

TO:	CHAIR AND MEMBERS PLANNING AND ENVIRONMENT COMMITTEE
FROM:	GEORGE KOTSIFAS, P.ENG. MANAGING DIRECTOR, DEVELOPMENT & COMPLIANCE SERVICES AND CHIEF BUILDING OFFICIAL
SUBJECT:	APPLICATION BY: 2425293 ONTARIO INC. C/O FARHAD NOORI 801 SARNIA ROAD PUBLIC PARTICIPATION MEETING ON DECEMBER 12, 2016

RECOMMENDATION

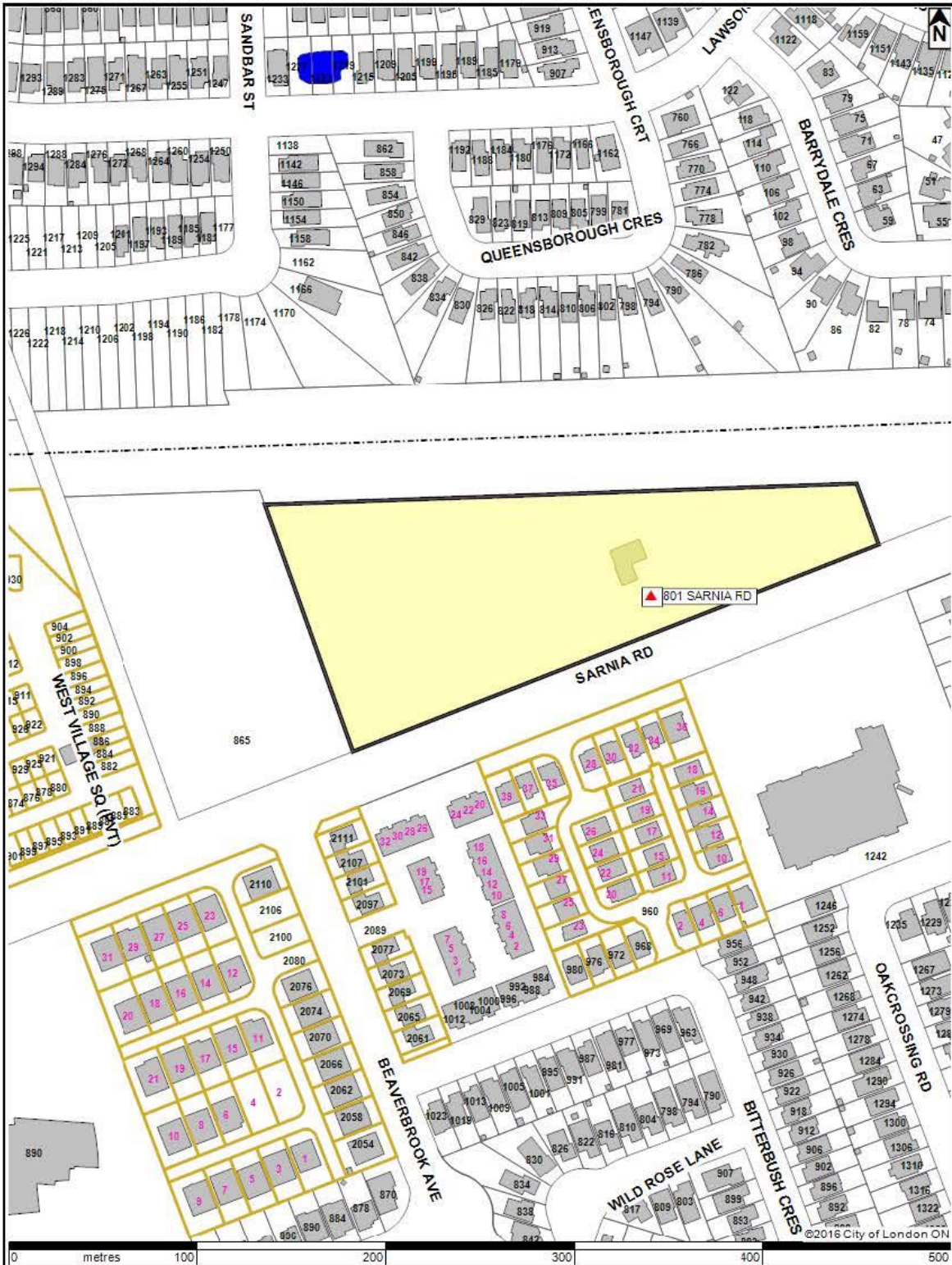
That, on the recommendation of the Senior Planner, Development Planning, the following actions be taken with respect to the application of 2425293 Ontario Inc. c/o Farhad Noori relating to the property located at 801 Sarnia Road:

- a) the attached proposed by-law **BE INTRODUCED** at the Municipal Council meeting on January 17, 2017 to amend Zoning By-law No. Z.-1 in conformity with the Official Plan, to change the zoning of the subject lands **FROM** an Urban Reserve (UR1) Zone, which permits uses such as existing dwellings; agricultural uses except for mushroom farms, commercial greenhouses livestock facilities, manure storage facilities; kennels; private outdoor recreation clubs; and riding stables **TO** an Open Space (OS1) Zone to allow for passive recreational uses including a pedestrian path system; and a Holding Residential R6 Special Provision (h.*h-34*h-65*R6-5()) Zone to permit cluster townhouse dwelling units with a special provision to include a minimum front yard setback of 4.0 metres, a maximum density of 39 units per hectare, a west interior side yard setback of 4.6 metres and a rear setback of 10 metres; and a Holding Residential R8* Bonus (h-*h-34*h-65*R8-4*B-) Zone to permit an apartment building with a maximum building height of 16 metres; a maximum density of 96 units per hectare; a minimum front yard setback of 4 metres, a minimum rear yard setback of 13 metres and parking ratio of 1 parking space per 1 dwelling unit in exchange for constructing a building which is in accordance with the illustration attached as Schedule "1", which shall be implemented through a development agreement in return for the following design features services and matters which are described in greater detail in the proposed by-law:
- Enhanced urban design features; and
 - Enhanced public open space pathway corridor including an entrance feature along Sarnia Road.

subject to holding provisions requiring the developer to enter into a development agreement with the City, that noise walls be discouraged and buildings are oriented to Sarnia Road and implement noise and vibration attenuation measures;

- b) Planning and Environment Committee **REPORT TO** the Approval Authority the issues, if any, raised at the public meeting with respect to this Vacant Land Condominium application;
- c) Planning and Environment Committee **REPORT TO** the Approval Authority the issues, if any, raised at the public meeting with respect to this Site Plan Approval application;

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LOCATION MAP	LEGEND										
Subject Site: 801 Sarnia Rd Applicant: 2425293 Ontario Inc. File Number: Z-8549 Planner: Craig Smith Created By: James Scott Date: 2016-12-05 Scale: 1:2500	<table border="0" style="width: 100%;"> <tr> <td style="width: 20px; text-align: center;">■</td> <td>Subject Site</td> </tr> <tr> <td style="width: 20px; text-align: center;">■</td> <td>Parks</td> </tr> <tr> <td style="width: 20px; text-align: center;">✕</td> <td>Assessment Parcels</td> </tr> <tr> <td style="width: 20px; text-align: center;">■</td> <td>Buildings</td> </tr> <tr> <td style="width: 20px; text-align: center;">123</td> <td>Address Numbers</td> </tr> </table>	■	Subject Site	■	Parks	✕	Assessment Parcels	■	Buildings	123	Address Numbers
■	Subject Site										
■	Parks										
✕	Assessment Parcels										
■	Buildings										
123	Address Numbers										
<div style="display: flex; justify-content: space-between; align-items: center;"> <div style="font-size: small;">Corporation of the City of London Prepared By: Planning and Development</div> <div style="font-size: 2em;">N</div> </div>											

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File: 39CD-15516/Z-8549/SPC15-036
Planner: C. Smith

PREVIOUS REPORTS PERTINENT TO THIS MATTER

None.

PURPOSE AND EFFECT OF RECOMMENDED ACTION

The purpose and effect of this application is to consider a change to the zoning of the lands to support the development of a draft plan of Vacant Land Condominium on 801 Sarnia Road. The plan consists of a 57 unit townhouse development and a 72 unit, 5 storey apartment building on private roads, with direct access from Sarnia Road.

RATIONALE

1. The recommended amendment is consistent with the PPS 2014.
2. The recommended amendment is consistent with the City of London Official Plan policies.
3. The bonusing of the subject site ensures the apartment building will fit within the surrounding area; have an enhanced design: and the development will provide for an enhanced public pathway corridor.
4. The requested zone to permit cluster townhouse dwellings and an apartment building will allow for a development which is compatible with existing development in this area.
5. The submitted draft plan of Vacant Land Condominium is in conformity with Official Plan policies, the City's Condominium Submission Review and Approval Guidelines and the regulations of the recommended R6-5 () Special Provision Zone.
6. The proposed Site Plan is in conformity with the Official Plan, proposed Zoning By-law and Site Plan Control guidelines.
7. The proposed residential uses and scale of development are generally consistent with the London Plan.

BACKGROUND

Date Application Accepted: September 22, 2015	Agent: Monteith Brown Planning Consultants c/o Jay McGuffin
REQUESTED ACTION: The purpose and effect of this application is to consider a proposed draft plan of vacant land condominium on 801 Sarnia Road. The plan consists of 57 residential units on a private road, with access from Sarnia Road on the westerly portion (approx. 1.6ha) and a 5 storey apartment building on the easterly portion (approx.0.755 ha). The apartment does not form part of the proposed vacant land condominium.	

