

## Report to Planning and Environment Committee

**To:** Chair and Members  
Planning & Environment Committee

**From:** George Kotsifas, P. Eng.  
Managing Director, Development and Compliance Services  
And Chief Building Official

**Subject:** Public Participation Meeting Report  
31675 Ontario Ltd (York Developments Inc)  
3080 Bostwick Road

**Public Participation Meeting on: October 9, 2018**

## Recommendation

That, on the recommendation of the Senior Planner, Development Services, the following actions be taken with respect to the application of 31675 Ontario Ltd (York Developments Inc) relating to a portion of the property located at 3080 Bostwick Road:

- (a) The comments received from the public during the Public Engagement process attached as Appendix "A" to the staff report dated September 28, 2018, **BE RECEIVED**
- (b) **IT BEING NOTED** that staff will continue to process the application and will consider the public, agency, and other feedback received during the review of the subject application as part of the staff evaluation of the subject application.

## Executive Summary

### Summary of Request

The requested amendment is to permit a draft plan of subdivision and Zoning By-law Amendments to allow for two development blocks, three new roads, a park block, an open space block and lands identified for future development.

### Purpose and the Effect of Recommended Action

The purpose and effect of the recommended action is to:

- i) Present the details of the requested amendment in conjunction with the statutory Public Meeting;
- ii) Preserve the appeal rights of the public and ensure the Municipal Council has had the opportunity to review the requested draft plan of subdivision and Zoning By-law Amendments prior to the expiration of the 180 day timeframe legislated for draft plan of subdivision and accompanying *Planning Act* applications;
- iii) Introduce the proposed development and identify matters raised to-date through the technical review and public consultation period; and
- iv) Bring forward a future recommendation report for consideration by the Planning and Environment Committee, once the technical review is complete.

## Analysis

### 1.0 Site at a Glance

#### 1.1 Property Description

The subject site consists of 5.8ha of vacant land, which also forms part of a larger parcel of land owned by the applicant (approximately 15ha) with frontage on Southdale Road West and Bostwick Road. The portion of the site that is the subject of the draft plan of subdivision and Zoning By-law Amendments is located south, southeast and southwest of the Bostwick Community Centre and the proposed development blocks of Sites 1, 3 and 5, which are the subject of separate *Planning Act* application sites.



Figure 1: Proposed Master Development Plan

#### 1.2 Current Planning Information (see more detail in Appendix C)

- Official Plan Designation – Multi-Family, High Density Residential (MFHDR) & Open Space (OS)
- The London Plan Place Type – Neighbourhoods, Green Space & High Density Residential Overlay
- Southwest Area Plan Designation – Multi-Family, High Density Residential (MFHDR), Open Space and Environmental Review
- Existing Zoning – Urban Reserve (UR4) Zone, Open Space (OS4) Zone, and Environmental Review (ER) Zone

#### 1.3 Site Characteristics

- Current Land Use – vacant
- Frontage – 84m (Bostwick Road)
- Depth – varies
- Area – 5.8ha
- Shape – Irregular

#### 1.4 Surrounding Land Uses

- North – Residential
- East – Vacant land & Future Place of Worship
- South – Vacant
- West – Vacant & Agricultural

1.5 Intensification (identify proposed number of units)

- 504 residential units are being proposed within the subject site which is located outside of the Built-area Boundary, and Primary Transit Area

1.6 Location Map



## 1.7 OZ-8941 (Site 1), Z-8942 (Site 3), and OZ-8943 (Site 5)

There are three separate Development Proposals for Sites 1, 3 and 5 which currently form part of the subject lands, but are not part of the draft plan of subdivision and Zoning By-law Amendment application 39T-18502/Z-8931. These three sites are part of separate Official Plan and Zoning By-law Amendments under consideration for development as well as three separate consent applications to create the lots, outside of the subject draft plan of subdivision proposal.

## 2.0 Description of Proposal

### 2.1 Development Proposal 39T-18502/Z-8931

The draft plan of subdivision has three new roads proposed (Street A, Street B and Street C), as part of the draft plan. A roundabout is proposed at the intersection of Street B and Street C.

The open space block (Block 11) is part of the Thornicroft Drain which is proposed to be zoned Open Space (OS4) permitting passive recreation and conservation activities. The park block (Block 4) is being proposed as a new Community Park located east of Street A and north of Street C, and will be zoned Open Space (OS2) to allow for a wide range of active recreational activities.

Lands to the south of Street C are being reserved for future development. The existing Urban Reserve (UR4) Zone will be retained, and special provisions are proposed to recognize reduced lot size and frontage.

Two development blocks (Blocks 2 & 6) are both proposed for high density residential development. Block 2 is located at the southwest portion of the site; east of Bostwick Road, north of Street C, and south of Site 1. Block 6 is located at the southeast portion of the site; east of Street B and south of Site 5.

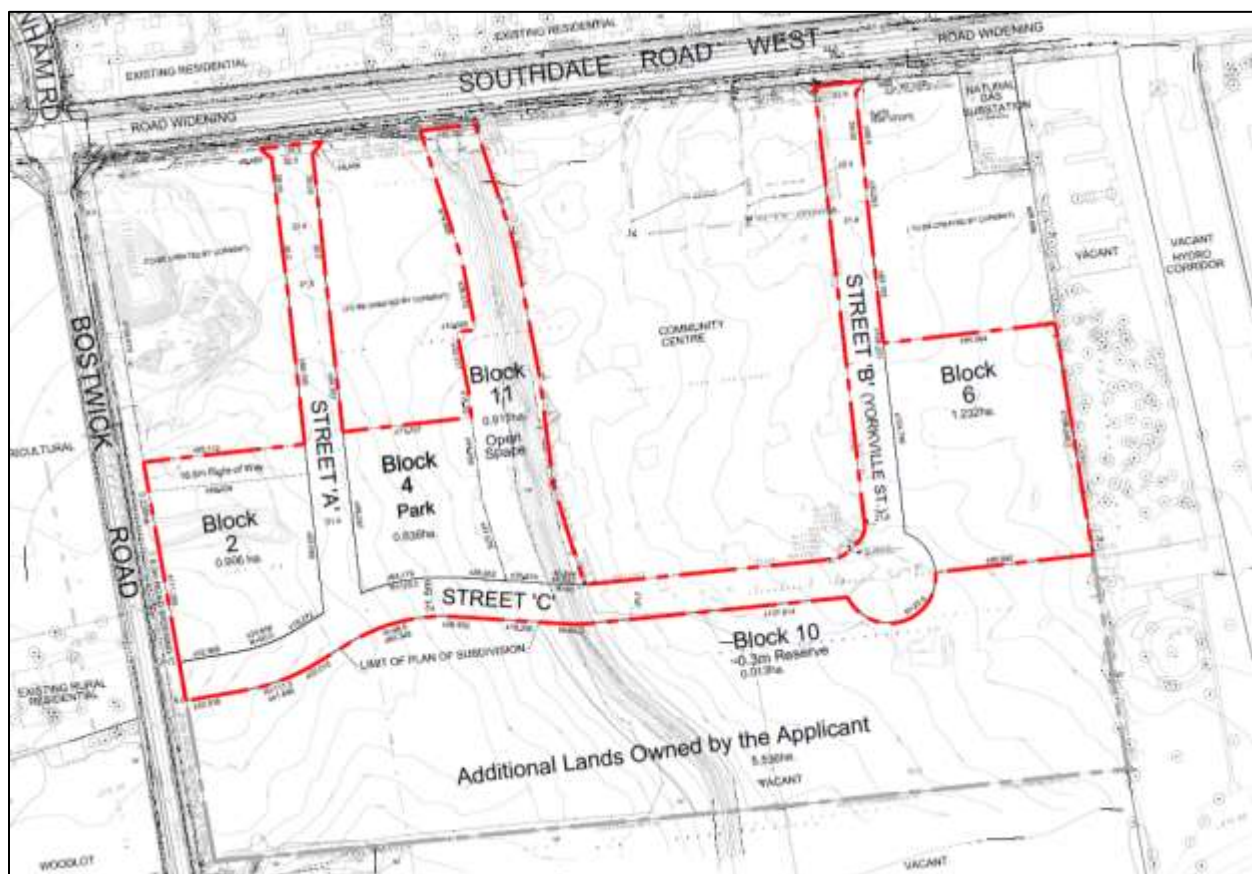


Figure 2: Proposed Draft Plan of Subdivision 39T-18502/Z-8931

## 2.2 Block 2

A site specific bonus zone is requested for Block 2 to permit the development for an 18 storey (70m) residential tower along Bostwick Road, and two 3.5 storey stacked townhouse blocks located along the eastern boundary of the site.



Figure 3: Conceptual Site Plan – Block 2

There are a total of 174 residential units proposed and a total density request of 193 units per hectare. Special provisions are requested to allow a minimum front yard setback of 6m, a height of 13m for the proposed stacked townhouses, and an exterior side yard setback of 0.4m and a rear yard setback of 22m for the proposed apartment building.



Figure 4: Conceptual Rendering – Block 2

A total of 254 parking spaces are proposed to support this development proposal consisting of 28 surface and 226 underground spaces. The access for Block 2 is

proposed from Street C, and a future east-west access is proposed to the north of the site.

### 2.3 Block 6

A site specific bonus zone is requested for Block 6 to permit the development of two 17 storey (68m) residential towers. The towers are connected by a 4 storey building in the middle and step down to 15 storeys along the north and east building edges.

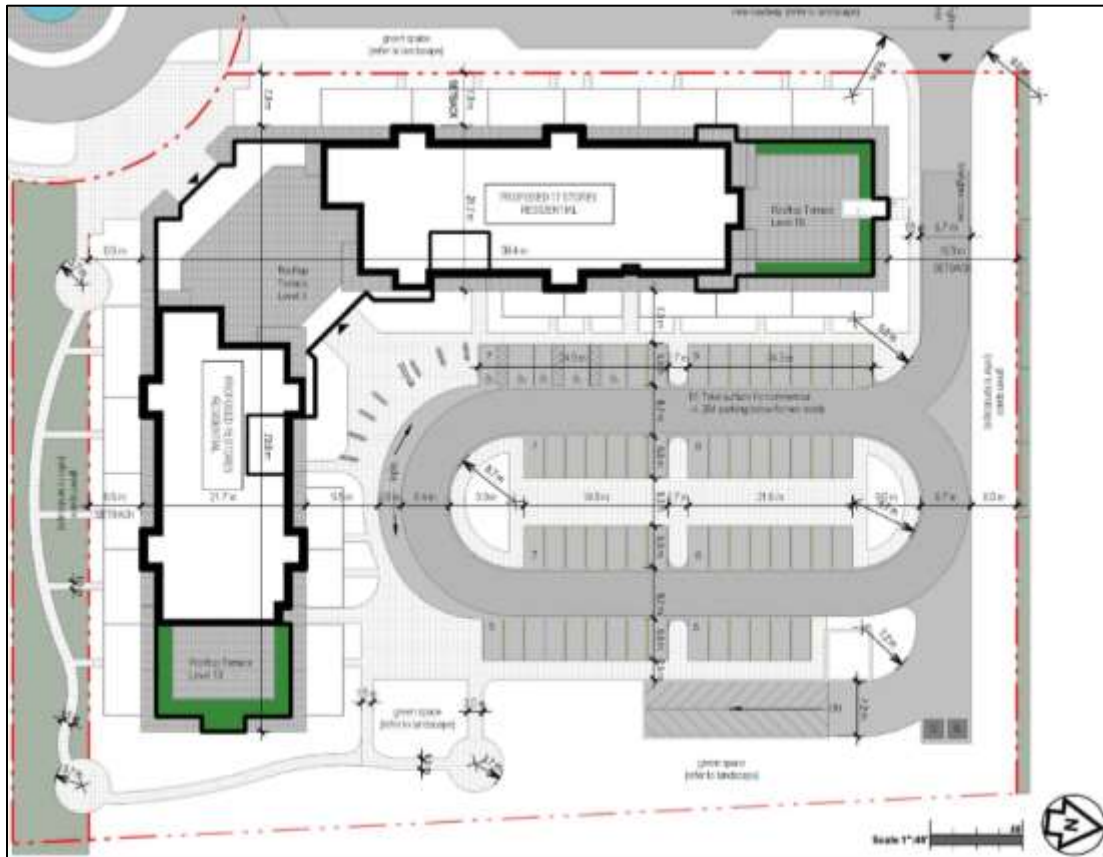


Figure 5: Conceptual Site Plan – Block 6

There are a total of 330 residential units proposed and a total density request of 269 units per hectare. Special provisions are being requested to permit a minimum front yard setback of 6.5m, an interior side yard setback of 12m, a rear yard setback of 12m, and a reduced number of parking spaces.



