommended special provision zoning provides for a range of permitted uses that are consistent with the policies. The zoning will utilize density, height, setback and other site development standards, including minimum parking and landscaped open space requirements, appropriate to the neighbourhood context. A Noise Assessment was also prepared and submitted with the application in order to assess potential noise impacts and recommend mitigation measures from vehicular traffic generated on Bradley Avenue.

The assessment report recommends mitigation measures for noise reduction through exterior wall construction, window glazing, installation of central air conditioning, and noise warning clauses for specific residential units. Noise walls will not be required in order to meet Provincial environmental noise guidelines.

The High Density Residential Overlay (from 1989 Official Plan)* applies to Block 152. The HDR overlay allows residential development to a maximum height of 12 storeys and density up to 150 units per hectare. The proposed development is consistent with HDR overlay policy.

The Green Space Place Type allows a range of open space uses including natural heritage features and natural resources, public parks, cemeteries, golf courses, recreational and community facilities. The permitted uses of the Green Space Place Type will be implemented by the recommended OS5 Zoning to be applied along the northerly perimeter of Blocks 151 and 152, and providing a buffer to the adjacent natural feature.

1433_ Development or site alteration on lands adjacent to features of the Natural Heritage System shall not be permitted unless the ecological function of the adjacent lands has been evaluated and it has been demonstrated that there will be no negative impacts on the natural features or on their ecological functions. Adjacent lands shall be identified and delineated by the trigger distances shown in Table 13 for requiring environmental evaluations.

A scoped Environmental Impact Study (EIS) was undertaken in conjunction with the rezoning application for Blocks 151 and 152 (BioLogic - February 5, 2019). The EIS evaluation concluded that there are no significant natural heritage features and functions present within Blocks 151 and 152. Mitigation measures have been recommended to protect the ecological function of the adjacent wetland/woodland feature, including an open space buffer zone, chain link fencing, sediment and erosion control measures, and provisions for temporary stormwater drainage. Based on comments received from UTRCA and City staff, further revisions to the parking layout will be required, and there are some outstanding concerns, including the proposed snow storage areas, exterior lighting, and on-site stormwater management, that will need to be addressed as part of the Site Plan Approval process prior to finalizing the EIS.

Our Tools Policies

*1650_ Type 2 Bonus Zoning may permit greater height or density in favour of a range of facilities, services, or matters that provide significant public benefit in pursuit of the City Building goals of this Plan. However, an applicant must demonstrate that this greater height or density represents good planning.**

*1652_ Under Type 2 Bonus Zoning, additional height or density may be permitted in favour of facilities, services, or matters such as:**

- 1. Exceptional site and building design.*
- 3. Dedication of public open space.*
- 10. Large quantities of secure bicycle parking, and cycling infrastructure such as lockers and change rooms accessible to the general public.*
- 15. Extraordinary tree planting, which may include large caliper tree stock, a greater number of trees planted than required, or the planting of rare tree species as appropriate.*

16. *Measures that enhance the Natural Heritage System, such as renaturalization, buffers from natural heritage features that are substantively greater than required, or restoration of natural heritage features and functions.*

As this zoning application includes a request for Type 2 Bonusing to permit heights exceeding four storeys on Blocks 150 and 151 (five and six storeys, respectively) the following summarizes a number of the bonusable items that were considered in the design of the development concept proposed for the subject lands:

- Exceptional site and building design - Enhanced building and site design features including an active street edge along Bradley Avenue, Chelton Road, and Meadowgate Boulevard; as well as incorporating other principles articulated in the Chelton Road Master Plan;
- Dedication of public open space – The open space (OS5) lands are to be dedicated to the City as publically owned lands;
- Measures that enhance the Natural Heritage System – This includes substantial buffer planting with native vegetation to protect adjacent natural features;
- Large quantities of secure bicycle parking and cycling infrastructure including indoor and outdoor storage facilities, and a multi-use pathway internal to the site providing connections to surrounding public sidewalks and pedestrian trails; and,
- Extraordinary tree planting of large caliper trees to be planted along the street frontage and outdoor amenity areas.