SITE CHARACTERISTICS:
<ul style="list-style-type: none"> • Current Land Use - vacant • Frontage – 300m • Depth – irregular • Area – 2.4 ha • Shape – triangle

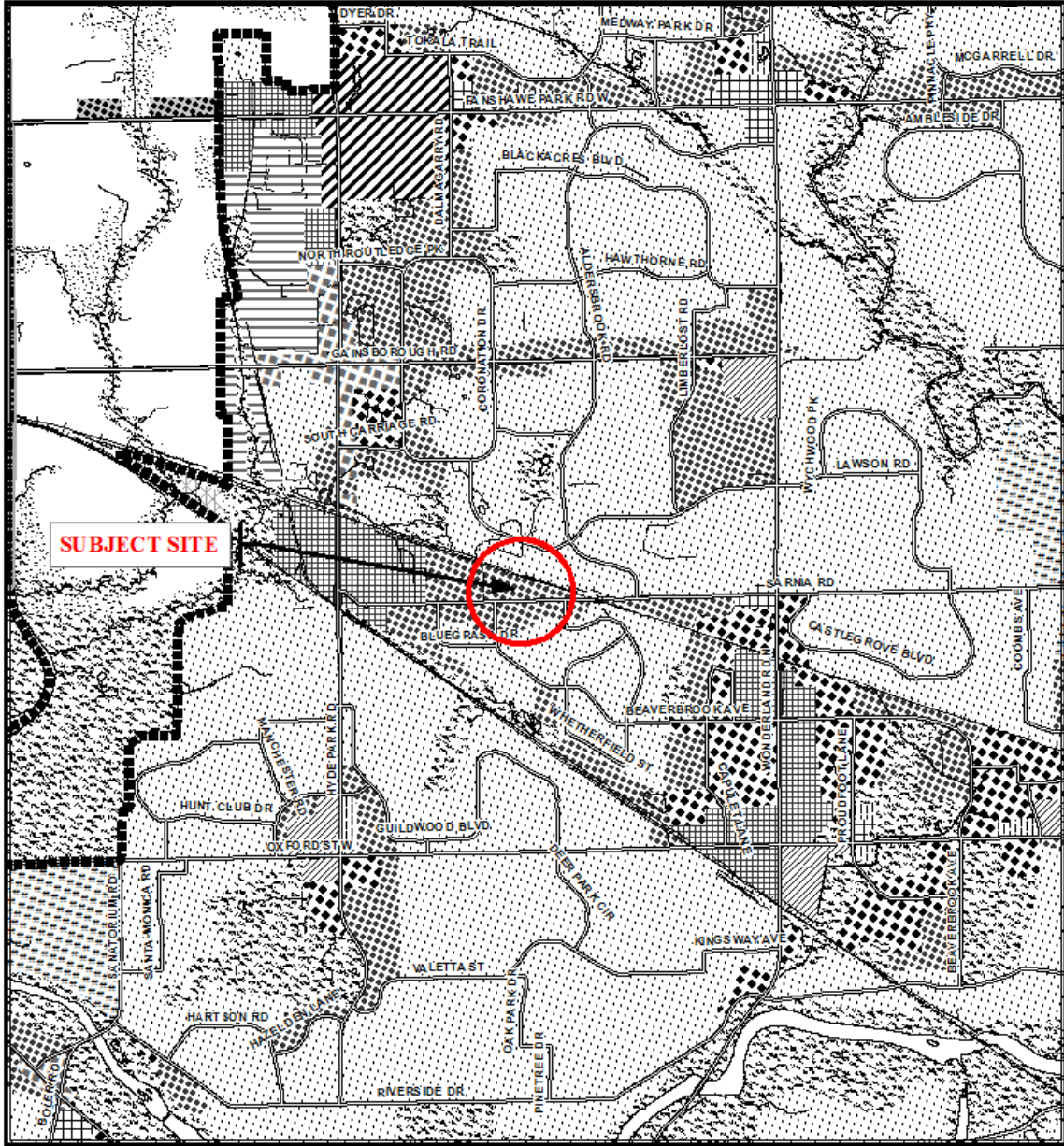
SURROUNDING LAND USES:
<ul style="list-style-type: none"> • North – CP rail line and existing single detached dwellings • South – Existing multifamily residential and retirement home • East – CP rail lands • West – Vacant, designated Multi Family Medium Density Residential lands.

OFFICIAL PLAN DESIGNATION: (refer to map)
<ul style="list-style-type: none"> • Multi-Family, Medium Density Residential
EXISTING ZONING: (refer to map)
<ul style="list-style-type: none"> • Urban Reserve (UR 1)

PLANNING HISTORY

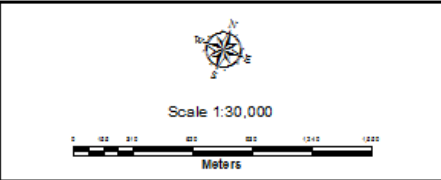
The subject lands are located within the Hyde Park Community Planning Area. This Community Planning Area is generally bounded by the urban growth boundary to the west, CN railroad right of way to the south, Fanshawe Park Road West to the north and Aldersbrook Road to the east. The Community Plan and associated amendments to the Official Plan were adopted by Council in January 2000.

On November 16, 2015 an application for Site Plan Approval was submitted. The third submission of the site plan drawings was submitted to Development Services for review on October 26, 2016.



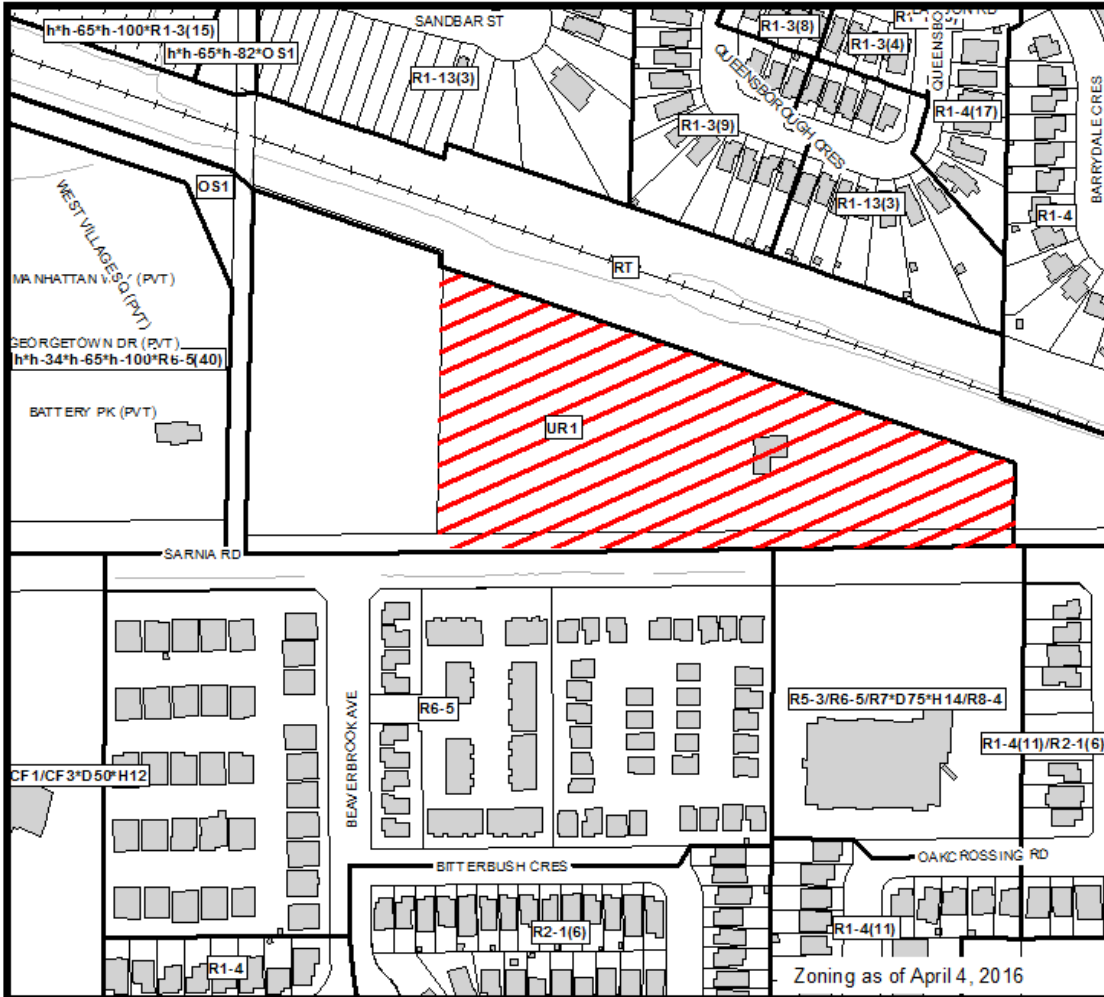
Legend		
Downtown	Multi-Family, Medium Density Residential	Office Business Park
Wonderland Road Community Enterprise Corridor	Low Density Residential	General Industrial
Enclosed Regional Commercial Node	Office Area	Light Industrial
New Format Regional Commercial Node	Office/Residential	Commercial Industrial
Community Commercial Node	Regional Facility	Transitional Industrial
Neighbourhood Commercial Node	Community Facility	Rural Settlement
Main Street Commercial Corridor	Open Space	Environmental Review
Auto-Oriented Commercial Corridor	Urban Reserve - Community Growth	Agriculture
Multi-Family, High Density Residential	Urban Reserve - Industrial Growth	Urban Growth Boundary

CITY OF LONDON
 Department of
 Planning and Development
OFFICIAL PLAN SCHEDULE A
 - LAND USE -
 PREPARED BY: Graphics and Information Services



FILE NUMBER: 39CD-15516/Z-8549
 PLANNER: CS
 TECHNICIAN: JTS
 DATE: November 25, 2016

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COUNCIL APPROVED ZONING FOR THE SUBJECT SITE:

1) **LEGEND FOR ZONING BY-LAW Z-1**

- | | |
|--|---|
| <ul style="list-style-type: none"> R1 - SINGLE DETACHED DWELLINGS R2 - SINGLE AND TWO UNIT DWELLINGS R3 - SINGLE TO FOUR UNIT DWELLINGS R4 - STREET TOWNHOUSE R5 - CLUSTER TOWNHOUSE R6 - CLUSTER HOUSING ALL FORMS R7 - SENIOR'S HOUSING R8 - MEDIUM DENSITY/LOW RISE APTS. R9 - MEDIUM TO HIGH DENSITY APTS. R10 - HIGH DENSITY APARTMENTS R11 - LODGING HOUSE
 DA - DOWNTOWN AREA RSA - REGIONAL SHOPPING AREA CSA - COMMUNITY SHOPPING AREA NSA - NEIGHBOURHOOD SHOPPING AREA BDC - BUSINESS DISTRICT COMMERCIAL AC - ARTERIAL COMMERCIAL HS - HIGHWAY SERVICE COMMERCIAL RSC - RESTRICTED SERVICE COMMERCIAL CC - CONVENIENCE COMMERCIAL SS - AUTOMOBILE SERVICE STATION ASA - ASSOCIATED SHOPPING AREA COMMERCIAL
 OR - OFFICE/RESIDENTIAL OC - OFFICE CONVERSION RO - RESTRICTED OFFICE OF - OFFICE | <ul style="list-style-type: none"> RF - REGIONAL FACILITY CF - COMMUNITY FACILITY NF - NEIGHBOURHOOD FACILITY HER - HERITAGE DC - DAY CARE
 OS - OPEN SPACE CR - COMMERCIAL RECREATION ER - ENVIRONMENTAL REVIEW
 OB - OFFICE BUSINESS PARK LI - LIGHT INDUSTRIAL GI - GENERAL INDUSTRIAL HI - HEAVY INDUSTRIAL EX - RESOURCE EXTRACTIVE UR - URBAN RESERVE
 AG - AGRICULTURAL AGC - AGRICULTURAL COMMERCIAL RRC - RURAL SETTLEMENT COMMERCIAL TGS - TEMPORARY GARDEN SUITE RT - RAIL TRANSPORTATION
 "h" - HOLDING SYMBOL "d" - DENSITY SYMBOL "H" - HEIGHT SYMBOL "B" - BONUS SYMBOL "T" - TEMPORARY USE SYMBOL |
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CITY OF LONDON
 PLANNING, ENVIRONMENTAL AND ENGINEERING SERVICES

ZONING BY-LAW NO. Z.-1
SCHEDULE A

THIS MAP IS AN UNOFFICIAL EXTRACT FROM THE ZONING BY-LAW WITH ADDED NOTATIONS

FILE NO:
 Z-8549 CS

MAP PREPARED:
 December 5, 2016 JTS

1:3,000

0 15 30 60 90 120 Meters

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SIGNIFICANT DEPARTMENT/AGENCY COMMENTS

Upper Thames River Conservation Authority (UTRCA)

- The UTRCA has no objections.