Figure 6: Conceptual Rendering Block 6 – Southeast View

A total of 325 parking spaces are proposed which is a reduction of 88 spaces from the minimum 413 that the by-law requires. The access for Block 6 is proposed to the north of the site from Street B that leads to 61 surface and 264 underground spaces. Both development blocks (Blocks 2 & 6) are proposing increased height and density through two separate site-specific bonus zones.

The proposed bonusable facilities, services or matters include:

- To support the provision of common open space that is functional for active or passive recreational use;
- To support the provision of underground parking;
- To encourage aesthetically attractive residential development through the enhanced provision of landscaped open space;
- To support innovative and environmentally sensitive development which incorporates notable design features, promotes energy conservation, waste and water recycling and use of public transit; and,
- To support the provision of design features that provide for universal accessibility in new construction and/or redevelopment

## 2.4 Submitted Studies

A number of reports and studies were submitted to support the requested amendment, including:

- Transportation Impact Assessment
- Urban Design Brief
- Final Proposal Review
- Sanitary Servicing and Feasibility Analysis
- Preliminary Geotechnical Investigation
- Storm Drainage and Stormwater Management Plan
- Environmental Impact Study
- Archaeological Assessment
- Hydrogeological and Water Balance Analysis
- Drain Erosion Assessment

## 2.5 Requested Amendment

The requested amendment to the Zoning By-law is intended to permit a high density residential form of development, with bonus zoning proposed for the two development blocks, and open space zones within the proposed park block and the limits of the Thornicroft Drain lands. The requested Amendments from an Urban Reserve (UR4) Zone and an Environmental Review (ER) Zone to:

Block 2: Residential R9 Bonus (R9-7\*B-(#)) Zone – to permit apartment buildings, lodging house class 2, senior citizens apartment buildings, handicapped persons apartment buildings, and continuum-of-care facilities. A bonus zone is requested to permit townhouses and stacked townhouses with a maximum height of 13m and a minimum front yard setback of 6m; an apartment building with a maximum height of 70m, a density of 193 units per hectare, a reduced exterior side yard setback of 0.4m, a reduced rear yard setback of 22m;

Block 6: Residential R9 Bonus (R9-7\*B-(##)) Zone – to permit apartment buildings, lodging house class 2, senior citizens apartment buildings, handicapped persons apartment buildings, and continuum-of-care facilities. A bonus zone is requested to permit townhouses, an apartment building with a maximum height of 68m, a density of 269 units per hectare, a minimum front yard setback of 6.5m, a reduced interior side yard setback of 12m, a reduced rear yard setback of 12m, and a reduced number of parking spaces (with 325 spaces provided);

Block 4: Open Space (OS2) Zone – to permit conservation lands, conservation works, cultivation of land for agricultural/horticultural purposes, golf courses, private parks, public parks, recreational golf courses, recreational buildings associated with

conservation lands and public parks, campground, and managed forest; commercial recreational establishments, community centres, institutions, private outdoor recreation clubs, public swimming pools, recreational buildings, riding stables, sports fields, golf driving range, miniature golf course, go kart track, batting cages, tennis court and playground;

Block 11: Open Space (OS4) Zone – to permit conservation lands, conservation works, golf courses, private parks, public parks, recreational golf courses cultivation or use of land for agricultural/horticultural purposes, and sports fields without structures;

Future Development Lands: Urban Reserve Special Provision UR4( ) Zone – to permit existing dwellings, agricultural uses, conservation lands, managed woodlots, wayside put, passive recreation uses, kennels, private outdoor recreation clubs, and riding stables with a special provision for a reduced lot size of 2ha.

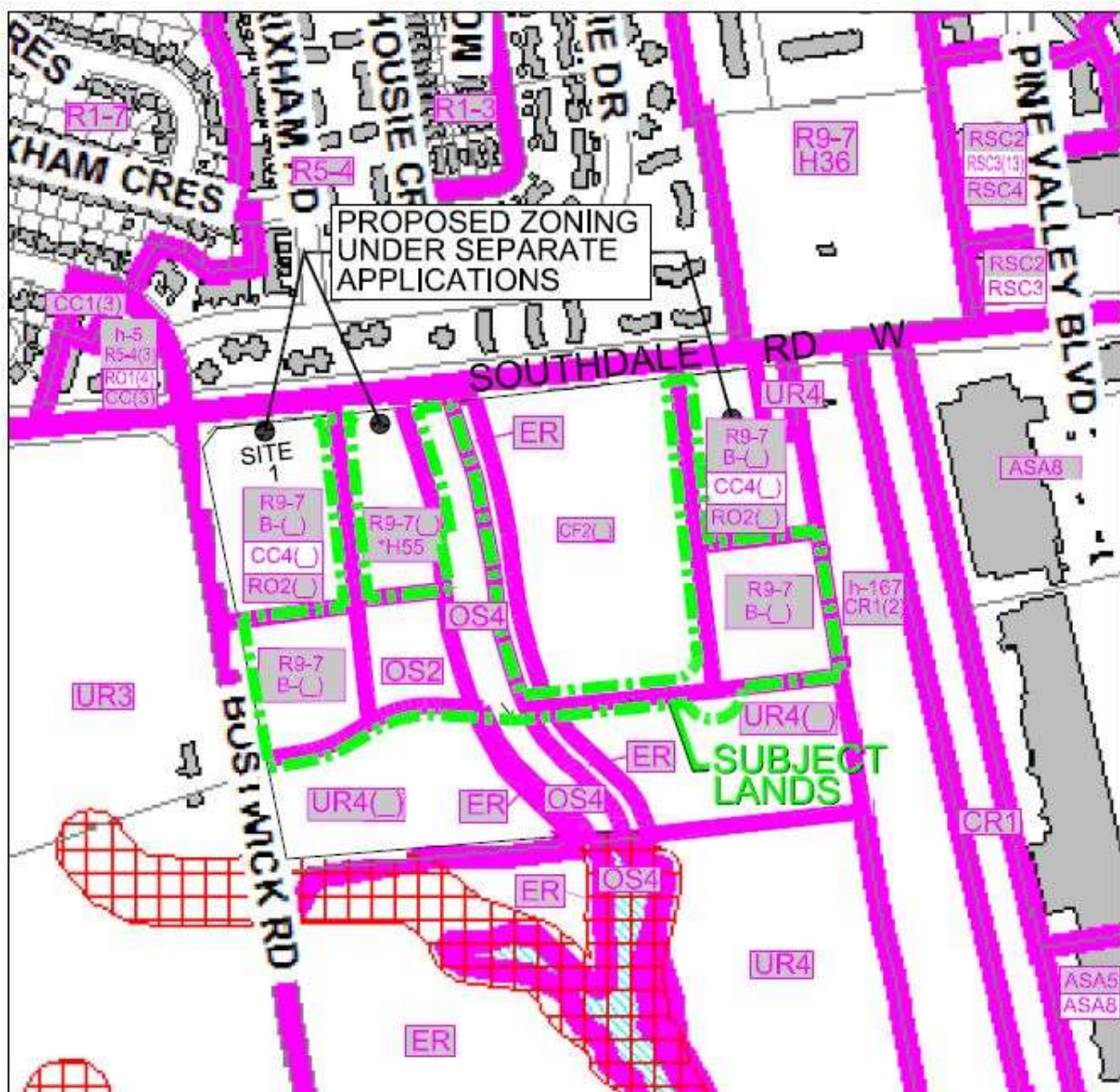


Figure 7: Proposed Zoning Amendment Map

### 3.0 Relevant Background

#### 3.1 Planning History

The subject lands previously formed part of the Town of Westminster which were annexed into the City of London in 1993. The lands were designated “Urban Reserve – Community Growth” and “Environmental Review” in 1996 when the Official Plan amendment for the annexed area was adopted.



In 2004, the current owner of 3080 Bostwick Road, in its entirety, applied for an Official Plan and Zoning By-law amendment (OZ-6662) to allow for a range of commercial and residential development on the lands. That planning application was considered to be premature in the absence of a comprehensive plan for the area, and was put 'on hold' to allow for the completion of the Southwest Area Secondary Plan. The Southwest Area Secondary Plan came into effect on April 29, 2014 (OPA No. 541) following an Ontario Municipal Board hearing. Recent amendments to the Plan were undertaken in April of 2017 to incorporate referenced policies from the 1989 Official Plan prior to the full London Plan coming into effect.

At the time of the draft plan in 2012, the recommended designation of the subject lands was for Multi-Family, Medium Density Residential. During the review of the SWAP, the owner requested a Multi-Family, High Density Residential designation instead of the Medium Density recommended, which was endorsed by the Planning and Environment Committee on October 15, 2012 as follows:

*ix) the portion of the property located at 3080 Bostwick Road west of the open space be designated "Multi-Family, High Density Residential"*

In 2014, a portion of the lands was the subject of a Zoning By-law Amendment Application (Z-8386) to facilitate development of the Bostwick Community Centre. A local road connection was created along the easterly boundary of the Community Centre lands and Municipal Services were extended along Southdale Road to support the Community Centre.

### **3.2 Community Engagement (see more detail in Appendix A)**

Notice of Application was circulated on August 17, 2018, and notice was published in the Londoner on August 16, 2018. There were 7 responses provided through the community consultation period. A summary of the comments include:

Concern for:

- Increased traffic and congestion (x5)
- Increased cut through traffic in the established neighbourhood to the north (x3)
- Pedestrian safety
- Road improvements should be implemented as recommended in the Southdale EA (x3)
- Only the ward 9 councillor was identified on the notice, not the nearby ward 10
- The local school capacity and ability to accommodate increased number of pupils (x2)
- Greater building heights are difficult to evacuate in emergencies and may block satellite signals
- Provide convenient drop-off/pick-up spaces for para transit vehicles
- Provide affordable housing options and small-lot, small home options

Support for:

- Positive to see the site finally develop
- Interest in investing in the project

### **3.3 Policy Context (see more detail in Appendix B)**

#### **Provincial Policy Statement, 2014**

The Provincial Policy Statement (PPS) 2014, provides policy direction on matters of provincial interest related to land use planning and development. The following policies support efficient and resilient development patterns through a range of uses and efficient use of land:

*Healthy, liveable and safe communities are sustained by accommodating an appropriate range and mix of residential (including second units, affordable housing and*

*housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs (1.1.1 b); Land use patterns within settlement areas shall be based on: a) densities and a mix of land uses which: 1) efficiently use land and resources; 2) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion (1.1.3.2); and Long-term economic prosperity should be supported by encouraging a sense of place,[and] by promoting well-designed built form (1.7.1. d).*  
In accordance with section 3 of the *Planning Act*, all planning decisions 'shall be consistent with the PPS'.

