

TO:	CHAIR AND MEMBERS CIVIC WORKS COMMITTEE MEETING ON FEBRUARY 20, 2019
FROM:	KELLY SCHERR, P. ENG., MBA, FEC MANAGING DIRECTOR, ENVIRONMENTAL AND ENGINEERING SERVICES AND CITY ENGINEER
SUBJECT:	AMENDMENTS TO THE TRAFFIC AND PARKING BY-LAW

RECOMMENDATION

That on the recommendation of the Managing Director, Environmental and Engineering Services and City Engineer, the attached proposed by-law (Appendix A) **BE INTRODUCED** at the Municipal Council meeting to be held on March 5, 2019, for the purpose of amending the Traffic and Parking By-law (PS-113).

2015-19 STRATEGIC PLAN

The following report supports the Strategic Plan through the strategic focus area of **Building a Sustainable City** by improving safety, traffic operations and residential parking needs in London’s neighbourhoods.

BACKGROUND

The Traffic and Parking By-law (PS-113) requires amendments (Appendix A) to address traffic safety, operations and parking concerns. The following amendments are proposed:

1. **No Stopping**

Charlotte Street

A review of the Schedule 1 ‘No Stopping Anytime’ revealed that the west side of Charlotte Street from Dundas Street to Lorne Avenue is mistakenly listed as ‘No Stopping’ and should be removed. The east side is designated as ‘No Parking Anytime’ from Dundas Street to Princess Avenue. Charlotte Street is 7.3 m wide and would allow for on-street parking on the west side except for during the annual ‘No Parking Anytime’ restrictions associated with the Western Fair.

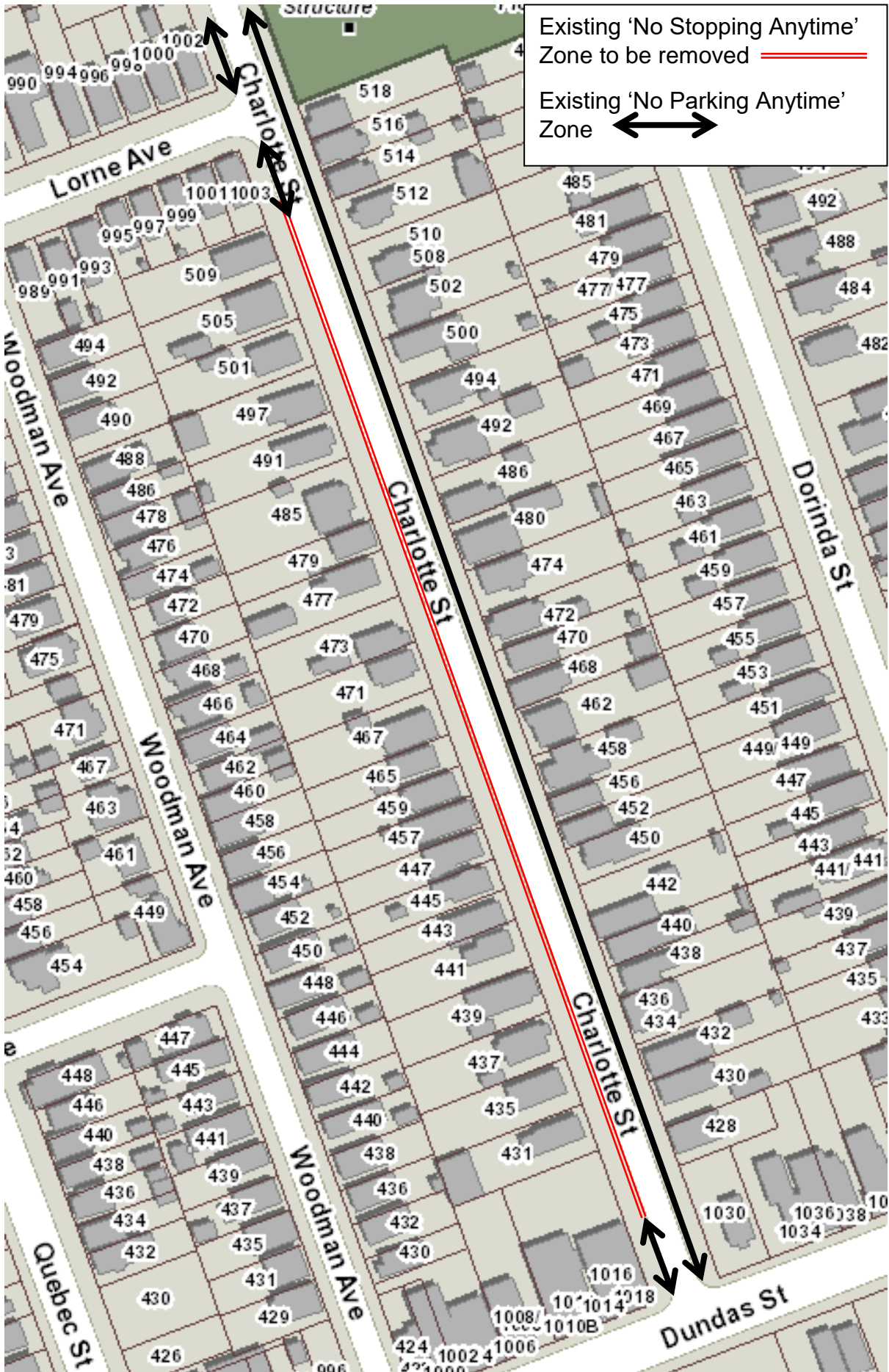


Figure 1: Charlotte Street

Duchess Avenue

At the request of a caregiver, St. Mary’s Catholic School and Southwestern Ontario Student Transportation Services (Transportation Services), a review was conducted of the existing ‘No Stopping’, ‘No Parking’, ‘School Bus Loading’ zones and available on-street parking zones to determine if they were appropriate for the needs of those dropping off and picking up students, school buses and area property owners.

Transportation Services has reduced the number of buses serving St. Mary’s from five to three, which allows for smaller ‘No Stopping’ and ‘School Bus Loading’ zones. Other changes were identified to better address the needs of all users. Figure 2a below shows the existing parking regulations and Figure 2b shows the proposed parking regulations. These changes will provide for an additional 47 m of unrestricted parking.

These changes will allow for more available on-street parking for the drop-off and pick-up students, as well as allowing for more parking opportunities for those attending the school and church for various functions and for area property owners and their guests.