Union Gas

- The owner/developer provide to Union Gas Limited the necessary easements and/or agreements required by Union Gas Limited for the provision of gas services for this project, in a form satisfactory to Union Gas Limited.

Staff Response: This will be addressed through a site plan approval and a condition of the draft plan of condominium.

Bell

- No conditions/objections to the above application as submitted

Canada Post

This subdivision will receive mail service to centralized mail facilities provided through our Community Mailbox program.

The owner shall complete to the satisfaction of the Director of Engineering of the City of London and Canada Post :

- a) include on all offers of purchase and sale a statement that advises the prospective purchaser :
 - i) that the home/business mail delivery will be from a designated Centralized Mail Box.
 - ii) that the developers/owners be responsible for officially notifying the purchasers of the exact Centralized Mail Box locations prior to the closing of any home sales.

- b) the owner further agrees to :
 - i) work with Canada Post to determine and provide temporary suitable Centralized Mail Box locations which may be utilized by Canada Post until the curbs, boulevards and sidewalks are in place in the remainder of the subdivision.
 - ii) install a concrete pad in accordance with the requirements of, and in locations to be approved by, Canada Post to facilitate the placement of Community Mail Boxes
 - iii) identify the pads above on the engineering servicing drawings. Said pads are to be poured at the time of the sidewalk and/or curb installation within each phase of the plan of subdivision.
 - iv) determine the location of all centralized mail receiving facilities in co-operation with Canada Post and to indicate the location of the centralized mail facilities on appropriate maps, information boards and plans. Maps are also to be prominently displayed in the sales office(s) showing specific Centralized Mail Facility locations.

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- c) Canada Post's multi-unit policy, which requires that the owner/developer provide the centralized mail facility at their own expense, will be in effect for buildings and complexes with a common lobby, common indoor or sheltered space.

Staff Response: This will be addressed through a site plan approval and a condition of the draft plan of condominium.

Canadian Pacific Railroad

- The natural grade differential between the CP Rail tracks and the development exceeds CP's 2.5 metre berm height and that no further berm is required.
- We have reviewed the Environmental Noise & Vibration Assessment prepared by Development Engineering (London) Limited and support the implementation of the recommendations contained therein.
- The applicant's setback of 20 metres to the nearest dwelling as depicted on the site plan is sufficient. The reduction in setback is acceptable given the increase in the natural berm height.
- We have reviewed the Proposed Site Servicing Study & Stormwater Management Plan prepared by Eng Plus Ltd. The report indicates that the proposed development will not cause a change in drainage patterns as they effect the CP property. We have one concern with item 4.3.3 Major Flows. All other items indicate that the flow onto CP lands will be less than the pre-development level. In this paragraph, no reference is made to the difference of flow into the CP Rail ditch pre and post development. Please confirm that the major flow onto CP lands is not greater than pre-development.
- A 1.83 metre high chain-link fence be constructed and maintained along the common property line of the Railway and the development by the developer at his expense, and the developer is made aware of the necessity of including a covenant running with the lands, in all deeds, obliging the purchasers of the land to maintain the fence in a satisfactory condition at their expense.

Staff Response; noise, vibration and safety attenuation measures will be addressed through the Site Plan Approval process. The proposed Zoning By-law amendment includes the h-65 holding provision requiring that noise and vibration measures are implemented prior to removal. A condition of the condominium draft approval will require warning clauses to be included in the condominium declaration. Issues dealing with drainage and fencing will be addressed in greater detail through the Site Plan Approval process.

Urban Design

The proposed Bonus zone is appropriate and includes the following:

Building

- a building design which, with minor variations at the discretion of the Managing Director, Planning and City Planner, matches the site concept and elevation drawings shown in Schedule "1";
- The building includes a differentiated base, middle and top;
- Individual entrances to the apartments are located on the ground floor of the apartment building adjacent to Sarnia Road and function as front doors rather than patio doors
- Ground floor amenity spaces are to be designed as open courtyards extending wider than the balconies above and into the front setback;

Site

- Provide glass or metal railings, or masonry walls to delineate the individual courtyards from the public realm;

- Railings are not to exceed 1m in height and masonry walls are not to exceed 0.9m in height in order to maintain visibility;
- Incorporate low landscaping to frame amenity area;
- Direct walkway access from the front doors to the public sidewalk will be provided; and
- All parking is to be located behind the building or enhanced screening.

Parks Planning

The proposed Bonus zone is appropriate and includes the following:

- Enhanced landscaping within the future public pathway corridor which includes additional plantings and public sitting areas; and
- Enhanced entrance feature along Sarnia Road which includes the use of differencing paving materials, vegetation and public sitting areas

PUBLIC LIAISON:	On November 10, 2015 and November 1, 2016, notices were sent to residents within 120 metres of the subject site. Notice of Application was also published in The Londoner on November 10, 2015 and November 3, 2016.	2 replies were received.
<p>Nature of Liaison: The purpose and effect of this application is to consider a proposed draft plan of vacant land condominium on 801 Sarnia Road. The plan consists of 57 residential units on a private road, with access from Sarnia Road on the westerly portion (approx. 1.6ha) and a 5 storey apartment building on the easterly portion (approx. 755 ha). Apartment use does not form part of the proposed vacant land condominium.</p> <p>Possible Amendment to Zoning By-law Z.-1 to change the zoning FROM an Urban Reserve (UR1) Zone which permits existing uses TO Open Space (OS1) Zone to allow for a recreational path system and a Residential R6 Special Provision (R6-5 (_)) Zone to permit 57 townhouse dwelling units at a maximum density of 39 units per hectare, a maximum height of 12 metres and a 4.0 metre minimum front yard setback, a 4.6 metre west interior side yard setback, a 10 metre rear yard setback to the Open Space (OS1) Zone and a Residential R8*Bonus Zone that provides for specific development concepts and site elements including, but not limited to an enhanced public pedestrian path system that includes an entrance feature at Sarnia Road, use of building materials to delineate, base, middle and top of the building, with an enhanced principle entrance features providing pedestrian scale and street orientation. The regulations of the Residential R8 Bonus (R8-4*B- _) Zone is to permit: a maximum building height of 16 metres; a maximum of 72 dwelling units; a maximum density of 96 units per hectare; a minimum front yard setback of 4 metres, a 13 metre rear yard setback to the Open Space (OS1) Zone and parking ratio of 1 parking spaces per 1 dwelling unit.</p> <p>The City is also considering the following amendments: The application of a Holding (h) Provision across the subject lands. The holding provision is to ensure the orderly development of lands and the adequate provision of municipal services and that that the development is consistent with the Hyde Park Community Plan Urban Design Guidelines.</p>		
<p>Responses:</p> <ul style="list-style-type: none"> • Height (5 storeys) is too high and will negatively impact the single detached dwellings on the north side of CP rail line; and • Increase in traffic will have a negative impact on Sarnia Road. 		

ANALYSIS

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File: 39CD-15516/Z-8549/SPC15-036
Planner: C. Smith

Subject site

The subject site is located on the north side of Sarnia Road, east of Hyde Park Road. The site is approximately 2.4 hectares in size, with 300 metres of frontage along Sarnia Road. The proposed draft plan of vacant land condominium site is located south of the CP rail line and was previously an agricultural use and a single detached dwelling. It is surrounded by multifamily medium density residential uses to the south and vacant (multifamily residential zoned) lands to the west.

The following report provides an analysis of this requested Zoning By-law amendment and the proposed Vacant Land Condominium. There is a Site Plan application that is currently under review for the subject lands. The Site Plan Approval process will address issues related to the physical form of development, such as the internal driveway layout, unit configuration/orientation, servicing, noise impacts from the CP rail line and Sarnia Road, and other development considerations/constraints

What is the nature of the application?

The applicant, 2425293 Ontario Inc. c/o Farhad Noori, has applied for a vacant land condominium, and has also submitted a site plan application to permit a 57 unit townhouse development and a 72 unit, five (5) storey apartment building at this location. Common element components of the Vacant Land Condominium includes private roadway including services and open space areas. The proposed site plan (SP-15-036), landscape plan and proposed elevations are shown on the following pages.