### **The London Plan**

The London Plan directs that all of the relevant policies of the Plan that relate to a planning and development applications should be read in their entirety and form the basis for evaluating consistency with the Plan (1577-1578). Proposed plans of subdivision will be evaluated based on all of the policies of The London Plan, including such policies as (1688):

1. Our Strategy
2. Our City
3. City Building Policies
4. Our Tools
5. Place Type Policies
6. Availability of Municipal Services
7. Potential impacts on adjacent and nearby properties
8. The degree to which the proposal fits within its context and policy goals
9. Relevant secondary plans and specific policies
10. Relevant guideline documents

### **Our Strategy**

Relevant planning strategies to support key directions to guide planning and development include the following:

*Ensure a mix of housing types within our neighbourhoods so that they are complete and support aging in place (59.5);  
Link land use and transportation plans to ensure they are integrated and mutually supportive (60.4);  
Design complete neighbourhoods by meeting the needs of people of all ages, incomes and abilities, allowing for aging in place and accessibility to amenities, facilities and services (61.2); and  
Implement "placemaking" by promoting neighbourhood design that creates safe, diverse, walkable, healthy, and connected communities, creating a sense of place and character (61.3).*

### **Our City – City Structure Plan**

The City Structure Plan provides a framework for London's growth and change for the future, including the following policies:

*The London Plan places an emphasis on growing "inward and upward" to achieve a compact form of development. This should not be interpreted to mean that greenfield forms of development will not be permitted, but rather there will be a greater emphasis on encouraging and supporting growth within the existing builtup area of the city (79); It is a target of this Plan that a minimum of 45% of all new residential development will be achieved within the Built-Area Boundary of the city, as defined by Figure 2. For the purposes of this Plan, this will be referred to as the "intensification target". The Built-Area Boundary is defined generally as the line circumscribing all lands that were substantively built out as of 2006. This boundary will be used on an on-going basis to*

*monitor intensification and will not change over time (81); and Subject to the Place Type, City Design, Our Tools and other relevant policies of this Plan, the most intense forms of development will be directed to the Downtown, Transit Villages, and at station locations along the Rapid Transit Corridors, where they can be most effective in meeting multiple objectives of this Plan (86).*

### City Building Policies

The City Building Policies provide over-arching direction for how the City grows, including the following:

*Development that is designed to be a good fit and compatible within its context (193); The built form will be designed to have a sense of place and character consistent with the planned vision of the place type, by using such things as topography, street patterns, lotting patterns and streetscapes (197); Throughout this Plan we have recognized that mobility and land use are inextricably linked. The design of a street and its associated public right-of-way will have a large impact on the use, intensity and form of development that can be supported along any corridor. In this way, how we plan our streets will dictate the quality of our neighbourhoods, our ability to facilitate positive infill and intensification along rapid transit corridors, and our success in promoting and supporting a viable transit system. It will also establish our ability to move people, goods, and services efficiently from one location to another within the city and to other parts of the world (309); and Utilize rapid transit services to strategically promote and stimulate intensification and support our growth management policies (313.3); To achieve a high level of connectivity that can support all forms of mobility, street networks within new neighbourhoods will be evaluated for their connectivity ratio. A ratio of 1.5 or higher will be used as a target (323); and Neighbourhoods should be designed to use public spaces and parks to serve as mobility linkages through and between neighbourhoods (333).*

### Neighbourhoods Place Type

The subject site is within the Neighbourhoods Place Type in The London Plan and located with frontage on two Civic Boulevards. The range of permitted uses include: single detached, semi-detached, townhouses, triplexes, small-scale community facilities, stacked townhouses, fourplexes, and low-rise apartment buildings. The development form is intended between a minimum of 2 storeys and a maximum of 4 storeys, with a potential to bonus up to 6 storeys (Tables 10-12).

### High Density Residential Overlay

The London Plan directs higher density uses towards strategic locations to support and take advantage of public transit, such as in transit villages and along rapid transit corridors; though also recognizes some remnant high density residential areas (954). The subject lands are designated in the 1989 Official Plan as High Density Residential, which are recognized in the High Density Residential (HDR) Overlay and retain greater development potential despite not being in a targeted growth location (955).

Lands located within the High Density Residential Overlay but outside of the Primary Transit Area may be permitted to develop up to 12 storeys with a density up to 150 units per hectare. On large sites or areas within the High Density Residential Overlay, capable of accommodating multiple buildings, a diversity of housing forms such as mid-rise and low-rise apartments and multiple attached dwellings will be required. Zoning may not allow for the full range of height and density identified in these policies. (958.2,3 & 5).

### 1989 Official Plan

The subject site is within the Multi-family, High Density Residential (MFHDR) designation, which primarily permits multiple-attached dwellings, and low and high-rise

apartment buildings (3.4.1).

### **Southwest Area Secondary Plan (SWAP)**

Both The London Plan and the 1989 Official Plan recognize the need for a Secondary Plan to provide more detailed policy guidance for a specific area that goes beyond the general policies. The Southwest Area Secondary Plan (SWAP) forms part of The London Plan and the 1989 Official Plan, and its policies prevail over the more general Official Plan policies if there is a conflict (1556 & 1558). The Secondary Plan serves as a basis for the review of planning applications, which will be used in conjunction with the other policies of the Official Plan. While the Plan is to be read and applied in its entirety, the most relevant policies for the consideration of the requested amendment include the following:

#### **20.5.1.4 Principles of the Secondary Plan**

The Southwest Area Plan is guided by a series of objectives and principles. Any amendments to the Secondary Plan shall be consistent with the following principles:

##### 20.5.1.4.i) Creation of a Diverse and Connected Community

- a) Provide for a range of land uses including residential, open space, public, commercial, office and mixed-uses and community facilities.*
- d) Provide for daily needs without reliance on a car;*
- j) Design the community street pattern to create or enhance view corridors.*

##### 20.5.1.4 ii) A Range of Housing Choices

- a) Ensure that a range and mix of housing types is provided within developments to achieve a balanced and inclusive residential community;*
- b) Ensure that housing developments and designs achieve compact residential development;*
- e) Provide opportunities for live-work opportunities to reduce the need for commuting; and*
- f) Provide affordable housing opportunities.*

##### 20.5.1.4 iv) A Green and Attractive Environment

- a) Protect and enhance natural heritage features such as woodlands, wetlands, river and creek systems*
- b) Develop publicly owned open spaces into linear parks with generous buffers to built areas.*
- c) Enhance livable neighbourhood ideals using public green spaces and urban squares/parkettes as significant design features and by designing walkable neighbourhoods.*
- d) Encourage development patterns that provide extensive visual and physical public access to natural features, provided there is minimal impact to the Natural Heritage System.*
- h) Encourage a built form and site design that is attractive and supportive of alternative modes of transportation.*

##### 20.5.1.4 v) A model of Sustainable Growth Management

- a) Extend infrastructure in a logical and cost-effective manner;*
- c) Design a road network of walkable connected streets and neighbourhoods;*
- e) Establish a high degree of connectivity between residential, open space, commercial and institutional uses within and between existing and new neighbourhoods; and*
- f) Ensure the use of housing densities and efficient development patterns that minimize land consumption and servicing costs.*

### 20.5.2 Community Structure Plan

The Community Structure Plan assists with implementing the vision for the built form, public realm and neighbourhood street pattern, including the following objectives:

- i) development patterns shall generally reflect a fine urban grid street network with a high level of connectivity;*
- iv) the arterial roads shall serve as key organizing elements and shall generally experience a higher intensity of development than the interior portions of the Planning Area;*
- viii) open space areas such as woodlands, river and creek systems and utility corridors may be used to provide pedestrian and cycling linkages between places within and outside the community, that complement the transportation opportunities offered by the street network.*

### 20.5.3 General Policies

The general policies of the Southwest Area Plan apply to all the lands within the secondary plan boundary as well as all the Neighbourhoods and designations, and include the following policies:

#### 20.5.3.1 Housing i) Affordable Housing

- a) where appropriate, density bonusing will be considered for proposals that have an affordable housing component above 25% of the total dwelling count in any one development;*
- b) opportunities for affordable housing shall be integrated into neighbourhoods and developments that also provide for regular market housing;*

#### 20.5.3.2 Sustainable/Green Development

##### i) Principles

*The Southwest Area Secondary Plan is based on a design in which one of the key goals is to maximize the potential for sustainable development. In a City Planning context, this is achieved through such features as enhanced connectivity to transit, mixed-use development, a modified grid road system, and a connected open space system.*

*Through planning applications, including subdivision design and layout, proponents are encouraged to design and construct development to meet the following criteria for sustainable development:*

- a) reduce the consumption of energy, land and other non-renewable resources;*
- b) minimize the waste of materials, water and other limited resources;*
- c) create livable, healthy, accessible and inclusive environments; and*
- d) reduce greenhouse gases.*

##### ii) Policies

- b) in new buildings, Leadership in Energy and Environmental Design (LEED) principles;*
- d) alternative energy sources, including solar and appropriately sized rooftop mounted wind collectors. Such technologies should be sensitively incorporated into buildings and community design;*
- f) a range of residential dwelling types that support life-cycle housing and provide opportunities to age-in-place;*
- i) food production opportunities throughout the site. This includes but is not limited to community gardens, private gardens, greenhouses, roof-top gardens and edible landscaping programs; and*
- l) the employment of building technologies such as “greenroofs.” Alternately, the use of reflective roof surface materials with high solar and thermal reflectivity to reduce the “heat island” effect is also desired.*

#### 20.5.3.4 Community Parkland and Trail Network i) Pathways and Trails

- a) Pedestrian pathways and trail development will be focused along the central corridor through the community extending from Dingman Creek to Southdale Road West (and beyond), and include the Sandra McInnis Woods, Thornicroft Drain, Pincombe Drain and hydro corridor. These corridors are intended to provide major pedestrian and cycling linkages within the overall community parkland network.*
- c) The multi-use pathway network shown on Schedule 2 is intended to function as recreational pathways for pedestrians and cyclists.*
- d) Subdivision design shall incorporate and provide connections of linear pathway/trail and park systems within residential neighbourhoods and between neighbourhoods where possible, and provide significant exposure of the open space feature to the residential community.*

#### 20.5.3.4 Community Parkland and Trail Network ii) Parks

- a) The general location of neighbourhood and district parks is illustrated on Schedule 2. Further refinement of the location, size and configuration of these parks will be undertaken at the subdivision approval stage*
- b) An adequate distribution and balance of active parkland and play equipment shall be provided within an 800 metre radius of new residential development, without crossing major barriers such as railways, rivers or major roads.*
- e) Through the subdivision design and approval process, efforts will be made to incorporate neighbourhood and district parks in proximity/adjacent to natural heritage features, and provide appropriate linkages to protect and enhance the natural heritage features.*

#### 20.5.3.6 Natural Heritage – i) Components of a Natural Heritage System

##### c) Other Natural Heritage Features

*Natural Heritage Features other than the Dingman Creek, which are identified on Schedule B-1 of the Official Plan will be confirmed and/or delineated through the recommendations of an approved Environmental Impact Study in accordance with Section 15 of the Official Plan. Ecological buffers will be established for Natural Heritage Features based upon the recommendations of an approved Environmental Impact Study (EIS) in accordance with section 15 of the Official Plan*

##### d) Development Limit

*Where development occurs within distances adjacent to natural heritage features that trigger the need for an Environmental Impact Study (EIS) as set out in Table 15-1 of the Official Plan, an EIS will be scoped to confirm and delineate the natural feature, to determine the appropriate ecological buffer and to provide details on the Open Space system and naturalization opportunities to integrate the system with the adjacent features to be protected.*

#### 20.5.3.6 Natural Heritage – ii) Enhanced Open Space Corridors

*In order to enhance open space opportunities within the Southwest Area, the City will seek to locate open space corridors adjacent to key natural heritage features. These corridors are intended to provide for uses such as trails, active and passive parkland and stewardship opportunities.*

*These enhanced open space corridors are intended to build upon the natural heritage system in the Southwest area and will help to create unique communities and neighbourhoods linked by an integrated open space system. Where there are no natural features to build upon, these corridors may, over time, provide open space connections between natural features. It is intended that these corridors will provide both active and passive recreation opportunities and will form a component of the Southwest area park system.*

#### 20.5.3.6 Natural Heritage – iii) Tree Planting Standards and Stewardship Practices

- a) All landscape plans for new development and the re-development of existing sites within the Southwest Area Secondary Plan shall comply with tree planting standards and other tree canopy cover targets established for each land use as identified in the Urban Forest Strategy;*
- b) Wherever possible, enhanced tree planting will be encouraged in exterior side yards along local streets; and*
- e) Encourage the use of large stock tree-planting for development adjacent to arterial roads. The use planting technologies and standards to provide for long term and sustainable growth is encouraged.*