*1653_ Type 2 Bonus Zoning will only be permitted where it is demonstrated that the resulting intensity and form of the proposed development represents good planning within its context.**

*1654_ Greater height or density offered through Type 2 Bonus Zoning will be commensurate with the public value of the facility, service or matter that is provided.**

The Type 2 Bonus Zoning criteria were reviewed and the planning merits and enhanced design elements outlined in the Chelton Road Master Plan, Urban Design Brief and Planning Justification Report have been considered. The applicant has demonstrated that the resulting intensity and form of development is appropriate for the development context, commensurate with the public benefit derived from the project enhancements, and represents good planning.

(1989) Official Plan

Block 150 is designated Community Commercial Node, and Block 151 is predominately designated Community Commercial Node with a portion along the northerly perimeter of the block designated as Open Space. Block 152 is predominantly designated Multi-family, High Density Residential on Schedule A – Land Use Map, and also shows lands inside the northerly perimeter of the block designated as Open Space. The Community Commercial Node permits a range of commercial and retail uses including food stores, pharmacies, convenience commercial uses, personal services, restaurants, financial institutions, limited automotive services, and various office uses. Multi-family, high density residential uses and community facilities may also be permitted in this designation through a zoning by-law amendment application, site plan application and consideration of design features which provide for the proper integration of the two uses, in accordance with Section 4.3.7.3.

The site plan has gone through the pre-application consultation process and a formal Application for Site Plan Approval will be made shortly. Consideration to various design

features presented in the Chelton Road Master Plan, Urban Design Brief, and Planning Justification Report are highlighted as follows:

Building Form, Massing and Articulation

- Proposed development includes mid-rise apartments and low-rise townhouses.
- Apartment buildings will be positioned to frame the street intersections, and include pedestrian entrances from the public sidewalks.
- Townhouses will also be front-facing to the surrounding public streets.
- Various architectural techniques will be employed to break up the massing of the apartment blocks, including projections and recessions, changes in building materials/colours, and incorporation of varying window sizes.

Architectural Treatment

- High quality materials including a large amount of glass are proposed to be incorporated into the building facades resulting in an attractive modern design.
- Repetition of vertical and horizontal lines, windows, projections, recesses, and setbacks will be applied to articulate the facades of larger buildings further breaking up the building mass and creating a rhythm along the streetscape.

Character and Image

- It is recognized that the north side of Bradley Avenue is undergoing a transition to a more urban character and form.
- Buildings will be designed to address the surrounding public streets to help establish, and support, a strong urban street edge along the developing road corridors.
- The proposed design is intended to set a positive example that will encourage an attractive a vibrant streetscape over the long term.

Public Realm / Pedestrian Environment

- Intent is to integrate the built form into the existing streetscape and reinforce the public realm at the pedestrian level.
- Back-to-back townhouses including street-facing units oriented to create an active street frontage.
- Landscaping to define the private / public realms along the street edges
- Walkways providing direct pedestrian connections from each building to the public street.
- An internal pedestrian/cycling pathway system with connections to the surrounding neighbourhood.
- Landscaped screening of surface parking areas adjacent all public streets

Through site planning and evaluation of design considerations as outlined above, the proposed development concept is generally supportive of, and in keeping with, the policies of the Official Plan. The proposed residential uses and zoning are considered appropriate and conform with the permitted use policies, as well as being consistent with the existing Multi-family, High Density Residential designation on the westerly portion of the subject lands, and the Open Space designation along the northerly limit of the subject blocks.

Recommended Zoning

Residential R5 (R5-7) - This zoning would be applied to the north half of the Block 150 to permit the 3-storey, back-to-back townhouses up to a maximum density of 60 units per hectare and maximum height of 12 metres. The area is 2.16 hectares and a total of 88 townhouse units are proposed yielding a density of approximately 41 units per hectare.