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Proposed Landscape Plan

Enhanced Public Pathway Entrance (see detail next page)

10m Enhanced Public Pathway

EXISTING TREE IDENTIFICATION

REMOVING TREE - NEW PLANT MATERIAL LIST

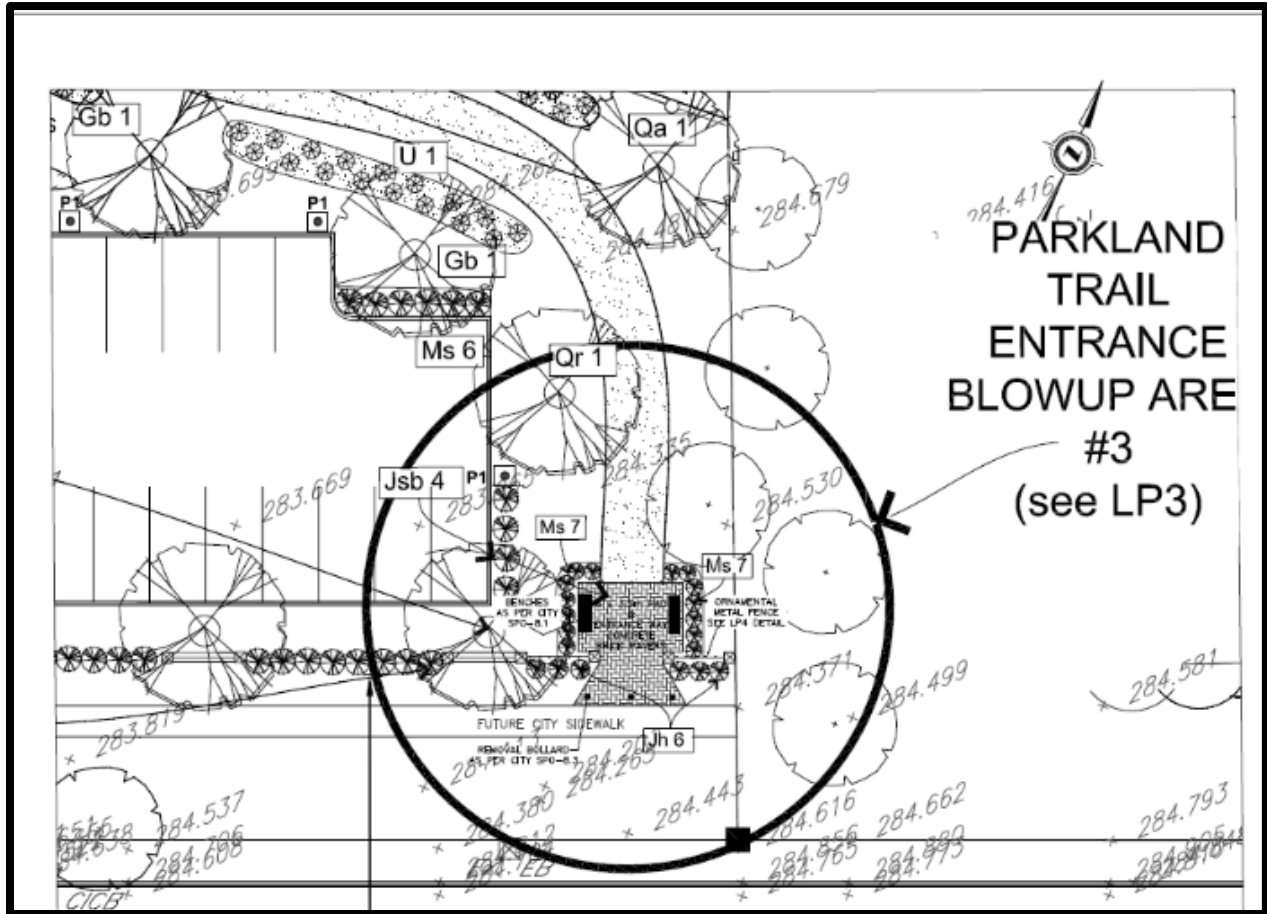
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GENERAL NOTES - PLANTING SPECIFICATIONS:

1. ALL PLANTING SHALL BE DONE IN ACCORDANCE WITH THE CITY OF TORONTO'S PLANTING SPECIFICATIONS.
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WESTERN PRESTIGE VILLAGE
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2425795 ONTARIO INC.
2425795 ONTARIO INC.

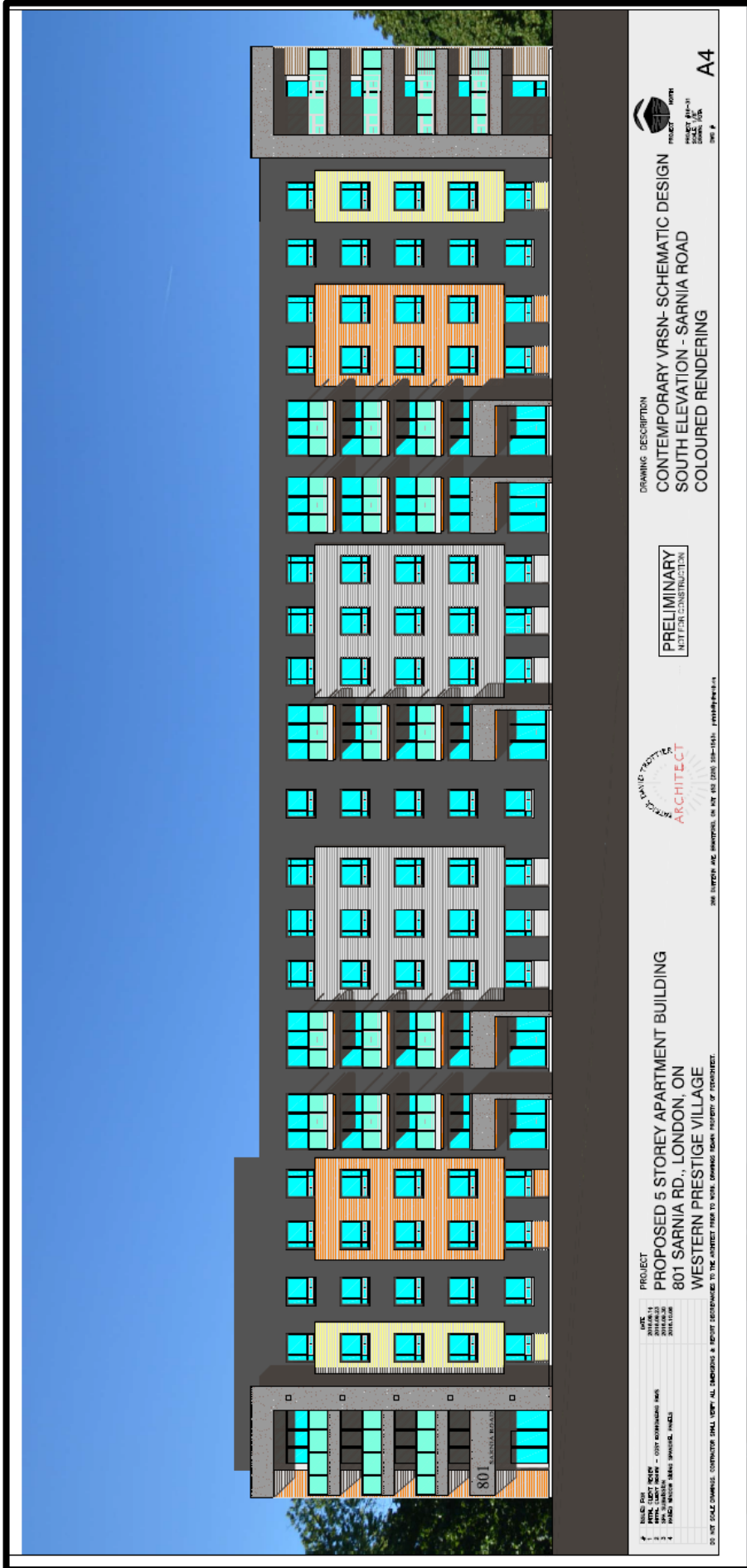
Proposed Enhanced Entrance Feature



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File: 39CD-15516/Z-8549/SPC15-036
 Planner: C. Smith

Front Elevation of Apartment- Sarnia Road



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File: 39CD-15516/Z-8549/SPC15-036
Planner: C. Smith

Rear, East and West Elevations

OVERALL WEST ELEVATION

OVERALL EAST ELEVATION

OVERALL NORTH ELEVATION

PROJECT
PROPOSED 5 STOREY APARTMENT BUILDING
801 SARNIA RD., LONDON, ON
WESTERN PRESTIGE VILLAGE

DATE
DATE PREPARED: 2015-05-15
DATE REVISED: 2015-05-15

SCALE
SCALE: 1/8" = 1'-0"

DO NOT SCALE DRAWING. CONTRACTOR SHALL VERIFY ALL DIMENSIONS & SPACING REQUIREMENTS TO THE PROJECT FROM THE MOST RECENT REVISIONS TO THE DRAWING.

ARCHITECT
PRELIMINARY
NOT FOR CONSTRUCTION

DRAWING DESCRIPTION
CONTEMPORARY VRSN- SCHEMATIC DESIGN
NORTH, WEST, EAST ELEVATIONS
COLOURED RENDERING

A4.3

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Front and Rear Elevations Townhome Block along Sarnia Road



Is the Proposed Application Appropriate?

Provincial Policy Statement (PPS)

This application has been reviewed for consistency with the 2014 Provincial Policy Statement. It is staff's position that the plan of condominium will provide for a healthy, livable and safe community. The proposed residential development will be on full municipal services. The site plan, development agreement and conditions of Vacant Land Condominium will address noise mitigation measures, landscaping, servicing and other development considerations. The site will not negatively impact any natural heritage areas or be affected by man-made hazards, and is located close to amenities and public open space.

The PPS supports publicly-accessible built and natural settings including parklands, open space areas, trails and linkages. The proposal incorporates a proposed enhanced public pathway that will connect the CP rail pedestrian overpass located to the west of the property, and Sarnia Road.

Based on our review, the proposed plan with appropriate conditions is consistent with the 2014 Provincial Policy Statement.

Hyde Park Community Plan

The lands were designated Multi Family Medium Density Residential through the Hyde Park Community Plan process. The Hyde Park Community Plan included Design Guidelines to assist in the review of development applications. The Design Guidelines required that noise walls be discouraged along arterial roads and that residential development be oriented to the arterial road.

The subject site will be accessed by the creation of a local driveway off of Sarnia Road. An enhanced public pedestrian path is proposed along the south boundary of the CP rail lands from Sarnia Road to the west property line. The public path system will be extended on the lands to the west (865 Sarnia Road) and will connect to the existing CP rail bridge overpass that connects to further pedestrian pathway systems to the north. The proposed path system meets the intent of the Community Plan's objective of strong pedestrian connectivity and will allow easy access to

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transit. The property will be developed with connection to full municipal services. Storm water management will be incorporated into the development to control water quantity and quality.

The proposed townhouse and apartment building will have four (4) metre front yard setbacks. The reduced front yard setback allows for a pedestrian oriented form of development. No noise walls are proposed along Sarnia Road and enhanced screening will be required to screen the parking area from Sarnia Road. The apartment building ground floor units are to have an operable door directly accessible to Sarnia Road in order to activate the streetscape and break up the bulk of the building. The townhomes that front onto Sarnia Road will have front doors oriented to Sarnia Road with direct sidewalk access. The development will provide a streetscape that is pedestrian oriented.

Given the location of the site and the manner in which it is to be developed, the current application provides for the integration of this site with the abutting land uses and is consistent with the Official Plan and Hyde Park Community Plan.