#### 20.5.3.8 Transportation - i) General Policies

*The transportation network within this Plan consists of Arterial, Primary and Secondary Collector roads. Local Streets may connect to appropriately designed arterial roads to provide new connections to the community neighbourhoods. The local street pattern will provide an organizing structure for each of the Neighbourhood areas.*

- a) The street patterns shall support pedestrian-oriented development patterns, with strong relationships to the natural heritage features in the Southwest Planning Area;*
- b) The Neighbourhood area street pattern shall support transit, cycling and walking;*
- c) At the subdivision and/or site plan application stage, traffic controls, including the provision of signalized intersections and turning movements, and street frontages that may be subject to full or partial restrictions on individual driveway access, shall be identified as part of the appropriate traffic studies required as part of a complete application;*
- h) Long stretches of on-street parking on local roads shall be broken-up with landscaped “bump-outs” sufficiently sized to support boulevard trees; and*
- i) Mitigation and replacement of any natural heritage feature that may be impacted or lost as a result of roads shall be required.*

#### 20.5.3.9 Urban Design

##### i) Development Design Policies

- a) All development, particularly in the Wonderland Boulevard, Lambeth Village Core, Neighbourhood Central Activity Nodes and residential areas, shall be designed in a form that is to be compact, pedestrian oriented and transit friendly;*
  - c) Development shall be based on a modified grid road system with interconnected networks of roads designed to disperse and reduce the length of vehicular and pedestrian trips and support the integration and long term viability of transit service. For local roads, the modified grid road system will respond to topography, the Open Space System and the nodal areas identified in the Plan. Cul-de-sacs will generally be permitted only when warranted by natural site conditions;*
  - d) The Open Space System forms a central feature of the Planning Area and the development form should reflect this fact. In addition, an interconnected system of trails will be developed that supports recreation, transit and transportation and connects the Wonderland Boulevard, Lambeth Village Core and the Neighbourhood Central Activity Nodes.*
  - e) Public safety, views and accessibility, both physically and visually to the Open Space System, as well as to parks, schools and other natural and civic features, will be an important consideration in community design. This will be accomplished through a range of different approaches including, but not limited to, the use of single loaded roads, combining public open space with other public or institutional facilities (e.g. school/park campuses, easements, stormwater management ponds adjacent to the Open Space System) and the location of high density residential and employment buildings.*
- Priority will be given to maintaining views and accessibility at key trail access points of the Open Space System. In addition, views to other public facilities, such as schools and parks, shall be ensured through the provision of a minimum of a combination of a*

*public right-of-way and/or open space immediately adjacent to a minimum of 50 percent of the perimeter of the property*

*i) The length of the block contributes significantly to creating a pedestrian-friendly environment. Blocks should be short and regular in length to make walking efficient and allow for variation in routes. Where it is impossible or undesirable to provide short blocks, wide public mid-block corridors should be provided to shorten walking distances. Development adjacent to such connections shall be designed to provide an active building facade for a minimum of 50 percent of the length of the pedestrian connection;*

*j) Views of the following features shall be created at appropriate locations:*

- Civic buildings;*
- Natural features and open spaces;*

*l) Safe Community Design is to promote safety, security and accessibility in public spaces through urban design including the design and siting of buildings and structures that:*

- Encourages continuous occupancy of public spaces;*
- Provides for opportunities for visual connections and ease of public access to adjacent streets, parks and other public areas;*
- Results in clear, unobstructed views of parks, school grounds, and open spaces from adjacent streets;*
- Ensures appropriate lighting, visibility and opportunities for informal surveillance are provided for walkways, parking lots, parking garages and open space areas;*
- Results in the selection and siting of landscape elements in a manner which maintains views for safety and surveillance;*
- Encourages the provision of views into, out of and through publicly accessible interior spaces;*
- Precludes entrapment or the perception of entrapment through properly identified exits and signage; and,*
- Results in accessibility for the disabled and elderly;*

*m) Community linkages will be established to connect other parts of the city where possible through road, transit, pedestrian and bicycle links, to ensure that the entire city functions in an integrated manner; and*

*m) Community linkages will be established to connect other parts of the city where possible through road, transit, pedestrian and bicycle links, to ensure that the entire city functions in an integrated manner.*

## *ii) Public Realm a) Local Streets*

*Local streets play a dual role as neighbourhood socialization spaces, as well as supporting transportation needs. The design requirements, while less substantial than for arterial and collector streets, must support the dual role of local streets;*

*b) Sidewalks shall generally be required on both sides of all streets;*

*c) Street furniture such as lighting, signage, parking meters, bicycle parking facilities, newspaper boxes, utilities, and garbage facilities shall be designed and placed within a consistent and integrated system of form, pattern, shape, colour, and texture to avoid clutter. Utilities will be grouped or clustered wherever possible and shall not compromise the overall intended character and design response for the street as identified in this section and associated Neighbourhood policies; and*

*d) Pedestrian/cyclist comfort and safety shall be considered in the streetscape design for roads under the control of the City of London.*

## *iii) Buildings and Site Design*

*a) Buildings, structures and landscaping shall be designed to provide visual interest to pedestrians, as well as a “sense of enclosure” to the street. Generally, heights of buildings shall also be related to road widths to create a more comfortable pedestrian environment, so that the wider the road width, the higher the building height;*

*c) Buildings on corner lots at the intersections of arterial and collector roads shall be sited and massed toward the intersection;*

*d) The rear and side building elevations of all buildings on corner lots shall be designed to take advantage of their extra visibility;*



- e) *In residential areas, garages shall be designed so that they are not the dominant feature in the streetscape. In particular, attached garages shall not:*
- *project beyond the façade of the dwelling or the façade (front face) of any porch; or*
  - *contain garage doors that occupy more than 50% of the frontage of a lot unless the City is satisfied through the submission of detailed plans by the applicant that the garage doors can be appropriately integrated with the streetscape;*
- g) *Off-street parking areas shall be designed to reduce their visual impact on both the adjoining streetscape and on people using the site and/or facility by:*
- *screening of the parking lot at the public right-of-way through the use of features such as low fences, walls and landscaping and in a manner which reflects the safe community design policies of this Section;*
  - *parking should be located underground for large buildings, such as high-rise residential buildings, office buildings, and mixed-use buildings;*
  - *the use of landscaping or decorative paving to reduce the visual expanse of large parking areas;*
  - *provision of pedestrian walkways adjacent to stores, between building clusters, and to provide pedestrian access to transit stops, public sidewalks and adjacent developments.*
- h) *All commercial and office development proposals shall demonstrate safe, effective and accessible pedestrian and bicycle and transit oriented transportation linkages from residential areas, and between and within these developments;*
- i) *Landscaping requirements shall ensure:*
- *the creation of a human scale within new development;*
  - *the enhancement of pedestrian comfort;*
  - *the provision of features which contribute to the definition of public open space, framing of views and focal points, direction of pedestrian movement and demarcation of areas with different functions; and*
  - *landscape design that promotes the use of native species and enhancement of ecological stability and integrity to reduce the heat island effect.*

#### 20.5.4.1 Residential

##### i) Function and Purpose

*It is intended that the Low, Medium and High Density Residential designations will support an urban housing stock, with residential intensity generally decreasing with greater distance from the Wonderland Road South corridor. Residential areas are to accommodate a diversity of dwelling types, building forms and heights, and densities in order to use land efficiently, provide for a variety of housing prices, and to allow for members of the community to “age-in-place”;*

##### iii) All Residential Designations in all Neighbourhoods

a) *Access to Arterial Roads The primary transit network is expected to be provided on the arterial roads. For all Draft Plan of Subdivision, Consent and Site Plan applications that include land within 400 metres of an arterial road, the requirements for a complete application shall include the submission of a plan that demonstrates the provision of viable, safe and effective pedestrian linkages to the arterial road, to provide pedestrian access to potential future transit services. Public streets are preferred, however, pathway connections may be considered on a case-specific basis;*

c) *Mix of Residential Forms Plans of subdivision shall accommodate a diversity of building types.*

#### 20.5.4.3 Open Space

##### i) Function and Purpose

*The Open Space designation will apply to lands within the Southwest Planning Area that are intended for active and passive recreation, and that are components of the city’s natural heritage system.*

*The Open Space designation is made of four sub-areas:*

- *Public Parkland – Active Recreation;*
- *Public Parkland – Urban Parks;*
- *Natural Heritage/Environmental; and*
- *Stormwater Management.*

ii) Character

*a) Public Parkland - Active Recreation – This area will have an active recreation character. The primary design focus will be to accommodate neighbourhood recreational needs such as multi-use and recreational pathways, play structures, basketball, skateboarding and playing fields. Opportunities for passive recreation are also to be integrated into active recreation park spaces; and*

*c) Natural Heritage/Environmental – This open space area is intended to protect the features and functions of the Natural Heritage System. In addition to providing opportunities to enhance the natural heritage system through naturalization and restoration of environmental buffers and linkages, it may allow for pedestrian trails and other forms of passive recreation, where appropriate.*

#### 20.5.5 Neighbourhoods and Land Use

This Secondary Plan is organized on the basis of Neighbourhood Areas which have specific functions and characteristics implemented by special policies pertaining specifically to the land use designations within that Neighbourhood. The subject lands are within the Bostwick Residential Neighbourhood which include the following policies:

#### **20.5.9 Bostwick Residential Neighbourhood**

i) Function and Purpose

*The Bostwick Neighbourhood will provide for residential development with the highest intensity of all of the Residential Neighbourhood Areas in the Southwest Planning Area, to support activities in the Wonderland Boulevard Neighbourhood.*

*Higher intensity mid-rise, transit-oriented development is encouraged along portions of the arterial road network to support the provision of transit services as detailed in Section 20.5.4.1 iv) of the General Residential policies.*

ii) Character

*The residential areas will develop as traditional suburban neighbourhoods, with characteristics similar to those found in the older areas of the city, reflecting a compact development, a diversity of building types, and walkable amenities to enhance the day to day living experience.*

#### 20.5.9.2 High Density Residential

i) Intent

*The High Density Residential designation provides for transit-oriented, mid-to high-rise, residential development that may be mixed use in nature.*

ii) Permitted Uses

*Permitted uses in the High Density Residential designation shall include mid-rise to high-rise apartment buildings, apartment hotels, nursing homes, rest homes, and homes for the aged. Convenience commercial uses and secondary permitted uses, including community centres, allowed in the High Density Residential designation of the Official Plan may be permitted within these areas.*

iii) Built Form and Intensity

*a) New development may be permitted to a maximum density of 150 units per hectare*

*and a maximum building height of 12 storeys, subject to further urban design review at the site plan approval stage;*

*b) The Urban Design policies of Section 20.5.3.9 and the General Residential policies of Section 20.5.4.1 of this Plan shall apply;*

*c) Notwithstanding Section 20.5.9.2(iii)(a), Sections 3.4.3(ii) and (iv) of the Official Plan shall apply.*

#### 20.5.17.1 Appendix 4: Official Plan Extracts – Policies

Relevant policies from the 1989 Official Plan have been included in the Secondary Plan to ensure that the policies that are required to fully implement the Secondary Plan are carried forward and become part of this Secondary Plan. Where policies of the 1989 Official Plan are referenced in the Secondary Plan and are not carried forward, it is the intent that this Secondary Plan is to be read in conjunction with the policies of The London Plan.