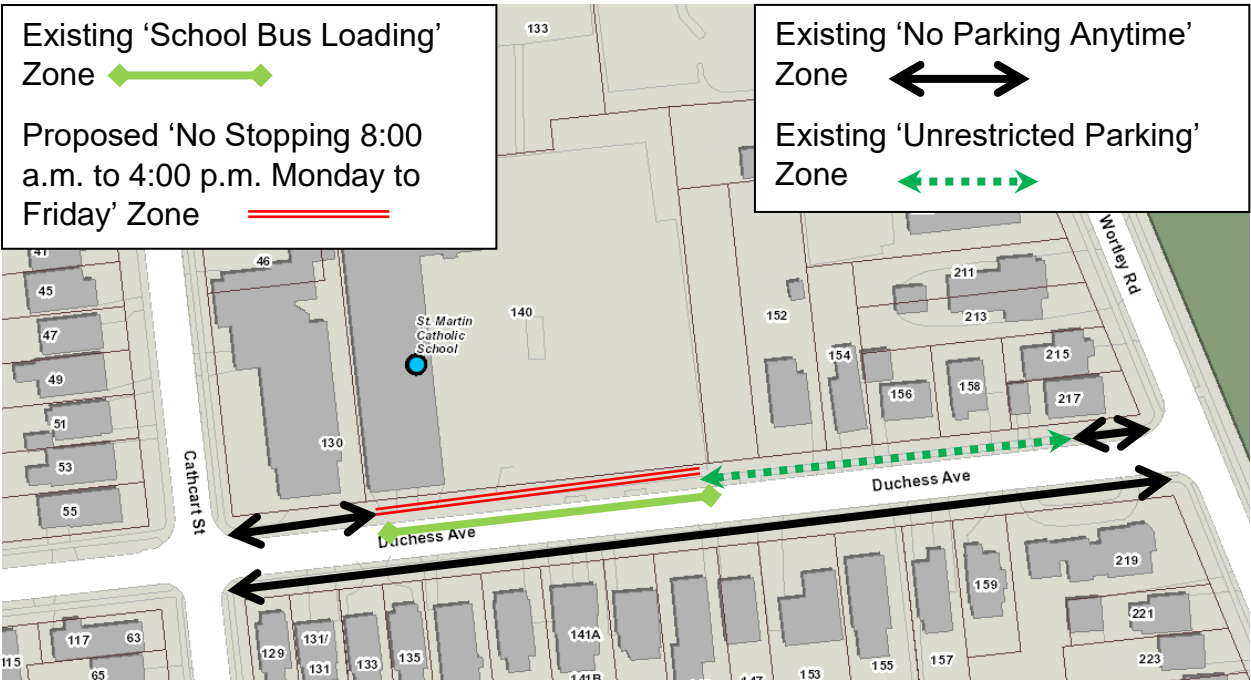


Figure 2a: Duchess Avenue Existing Regulations

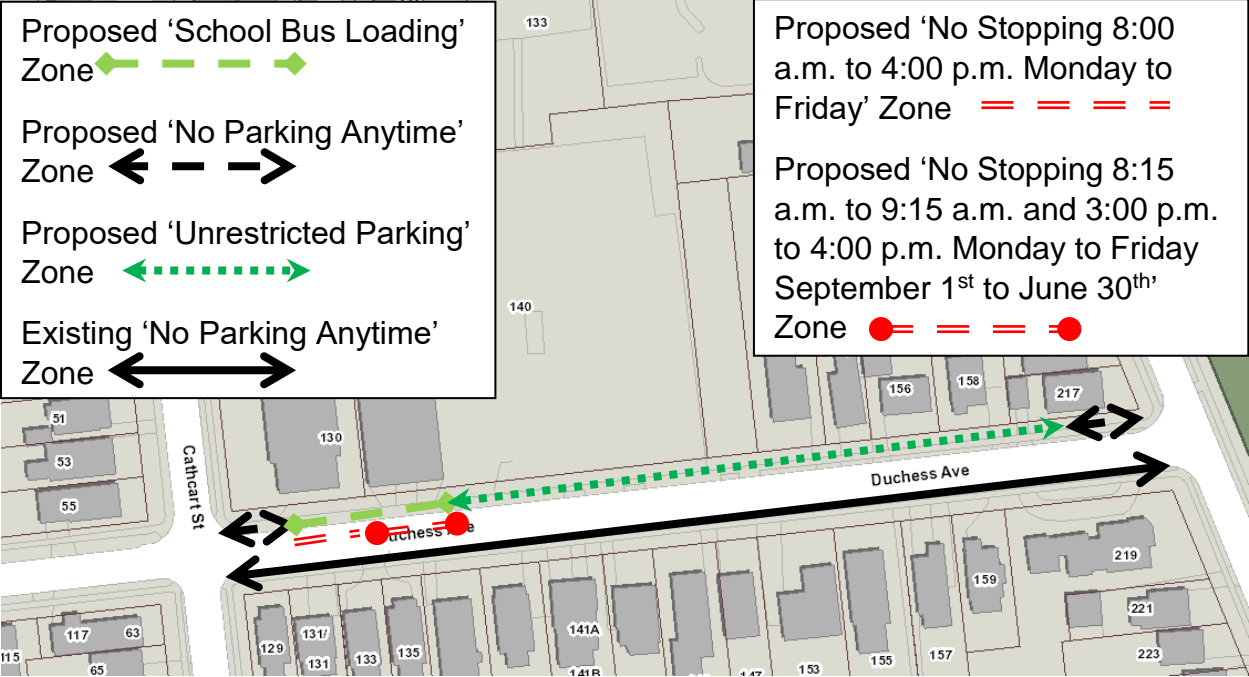


Figure 2b: Duchess Avenue Proposed Regulations

Amendments are required to Schedule 1 (No Stopping), Schedule 2 (No Parking), Schedule 6 (Limited Parking) and Schedule 16 (School Bus Loading Zones) to address the above changes.

2. **No Parking**

Staff was requested to review the current parking restrictions within the vicinity of Canada Post Community Mail Boxes (CMB) due to concerns that residents have difficulty retrieving their mail when vehicles are parked in front of the CMB.

The Traffic and Parking By-law currently does not restrict parking within the vicinity of a CMB. It is recommended to implement parking restrictions within 1.0 meter of a CMB when signage is present and if the CMB is facing the street and has no sidewalk access as shown in Figure 3.



Figure 3: No Parking Anytime within 1m of street facing Canada Post Mailboxes

An amendment to PS-113 Traffic and Parking By-law Section 10 (No Parking In Posted Locations) is needed to address the above change.

Frobisher Crescent

At the request of local residents, a mail-back survey was sent to the property owners on Frobisher Crescent where the majority of the respondents supported implementing 'No Parking 8:00 a.m. to 4:00 p.m. Monday to Friday' zones on both sides of Frobisher Crescent to 190 m north of Hudson Drive.

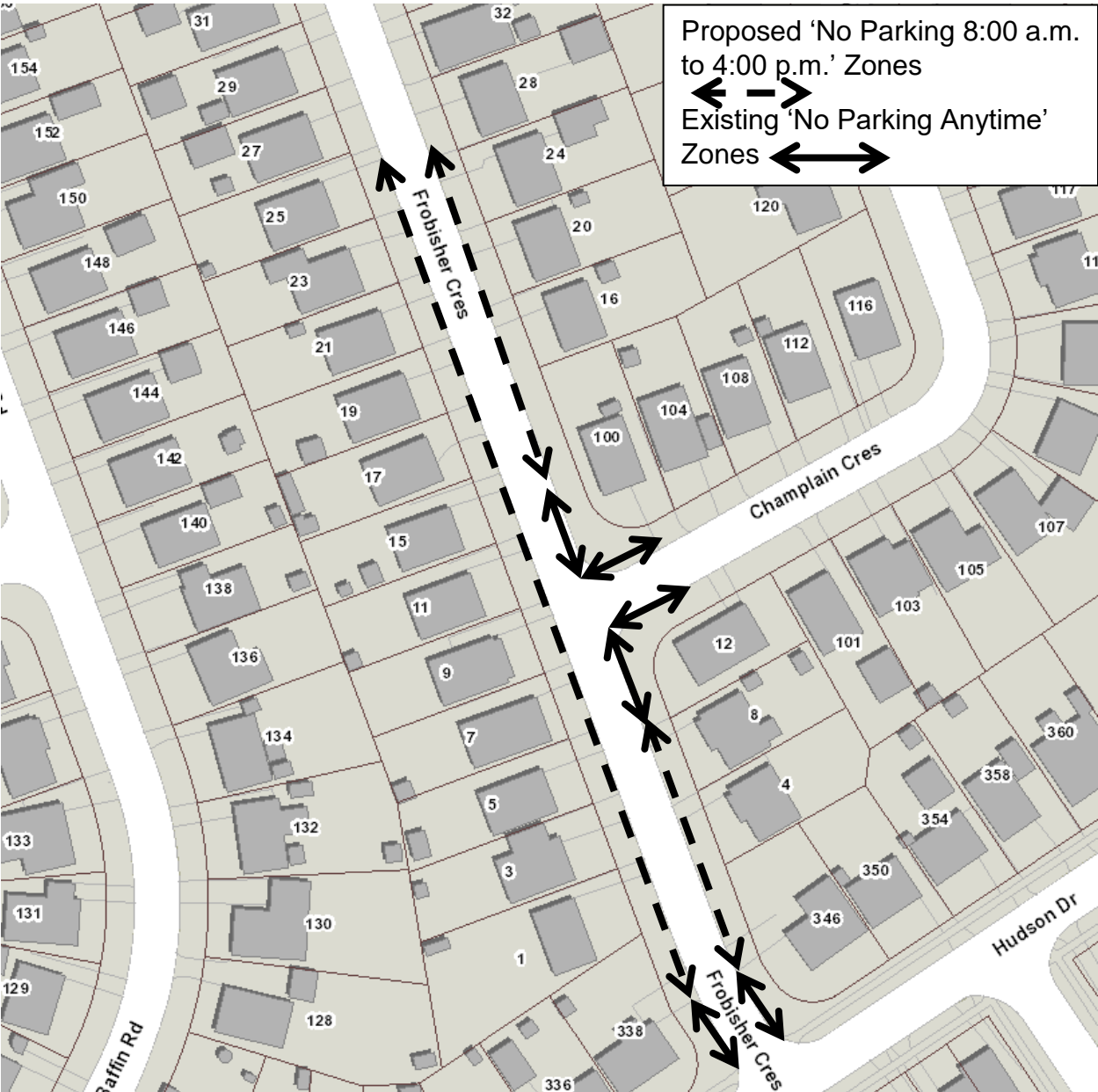


Figure 4: Frobisher Crescent

Shore Road

At the request of an area resident and the St. Nicholas Catholic School Principal, staff reviewed the on-street parking regulations for Shore Road fronting St. Nicholas Catholic School. The south side of Shore Road is currently No Stopping Anytime from Riverbend Road to a point 205 m west. The north side is currently unrestricted parking, which is required for the drop-off and pick up of students and for school functions. School buses use the U-driveway fronting the school and the school is considering permitting student drop-off and pick-up in the parking area to the east of the building. To improve ingress and egress for the school buses and caregivers' vehicles, a 'No Parking Anytime' zone extending 10m both east and west of the west end of the U-driveway is recommended. A 'No Parking Anytime' zone is also recommended from 10 m west of the U-drive is recommended east entrance to the west side of the parking lot access and for 10 m east of that access.



Figure 5: Shore Road

Amendments are required to Schedule 2 (No Parking) address the above changes.

3. **Regulatory Signs**

Blackfriars Bridge

The construction of Blackfriars Bridge, Blackfriars Street and Ridout Street North requires amendments to a number of PS-113 Traffic By-law Schedules, including Schedule 8 (Prohibited Turns), Schedule 9.1 (Reserved Lanes), Schedule 10 (Stop Signs), Schedule 12 (One Way Streets) and Schedule 13.1 (Pedestrian Crossovers) to reflect the changes implemented to support safe operations for motorists, cyclists and pedestrians.