Residential R5 Special Provision R5-7() - This zoning would be applied to a small area (0.24 hectares) on the westerly portion of Block 152 to permit townhouses up to a maximum density of 60 units per hectare and maximum height of 12 metres. The site concept plan shows two rows of 12 townhouse units. Special zone provisions are

recommended to allow for a minimum interior side yard setback of 2.0 metres, and minimum setback from the OS5 Zone of 3.0 metres. The west side yard is adjacent undeveloped fields that are currently zoned Urban Reserve (UR1) and the east building setback is adjacent to parking and common amenity areas internal to the development site. The application request was for a 0.0 metre rear yard setback adjacent the proposed Open Space (OS5) zoning. A zero building setback is not possible as the zone line must be fenced to demarcate the open space lands to be dedicated to the City. Recognizing that this part of the development site forms a pinch point, and that further adjustments to shift the individual townhouse units and driveways as shown on the site concept plan will be required, staff are prepared to recommend a reduced minimum setback from the Open Space zone of 3.0 metres.

Residential R9 Special Provision Bonus (R9-7()•H16•B-) - This zone would be applied to the south half of the Block 150 to permit the proposed apartment buildings up to a maximum density of 150 units per hectare. The area is 2.02 hectares and a total of 200 units are proposed yielding a density of approximately 99 units per hectare. This zoning would also be applied to portions within Blocks 151 and 152 and the developable area here is 2.29 hectares. A total of 255 units are proposed yielding a density of approximately 111 units per hectare.

Special zone provisions are recommended to permit a front yard building setback of 3.0 metres (minimum) and 6.0 metres (maximum), as well as an exterior side yard setback of 3.0 metres (minimum) and 6.0 metres (maximum) in order to ensure buildings are located and oriented to the abutting public streets consistent with goals of placemaking. The requested zoning here also included a 0.0 metre minimum setback from the OS5 Zone. However, this is no longer required as subsequent revisions were made to the site concept plans to relocate buildings away from the open space zone line and eliminate a proposed parking structure. The recommended special provision includes a maximum building height of four storeys (16 metres) which will be applied as the standard regulation.

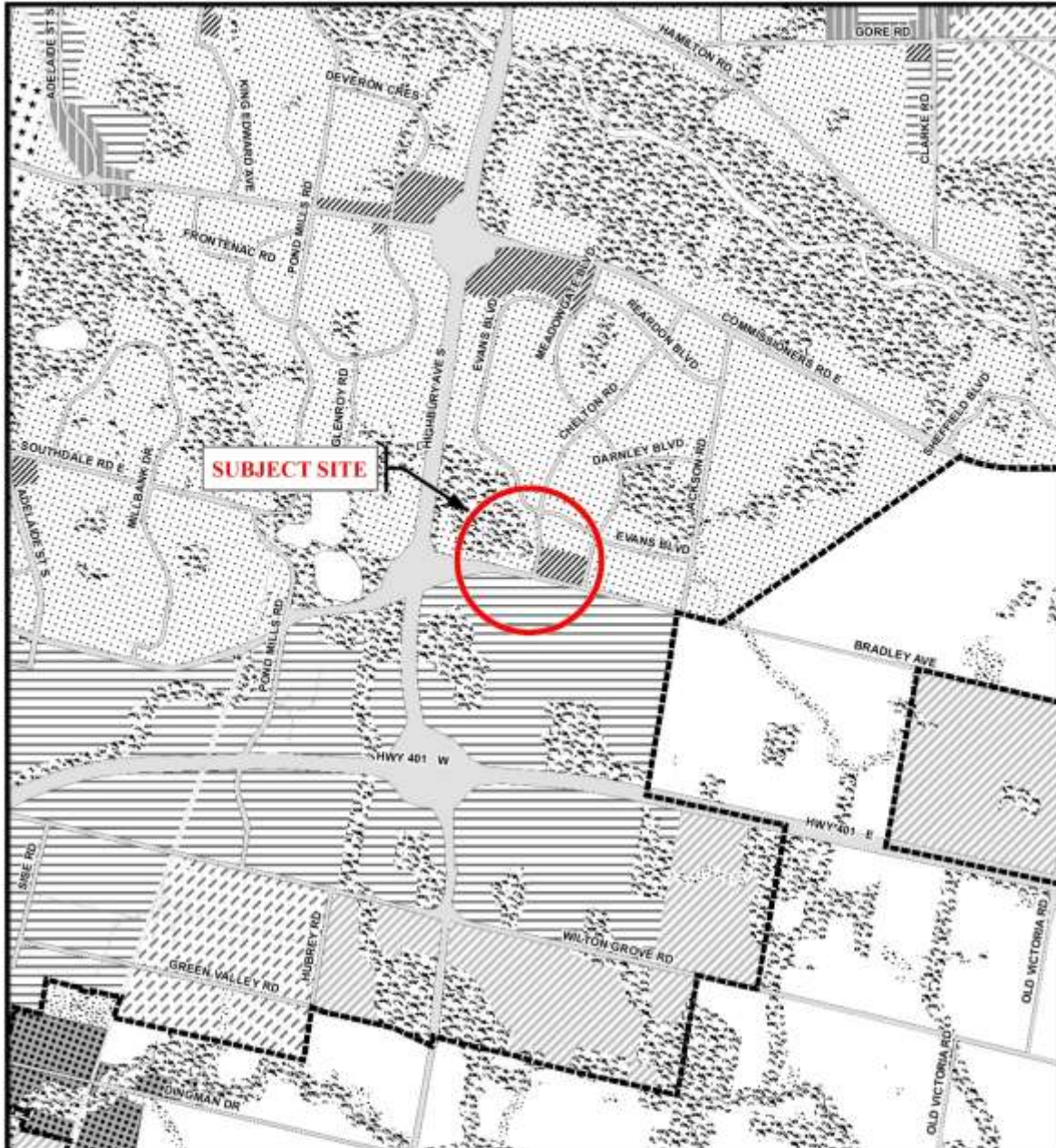
Building heights may be permitted to exceed four storeys in order to facilitate the development of apartment buildings up to a maximum of five (5) storeys and six (6) storeys through bonus zoning. The Bonus (B-) Zone will be implemented through the site plan(s) and development agreement(s). The final development plans for the proposed apartment buildings must adhere to the vision and principles of the Chelton Road Master Plan for 800, 805 and 810 Chelton Road, and site concept plans attached to the amending by-law. The bonus for increase in height will be permitted in exchange for the following matters:

- Exceptional site and building design - Enhanced building and site design features including an active street edge along Bradley Avenue, Chelton Road, and Meadowgate Boulevard.
- Dedication of public open space – The open space (OS5) lands are to be dedicated to the City as publically owned lands.
- Measures that enhance the Natural Heritage System – This includes substantial buffer planting with native vegetation to protect adjacent natural features.
- Large quantities of secure bicycle parking and cycling infrastructure including indoor and outdoor storage facilities, and a multi-use pathway internal to the site providing connections to surrounding public sidewalks and pedestrian trails; and,
- Extraordinary tree planting of large caliper trees to be planted along the street frontage and outdoor amenity areas.

Open Space OS5 – This zoning will be applied to the open space buffer on Blocks 151 and 152 and is consistent with the zoning of the large open space lands to the north.

Appendix D – Relevant Background

London Plan Map Excerpt



Legend

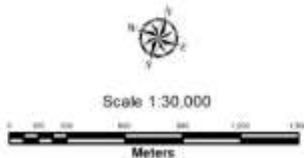
Downtown	Future Community Growth	Environmental Review
Transit Village	Heavy Industrial	Farmland
Shopping Area	Light Industrial	Rural Neighbourhood
Rapid Transit Corridor	Future Industrial Growth	Waste Management Resource Recovery Area
Urban Corridor	Commercial Industrial	Urban Growth Boundary
Main Street	Institutional	
Neighbourhood	Green Space	

This is an excerpt from the Planning Division's working consolidation of Map 1 - Place Types of the London Plan, with added notations.

At the time of the printing of this map, the Rapid Transit EA is in progress. This map shows the Rapid Transit Corridors and Urban Corridors to recognize potential alignments. These Place Types will be modified to align with the results of the EA process for the final version of The London Plan.

