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TO:	CHAIR AND MEMBERS BUILT AND NATURAL ENVIRONMENT COMMITTEE MEETING ON OCTOBER 17, 2011
FROM:	JOHN LUCAS, P.ENG. ACTING DIRECTOR, ROADS AND TRANSPORTATION PLANNING, ENVIRONMENTAL & ENGINEERING SERVICES
SUBJECT:	SUNNINGDALE ROAD – INTERIM MAINTENANCE STRATEGY

RECOMMENDATION

That, on the recommendation of the Acting Director, Roads and Transportation, Planning, Environmental and Engineering Services, the following report with respect to road interim maintenance strategies being implemented on Sunningdale Road **BE RECEIVED** for information.

PREVIOUS REPORTS PERTINENT TO THIS MATTER
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None

BACKGROUND

Purpose:

This report provides background to current pavement rehabilitation work on Sunningdale Road between Wonderland Road and Clarke Road.

Discussion:

Existing Situation

Sunningdale Road is an arterial road travelling east-west near the northerly limits of the City. It was annexed into the City of London in 1993. Previously it was under the jurisdiction of the Township of London. Some sections of the road have been reconstructed in conjunction with recent development activity, but the majority of the road still reflects its rural history:

- narrow roadway and shoulders;
- substandard profile with less than desirable sight distances at some locations;
- discontinuous sidewalks and streetlighting;
- substandard drainage;
- seasonal frost heaving; and,
- thin pavement structures in poor condition.

The City's Growth Management Implementation Strategy (GMIS) and DC Background Study identified improvements to Sunningdale Road in the area of greatest development related to traffic demands between Wonderland Road North and Adelaide Street North. Reconstruction and capacity improvements from Richmond Street to Adelaide Street were scheduled for 2014, and from Wonderland Road to Richmond Street in 2017. The Environmental Assessment for these projects is currently underway and identifies improvements to address all the deficiencies identified above. The EA is expected to be completed in the coming months.

The Sunningdale Road GMIS projects are funded predominantly from Development Charges at rates in the order of 71 to 86% depending on project phase. The balance of the funding is rate supported. The funding sources acknowledge that the projects are primarily driven by development but there are also life-cycle benefits to the infrastructure that occurs during a coordinated widening project.

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The Sunningdale Road GMIS projects, with the exception of two localized intersection improvements, were recently deferred for 10 years as part of a Development Charges By-Law OMB Appeal Settlement. The deferrals, along with increased usage by new development, are creating stresses on the existing infrastructure. In similar situations on Commissioners Road West (between Viscount and Wonderland) and Oxford Street West (between Hyde Park and Sanatorium), an asphalt rehabilitation was used to address poor road structure while delaying reconstruction and widening.

Maintenance Strategies

The Sunningdale Road pavement condition has been deteriorating as a result of growing traffic volumes. A geotechnical investigation was undertaken which indicated the pavements comprise thin structures typical of a low-volume rural road and inadequate for current or future use. Maintenance strategies with life expectancies of between 7 and 9 years are being applied to the poorest locations to maintain acceptable levels of service. These strategies are routinely used by the Department, often to align urgent road needs with future major construction that could also involve other utilities. The work predominantly includes milling & paving a layer of asphalt, and localized installation of sub drains to assist with road drainage. The locations for road rehabilitation are shown in Appendix 'A'.

The current construction is being funded from the 2011 Arterial Road Rehabilitation program which addresses Life Cycle maintenance of our arterial roads. Sunningdale Road was not anticipated to be rehabilitated in 2011, but the deterioration of the road has accelerated. Due to favourable tenders this year, unallocated funds are available within the program. The Sunningdale work totals about \$650,000, which is 6% of the program budget.

Road capacity is becoming critical at intersections first. This need is addressed by growth projects that have not been deferred. New intersections created by development will include turn lanes and localized lighting for safety. These improvements will usually be temporary and replaced during the future growth projects due to the subsequent construction of sewers and changes in horizontal and vertical alignment.

Pedestrian volumes are also increasing and sidewalks have been installed in some areas adjacent to developments. Upon completion of the Sunningdale Road EA, the corridor will be reviewed and prioritized for sidewalk implementation, particularly to address links where gaps exist. Sidewalks would be constructed at the permanent location where possible, but this is challenging where profile changes are proposed or where existing ditches preclude it. There may be locations that would warrant a temporary sidewalk and others where a sidewalk will have to wait for the future reconstruction program.

The corridor has been reviewed for coordination of future underground service installations. There are isolated locations where utilities will cross the road, but nothing substantial along its length.

A bicycle primary commuter route is proposed on Sunningdale Road from Wonderland Road to Highbury Avenue in the Bicycle Master Plan. Bicycle lanes will be implemented in the growth widening projects.

Transit routing in the area primarily follows nearby collector roads. London Transit's 38-Stoney Creek route travels Sunningdale Road at two locations with only one stop on Sunningdale Road. Future transit needs, such as bus stop improvements, will be addressed as the need arises.