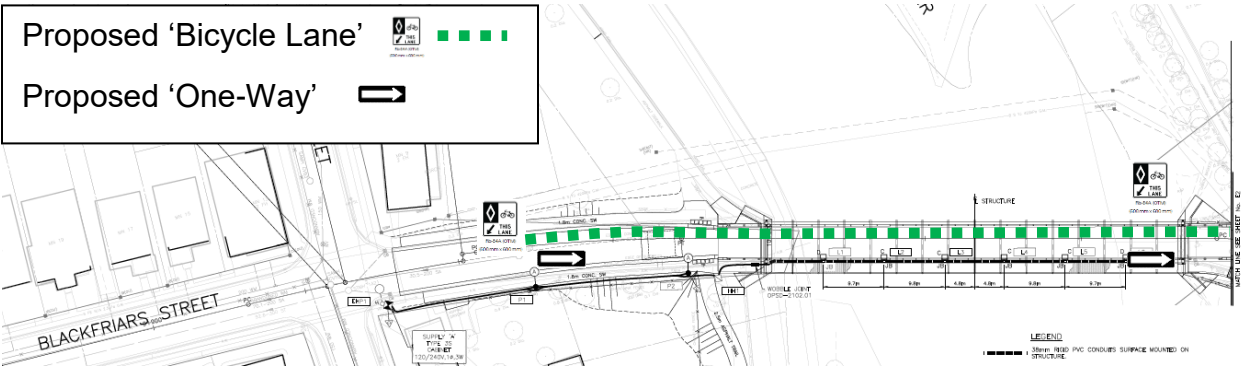


Figure 7: Blackfriars Street and Blackfriars Bridge

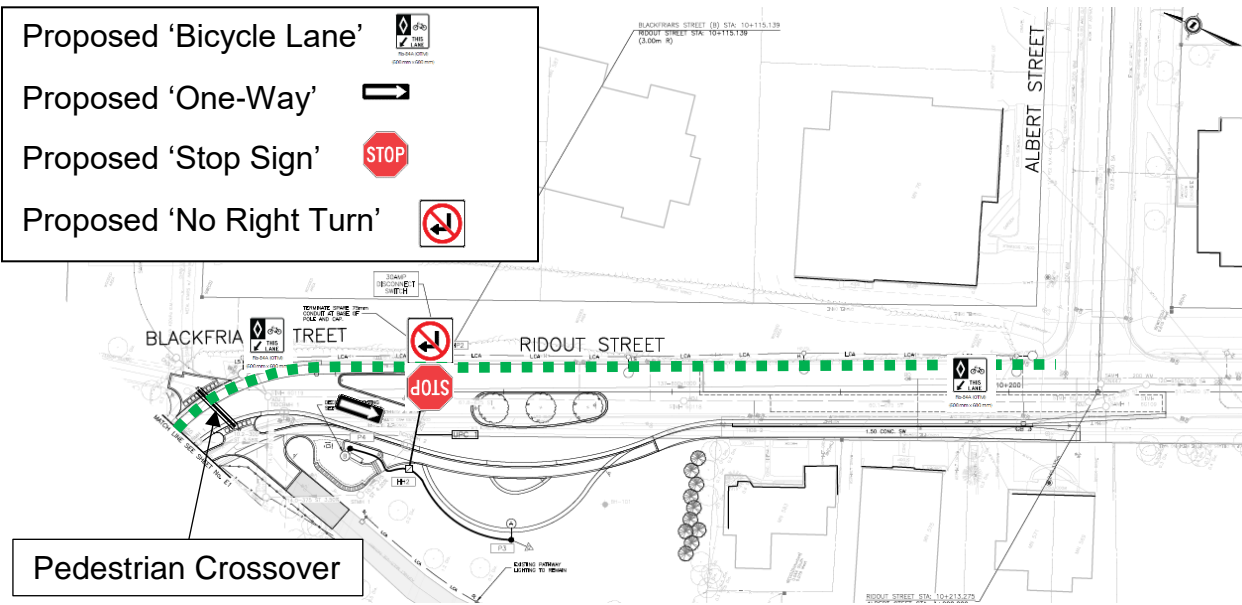


Figure 6: Ridout Street North and Blackfriars Bridge

4. **Prohibited Turns**

Sarnia Road

It is recommended that 'No U-Turn' be implemented for both the eastbound and westbound directions of Sarnia Road at Middlewoods Drive to address safety concerns.



Figure 7: Sarnia Road

Amendments are required to Schedule 8 (Prohibited Turns) to address the above change.

5. **Regulatory Signs**

King Edward Avenue and Scenic Drive

In order to address pedestrian and vehicle safety concerns, it is recommended that an ‘All-way Stop’ be implemented at the intersection of King Edward Avenue and Scenic Drive.

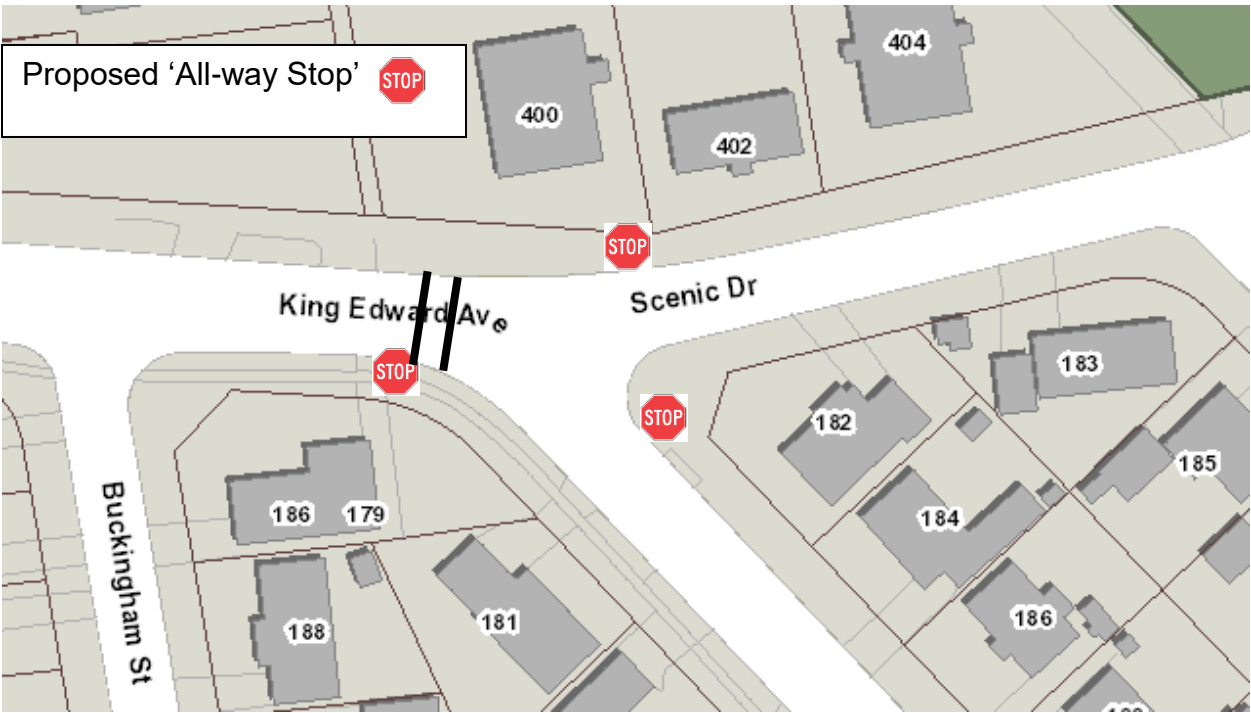


Figure 8 King Edward Avenue and Scenic Drive

Amendments are required to Schedule 10 (Stop Sign Locations) to address the above change.

6. Yield Sign to Stop Sign

Due to safety concerns, it is recommended to replace the existing Yield Signs with Stop Signs at the following locations:

- Hillsborough Road at St. Lawrence Boulevard;
- Penrith Crescent at Grasmere Crescent ;
- Robin's Hill Road at Crumlin Sideroad;
- Smallman Drive at Carnforth Road; and
- Thirlmere Road at Carnforth Road.