#### 20.5.17.3 - 3.4.3 Scale of Development

Further to the built form and intensity policies in section 20.5.9.2 iii) of SWAP, the ‘Scale of Development’ policies set out in section 3.4.3 ii) & iv) apply and include the following: Height and Density outside of the Downtown and Central London Areas are guided by the following policies:

##### i) Height and Density outside of the Downtown and Central London Areas

*Outside of the Downtown and Central London areas it is Council's intention that a mixing of housing types, building heights and densities shall be required in large designated Multi-Family, High Density Residential areas. Such areas, which will normally exceed 3 hectares (7.4 acres) in size, will be guided by the following criteria:*

*(a) a transition in scale shall be encouraged, where appropriate, to avoid extremes in building height and bulk between the new development and the existing built fabric of adjacent properties;*

*(b) all areas shall include a diversity of housing forms such as midrise and low-rise apartments and multiple attached dwellings, in order to minimize the overwhelming effect of large high-rise developments;*

*(c) high-rise structures shall be oriented, where possible, closest to activity nodes (shopping and employment centres) and points of high accessibility (arterial roads, transit service) with densities and building heights decreasing as the distance from an activity node increases;*

*(d) massive, at-grade or above-grade parking areas shall not dominate the site. Pedestrian circulation and access to transit services should be facilitated through site design and building orientation; and*

*(e) conformity with this policy and the urban design principles in Section 11.1, shall be demonstrated through the preparation of an secondary plan or a concept plan of the site, and the final approval of zoning may be withheld pending a public participation meeting on the site plan, and the enactment of a satisfactory agreement with the City.*

##### ii) Criteria for Increasing Density

*Notwithstanding Section i) above, on any lands designated Multi-Family High Density Residential, Council may consider proposals to allow higher densities than would normally be permitted. Zoning to permit higher densities will only be approved where a development will satisfy all of the following criteria:*

*(a) the site or area shall be located at the intersection of two arterial roads or an arterial and primary collector road, and well-served by public transit;*

*(b) the development shall include provision for unique attributes and/or amenities that may not be normally provided in lower density projects for public benefit such as, but not limited to, enhanced open space and recreational facilities, innovative forms of housing and architectural design features;*

*(c) parking facilities shall be designed to minimize the visual impact off-site, and provide for enhanced amenity and recreation areas for the residents of the development;*

- (d) conformity with this policy and urban design principles in Section 11.1 shall be demonstrated through the preparation of an secondary plan or a concept plan of the site which exceed the prevailing standards; and
- (e) the final approval of zoning shall be withheld pending a public participation meeting on the site plan and the enactment of a satisfactory agreement with the City.

iv) Density Bonusing

*Council, under the provisions of policy 19.4.4. and the Zoning By-law, may allow an increase in the density above the limit otherwise permitted by the Zoning By-law in return for the provision of certain public facilities, amenities or design features. The maximum cumulative bonus that may be permitted without a zoning by-law amendment (as-of-right) on any site shall not exceed 25% of the density otherwise permitted by the Zoning Bylaw. Bonusing on individual sites may exceed 25% of the density otherwise permitted, where Council approves site specific bonus regulations in the Zoning By-law. In these instances, the owner of the subject land shall enter into an agreement with the City, to be registered against the title to the land.*

1989 Official Plan 19.4.4 Bonus Zoning

*Under the provisions of the Planning Act, a municipality may include in its Zoning By-law, regulations that permit increases to the height and density limits applicable to a proposed development in return for the provision of such facilities, services, or matters, as are set out in the By-law. This practice, commonly referred to as bonus zoning, is considered to be an appropriate means of assisting in the implementation of this Plan.*

i) Principle

*The facilities, services or matters that would be provided in consideration of a height or density bonus should be reasonable, in terms of their cost/benefit implications, for both the City and the developer and must result in a benefit to the general public and/or an enhancement of the design or amenities of a development to the extent that a greater density or height is warranted. Also, the height and density bonuses received should not result in a scale of development that is incompatible with adjacent uses or exceeds the capacity of available municipal services.*

ii) Objectives

*Bonus Zoning is provided to encourage development features which result in a public benefit which cannot be obtained through the normal development process. Bonus zoning will be used to support the City's urban design principles, as contained in Chapter 11 and other policies of the Plan, and may include one or more of the following objectives:*

- (a) to support the provision of the development of affordable housing as provided for by 12.2.2;
- (b) to support the provision of common open space that is functional for active or passive recreational use;
- (c) to support the provision of underground parking;
- (d) to encourage aesthetically attractive residential developments through the enhanced provision of landscaped open space;
- (e) to support the provision of, and improved access to, public open space, supplementary to any parkland dedication requirements;
- (f) to support the provision of employment-related day care facilities;
- (g) to support the preservation of structures and/or districts identified as being of cultural heritage value or interest by the City of London;
- (h) to support innovative and environmentally sensitive development which incorporates notable design features, promotes energy conservation, waste and water recycling and use of public transit;
- (i) to support the preservation of natural areas and/or features; and
- (j) to support the provision of design features that provide for universal accessibility in new construction and/or redevelopment.

The London Plan - City Design Policies 193 (1989 Official Plan – Chapter 11 Policies)

In all of the planning and development we do and the initiatives we take as a municipality, we will design for and foster:

1. *A well-designed built form throughout the city;*
2. *Development that is designed to be a good fit and compatible within its context;*
3. *A high-quality, distinctive and memorable city image;*
4. *Development that supports a positive pedestrian environment;*
5. *A built form that is supportive of all types of active mobility and universal accessibility;*
6. *High-quality public spaces that are safe, accessible, attractive and vibrant;*
7. *A mix of housing types to support ageing in place and affordability;*
8. *Sustainably designed development that is resilient to long-term change; and*
9. *Healthy, diverse and vibrant neighbourhoods that promote a sense of place and character.*

The London Plan - Bonusing Provisions Policy 1652

Under Type 2 Bonus Zoning, additional height or density may be permitted in favour of facilities, services, or matters such as:

- 1) *Exceptional site and building design.*
- 2) *Cultural heritage resources designation and conservation.*
- 3) *Dedication of public open space.*
- 4) *Provision of off-site community amenities, such as parks, plazas, civic spaces, or community facilities.*
- 5) *Community garden facilities that are available to the broader neighbourhood.*
- 6) *Public art.*
- 7) *Cultural facilities accessible to the public.*
- 8) *Sustainable forms of development in pursuit of the Green and Healthy City policies of this Plan.*
- 9) *Contribution to the development of transit amenities, features and facilities.*
- 10) *Large quantities of secure bicycle parking, and cycling infrastructure such as lockers and change rooms accessible to the general public.*
- 11) *The provision of commuter parking facilities on site, available to the general public.*
- 12) *Affordable housing.*
- 13) *Day care facilities, including child care facilities and family centres within nearby schools.*
- 14) *Car parking, car sharing and bicycle sharing facilities all accessible to the general public.*
- 15) *Extraordinary tree planting, which may include large caliper tree stock, a greater number of trees planted than required, or the planting of rare tree species as appropriate.*
- 16) *Measures that enhance the Natural Heritage System, such as renaturalization, buffers from natural heritage features that are substantively greater than required, or restoration of natural heritage features and functions.*
- 17) *Other facilities, services, or matters that provide substantive public benefit.”*

The London Plan - Affordable Housing 516

*New neighbourhoods will be planned to include a variety of different housing types such that it is possible for people to remain in a neighbourhood as their housing needs change over time (509);*

*A target of 25% of new housing, in aggregate, is to be affordable to low- and moderate-income households as defined by the Provincial Policy Statement and this Plan. This target may be met through residential greenfield development and the many forms of intensification identified in the City Structure policies of this Plan (517); and Secondary plans and larger residential development proposals should include a 25% affordable housing component through a mix of housing types and sizes. In keeping with this intent, 40% of new housing units within a secondary plan, and lands exceeding five hectares in size outside of any secondary plan, should be in forms other than single detached dwellings (518).*

## 4.0 Matters to be Considered

A complete analysis of the applications is underway and includes a review of the following matters, which have been identified to date:

### Provincial Policy Statement (PPS)

- Consideration for the consistency with policies related to the provision of an appropriate mix and intensity of uses, and efficient use of land, infrastructure and services

### Range of Uses

- If the range of residential, open space and park uses are appropriate

### Intensity

- If the requested intensity for each of the two proposed development blocks is appropriate for their site, surrounding context, and able to be serviced
- If the proposed intensity is consistent with the Our City, Our Strategy, City Building, Intensification Targets, City Structure, and Place Type policies

### Bonusing

- If the requested bonus zone results in enhanced public benefit and is commensurate to the increased height and density requested

### SWAP

- Conformity to policies related to the appropriateness of the level of proposed intensity in the Bostwick Neighbourhood and broader secondary plan

### Technical Review

- Functional servicing analysis and available sanitary capacity to accommodate the proposed intensity
- A review of the Transportation Impact Assessment to ensure no negative impacts on existing roads, and to ensure future road construction can be managed through the consent/subdivision applications
- Ensure appropriate and desirable design of towers and consideration before the Urban Design Peer Review Panel

### Zoning

- Suitability of the requested bonus zones and amendments in relation to the proposed development and neighbourhood
- Suitability of the requested Open Space (OS4) to delineate the extent of the Thornicroft Drain

More information and detail is available in the Appendices of this report.

## 5.0 Conclusion

Development Services staff will continue to review the merits of the draft plan of subdivision and Zoning By-law Amendment applications and the comments received with respect to the requested planning applications. A subsequent planning report will be prepared when the review is complete, including a recommended action for the consideration of the Planning and Environment Committee and Municipal Council.

<b>Recommended by:</b>	<b>Sonia Wise, MCIP, RPP Senior Planner, Development Services</b>
<b>Reviewed by:</b>	<b>Lou Pompili, MCIP RPP Manager, Development Planning (Subdivision)</b>
<b>Concurred in by:</b>	<b>Paul Yeoman, RPP, PLE Director, Development Services</b>
<b>Submitted by:</b>	<b>George Kotsifas, P.ENG Managing Director, Development and Compliance Services and Chief Building Official</b>
Note: The opinions contained herein are offered by a person or persons qualified to provide expert opinion. Further detail with respect to qualifications can be obtained from Development Services	

October 1, 2018  
/sw

CC: Matt Feldberg, Manager, Development Services (Subdivisions)

Z:\Shared\ADMIN\1- PEC Reports\2018 PEC Reports\15 - Oct 09 '18 PEC\draft\_39T-18502\_Z-8931-3080\_Bostwick\_Rd\_subdivision\_PEC\_Report\_1\_of\_1.docx

## Appendix A – Public Engagement

### Community Engagement

**Public liaison:** On August 17, 2018, Notice of Application was sent to 552 property owners and residents in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on August 16, 2018. A “Planning Application” sign was also posted on the site.

7 replies were received

**Nature of Liaison:** The purpose and effect of these applications would be the creation of a high density residential subdivision consisting of apartment buildings, stacked townhouse dwellings, a park and open space, and access via new street connections to Southdale Road and Bostwick Road.

Consideration of a Residential Draft Plan of Subdivision with two (2) high density residential blocks with an estimated total of 504 residential units (consisting of multiple apartment buildings and stacked townhouse dwellings), one (1) park block, one (1) open space block, and one (1) 0.3 m reserve, all served by three (3) new local roads (Street A, Street B, and Street C).

Possible Amendment to Zoning By-law Z.-1 to change the zoning FROM an Urban Reserve (UR4) Zone and an Environmental Review (ER) Zone TO: a) a Residential R9 Bonus (R9-7\*B-(#)) Zone – to permit apartment buildings, lodging house class 2, senior citizens apartment buildings, handicapped persons apartment buildings, and continuum-of-care facilities. A bonus zone is requested to permit townhouses and stacked townhouses with a maximum height of 13m and a minimum front yard setback of 6m; an apartment building with a maximum height of 70m, a density of 193 units per hectare, a reduced exterior side yard setback of 0.4m, a reduced rear yard setback of 22m, in return for such facilities, services and matters identified in section 19.4 of the 1989 Official Plan, and policies 1638-1655 of The London Plan such as underground parking and enhanced urban design; b) a Residential R9 Bonus (R9-7\*B-(##)) Zone – to permit apartment buildings, lodging house class 2, senior citizens apartment buildings, handicapped persons apartment buildings, and continuum-of-care facilities. A bonus zone is requested to permit townhouses, an apartment building with a maximum height of 68m, a density of 269 units per hectare, a minimum front yard setback of 6.5m, a reduced interior side yard setback of 12m, a reduced rear yard setback of 12m, and a reduced number of parking spaces (with 325 spaces provided), in return for such facilities, services and matters identifies in section 19.4 of the 1989 Official Plan, and policies 1638-1655 of The London Plan such as underground parking and enhanced urban design; c) Open Space (OS2) Zone – to permit conservation lands, conservation works, cultivation of land for agricultural/horticultural purposes, golf courses, private parks, public parks, recreational golf courses, recreational buildings associated with conservation lands and public parks, campground, and managed forest; commercial recreational establishments, community centres, institutions, private outdoor recreation clubs, public swimming pools, recreational buildings, riding stables, sports fields, golf driving range, miniature golf course, go kart track, batting cages, tennis court and playground; d) Open Space (OS4) Zone – to permit conservation lands, conservation works, golf courses, private parks, public parks, recreational golf courses cultivation or use of land for agricultural/horticultural purposes, and sports fields without structures; e) Urban Reserve Special Provision UR4( ) Zone – to permit existing dwellings, agricultural uses, conservation lands, managed woodlots, wayside put, passive recreation uses, kennels, private outdoor recreation clubs, and riding stables with a special provision for a reduced lot size of 2ha. Holding provisions may be considered for urban design, municipal servicing, and phasing.