Official Plan Policies

The subject lands are designated Multi-Family, Medium Density Residential (MDR) which permits multiple-unit residential developments having a low-rise profile:

- Section 3.3.3 i) states: normally height limitations will not exceed four storeys. In some instances, height may be permitted to exceed this limit, if determined through a compatibility report as described in Section 3.7.3. to be appropriate subject to a site specific zoning by-law amendment and/or bonus zoning provisions; and
- Section 3.3.3 ii) states: Exceptions to the density limit may be made without amendment to the Official Plan for developments which qualify for density bonusing. Developments which are permitted to exceed the density limit of 75 units per hectare (30 units per acre) shall be limited to a maximum density of 100 units per hectare (40 units per acre).

The proposed apartment building has a total proposed height of 16m which results in a five-storey form. There are a total of 72 residential units proposed, which equates to a density of 96 units per hectare, and is consistent with development within the Multi-Family, Medium Density Residential Designation that contemplates a maximum of 100 units per hectare in return for the following design features, services and matters:

- Enhanced urban design features; and
- Enhanced public open space pathway corridor including an entrance feature along Sarnia Road.

The proposed 57 unit vacant land condominium development is a townhouse form of development with a maximum density of 40 units per hectare.

The proposed form of development meets the intent of the Multi Family Medium Density Residential Official Plan Policies.

Compatibility

The bonus provisions contained in the recommended Zoning By-law amendment to allow for the proposed apartment requires the development to be in accordance with the site concept (revised October 24, 2016) and the elevations contained in this report, with some minor variations to the building elevations at the discretion of the City Planner. They also identify the key design components that must be met, addressing the massing and location of the building, locations of main and supplementary entrances, architectural treatment, the placement of parking in unobtrusive locations, an enhanced public open space path corridor and entrance feature.

The proposed bonus zone will allow for site specific increases to the height and density of the apartment building in return for the above described matters which provide a public benefit. The proposed apartment is compatible with the abutting uses.

The applicant has requested special provisions to the zoning to allow for reduced front yard

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setback. The proposed reduced setback of four (4) metres along Sarnia Road allows for the street orientation of the townhouse units. The proposed townhouse units will be compatible with the size of housing in the existing adjacent neighbourhoods.

Ability of Site to Accommodate Development

The subject land is 2.4 hectares in size. The proposed site plan and draft plan of vacant land condominium illustrates how these lands are to develop for an apartment use and cluster townhouse dwellings. The site is of sufficient size to allow for the development of 57 townhouse units with on-site parking and common outdoor space and a 72 unit apartment building that is street oriented with screened parking.

Vacant Land in the Area

This parcel is within the Hyde Park Community Plan Area. This parcel of land is located on the north side of Sarnia Road and is designated Multi Family Medium Density Residential. The vacant lands located to the west are also designated for Multi Family Medium Density uses.

Building Size and Siting

As per the Official Plan policies of 3.3.3, the height of low-rise apartment buildings in the Multi-Family, Medium Density Residential Designation are normally up to four storeys with a maximum density of 75 units per hectare, however height and density may be increased in some instances when considered through a bonus zoning and where determined through a compatibility report. The proposed site is located adjacent to Sarnia Road and south of the CP rail line. A three storey retirement building is located immediately across from this site on the south side of Sarnia Road at 1242 Oakcrossing Road. A second three storey retirement building is currently being constructed on this site and will be located directly abutting Sarnia Road. The five-storey building will not adversely impact the amenities and character of the surrounding area. The site is located on an arterial road and is separated by the CP rail line from the low density residential uses to the north and is a suitable location for this higher built form.

The size of townhouses within this development will be regulated by the R6-5() Zoning regulations which includes a maximum height of 12 metres, lot coverage of 45% and minimum landscaped open space of 30%. The proposed development has a lot coverage of 31% and 41.5% landscaped open space. The applicant has proposed a maximum density of 40 units per hectare. The proposed locations, size and form of the townhouse development is compatible with surrounding land uses.

The requested R8-4*B_) bonus zone and special provisions to the R6-5 zone are appropriate and represent sound land use planning.

Site Access

This development will be accessed by the creation of a local driveway off of Sarnia Road. Full turning access into and out of the site is proposed. Through the Site Plan process right and left turn lanes will be required to be constructed on Sarnia Road.

Development Constraints

The subject lands are on the north side of Sarnia Road which is an arterial road and is bounded by the CP rail line on the north. Due to the impacts of noise generated by Sarnia Road and the CP rail line, the applicant has submitted a noise study to address this issue. CP rail has provided comments and recommendation that will permit the development as proposed to proceed. The C.P. recommendation and the recommendations in the accepted noise study will be included in the development agreement and the declaration and description of the condominium.

Staff has reviewed the rezoning request of the applicant and subject to staffs proposed amendments, bonus zone, special provision zone and holding provisions, the recommended zoning of the subject property to implement this development is appropriate and represents good land use planning.

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London Plan

The London Plan was approved by Council on June 23, 2016 and will provide the future framework for planning and development in the City of London upon adoption and endorsement by the Province of Ontario.

The London Plan place type for 801 Sarnia Road is 'Neighbourhood' with frontage onto a Civic Boulevard (Sarnia Road). The permitted uses for this site would range from stacked townhouses to low-rise apartments as well as emergency care establishments, rooming houses, and supervised correctional residences. The form of low-rise apartment buildings is contemplated up to four storeys, with the ability to increase to six storeys through bonusing. The proposed residential uses and scale of development are generally consistent with the intended function of the Neighbourhood Place Type.

Zoning By-law

The proposed Zoning By-law amendment is to remove the Urban Reserve (UR1) Zone. This zone permits existing dwellings and limited agricultural uses.

Residential R6

The proposed Residential Holding Residential R6 Special Provision (h.*h-34*h-65*h-100*R6-5(_)) Zone is used to implement Medium Density Forms of development and allows for cluster townhouse dwellings. Special provisions are recommended to provide the following:

- a minimum front yard setback of 4.0 metres,
- a maximum density of 39 units per hectare (57 units),
- a west interior side yard setback of 4.6 metres; and
- a rear setback of 10 metres

Residential R8

The proposed Residential R8-4 Zone is used to implement Medium Density Forms of development and allows for apartment forms of use. The proposed base Residential (R8-4) Zone would permit apartment uses with a maximum height of 13m and density of 75 units per hectare.

Residential R8 Bonus

The proposed Residential R8-4*B Zone implements a Multi Family Medium Density form of development that is bonused to allow:

- a maximum building height of 16 metres;
- a maximum of 72 dwelling units;
- a maximum density of 96 units per hectare;
- a minimum front yard setback of 4 metres,
- a minimum rear yard setback of 13 metres: and
- parking ratio of 1 parking space per 1 dwelling

In return for the following design features services and matters:

Building

- a building design which, with minor variations at the discretion of the Managing Director, Planning and City Planner, matches the site concept and elevation drawings shown in Schedule "1";
- The building includes a differentiated base, middle and top;
- Individual entrances to the apartments are located on the ground floor of the apartment building adjacent to Sarnia Road and function as front doors rather than patio doors
- Ground floor amenity spaces are to be designed as open courtyards extending wider than the balconies above and into the front setback;

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Site

- Provide glass or metal railings, or masonry walls to delineate the individual courtyards from the public realm.
- Railings are not to exceed 1m in height and masonry walls are not to exceed 0.9m in height in order to maintain visibility.
- Incorporate low landscaping to frame amenity areas.
- Direct walkway access from the front doors to the public sidewalk will be provided.
- Enhanced landscaping within the future public pathway corridor which includes additional plantings and public sitting areas;
- Enhanced entrance feature along Sarnia Road which includes the use of differencing paving materials, vegetation and public sitting areas: and
- All parking is to be located behind the building or enhanced screening.

The proposed Holding Residential R6 Special Provision (h.*h34-h-65*R6-5 (_)) Zone and Holding Residential R8*Bonus (h.*h34-h-65*R8-4* _)) Zone would permit a form of development that is consistent with the City of London Official Plan and the Hyde Park Community Plan.

Open Space OS1 (parkland dedication)

A ten (10) metre wide public pathway corridor located south of the CP rail lands extending from Sarnia Road to the westerly boundary of the site will be dedicated as parkland. The Open Space (OS1) Zone will permit the utilization of the 10 metre corridor to provide a public pathway from Sarnia Road to the western boundary. The intent is when the lands to the west of the site (865 Sarnia) are developed the 10 metre pathway corridor will be extended to the CP Rail Bridge located at 895 Sarnia Road. The parkland dedicated will allow the lands to be utilized as a multi-use pedestrian pathway that provides a connection from Sarnia Road to the subdivisions to the north of the railroad lines.

Proposed Holding Provisions

To ensure the orderly development of lands the following holding provisions are included in the proposed zoning amendment.

h - Purpose: To ensure the orderly development of lands and the adequate provision of municipal services, the "h" symbol shall not be deleted until the required security has been provided for the development agreement or subdivision agreement, and Council is satisfied that the conditions of the approval of the plans and drawings for a site plan, or the conditions of the approval of a draft plan of subdivision, will ensure a development agreement or subdivision agreement is executed by the applicant and the City prior to development.

h-34 Purpose: To encourage street oriented development and discourage noise attenuation walls along arterial roads, a development agreement shall be entered into to ensure that new development is designed and approved consistent with the design guidelines in the Hyde Park Community Plan, to the satisfaction City of London, prior to removal of the "h-34" symbol.

h-65 Purpose: To ensure there are no land use conflicts between the adjacent arterial roads and/or rail line and the proposed residential uses, the "h-65" shall not be deleted until the owner agrees to implement all noise and vibration attenuation measures, recommended in noise and vibration assessment reports acceptable to the City of London.

Evaluation of the Vacant Land Condominium Application

The same considerations and requirements for the evaluation of draft plans of subdivision also apply to draft plans of vacant land condominiums, such as:

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Planner: C. Smith

- This proposal is consistent with the objectives and policies of the Official Plan, and the Hyde Park Community Plan.
- The site can be serviced by sewer and water.
- The proposed development is in close proximity to community parks, open space and walking trails.
- A noise study was provided as part of site plan approval application, noise attenuation measures will be constructed to protect the outdoor amenity area for units within the development. Warning clauses will also be registered on title for the development.
- The proposed plan is designed to be integrated with adjacent lands.
- From a Placemaking perspective, the proposed development is oriented to Sarnia Road and the City's pedestrian path system which creates a sense of place by providing an active street frontage and path system. The proposed development meets the intent of the Placemaking principles.
- The applicant is proposing to construct 57 new townhouse dwellings on vacant land condominium units (lots). The proposed townhouses satisfy the locational and compatibility criteria of the Official Plan. Based on the size of the proposed lots and building footprints it is anticipated that the design of these townhomes will not have a negative impact on the character of the surrounding neighbourhood.
- The subject land is 2.4 hectares in size. The draft plan of Vacant Land Condominium illustrates how these lands are to develop for townhouse dwellings. The size of units and proposed buildings meet the community demand for housing type, tenure and affordability with the existing development in the area.
- The applicant must ensure that the proposed grading and drainage of this development does not adversely impact adjacent properties. All grading and drainage issues will be addressed by the applicants consulting engineer to the satisfaction of the City through the Site Plan Approval process.