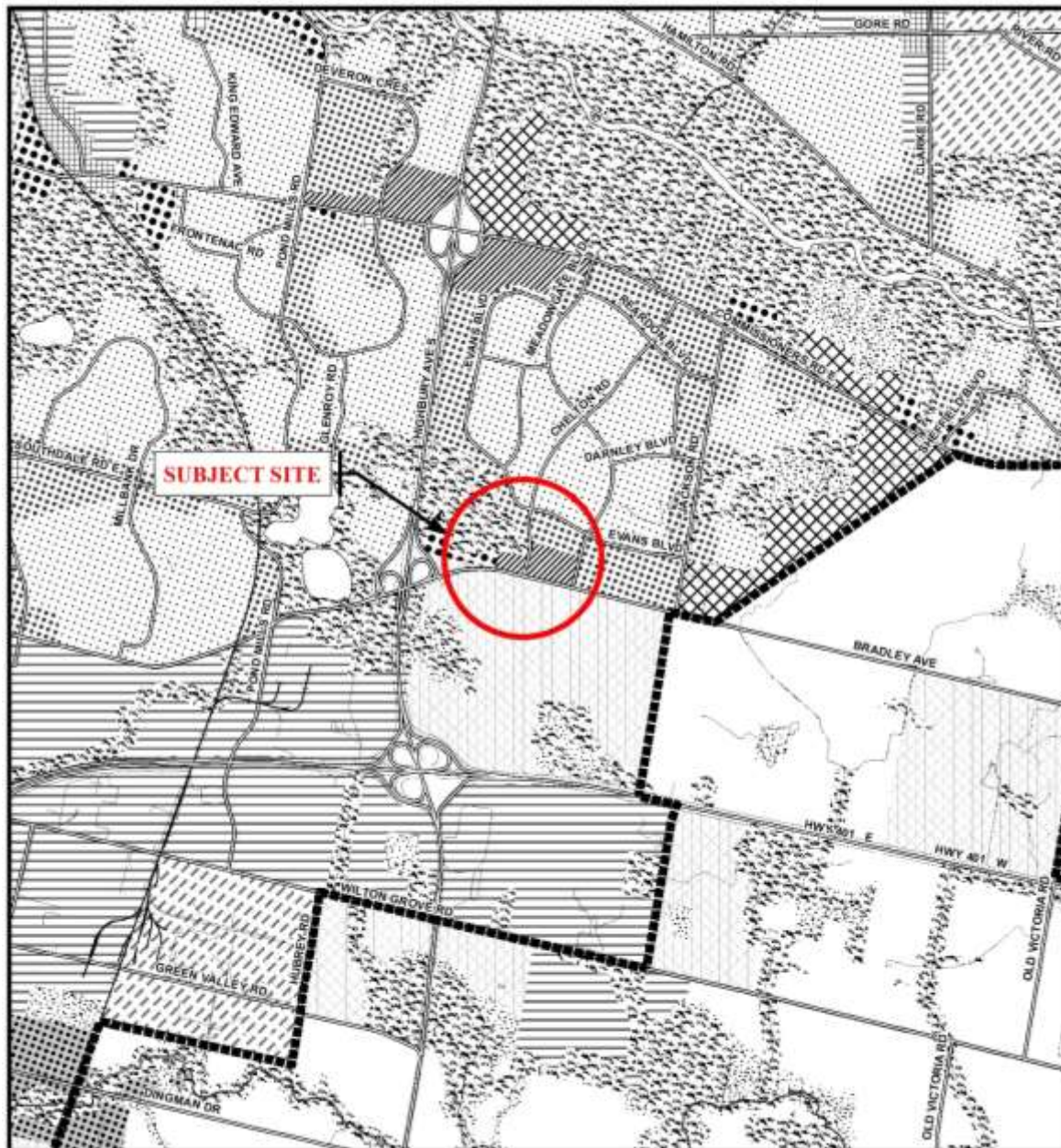
CITY OF LONDON
 Planning Services /
 Development Services