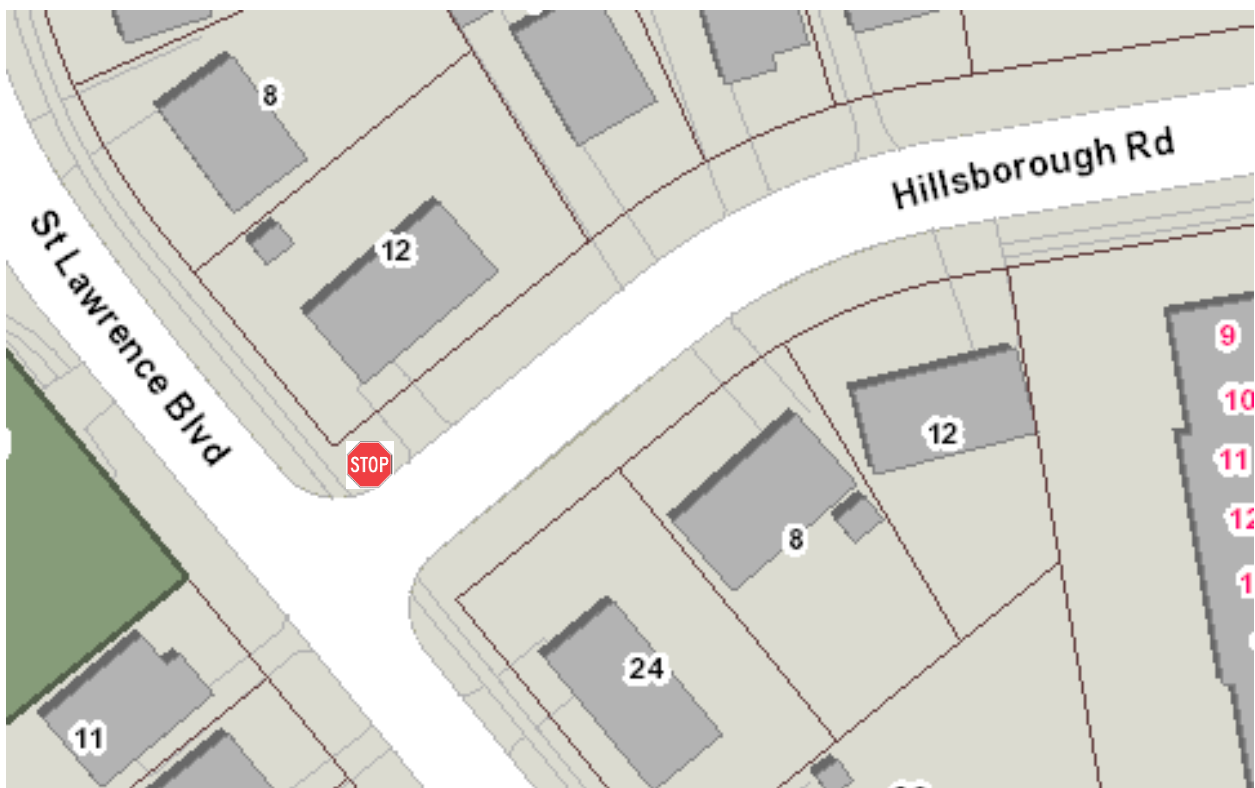


Figure 9: Hillsborough Road at St. Lawrence Boulevard

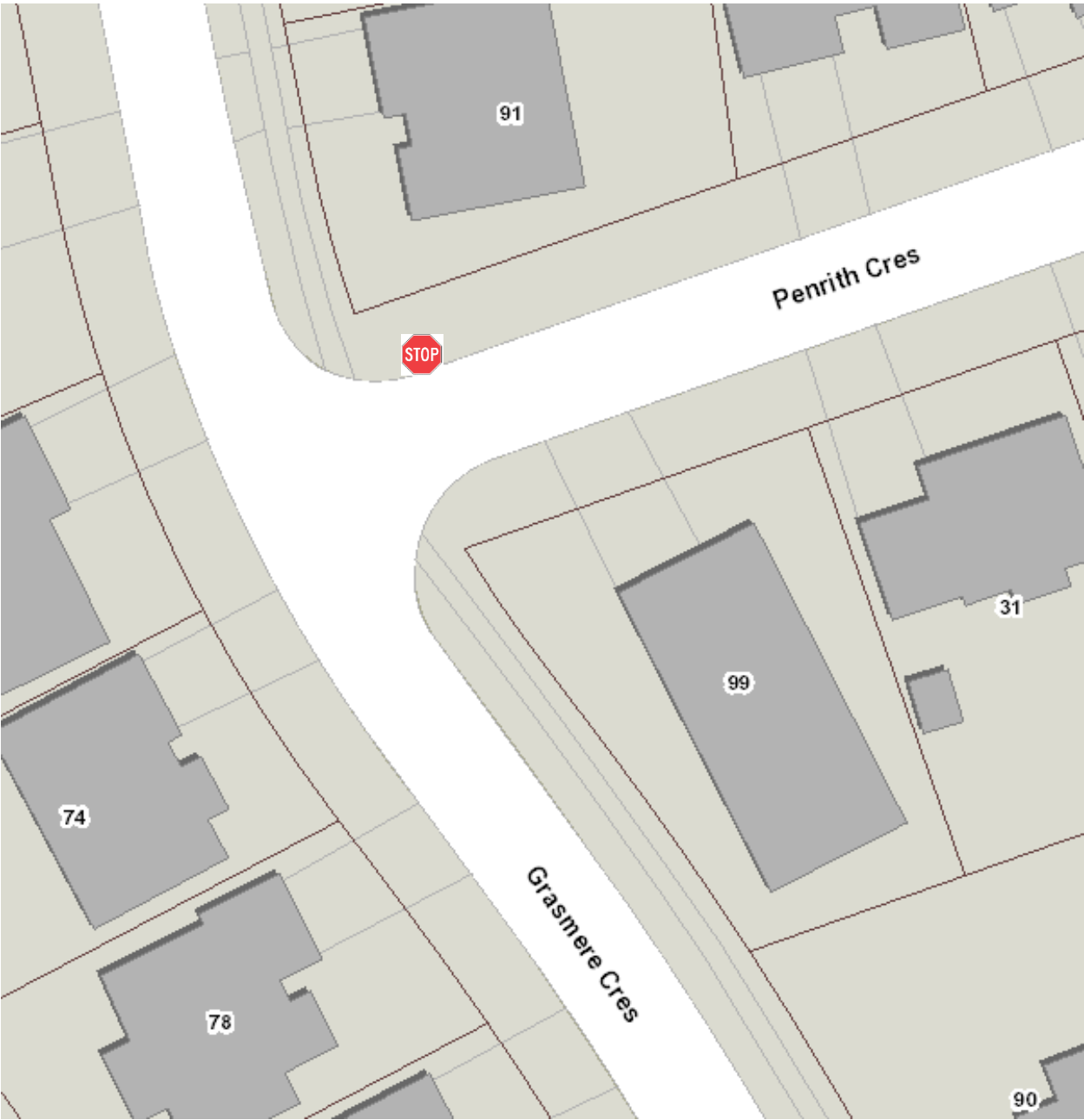


Figure 10: Penrith Crescent at Grasmere Crescent

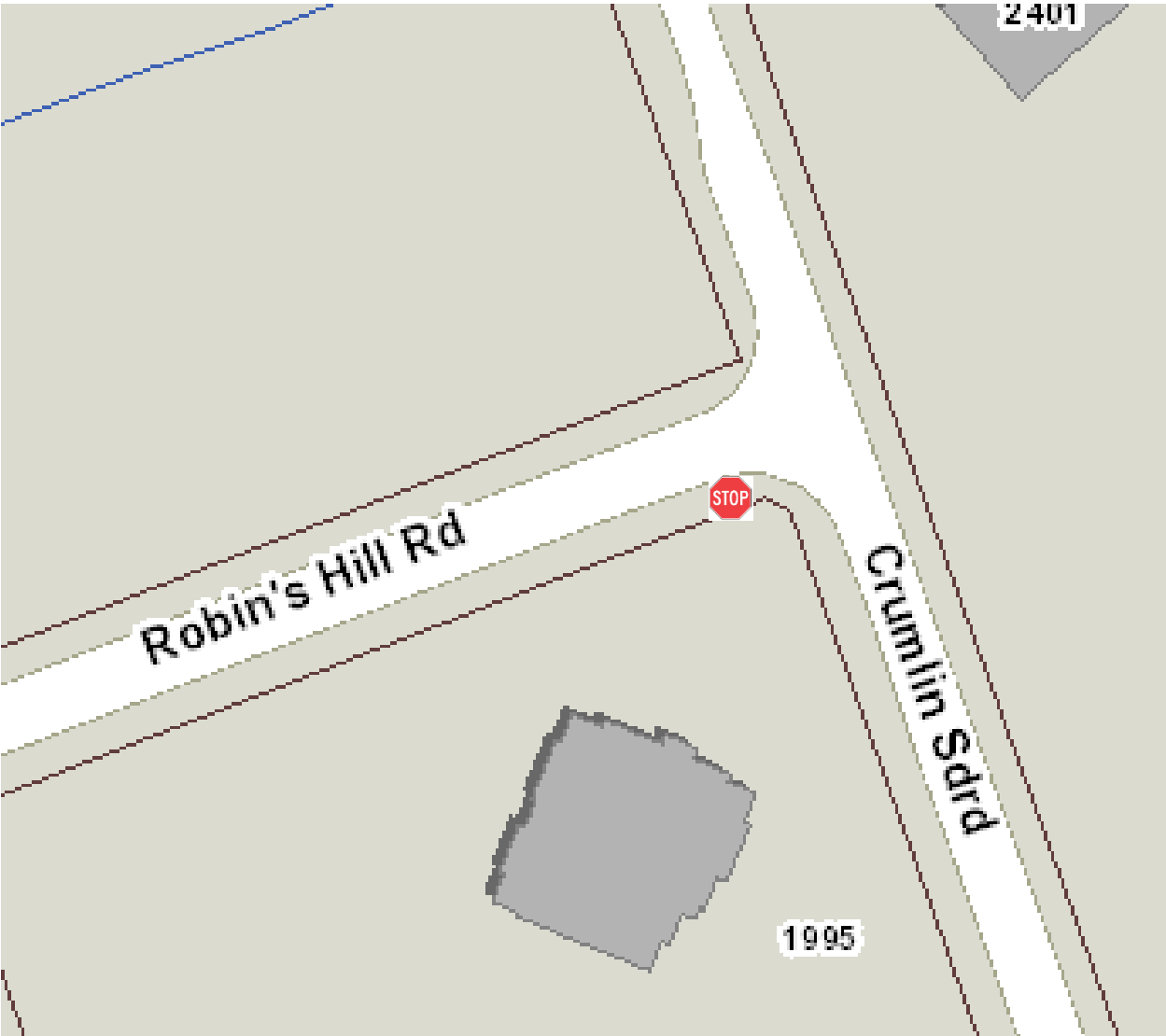


Figure 11: Robin's Hill Road at Crumlin Sideroad

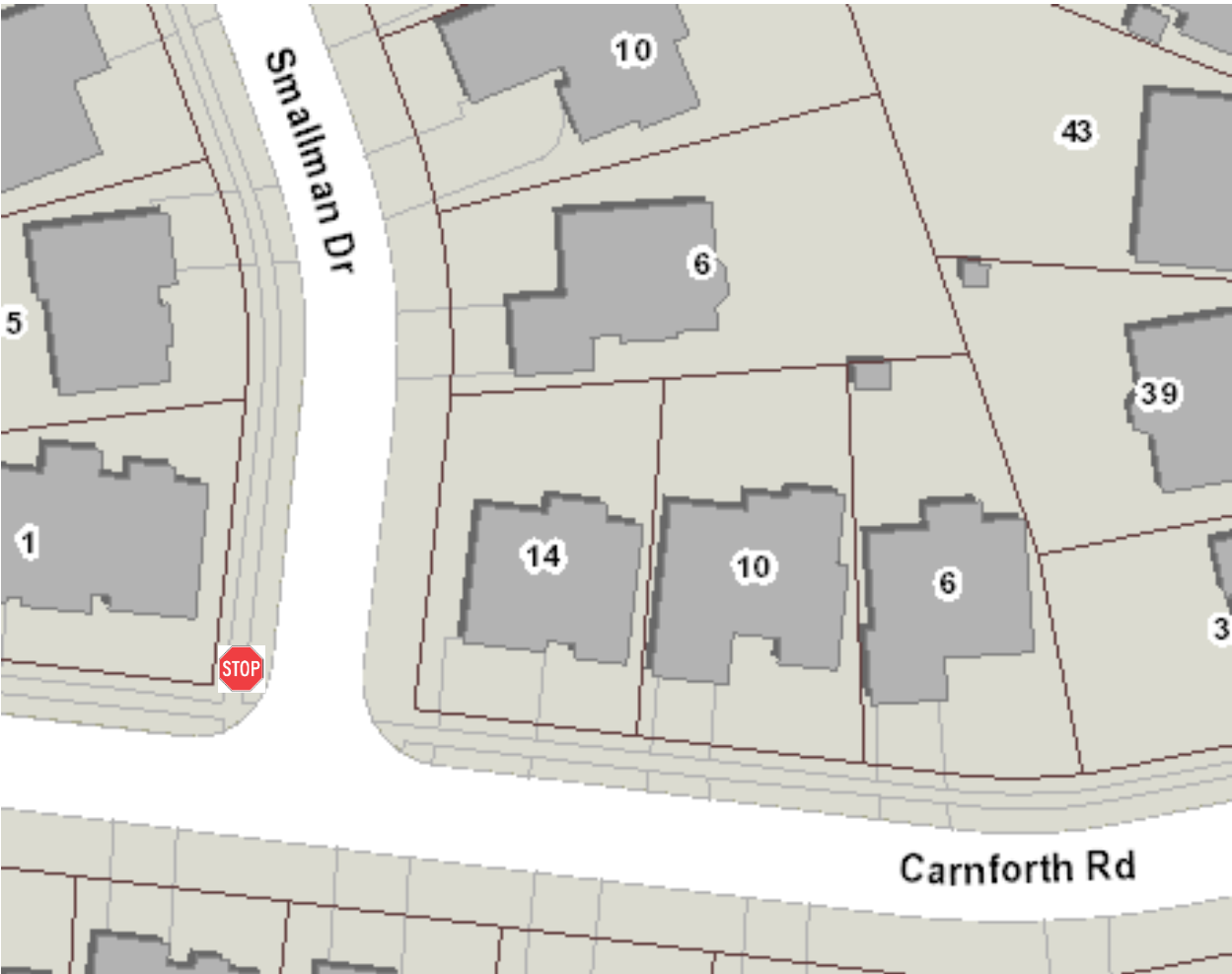


Figure 12: Smallman Drive at Carnforth Road

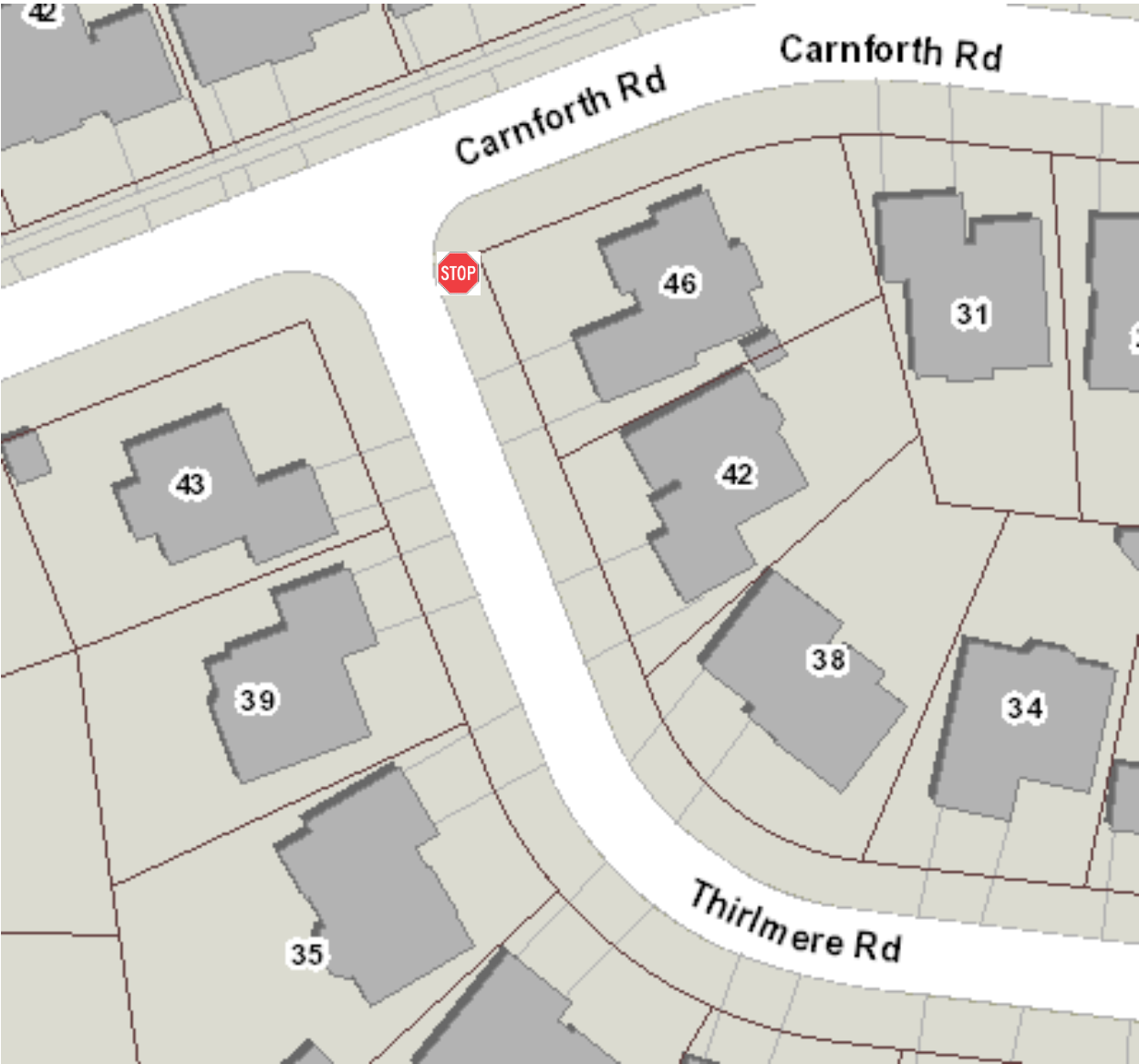


Figure 13: Thirlmere Road at Carnforth Road

Amendments are required to Schedule 10 (Stop Sign Locations) and to Schedule 11 (Yield Signs) to address the above changes.

7. **Heavy Truck Routes**

Due to safety and maintenance concerns, removal of the ‘24 Hour Heavy Truck Route’ on Evelyn Drive from Rebecca Road to the east City Limit and on Rebecca Road from Evelyn Drive to Robin’s Hill Road is recommended. Heavy trucks will still have 24 hour access on Robin’s Hill Road from the east City Limit to Huron Street and on Rebecca Road from Robin’s Hill Road to the north City Limit.