The City may require applicants to satisfy reasonable conditions prior to final approval and registration of the plan of condominium, as authorized under the provisions of subsection 51(25) of the Planning Act. The applicant will be required to meet conditions of draft plan approval within a specified time period, failing which, draft plan approval will lapse.

In order to ensure that this vacant land condominium development functions properly, the following issues among others will be addressed through conditions of draft approval:

- completion of site works in the common elements and the posting of security in addition to that held under the Development Agreement (if applicable), in the event these works are not completed prior to registration of the plan of condominium;
- installation of fire route signs prior to registration;
- confirmation of addressing information;
- payment of outstanding taxes or local improvement charges, if any;
- provision of servicing easements for utility providers (such as London Hydro, Union Gas, Bell, etc.);
- ensuring that no homes are constructed prior to registration of the Vacant Land Condominium;
- ensuring that the Condominium Declaration to be registered on title adequately addresses the distribution of responsibilities between the unit owners and the condominium corporation for the maintenance of services, the internal driveway, fencing, and any other structures in the common elements;
- appropriate fencing is provided to mitigate any privacy issues;
- relocation of the existing municipal sidewalk;
- drainage and stormwater management; and
- noise attenuation.

SERVICING

Sanitary

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Planner: C. Smith

Sanitary servicing for this site will need to connect into the future sanitary sewer on Sarnia Road (to be constructed in 2017). Alternative extension of the sanitary sewer at the developer cost may be permitted prior to the City's work.

Storm

The proposed development will be required to provide for its storm water management through the implementation of permanent private system on-site (PPS) controls which will outlet to the existing storm drain on CPR right-of-way.

Transportation

An 18 metre road widening from the centreline is required along the Sarnia Road frontage of this plan and a left turn land and right turn taper is required to provide safe ingress and egress to this development. This will be required prior to site plan approval.

Water

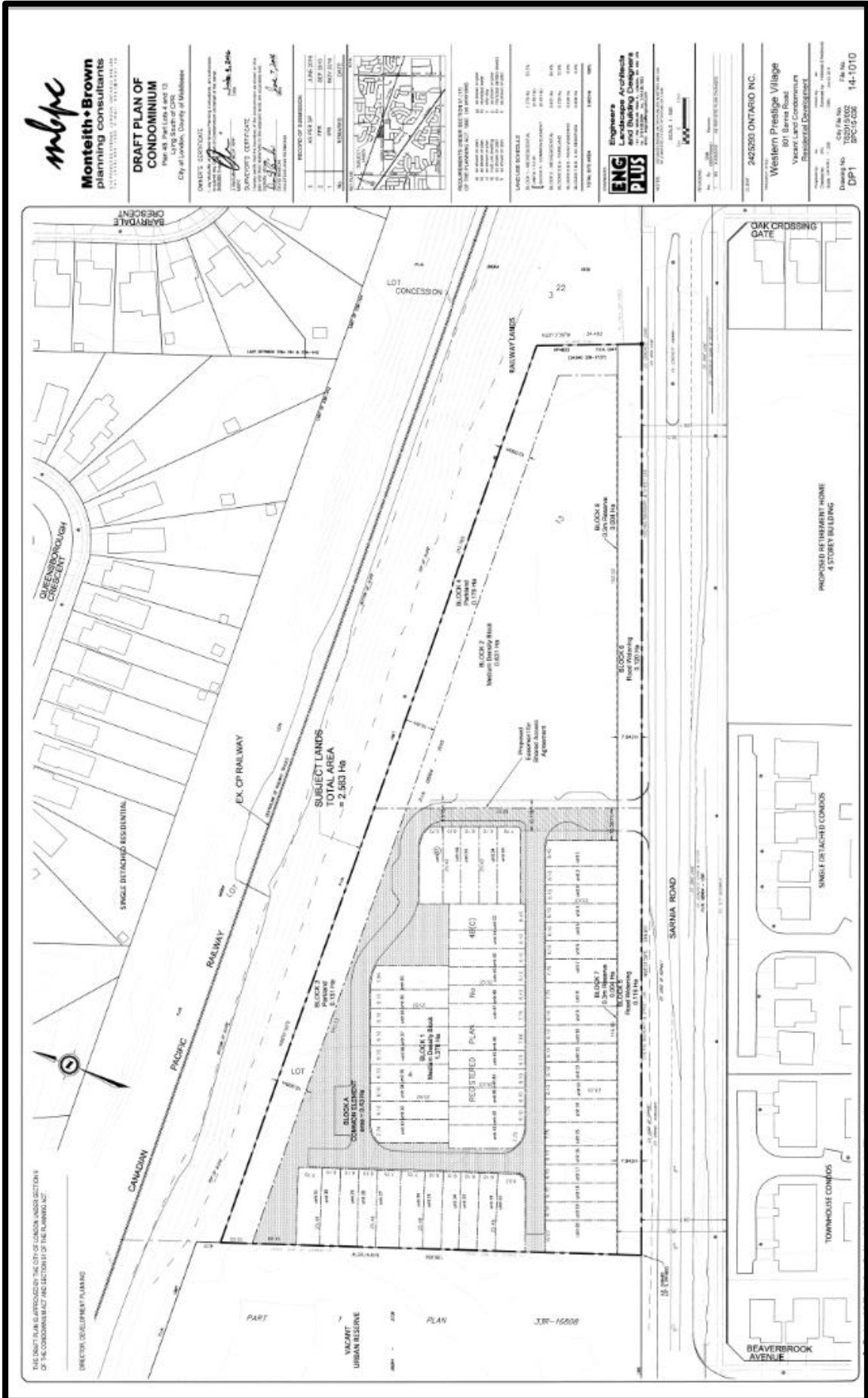
Water will be provided from the 400 mm diameter watermain on the south side of Sarnia Road pending compliance with the Ministry of the Environment and Climate Change Safe Drinking Water Act (SDWA) and O. Reg. 170/03 for regulated drinking water systems.

The Act is intended to protect public drinking water systems from contamination. The Act requires that regulated drinking water systems be monitored and maintained to ensure the provision of clean drinking water. Development that is deemed to be a regulated system must take appropriate steps to implement and maintain a clean drinking water system. Obligation to conform to the Act is ultimately the City's.

The City is evaluating and considering various resolutions to ensure that the intent of the Act is maintained and to allow for this development to move forward. Through the site plan approval process and through conditions of the vacant land condominium draft approval, the City will ensure that this issue is satisfied.

Holding Provisions to support the Engineering comments have been added to the amended Zoning By-law and these issues will also be addressed through Site Plan approval and conditions of draft approval for the Vacant Land Condominium.

Proposed Plan of Condominium



mbpc
Monteith • Brown
 planning consultants
 157 UNIVERSITY AVE. SUITE 200 TORONTO, ONT. M5S 1A6
 TEL: (416) 593-8888 FAX: (416) 593-8889
DRAFT PLAN OF SUBDIVISION
 Part 4B Part Lots 4 and 13
 City of London, County of Middlesex

OWNER'S CERTIFICATE
 I, the undersigned, being the owner of the land shown on the plan, hereby certify that the information contained therein is true and correct to the best of my knowledge and belief.
 Date: June 1, 2016
 Signature: [Signature]
SURVEYOR'S CERTIFICATE
 I, the undersigned, being a duly qualified and licensed surveyor, hereby certify that the information contained in this plan is true and correct to the best of my knowledge and belief.
 Date: June 7, 2016
 Signature: [Signature]

RECORD OF SUBMISSION

NO.	DATE	DESCRIPTION
1	2016.06.01	AS PER SP
2	2016.06.07	SPC 15-036
3	2016.06.07	PLAN
4	2016.06.07	CONTRACT



- REQUIREMENTS UNDER SECTION 37 (1) OF THE PLANNING ACT, 1968 (IN PARTIAL)**
- 1. TO BE SUBMITTED TO THE BOARD OF ZONING APPEALS AND REVIEW
 - 2. TO BE SUBMITTED TO THE BOARD OF ZONING APPEALS AND REVIEW
 - 3. TO BE SUBMITTED TO THE BOARD OF ZONING APPEALS AND REVIEW
 - 4. TO BE SUBMITTED TO THE BOARD OF ZONING APPEALS AND REVIEW
 - 5. TO BE SUBMITTED TO THE BOARD OF ZONING APPEALS AND REVIEW
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 - 8. TO BE SUBMITTED TO THE BOARD OF ZONING APPEALS AND REVIEW
 - 9. TO BE SUBMITTED TO THE BOARD OF ZONING APPEALS AND REVIEW
 - 10. TO BE SUBMITTED TO THE BOARD OF ZONING APPEALS AND REVIEW

LANDSCAPE SCHEDULE

BLOCK 1 - RESIDENTIAL (1.51 Ha)
 BLOCK 2 - RESIDENTIAL (1.31 Ha)
 BLOCK 3 - RESIDENTIAL (1.31 Ha)
 BLOCK 4 - RESIDENTIAL (1.31 Ha)
 BLOCK 5 - RESIDENTIAL (1.31 Ha)
 BLOCK 6 - RESIDENTIAL (1.31 Ha)
 BLOCK 7 - RESIDENTIAL (1.31 Ha)

ENGINEERS
ENG PLUS
 Landscape Architects
 801 Sarnia Road
 Sarnia, Ontario N6A 4K1
 Tel: (519) 336-1111
 Fax: (519) 336-1112
 Email: info@engplus.ca
 Website: www.engplus.ca

2452555 ONTARIO INC.
 Western Prestige Village
 801 Sarnia Road
 Vacant Land Condominium
 Residential Development

City File No. 7-18
 Drawing No. 39R-15516
 Project Code 14-1010
 Date 14-10-10

THIS DRAFT PLAN IS APPROVED BY THE CITY OF LONDON, UNDER SECTION 37 (1) OF THE CONDOMINIUM ACT AND SECTION 37 (1) OF THE PLANNING ACT.
 DIRECTOR, DEVELOPMENT PLANNING

PART 7 VACANT URBAN RESERVE PLAN 39R-15516

OAK CROSSING GATE
 PROPOSED RETIREMENT HOME
 4 STOREY BUILDING
 SINGLE DETACHED CONDOS
 TOWNHOUSE CONDOS
 BEAVERBROOK AVENUE

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Public Circulation Comments

- Height (5 storeys) is too high and will negatively impact the single detached dwellings on the north side of CP rail line; and
- Increase in traffic will have a negative impact on Sarnia Road.