**Responses:** A summary of the various comments received include the following:  
Concern for:

- Increased traffic and congestion (x5)
- Increased cut through traffic in the established neighbourhood to the north (x3)
- Pedestrian safety



- Road improvements should be implemented as recommended in the Southdale EA (x3)
- Only the ward 9 councillor was identified on the notice, not the nearby ward 10
- The local school capacity and ability to accommodate increased number of pupils (x2)
- Greater building heights are difficult to evacuate in emergencies and may block satellite signals
- Provide convenient drop-off/pick-up spaces for para transit vehicles
- Provide affordable housing options and small-lot, small home options

Support for:

- Positive to see the site finally develop
- Interest in investing in the project

### Responses to Public Liaison Letter and Publication in “The Londoner”

Telephone	Written
Tom Brimson 12-1015 Farnham Rd London ON N6K 1S3	Amanda Nash 1172 Dalhouse Dr London ON N6K 2Y1
Jim Cressman 957 Dalhousie Dr London ON N6K 1M8	Susan Spencer-Paton 31 Brixham Road London ON NK 1P5
Wing Man Lin	Esther Corcoran 143 McMaster Drive London ON N6K 1J5
	Ed Morrison

From: amanda nash [mailto: ]

Sent: Monday, August 27, 2018 3:23 PM

To: Wise, Sonia <swise@london.ca>; Hopkins, Anna <ahopkins@london.ca>

Subject: 3080 bostwick rd development

As a resident of Dalhousie dr. We are concerned about traffic on surrounding streets. Traffic is already horrific on southdale and very often backs up through the intersection of Bostwick rd and Southdale rd. It will often back up through the lights at Wonderland and as far west as Colonel Talbot at times. If you add in another potential 1000 plus units that could make this neighbourhood basically inaccessible at many times of day.

With the added population density where will the children go to school ? If even half of the 1000 plus units even have just 2 children where do you suggest these schools put 1000 more students ? The plans do not show any plans for a new school building, so that means our children who already go to these crowded schools will suffer greatly from the overpopulation of their class rooms and school fundings will be stretched even thinner.

LONDON CONDOMINIUM CORPORATON # 15  
31 Brixham Road, London ON N6K 1P5

September 4, 2018

Sonia Wise  
[swise@london.ca](mailto:swise@london.ca)

Dear Ms. Wise:

**Re: Planning Application, 3080 Bostwick Road**

We represent London Condominium Corporation # 15 which consists of 43 townhouse units along McMaster Drive, Brixham Road, Dalhousie Crescent and Farnham Road. Although we are pleased to finally see that the weeds and mounds of dirt at the corner of Bostwick and Southdale replaced by development, we have concerns about the increase in traffic that it will bring to the area and through our streets.

Although, the majority of our condominium residents are seniors and mature adults, there are also many children living in the area and using these streets to attend the three schools in Westmount. Even without the addition of the proposed 1,670 new residents, plus expanse of commercial retail and office use, the traffic along Southdale Road and through our streets has increased tremendously over the past couple of years. With the addition of this number of residents without improvements, the traffic congestion and safety of pedestrians is a great concern.

We urge the City to implement the recommendations for road improvements proposed in the Environmental Study presented by York Developments in 2017, prior to the completion of and new high rise buildings on Southdale Road

Respectfully submitted,

*Susan Spencer-Paton*  
Susan Spencer-Paton  
President

*Doris E. Hall*  
Doris E. Hall  
Treasurer

Email: [REDACTED]

Cc: Anna Hopkins [ahopkins@london.ca](mailto:ahopkins@london.ca)  
Virginia Ridley, [vr Ridley@london.ca](mailto:vr Ridley@london.ca)

1

**From:** Esther Corcoran [mailto:[REDACTED]]  
**Sent:** Monday, September 17, 2018 8:16 PM  
**To:** Wise, Sonia <[swise@london.ca](mailto:swise@london.ca)>  
**Subject:** Re: 3080 Bostwick Road Planning Applications

Dear Ms. Wise,

I also forgot to add that to better serve the community and our city, with all the new developments that there is limited affordable homes and rentals with the tightening of the requirements for obtaining a mortgage this greatly impacts young families, single people and those that have already retired and those approaching retirement. People on restricted income or a lack of income growth (eg - seniors), that affordable housing or single family homes are also required. There is a small circle of homes within our subdivision of detached homes that have yards slightly larger than townhome has. Many of them are single floors and this is something that is also needed for those just entering the housing market, downsizing or disabled.

Thank you

Esther Corcoran

-----original message-----

**From:** Esther Corcoran [mailto: ]  
**Sent:** Sunday, September 16, 2018 8:39 PM  
**To:** Hopkins, Anna <ahopkins@london.ca>  
**Cc:** Wise, Sonia <swise@london.ca>  
**Subject:** Re: 3080 Bostwick Road Planning Applications

Dear Anna,

We reside in Westmount and would like to share some of our concerns for the proposals for 3080 Bostwick Road. At this time, traffic from Wonderland Rd. S., to Colonel Talbot Rd along Southdale Rd is extremely heavy especially during the early hours and later afternoon (eg - 7am-10am & 4pm-6pm). With the proposed additions of housing the traffic will increase drastically and Southdale is not equipped to handle the increase in traffic and should be expanded prior to any more additional housing being built.

Proposal for Site 1 - the residential apartment building(s) are too high for the surrounding neighborhood as proposed at 21 & 18 storeys high. They should be no higher than 14 storeys. Concern is two fold in that should there be a power outage or fire, any elderly, disabled, expectant mother, young children would have a difficult time in descending 21 or 18 storeys to safety. The other concern is for surrounding homes that may be utilizing satellite signals that the height of such a high rise may disrupt the signal.

Proposal for Site 3 - would be the same as above as it is being proposed for a 17 storey apartment building.

Proposal for Site 5 - It is not clear as to how many parking spaces will be available for 168 units. Many seniors continue to drive.

Our final concern for all proposals is that there should be a spot for each apartment building that allows for easy access for para transit vehicles (eg Voyager etc.) so that they can easily get in and out for pick ups and drop offs. Many newer buildings aren't allowing easy access for these vehicles.

Sincerely,

Esther Corcoran

**From:** Ed Morrison [mailto: ]  
**Sent:** Thursday, September 27, 2018 11:54 AM  
**To:** Wise, Sonia <swise@london.ca>  
**Subject:** Re: 3080 Bostwick Road Development

Hello Mrs. Wise

My biggest concern so far is when the development will take place. The widening of Southdale from Pine Valley to Colonel Talbot is slated to be finished by 2030. If this development takes place before the widening it will further aggravate the already congested area.

Ed Morrison

**Agency/Departmental Comments**

August 1, 2018 – Urban Regeneration: Email Excerpt

No further archaeological work is required for the assessed area

August 8, 2018: Bell Canada – Email Excerpt

No conditions or objections at this time

August 8, 2018: Hydro one – Email Excerpt

No comments or concerns

August 30, 2018: Water Engineering – Email

*Zoning By-Law Amendment Z-8931*

Regarding the water servicing, as part of the Zoning By-Law Amendment Application for the subject parcels, we would request Holding Zone Provisions be applied as follows:

- A general “h” provision to ensure the orderly development of lands and the adequate provision of municipal services (i.e. to ensure the detailed design and agreement to construct the required watermain has been satisfied).
- An “h-100” provision to ensure the looped watermain discussed above is constructed, commissioned, and put into service.

*Draft Plan of Subdivision 39T-18502*

A review of Section 9.0 (Water Servicing) of the Final Proposal Report dated May 2018 identified a number of faulty assumptions pertaining to the water servicing to the proposed Draft Plan of Subdivision; we note the following:

- A looped watermain from the 400mm high-level watermain on Southdale Road through the Plan to the low-level 600mm watermain on Bostwick Road would not be permitted; no interconnection between the high and low-level systems, through a check-valve connection or otherwise.
- For watermain looping purposes, a Street ‘A’ watermain and secondary connection to the high-level 400mm watermain on Southdale Road is required. Depending on the existing Southdale Road watermain valving a new line valve may need to be installed on the 400mm main between the two connections to complete the loop.
- Direct water service connections for development Blocks 2 & 6 (and 1, 3, 5 from the original proposal) to the Southdale and Bostwick Road watermains would not be supported; the water servicing strategy for the subject lands is for these development Blocks to connect to the internal subdivision watermain.

*Draft Plan Conditions – Water Servicing*

1. The lands subject to this Draft Plan of Subdivision application are located within a high-level water distribution system service area; an adequately sized high-level watermain is required to be designed and extended along the internal local road network (Streets ‘A’, ‘B’ & ‘C’) with a looped connection to the 400mm watermain on Southdale Road West.
2. As a Condition of Draft Plan Approval the Owner shall have their consulting engineer develop to the satisfaction of the City Engineer a water servicing area plan for the external lands south of this proposed Draft Plan along Bostwick Road. This water servicing area plan shall size the watermains considering future development demands (fire & domestic), establish looped watermain alignment(s) to service the study limits, identify connections to existing watermains (high and low-level), and identify any required external works.

3. In conjunction with the consolidated detailed design the Owner shall have their consulting engineer prepare and submit a water servicing design study which addresses the following, all to the satisfaction of the City Engineer:
  - a) Water distribution system analysis & modeling and hydraulic calculations for the Draft Plan of Subdivision confirming system design requirements are being met;
  - b) Identify domestic and fire flows for the residential\development Blocks from the high-level water distribution system. In addition to Blocks 2 & 6 internal to the Plan, Blocks 1, 3 & 5 “external” to the Plan being created by Consent (B.032/18, B.033/18, B.034/18) are to be serviced off the subdivision internal watermain;
  - c) Address water quality and identify measures to maintain water quality from zero build-out through full build-out of the subdivision;
  - d) Include a staging and phasing report as applicable which addresses the requirement to maintain interim water quality;
  - e) Include modeling for two fire flow scenarios as follows:
    - i. Max Day + Fire confirming velocities and pressures within the system at the design fire flows, and
    - ii. Max Day + Fire confirming the available fire flows at fire hydrants at 20PSI residual. Identify fire flows available from each proposed hydrant to be constructed and determine the appropriate colour hydrant markers (identifying hydrant rated capacity);
  - f) Identify any water servicing requirements necessary to provide water servicing to external lands through this Plan of Subdivision, incorporating existing area plans as applicable. This would not only encompass the lands immediately adjacent of Street ‘C’ to the south (part of the parent parcel), but also the external lands further to the south along Bostwick Road;
  - g) Identify any need for the construction of or improvement to external works necessary to provide water servicing to this Plan of Subdivision;
  - h) Identify any required watermain oversizing, if necessary, and any cost sharing agreements;
  - i) Identify the effect of development on existing water infrastructure – identify potential conflicts;
  - j) Include full-sized water distribution and area plan(s) which identifies the location of valves & hydrants, the type and location of water quality measures to be implemented (including automatic flushing device settings), the fire hydrant rated capacity & marker colour, and the design fire flow applied to development Blocks.
4. In conjunction with the consolidated detailed design the Owner shall have their consulting engineer prepare and submit a design acceptable to the City Engineer for the proposed watermain crossing the Dingman Creek Tributary “D” watercourse (Thorncroft Drain); considerations could include utilization of a specific product(s), joint restraint systems, casing pipe, strategic valve placement, and insulation.
5. Prior to the issuance of any Certificates of Conditional Approval the Owner shall install and commission the accepted water quality measures required to maintain water quality within the water distribution system during build-out, all to the satisfaction of the City Engineer, at no cost to the City. The measures which are necessary to meet water quality requirements, including their respective flow settings, etc shall be shown clearly on the engineering drawings.
6. The Owner shall ensure implemented water quality measures shall remain in place until there is sufficient occupancy demand to maintain water quality within the Plan of Subdivision without their use. The Owner is responsible for the following:
  - i. to meter and pay the billed costs associated with any automatic flushing devices including water discharged from any device at the time of their installation until removal;
  - ii. any incidental and/or ongoing maintenance of the automatic flushing devices;