Conclusions:

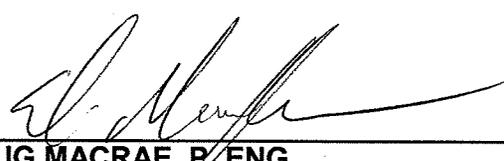
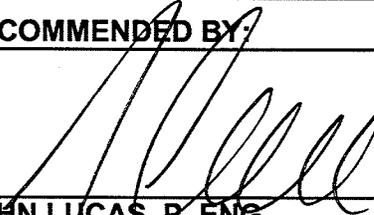
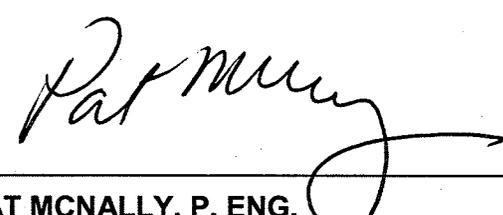
Continued development adjacent to Sunningdale Road combined with the deferral of GMIS projects is creating stresses on the existing transportation infrastructure and resulting in deficiencies as this corridor shifts from rural to urban use. Interim maintenance strategies are being implemented to bridge the gap and relieve poor existing conditions until future reconstruction.

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Localized pavement rehabilitation is being implemented this fall with unallocated funding from the 2011 Arterial Road Rehabilitation program which does not affect the planned program for this year. Other road user needs, including pedestrian and transit, will be reviewed as needs arise and with regard for the Sunningdale Road (Wonderland Road to Adelaide Street) EA that is nearing completion. Implementation could be either in conjunction with development agreements or by City Services Reserve Fund minor works installations.

Acknowledgements:

This report was prepared with assistance from Karl Grabowski, P.Eng. of the Transportation Planning & Design Division.

PREPARED BY:	RECOMMENDED BY:
	
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REVIEWED & CONCURRED BY:	
	
PAT MCNALLY, P. ENG. EXECUTIVE DIRECTOR, PLANNING, ENVIRONMENTAL AND ENGINEERING SERVICES DEPARTMENT	

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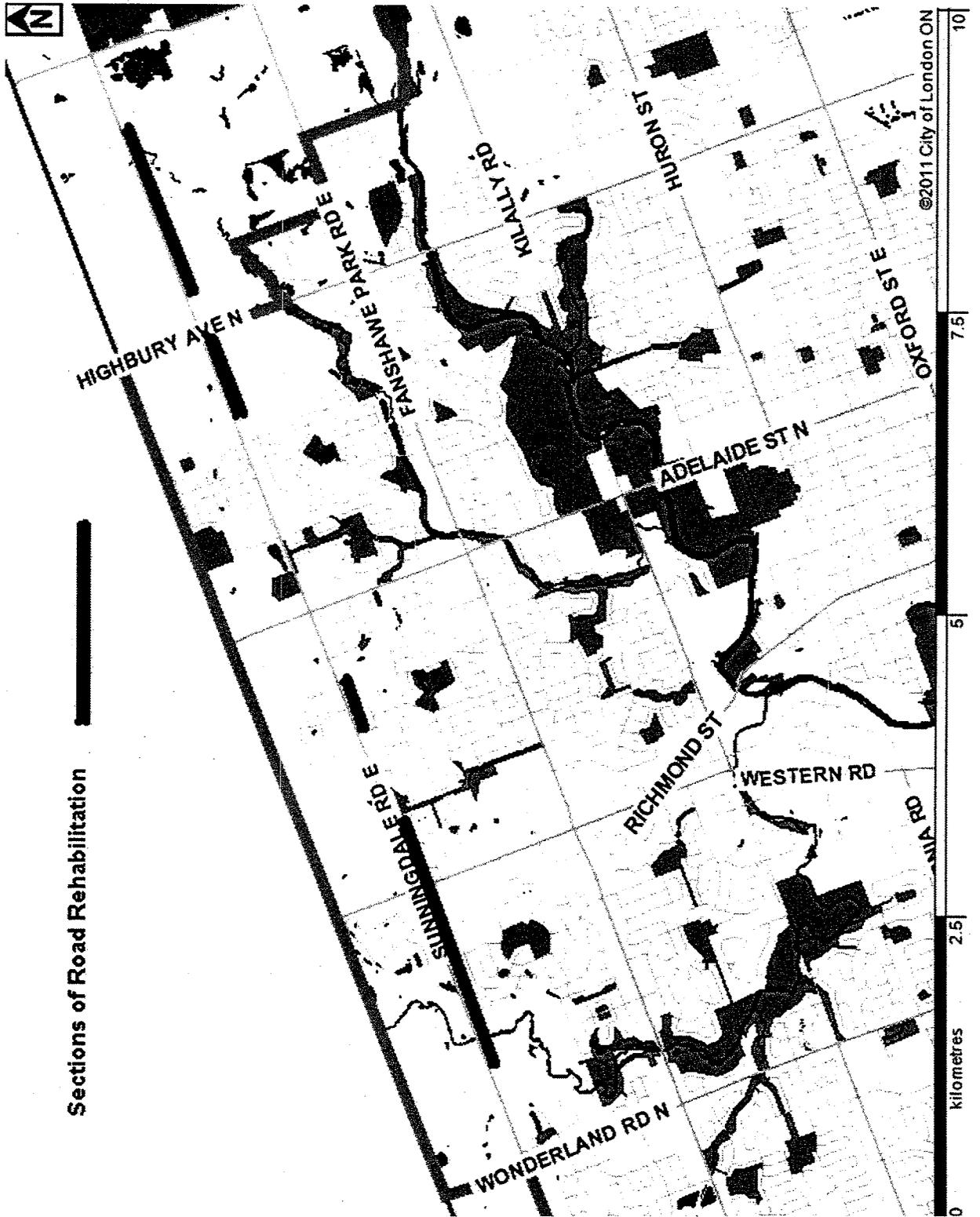
Appendix 'A' – Road Rehabilitation Locations on Sunningdale Road

- c: John Braam
- John Parsons

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Appendix 'A'

Road Rehabilitation Locations on Sunningdale Road



Sections of Road Rehabilitation