View

The apartment is located within an area that is designated for multifamily forms of residential development. A reasonable expectation is that these lands will develop in a form that is compatible with the abutting land uses. Compatibility is not inclusive to or the same as existing development but should be consistent and in harmony with existing uses. The lands immediately to the north of the property (approximately 150 metres from the apartment building) are single detached dwellings. The single detached dwellings are separated from these lands by the CP rail line and a 2.4 metre solid noise attenuation wall. The increase in height (from 4 to 5 storeys); the separation of these uses; and the enhanced design should not have a significant impact on the adjacent single family dwelling neighbourhood. The proposed zoning permits an acceptable form of multifamily development along Sarnia Road and is compatible with the existing uses.

Traffic

A Traffic Impact Analysis report was submitted with the complete application and was reviewed by the City of London Transportation Division. No concerns were identified by the City. Traffic generated by the development will be accommodated by the existing road network.

CONCLUSION

Based on the above analysis, the proposed Zoning By-law Amendment, Site Plan and Vacant Land Condominium represents an efficient use of land and encourages compact urban form. The Provincial Policy Statement and the City’s Official Plan encourage this form of development. The applicant’s proposal for an apartment and townhouse development in this area is appropriate. Overall, this application represents good planning and is appropriate.

PREPARED and RECOMMENDED BY:	REVIEWED BY:
C. SMITH SENIOR PLANNER, DEVELOPMENT SERVICES	ALLISTER MACLEAN MANAGER, DEVELOPMENT PLANNING
REVIEWED BY:	SUBMITTED BY:
TERRY GRAWAY, MCIP, RPP MANAGER, DEVELOPMENT SERVICES & PLANNING LIAISON	G. KOTSIFAS, P.ENG MANAGING DIRECTOR, DEVELOPMENT & COMPLIANCE SERVICES & CHIEF BUILDING OFFICIAL

JCS/

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File: 39CD-15516/Z-8549/SPC15-036
Planner: C. Smith

Bibliography of Information and Materials

Request for Approval:

City of London Condominium Application Form, completed by Monteith Brown Planning Consultants, September 22, 2015.
Draft Plan of Vacant Land Condominium, September 22, 2015 and amended October 24, 2016.
Site Plan Approval Application, completed by Monteith Brown Planning Consultants, November 16, 2015.

Reference Documents:

City of London. Official Plan, June 19, 1989, as amended.
City of London. Zoning By-law No. Z.-1, May 21, 1991, as amended.
Ontario. Ministry of Municipal Affairs and Housing. Provincial Policy Statement, April 30, 2014.
City of London, Site Plan Control Area By-Law, September 19, 2011, as amended.
City of London, Condominium Submission, Review and Approval Guidelines, March 2003.

Correspondence:

*all located in City of London File No. 39CD-15516, Z-8549 or SP15-0036107 unless otherwise stated

Reports submitted with Application:

Noise and Vibration Study, Development Engineering, September 2015
Phase 1 Environmental Site Assessment exp Services Inc. June 2014
Tree Preservation Plan Eng Plus August 2014
Urban Design Brief Monteith Brown Planning Consultants September 2015
Proposed Site Servicing and SWM Plan, Eng Plus, September 2015
Water Distribution System Analysis, Eng Plus, September 2015

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Planner: C. Smith

Public Comments

This is in response to requests for comments contained in the Notice of Revised Application dated November 1, 2016.

Our property is across the railway tracks to the north-east of the subject location, with the back of our house and backyard in direct line of sight. Our concern revolves around the 5 story apartment building which, we believe, is much too high for the location. The apartment is to be built on the highest elevation of land in the entire area which will make it appear even higher. It will absolutely dominate the skyline of all existing property owners on the north side of the tracks and change the character of the existing neighbourhood.

With 72 units proposed in this high density building, a secondary concern is that there will be relatively high volumes of automobile traffic in and out of the surface parking area. According to the plan, entry to the parking area first requires driving around the westerly end of the apartment which will create a constant glare of headlights sweeping across properties north of the track. Cars exiting from the east portion of the parking area must drive around the eastern end of the apartment building – again causing a sweeping glare of headlights as they do so. The plan shows the planting of some trees and shrubs, but these seem inadequate to address this issue – especially in winter when leaves are off many trees.

The property in question is currently zoned for multi-family medium density residences which, we believe, results in appropriate height of buildings for this location and reasonable amounts of traffic entering and exiting parking areas. If there must be an apartment building included, we believe its height should be limited to no more than 3 stories which will provide a better fit with adjoining townhouses and the rest of the neighbourhood, as well as a more reasonable flow of in-site traffic.

Ron and Heather Madill
94 Barrydale Crescent
London, N6G 2X4

Mr. Smith,

Recently we received an "Notice of Revised Application" 39CD-15516/Z-8549. After reading through the proposed revisions, it becomes even more clear that this is not a proposal that we want to endorse. The proposal clearly states the land is designated as "Multi Family Medium Density Residential". The "Residential R8 Special Provision Bonus (R8-4()*B-) Zone to permit a 5 storey apartment unit" is completely unacceptable. Given the close proximity to Western University, this apartment building will become a student complex and not a "Family" residence.

We currently can see the 3 storey high "People's Care" Complex that is going in between Sarnia Road and the existing complex. To now tell us that a massive 5 storey apartment building will now even block this view is disheartening. We see that the proposal speaks of trees and shrubs, but unless these are mature trees and shrubs, then we will see the monstrosity that is being proposed.

We were under the impression when we built in this area 13 years ago, that it would remain an area of single family dwellings. An apartment building does not fit with this mold. Additional town or row houses is one thing, but a 16 metre high apartment building is just wrong.

We trust that our fair City of London **will not endorse** this "apartment building" that is being proposed.

Sincerely,

Betty Ann Westelaken
Jorge Teodoro

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File: 39CD-15516/Z-8549/SPC15-036
Planner: C. Smith

Bill No. (number to be inserted by Clerk's Office)
2017

By-law No. Z.-1-_____

A by-law to amend By-law No. Z.-1 to rezone an area of land located at 801 Sarnia Road.

WHEREAS 2425293 Ontario Inc. c/o Farhad Noori has applied to rezone an area of land located at 801 Sarnia Road, as shown on the map attached to this by-law, as set out below;

AND WHEREAS this rezoning conforms to the Official Plan;

THEREFORE the Municipal Council of The Corporation of the City of London enacts as follows:

- 1) Schedule "A" to By-law No. Z.-1 is amended by changing the zoning applicable to lands located at 801 Sarnia Road, as shown on the attached map, from an Urban Reserve (UR1) Zone to an Open Space (OS1) Zone, a Holding Residential R6 Special Provision (h.*h-34*h-65*R6-5(_)) Zone and a Holding Residential R8*Bonus (h.*h-34*h-65*R8-4*B-_) Zone.
- 2) Section Number 4.3 iv) – Site Specific Bonus Provisions is amended by adding the following Bonus Provision:

) B-___ 801 Sarnia Road

The development shall be in accordance with the site concept and elevations attached as Schedule "1" of this By-law which includes an apartment building with a maximum height of 16 metres with total maximum of 72 units (96 units per hectare). The development shall specifically incorporate the following services, facilities and matters:

Building

- a building design which, with minor variations at the discretion of the Managing Director, Planning and City Planner, matches the site concept and elevation drawings shown in Schedule "1";
- The building includes a differentiated base, middle and top;
- Individual entrances to the apartments are located on the ground floor of the apartment building adjacent to Sarnia Road and function as front doors rather than patio doors
- Ground floor amenity spaces are to be designed as open courtyards extending wider than the balconies above and into the front setback;

Site

- Provide glass or metal railings, or masonry walls to delineate the individual courtyards from the public realm.
- Railings are not to exceed 1m in height and masonry walls are not to exceed 0.9m in height in order to maintain visibility.
- Incorporate low landscaping to frame amenity areas.
- Direct walkway access from the front doors to the public sidewalk will be provided.
- Enhanced landscaping within the future public pathway corridor which includes additional plantings and public sitting areas;
- Enhanced entrance feature along Sarnia Road which includes the use of differencing paving materials, vegetation and public sitting areas: and
- All parking is to be located behind the building or enhanced screening.

Notwithstanding anything in the By-law to the contrary the following regulations shall

apply:

- | | | |
|------|--|---|
| i) | Front Yard Depth
(minimum) | 4.0 metres (13.1 feet) |
| ii) | Rear Yard Setback to Open Space
(OS1) Zone
(minimum) | 13 metres (42.6 feet) |
| iii) | Height
(maximum) | 16 metres (52.4 feet) |
| iv) | Density
(maximum) | 96 units per hectare
(72 dwelling units) |
| v) | Parking
(minimum) | 1 space per dwelling unit |

3) Section Number 10.4 of the Residential R6 (R6-5) Zone is amended by adding the following Special Provision:

) R6-5 ()

a) Regulations:

- | | | |
|------|---|------------------------|
| i) | Front Yard
Setback
(Minimum): | 4 metres (13.1 feet) |
| ii) | West Interior Side
Yard Setback
(Minimum): | 4.6 metres (15.0 feet) |
| iii) | Rear Yard Setback to Open Space
(OS1) Zone
(Minimum): | 10 metres (13.1 feet) |
| iv) | Density
(Maximum) | 39 units per hectare |

The inclusion in this By-law of imperial measure along with metric measure is for the purpose of convenience only and the metric measure governs in case of any discrepancy between the two measures.

This By-law shall come into force and be deemed to come into force in accordance with Section 34 of the *Planning Act, R.S.O. 1990, c. P.13*, either upon the date of the passage of this by-law or as otherwise provided by the said subsection.

PASSED in Open Council on January 17, 2017.