LONDON PLAN MAP 1
- PLACE TYPES -
 PREPARED BY: Planning Services



File Number: Z-9089
Planner: LM
Technician: DM
Date: September 12, 2019

Official Plan Map Excerpt



Legend

- | | |
|--|-----------------------------------|
| Downtown | Office Business Park |
| Enclosed Regional Commercial Node | General Industrial |
| New Format Regional Commercial Node | Light Industrial |
| Community Commercial Node | Regional Facility |
| Neighbourhood Commercial Node | Community Facility |
| Main Street Commercial Corridor | Open Space |
| Auto-Oriented Commercial Corridor | Urban Reserve - Community Growth |
| Multi-Family, High Density Residential | Urban Reserve - Industrial Growth |
| Multi-Family, Medium Density Residential | Rural Settlement |
| Low Density Residential | Environmental Review |
| Office Area | Agriculture |
| Office/Residential | Urban Growth Boundary |

CITY OF LONDON
 Department of
 Planning and Development
 OFFICIAL PLAN SCHEDULE A
 - LANDUSE -

PREPARED BY: Graphics and Information Services



Scale 1:30,000



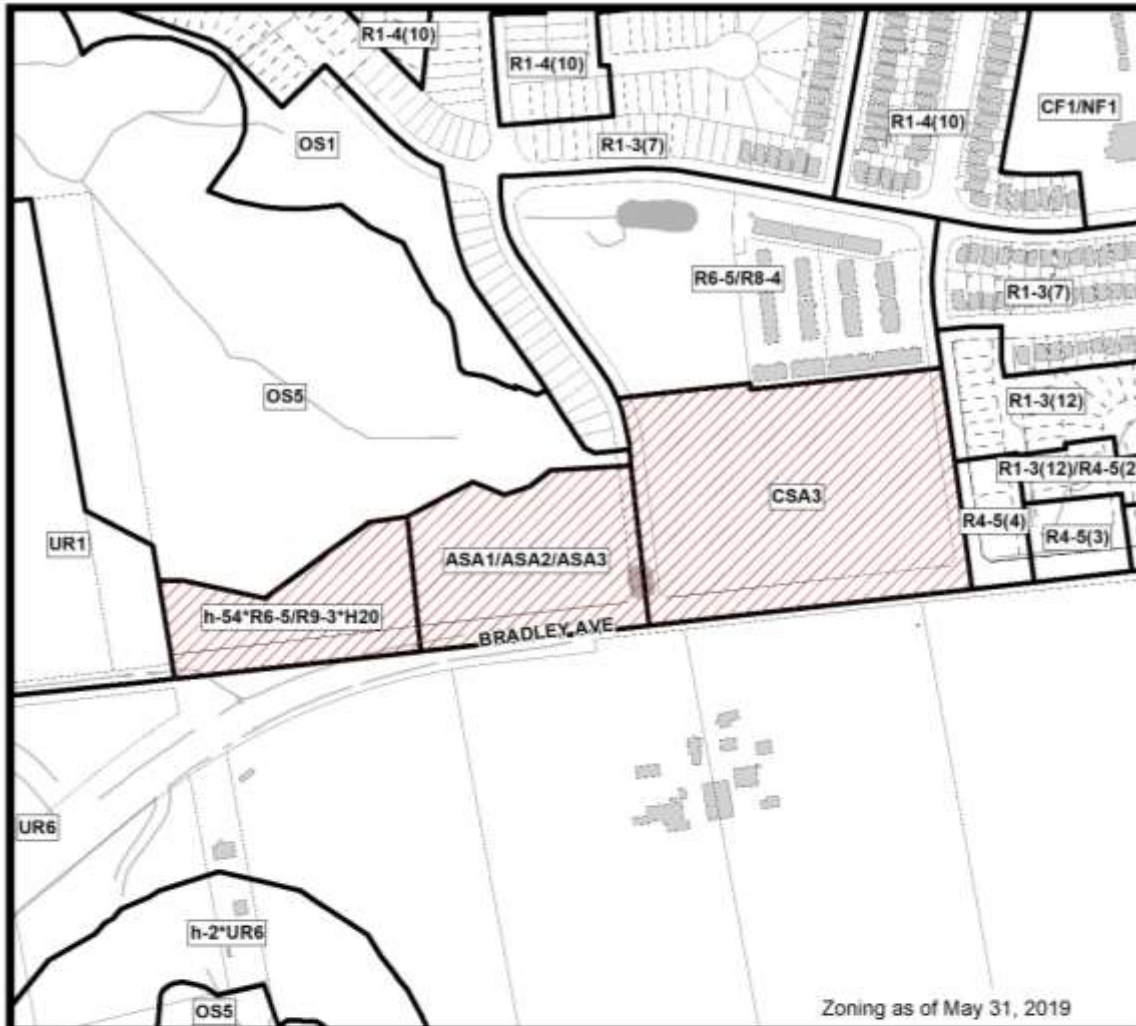
FILE NUMBER: Z-9089

PLANNER: LM

TECHNICIAN: DM

DATE: 2019/09/12

Zoning By-law Map Excerpt



COUNCIL APPROVED ZONING FOR THE SUBJECT SITE:

1) LEGEND FOR ZONING BY-LAW Z-1

- | | |
|--|---|
| <ul style="list-style-type: none"> R1 - SINGLE DETACHED DWELLINGS R2 - SINGLE AND TWO UNIT DWELLINGS R3 - SINGLE TO FOUR UNIT DWELLINGS R4 - STREET TOWNHOUSE R5 - CLUSTER TOWNHOUSE R6 - CLUSTER HOUSING ALL FORMS R7 - SENIOR'S HOUSING R8 - MEDIUM DENSITY/LOW RISE APTS. R9 - MEDIUM TO HIGH DENSITY APTS. R10 - HIGH DENSITY APARTMENTS R11 - LOOGING HOUSE