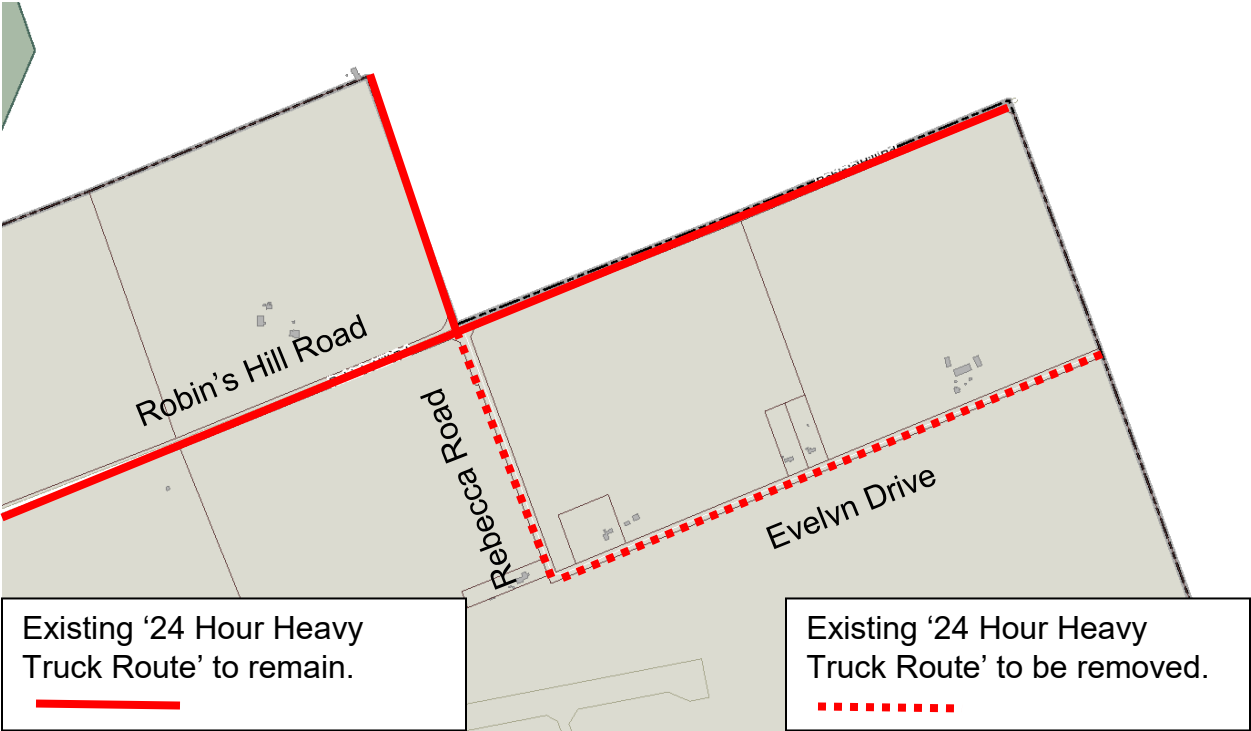


Figure 14: 24 Hour Heavy Truck Route

An amendment is required to Schedule 14 (Heavy Truck Restrictions) to address the above changes.

8. **School Zone Speed Limit**

It recommended that the speed limit be reduced to 40 km/h at the following locations as per the School Zone Speed Limit Policy approved by Council:

Al-Taqwa Academy Private School

Dumont Street	Merlin Crescent to Avondale Road
Edmonton Street	Wavell Street to Hilton Avenue
Merlin Crescent	Wavell Street to Dumont Street

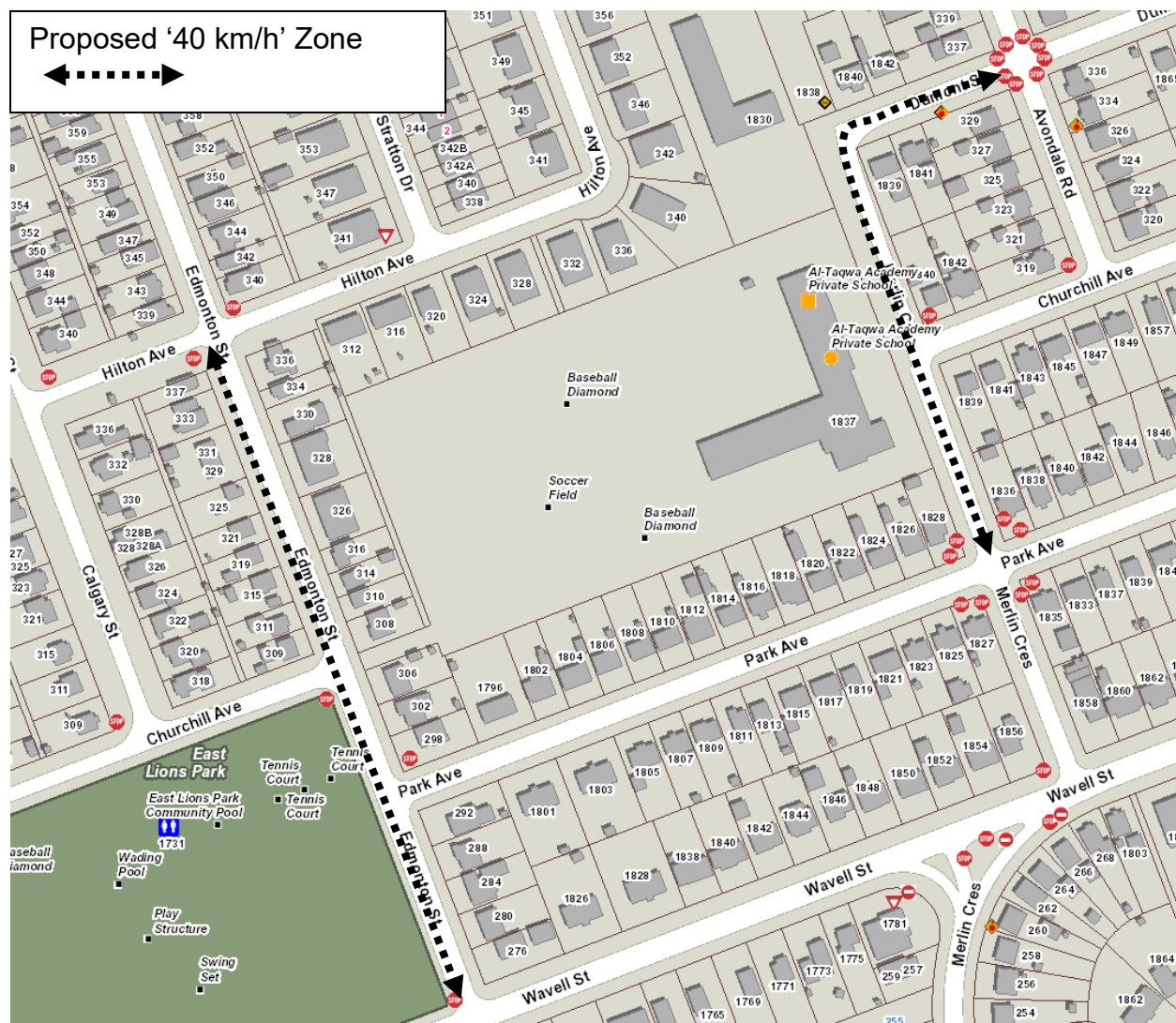


Figure 16: Al-Taqwa Academy Private School

Amendments are required to Schedule 17.1 (Lower Speed Limits) to address the above changes.

9. Designated Parking Space

Staff received a request to review the on-street parking on the south side of Dundas Street between Adelaide Street North and Lyle Street to accommodate the need for accessible parking. It is recommended to implement a designated parking space for disabled persons on the south side of Dundas Street from 74 m east of Adelaide Street North to 82m east of Adelaide Street North.