- iii. payment for maintenance costs for these devices incurred by the City on an ongoing basis until removal;
  - iv. all works and the costs of removing the devices when no longer required;
7. The Owner shall ensure the limits of any request for Conditional Approval shall conform to the staging and phasing plan as set out in the accepted water servicing report and shall include the implementation of the interim water quality measures. In the event the requested Conditional Approval limits differ from the staging and phasing Issued: August 30, 2018 as set out in the accepted water servicing report, the Owner shall be required to submit revised plans and hydraulic modeling as necessary to address water quality.
8. Prior to the issuance of any Certificates of Conditional Approval, and in accordance with City standards, or as otherwise required by the City Engineer, the Owner shall complete the following for the provision of water servicing to this Draft Plan of Subdivision:
- v. Construct a looped watermain to serve this Plan along the internal road network (Streets 'A', 'B' & 'C') and connect them to the existing high-level municipal system, namely the existing 400mm diameter watermain on Southdale Road West;
  - vi. Available fire flows and appropriate hydrant rated capacity colour code markers are to be shown on the engineering drawings; the coloured fire hydrant markers will be installed by the City of London at the time of Conditional Approval; and
  - vii. Have their consulting engineer confirm to the City that the watermain system has been constructed, is operational, and is looped from the watermain on Southdale Road West through this Plan.
9. The Owner shall obtain all necessary approvals from the City Engineer for the servicing of all Blocks in this Plan of Subdivision prior to the installation of any water services to or within these Blocks.
10. Where the allowable density of any development Block serviced internal to this Plan would trigger the requirement for a looped private water service connection, incorporate strategic valves on the internal watermains such that dual water service connections could be accommodated without having to cut-in valves in the future.
11. With respect to any proposed development Blocks the Owner shall include in all agreements of purchase and sale, and/or lease of Blocks in this Plan, a warning clause advising the purchaser/transferee that if it is determined by the Ministry of Environment and Climate Change (MOECC) that the water servicing for the Block is a regulated drinking water system, then the Owner or Condominium Corporation may be required to meet the regulations under the Safe Drinking Water Act and the associated regulation O.Reg. 170/03.

If deemed a regulated system, there is potential the City of London could be ordered to operate this system in the future. As such, the system would be required to be constructed to City standards and requirements.

## Appendix B – Policy Context

The following policy and regulatory documents were considered in their entirety as part of the evaluation of this requested land use change. The most relevant policies, by-laws, and legislation are identified as follows:

### Provincial Policy Statement, 2014

- 1.1 Managing and Directing Land Use to Achieve Efficient and Resilient Development and Land Use Patterns
  - 1.1.3 Settlement Areas
- 1.7 Long-term economic prosperity

### London Plan

- 54 Our Strategy
- 79 Our City – City Structure Plan
- 193 City Design Policies
- 309 City Building Policies
- 516 Affordable Housing
- 916 Neighbourhoods
- 954 High Density Residential Overlay
- 1556 Secondary Plans
- 1577 Evaluation of Planning Applications
- 1645-1655 Bonus Zoning

### Southwest Area Secondary Plan

- 20.5.1.4 Principles of the Secondary Plan
- 20.5.2 Community Structure Plan
- 20.5.3 General Policies
  - 20.5.4.1 General Land Use Policies
- 20.5.5 Neighbourhoods
- 20.5.9 Bostwick Neighbourhood
- 20.5.17 Appendix 4: Official Plan Excerpts – Policies

### Official Plan

- 2.1 Council Strategic Plan
- 3.4. Multi-Family, High Density Residential
  - 11.1 Urban Design
  - 12 Housing
  - 15 Environmental Policies
  - 16 Parks & Recreation Policies
  - 19.4.4 Bonus Zoning
- 20 Secondary Plans

### Z.-1 Zoning By-law

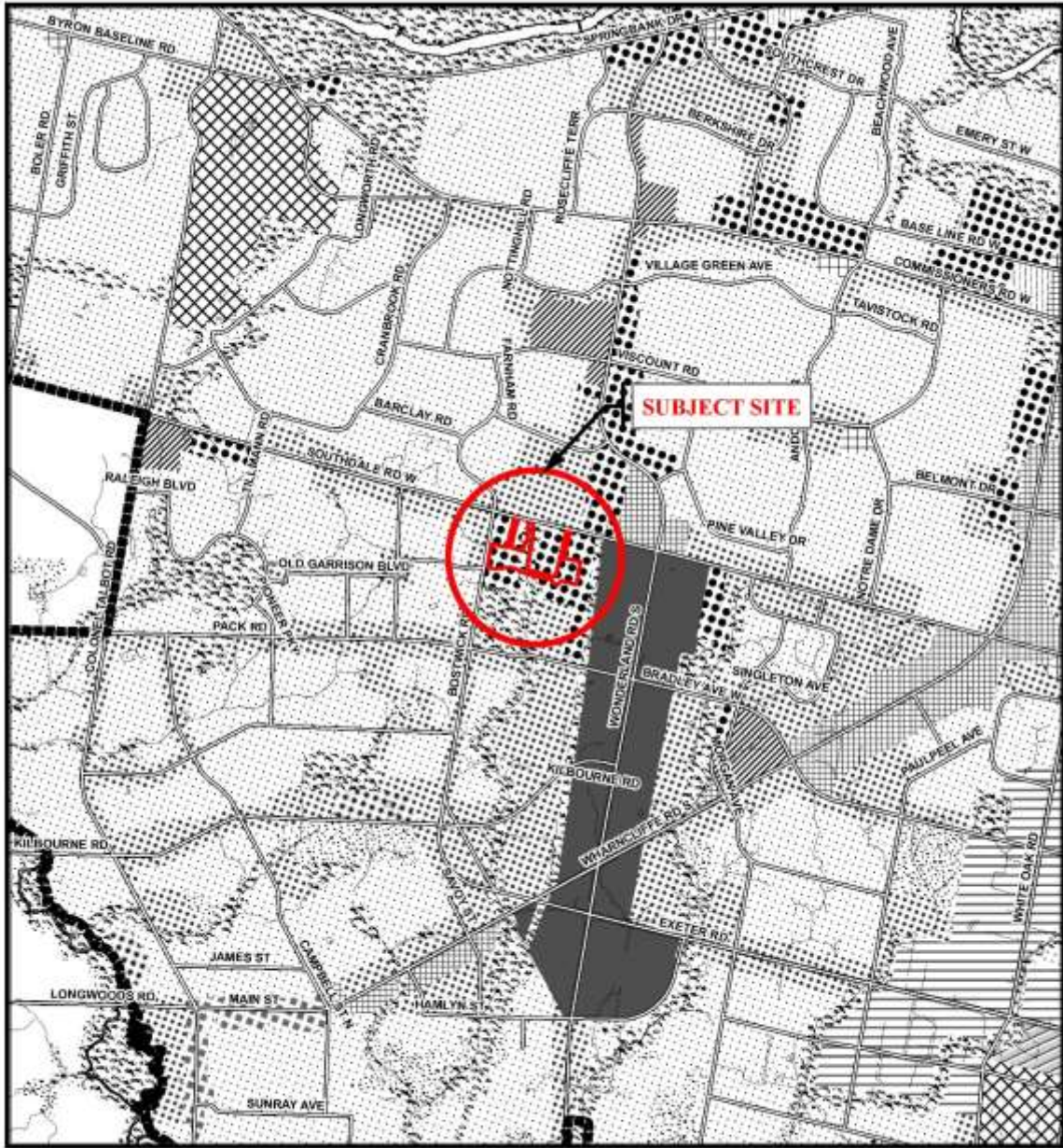
- Section 3: Zones and Symbols
- Section 4: General Provisions
- Section 13: Residential R9 Zone
- Section 36: Open Space
- Section 49: Urban Reserve

**Appendix C – Additional Information**

**Additional Maps**



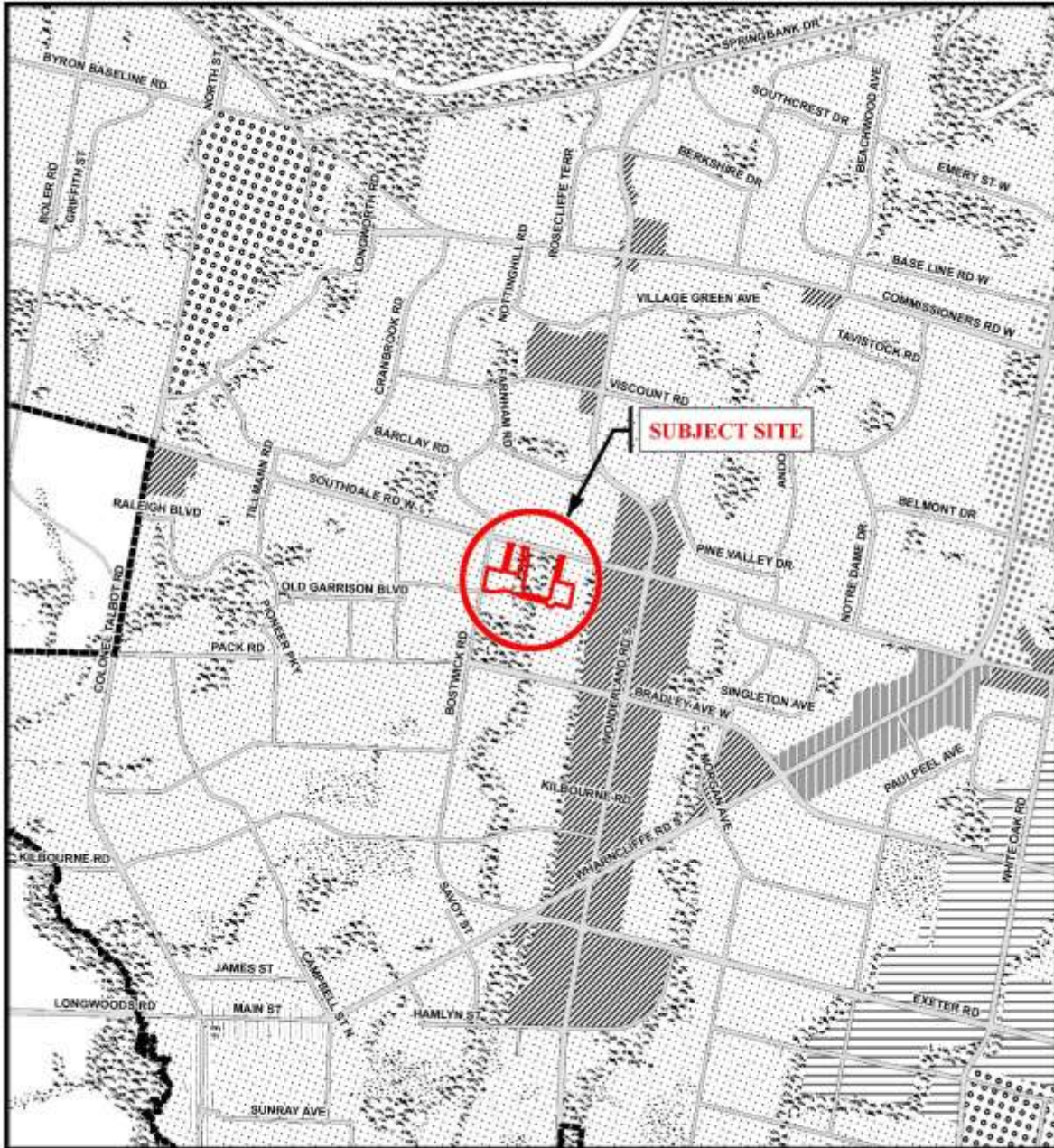




Legend		
Downtown	Multi-Family, Medium Density Residential	Office Business Park
Wonderland Road Community Enterprise Corridor	Low Density Residential	General Industrial
Enclosed Regional Commercial Node	Office Area	Light Industrial
New Format Regional Commercial Node	Office/Residential	Commercial Industrial
Community Commercial Node	Regional Facility	Transitional Industrial
Neighbourhood Commercial Node	Community Facility	Rural Settlement
Main Street Commercial Corridor	Open Space	Environmental Review
Auto-Oriented Commercial Corridor	Urban Reserve - Community Growth	Agriculture
Multi-Family, High Density Residential	Urban Reserve - Industrial Growth	Urban Growth Boundary