 DA - DOWNTOWN AREA RSA - REGIONAL SHOPPING AREA CSA - COMMUNITY SHOPPING AREA NSA - NEIGHBOURHOOD SHOPPING AREA BDC - BUSINESS DISTRICT COMMERCIAL AC - ARTERIAL COMMERCIAL HS - HIGHWAY SERVICE COMMERCIAL RSC - RESTRICTED SERVICE COMMERCIAL CC - CONVENIENCE COMMERCIAL SS - AUTOMOBILE SERVICE STATION ASA - ASSOCIATED SHOPPING AREA COMMERCIAL
 OR - OFFICE/RESIDENTIAL OC - OFFICE CONVERSION RO - RESTRICTED OFFICE OF - OFFICE | <ul style="list-style-type: none"> RF - REGIONAL FACILITY CF - COMMUNITY FACILITY NF - NEIGHBOURHOOD FACILITY HER - HERITAGE DC - DAY CARE
 OS - OPEN SPACE CR - COMMERCIAL RECREATION ER - ENVIRONMENTAL REVIEW
 OB - OFFICE BUSINESS PARK LI - LIGHT INDUSTRIAL GI - GENERAL INDUSTRIAL HI - HEAVY INDUSTRIAL EX - RESOURCE EXTRACTIVE UR - URBAN RESERVE
 AG - AGRICULTURAL AGC - AGRICULTURAL COMMERCIAL RRC - RURAL SETTLEMENT COMMERCIAL TGS - TEMPORARY GARDEN SUITE RT - RAIL TRANSPORTATION
 "h" - HOLDING SYMBOL "d" - DENSITY SYMBOL "H" - HEIGHT SYMBOL "B" - BONUS SYMBOL "T" - TEMPORARY USE SYMBOL |
|--|---|

CITY OF LONDON

PLANNING SERVICES / DEVELOPMENT SERVICES

**ZONING
 BY-LAW NO. Z-1
 SCHEDULE A**



FILE NO:

Z-9089

LM

MAP PREPARED:

2019/09/12

DM

1:5,000

0 30 60 120 180 240

Meters

THIS MAP IS AN UNOFFICIAL EXTRACT FROM THE ZONING BY-LAW WITH ADDED NOTATIONS

Additional Reports

December 12, 2016 – Planning and Environment Committee Public Participation Meeting– Application by Drewlo Holdings Inc. – Application for Red-Line Revisions and Extension of Draft Plan Approval re: lands located on the north side of Bradley Avenue, east of Highbury Avenue - File No. 39T-92020 / 39T-92020-E (Agenda Item #10).