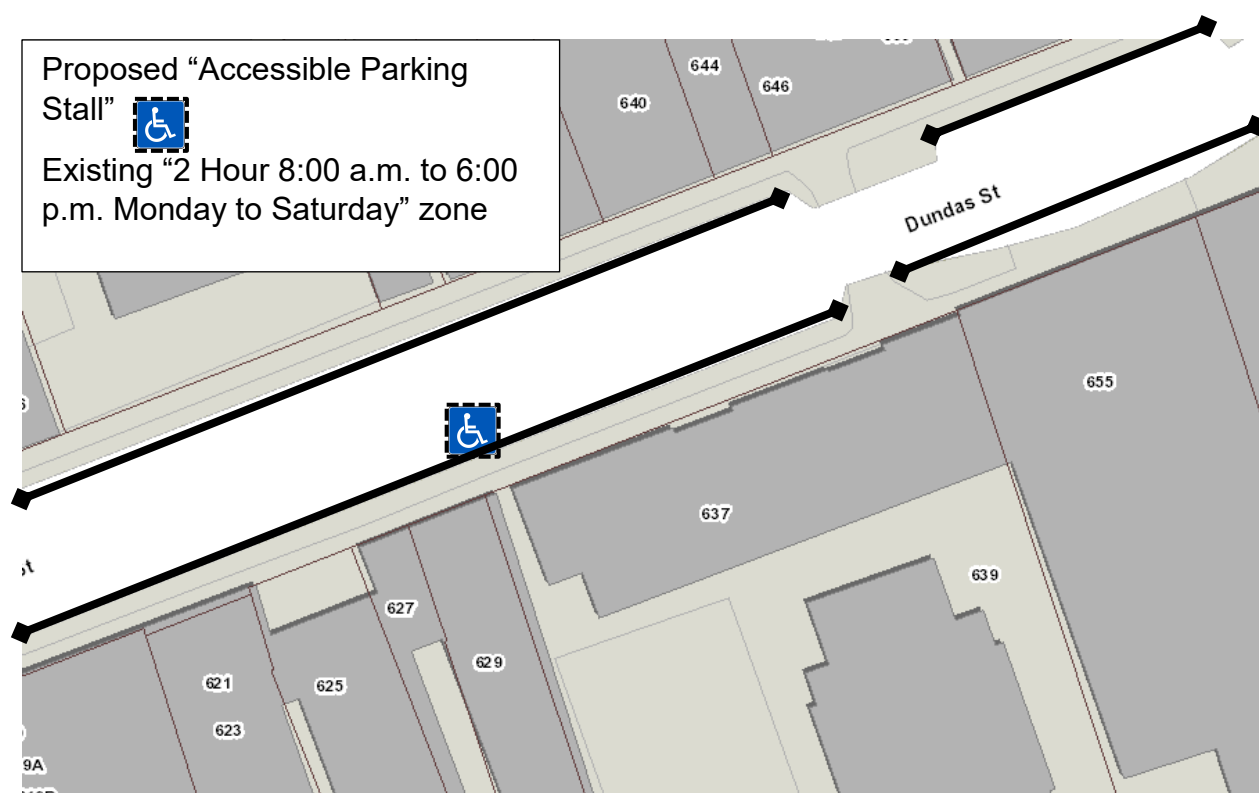


Figure 17: Dundas Street between Adelaide Street North and Lyle Street

An amendment is required to Schedule 27 (Designated Spaces – Disabled Persons) to address the above change.

10. **Metered Municipal and Public Parking Lots**

The private parking lot at 175-193 Mill Street, 52-64 St. George Street and 174-192 John Street has been added to the list of parking lots managed by the City for the property owner.

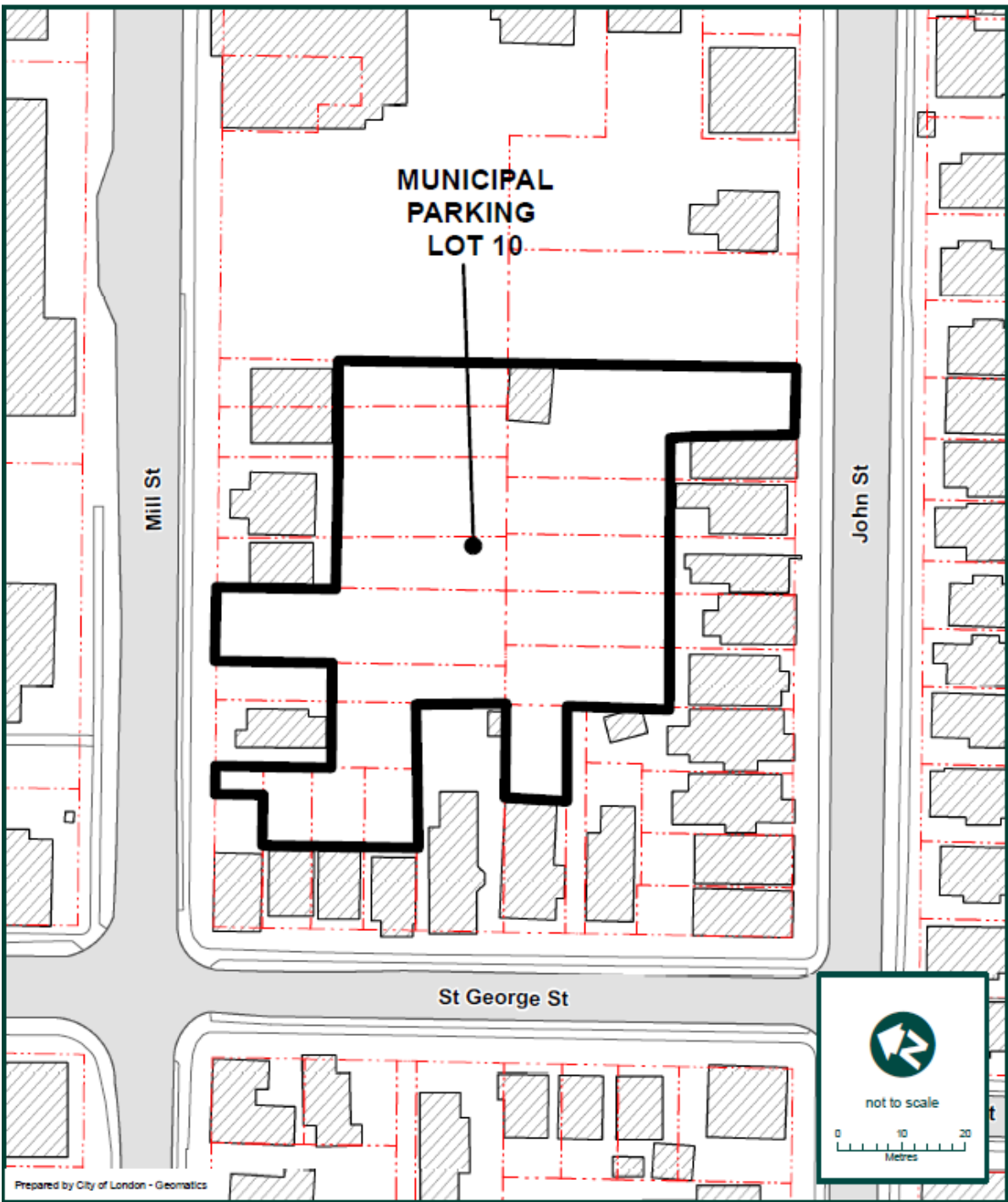


Figure 18: Municipal Lot 10

Amendments are required to Schedule 22 (Metered Off-street Municipal Parking Lots) and Schedule 30 (Metered Municipal and Public Parking Lots) to address the above change.

This report was prepared by Doug Bolton and Shane Maguire of the Roadway Lighting and Traffic Control Division.

PREPARED BY:	REVIEWED AND CONCURRED BY:
SHANE MAGUIRE, P. ENG. DIVISION MANAGER, ROADWAY LIGHTING AND TRAFFIC CONTROL	DOUG MACRAE, P.ENG., MPA DIRECTOR, ROADS AND TRANSPORTATION
RECOMMENDED BY:	
KELLY SCHERR, P.ENG., MBA, FEC MANAGING DIRECTOR, ENVIRONMENTAL AND ENGINEERING SERVICES AND CITY ENGINEER	

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February 11, 2019/db

Attach: Appendix A: Proposed Traffic and Parking By-Law Amendments

cc. City Solicitor’s Office
 Parking Office

APPENDIX A

BY-LAW TO AMEND THE TRAFFIC AND PARKING BY-LAW (PS-113)

Bill No.

By-law No. PS-113

A by-law to amend By-law PS-113 entitled, “A by-law to regulate traffic and the parking of motor vehicles in the City of London.”

WHEREAS subsection 10(2) paragraph 7. Of the *Municipal Act, 2001*, S.O. 2001, c.25, as amended, provides that a municipality may pass by-laws to provide any service or thing that the municipality considers necessary or desirable to the public;

AND WHEREAS subsection 5(3) of the *Municipal Act, 2001*, as amended, provides that a municipal power shall be exercised by by-law;

NOW THEREFORE the Municipal Council of The Corporation of the City of London enacts as follows

1. No Parking in Posted Locations

Section 10 No Parking In Posted Locations of the By-law PS-113 is hereby amended by **adding** the following:

- m) within 1 meter of a Canada Post Community Mail Box or where the Community Mail Box is set back from the roadway, within 1 meter of the point at which defines the outer edge of the Community Mail Box at the right angles to the edge of the roadway intersects such edge at any time, when the Community Mailbox is facing the roadway and the presence of sidewalk access is absent.