<p><b>CITY OF LONDON</b> Planning Services / Development Services OFFICIAL PLAN SCHEDULE A - LANDUSE -</p> <p>PREPARED BY: Graphics and Information Services</p>	<p>Scale 1:30,000</p> <p>Meters</p>	<p>FILE NUMBER: 39T-18502/Z-8931</p>
		<p>PLANNER: SW</p> <p>TECHNICIAN: RC</p> <p>DATE: 2018/09/29</p>



**Legend**

- |                        |                          |   |
|------------------------|--------------------------|---|
| Downtown               | Future Community Growth  | Environmental Review                    |
| Transit Village        | Heavy Industrial         | Farmland                                |
| Shopping Area          | Light Industrial         | Rural Neighbourhood                     |
| Rapid Transit Corridor | Future Industrial Growth | Waste Management Resource Recovery Area |
| Urban Corridor         | Commercial Industrial    | Urban Growth Boundary                   |
| Main Street            | Institutional            |   |
| Neighbourhood          | Green Space              |   |

*This is an excerpt from the Planning Division's working consolidation of Map 1 - Place Types of the London Plan, with added notations:*

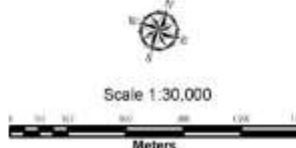
*At the time of the printing of this map, the Rapid Transit EA is in progress. This map shows the Rapid Transit Corridors and Urban Corridors to recognize potential alignments. These Place Types will be modified to align with the results of the EA process for the final version of The London Plan.*

**CITY OF LONDON**

Planning Services /  
Development Services

**LONDON PLAN MAP 1  
- PLACE TYPES -**

PREPARED BY: Planning Services

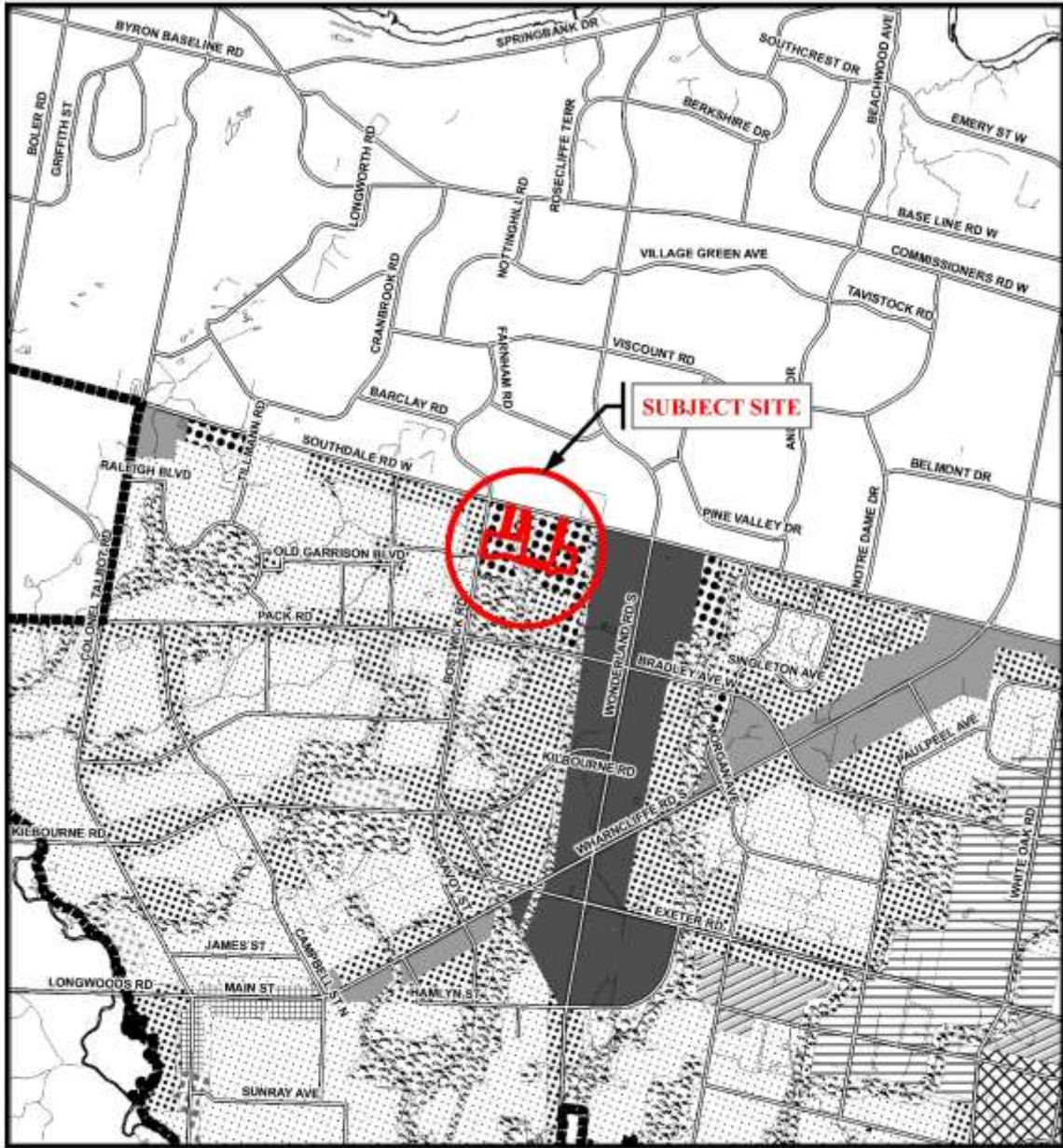


**File Number:** 39T-18502/Z-8931

**Planner:** SW

**Technician:** RC

**Date:** August 29, 2018



Legend	
	High Density Residential
	Medium Density Residential
	Low Density Residential
	Commercial
	Office
	Wonderland Road Community Enterprise Corridor
	Main Street Lambeth North
	Main Street Lambeth South
	Open Space
	Institutional
	Industrial
	Commercial Industrial
	Transitional Industrial
	Urban Reserve Community Growth
	Urban Reserve Industrial Growth
	Rural Settlement
	Urban Growth Boundary

<p><b>CITY OF LONDON</b> Planning Services / Development Services SOUTHWEST AREA STUDY SECONDARY PLAN - LANDUSE -</p> <p>PREPARED BY: Graphics and Information Services</p>	 Scale 1:30,000 Meters	FILE NUMBER: 39T-18502/Z-8931
		PLANNER: SW
		TECHNICIAN: RC
		DATE: 2018/08/29



**COUNCIL APPROVED ZONING FOR THE SUBJECT SITE:**

1) **LEGEND FOR ZONING BY-LAW Z-1**

- |  |   |
|--|---|
| <ul style="list-style-type: none"> <li>R1 - SINGLE DETACHED DWELLINGS</li> <li>R2 - SINGLE AND TWO UNIT DWELLINGS</li> <li>R3 - SINGLE TO FOUR UNIT DWELLINGS</li> <li>R4 - STREET TOWNHOUSE</li> <li>R5 - CLUSTER TOWNHOUSE</li> <li>R6 - CLUSTER HOUSING ALL FORMS</li> <li>R7 - SENIOR'S HOUSING</li> <li>R8 - MEDIUM DENSITY/LOW RISE APTS.</li> <li>R9 - MEDIUM TO HIGH DENSITY APTS.</li> <li>R10 - HIGH DENSITY APARTMENTS</li> <li>R11 - LODGING HOUSE</li> <br/> <li>DA - DOWNTOWN AREA</li> <li>RSA - REGIONAL SHOPPING AREA</li> <li>CSA - COMMUNITY SHOPPING AREA</li> <li>NSA - NEIGHBOURHOOD SHOPPING AREA</li> <li>BDC - BUSINESS DISTRICT COMMERCIAL</li> <li>AC - ARTERIAL COMMERCIAL</li> <li>HS - HIGHWAY SERVICE COMMERCIAL</li> <li>RSC - RESTRICTED SERVICE COMMERCIAL</li> <li>CC - CONVENIENCE COMMERCIAL</li> <li>SS - AUTOMOBILE SERVICE STATION</li> <li>ASA - ASSOCIATED SHOPPING AREA COMMERCIAL</li> <br/> <li>OR - OFFICE/RESIDENTIAL</li> <li>OC - OFFICE CONVERSION</li> <li>RO - RESTRICTED OFFICE</li> <li>OF - OFFICE</li> </ul> | <ul style="list-style-type: none"> <li>RF - REGIONAL FACILITY</li> <li>CF - COMMUNITY FACILITY</li> <li>NF - NEIGHBOURHOOD FACILITY</li> <li>HER - HERITAGE</li> <li>DC - DAY CARE</li> <br/> <li>OS - OPEN SPACE</li> <li>CR - COMMERCIAL RECREATION</li> <li>ER - ENVIRONMENTAL REVIEW</li> <br/> <li>OB - OFFICE BUSINESS PARK</li> <li>LI - LIGHT INDUSTRIAL</li> <li>GI - GENERAL INDUSTRIAL</li> <li>HI - HEAVY INDUSTRIAL</li> <li>EX - RESOURCE EXTRACTIVE</li> <li>UR - URBAN RESERVE</li> <br/> <li>AG - AGRICULTURAL</li> <li>AGC - AGRICULTURAL COMMERCIAL</li> <li>RRC - RURAL SETTLEMENT COMMERCIAL</li> <li>TGS - TEMPORARY GARDEN SUITE</li> <li>RT - RAIL TRANSPORTATION</li> <br/> <li>"Y" - HOLDING SYMBOL</li> <li>"D" - DENSITY SYMBOL</li> <li>"H" - HEIGHT SYMBOL</li> <li>"B" - BONUS SYMBOL</li> <li>"T" - TEMPORARY USE SYMBOL</li> </ul> |
|--|---|

**CITY OF LONDON**

PLANNING SERVICES / DEVELOPMENT SERVICES

**ZONING  
BY-LAW NO. Z-1  
SCHEDULE A**



THIS MAP IS AN UNOFFICIAL EXTRACT FROM THE ZONING BY-LAW WITH ADDED NOTATIONS

FILE NO:  
39T-18502/Z-8931 SW

MAP PREPARED:  
2018/08/29 RC

1:5,000  
0 25 50 100 150 200 Meters

**Additional Reports**

OZ-6662: 2004 Request for Official Plan and Zoning By-law Amendments to develop site for various residential and commercial uses

O-7609: 2012 Council Approved Official Plan Amendments associated with Southwest Area Plan

Z-8386: 2014 Zoning by-law Amendment to facilitate the development of the Bostwick Community Centre