1. No Stopping

Schedule 1 (No Stopping) of the By-law PS-113 is hereby amended by **deleting** the following rows:

Charlotte Street	West	Dundas Street	Lorne Avenue	Anytime
Duchess Avenue	North	A point 52 m east of Cathcart Street	A point 113 m east of Cathcart Street	8:00 am to 4:00 pm Monday to Friday

Schedule 1 (No Stopping) of the By-law PS-113 is hereby amended by **adding** the following rows:

Duchess Avenue	North	A point 20 m east of Cathcart Street	A point 32 m east of Cathcart Street	8:00 a.m. to 4:00 p.m. Monday to Friday September 1 st to June 30 th
Duchess Avenue	North	A point 32 m east of Cathcart Street	A point 56 m east of Cathcart Street	8:15 a.m. to 9:15 a.m. and 3:00 p.m. to 4:00 p.m. Monday to Friday September 1 st to June 30 th

2. **No Parking**

Schedule 2 (No Parking) of the By-law PS-113 is hereby amended by **deleting** the following row:

Duchess Avenue	North	Cathcart Street	A point 52 m east of Cathcart Street	Anytime
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Schedule 2 (No Parking) of the By-law PS-113 is hereby amended by **adding** the following rows:

Frobisher Crescent	Both	Hudson Drive	A point 190 m north of Hudson Drive	8:00 a.m. to 4:00 p.m. Monday to Friday
Shore Road	North	A point 210 m west of Riverbend Road	A point 175 m west of Riverbend Road	Anytime
Shore Road	North	A point 128 m west of Riverbend Road	A point 55 m west of Riverbend Road	Anytime

3. **Prohibited Turns**

Schedule 8 (Prohibited Turns) of the By-law PS-113 is hereby amended by **adding** the following rows:

Ridout Street North at a point 98 m north of Albert Street with Blackfriars Street	Northbound	Right
Sarnia Road with Middlewoods Drive	Eastbound and Westbound	“U” Turn

4. **Reserved Lanes**

Schedule 9.1 (Reserved Lanes) of the By-law PS-113 is hereby amended by **adding** the following rows:

Blackfriars Street	a point 24 m east of Napier Street to Ridout Street N	1 st lane from the north	Anytime	Northbound	Bicycle
Ridout Street N	Blackfriars Street to Albert Street	1 st lane from the north	Anytime	Northbound	Bicycle

5. **Stop Signs**

Schedule 10 (Stop Sign Locations) of the By-law PS-113 is hereby amended by **adding** the following rows:

Westbound	Hillsborough Road	St. Lawrence Boulevard
Eastbound	King Edward Avenue	King Edward Avenue
Westbound	Penrith Crescent	Grasmere Crescent

Northbound	Ridout Street North	At a point 98 m north of Albert Street
Eastbound	Robin’s Hill Road	Crumlin Sideroad
Westbound	Scenic Drive	King Edward Avenue
Southbound	Smallman Drive	Carnforth Road
Northbound	Thirlmere Road	Carnforth Crescent

6. **Yield Signs**

Schedule 11 (Yield Sign Locations) of the By-law PS-113 is hereby amended by **deleting** the following rows:

Westbound	Hillsborough Road	St. Lawrence Boulevard
Westbound	Penrith Crescent	Grasmere Crescent
Eastbound	Robin’s Hill Road	Crumlin Sideroad
Southbound	Smallman Drive	Carnforth Road
Northbound	Thirlmere Road	Carnforth Crescent

7. **One-Way Streets**

Schedule 12 (One-Way Streets) of the By-law PS-113 is hereby amended by **adding** the following row:

Blackfriars Street	Napier Street	Ridout Street N	Eastbound
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8. **Pedestrian Crossovers**

Schedule 13.1 (Pedestrian Crossovers) of the By-law PS-113 is hereby amended by **adding** the following row:

Blackfriars Street	A point 130 m north of Albert Street
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9. **Heavy Truck Restrictions**

Schedule 14 (Heavy Truck Restrictions) of the By-law PS-113 is hereby amended by **deleting** the following rows:

Evelyn Road	Rebecca Road	East City Limits	24 Hours
Rebecca Road	North City Limit	Evelyn Road	24 Hours

Schedule 14 (Heavy Truck Restrictions) of the By-law PS-113 is hereby amended by **adding** the following row:

Rebecca Road	Robin's Hill Road	North City Limit	24 Hours
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10. **School Bus Loading Zones**

Schedule 16 (School Bus Loading Zones) of the By-law PS-113 is hereby amended by **deleting** the following row:

Duchess Avenue	North	A point 52 m east of Cathcart Street	A point 113 m east of Cathcart Street
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Schedule 16 (School Bus Loading Zones) of the By-law PS-113 is hereby amended by **adding** the following row:

Duchess Avenue	North	A point 20 m east of Cathcart Street	A point 56 m east of Cathcart Street
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11. **Lower Speed Limits**

Schedule 17.1 (Lower Speed Limit) of the PS-113 By-law is hereby amended by **adding** the following rows:

Dumont Street	Merlin Crescent	Avondale Road	40 km/h
Edmonton Street	Wavell Street	Hilton Avenue	40 km/h
Merlin Crescent	Park Avenue	Dumont Street	40 km/h

12. **Metered Off-street Municipal Parking Lots**

Schedule 22 (Metered Off-street municipal Parking Lots) of the said By-law PS-113 is hereby amended by inserting attached in Appendix ‘B’.

13. **Designated Parking Spaces - Disabled Persons**

Schedule 27 (Designated Parking Spaces – Disabled Persons) of the PS-113 By-law is hereby amended by **adding** the following row:

Dundas Street	South	From a point 74 m east of Adelaide Street North to a point 82 m east of Adelaide Street North	2 Hours
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14. **Metered Municipal and Public Parking Lots**

Schedule 30 (Metered Municipal and Public Parking Lots) of the PS-113 By-law is hereby amended by **deleting** the following row:

10

Schedule 30 (Metered Municipal and Public Parking Lots) of the PS-113 By-law is hereby amended by **adding** the following row:

10	Mill Street	The premises bounded by 175-193 Mill Street, 52-64 St. George Street and 174-192 John Street	130
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This by-law comes into force and effect on the day it is passed.

PASSED in Open Council on March 5, 2019

Ed Holder, Mayor

Catharine Saunders, City Clerk

First Reading – March 5, 2019

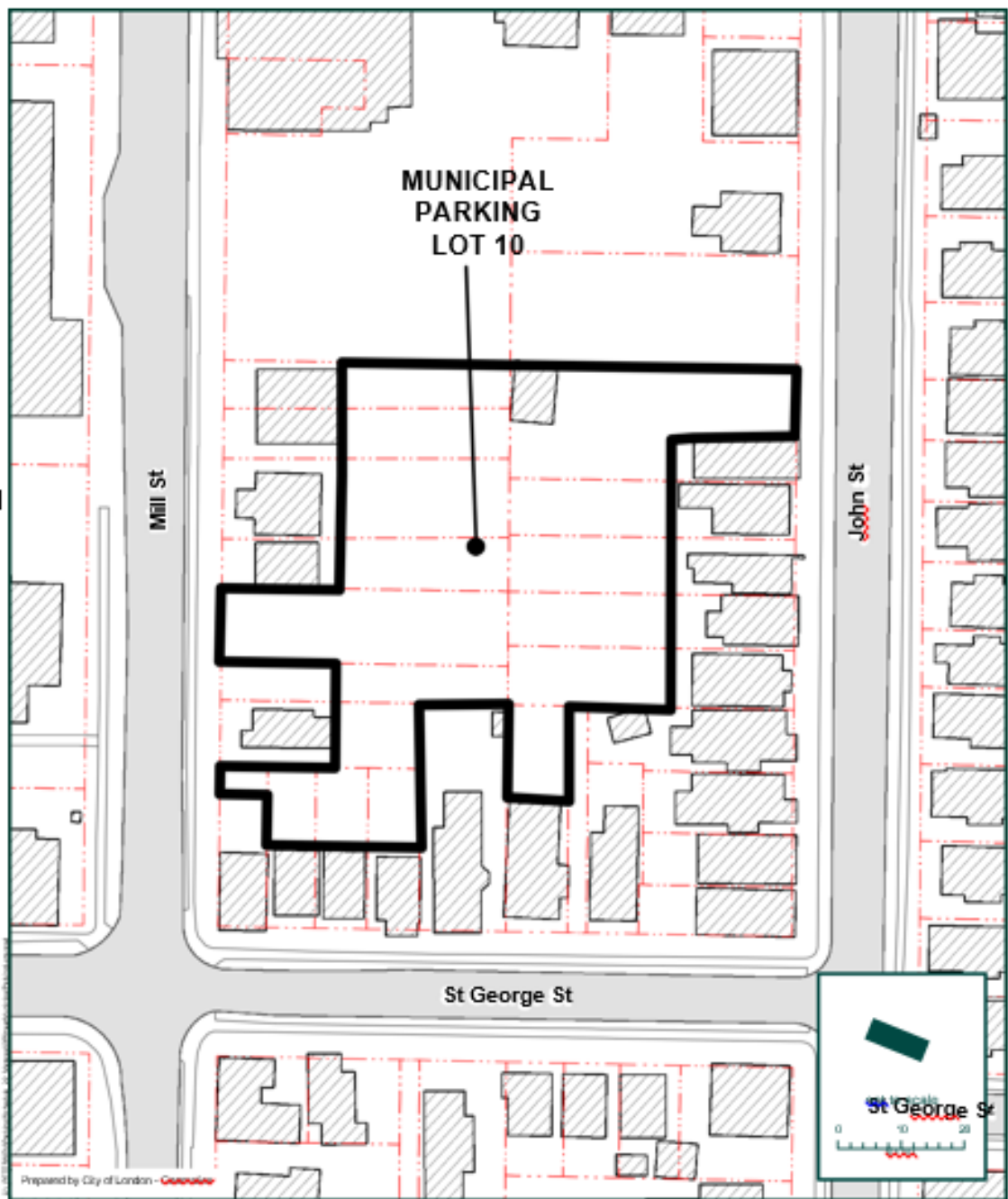
Second Reading – March 5, 2019

Third Reading – March 5, 2019

APPENDIX B

SCHEDULE 22
of
TRAFFIC & PARKING BY-LAW

METERED
OFF-STREET MUNICIPAL PARKING LOTS



 2017 Public Access PD Rev 0220 London, Ontario VLS v1.0 General Inquiries: 810-661-0220 www.london.ca	Parking Lot for: 175-193 Mill Street, 52-64 St. George Street, & 174-192 John Street	
	Date: Nov 08, 2018	Lot 10