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Planner: C. Smith

Matt Brown
Mayor

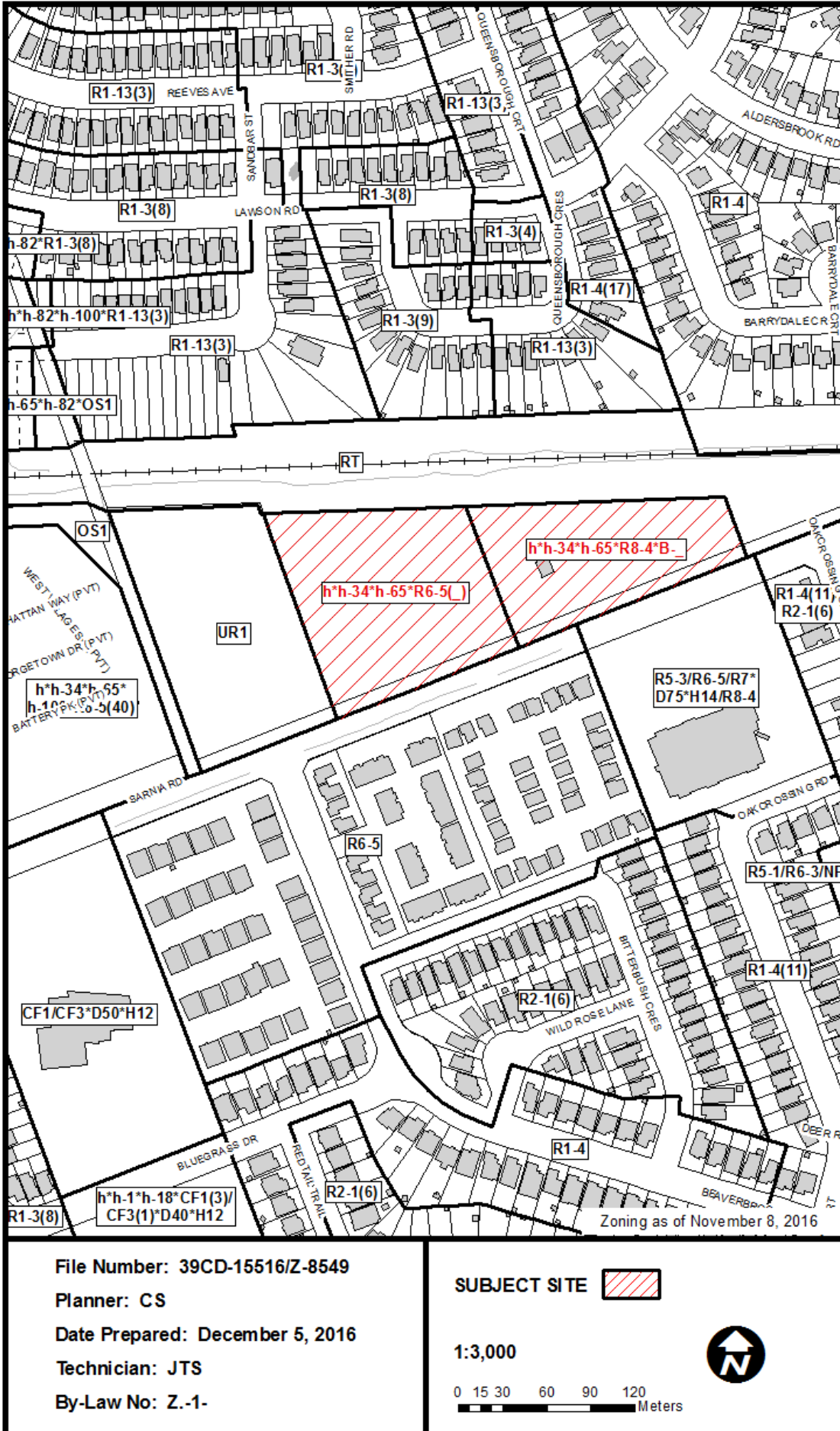
Catharine Saunders
City Clerk

First Reading - January 17, 2017
Second Reading - January 17, 2017
Third Reading - January 17, 2017

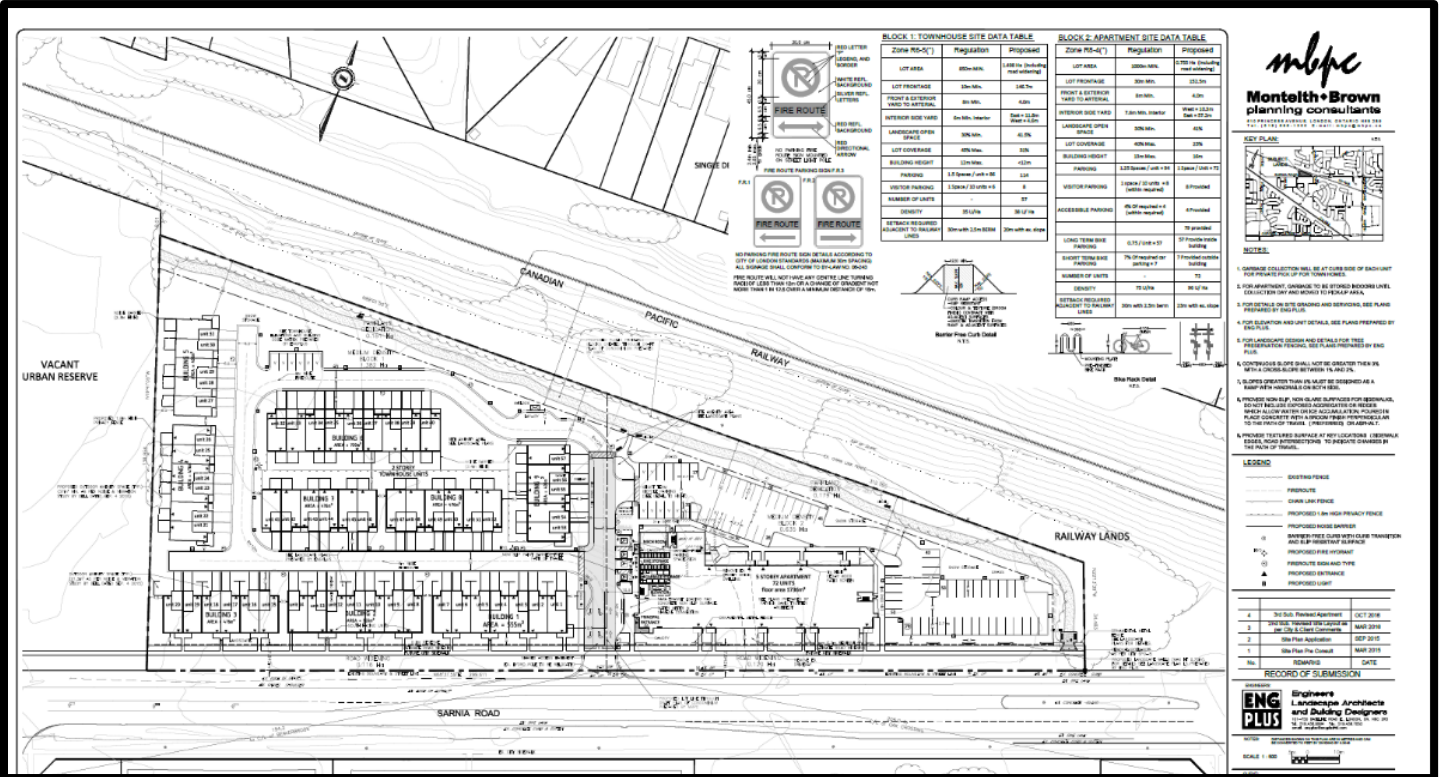
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File: 39CD-15516/Z-8549/SPC15-036
Planner: C. Smith

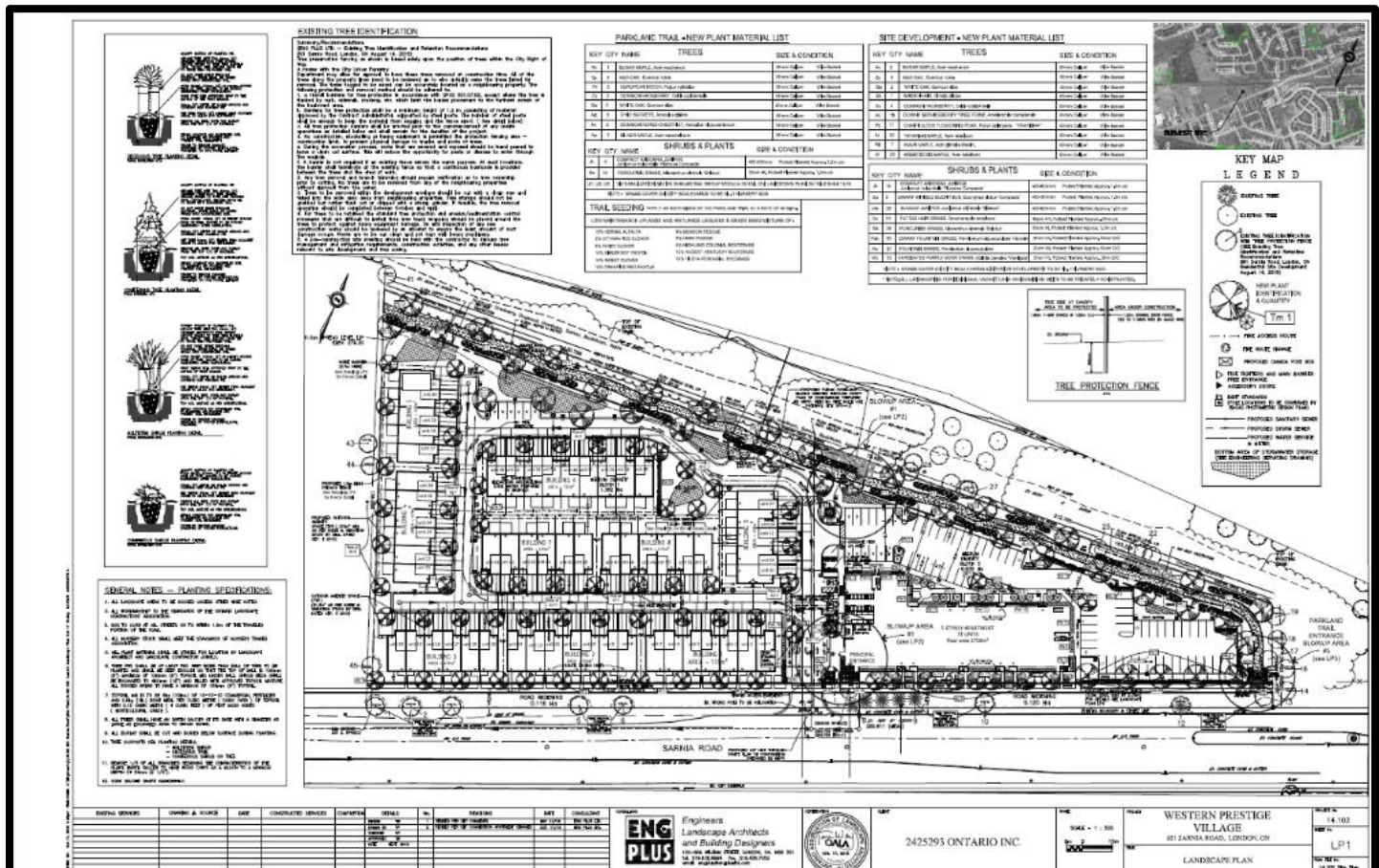
AMENDMENT TO SCHEDULE "A" (BY-LAW NO. Z.-1)



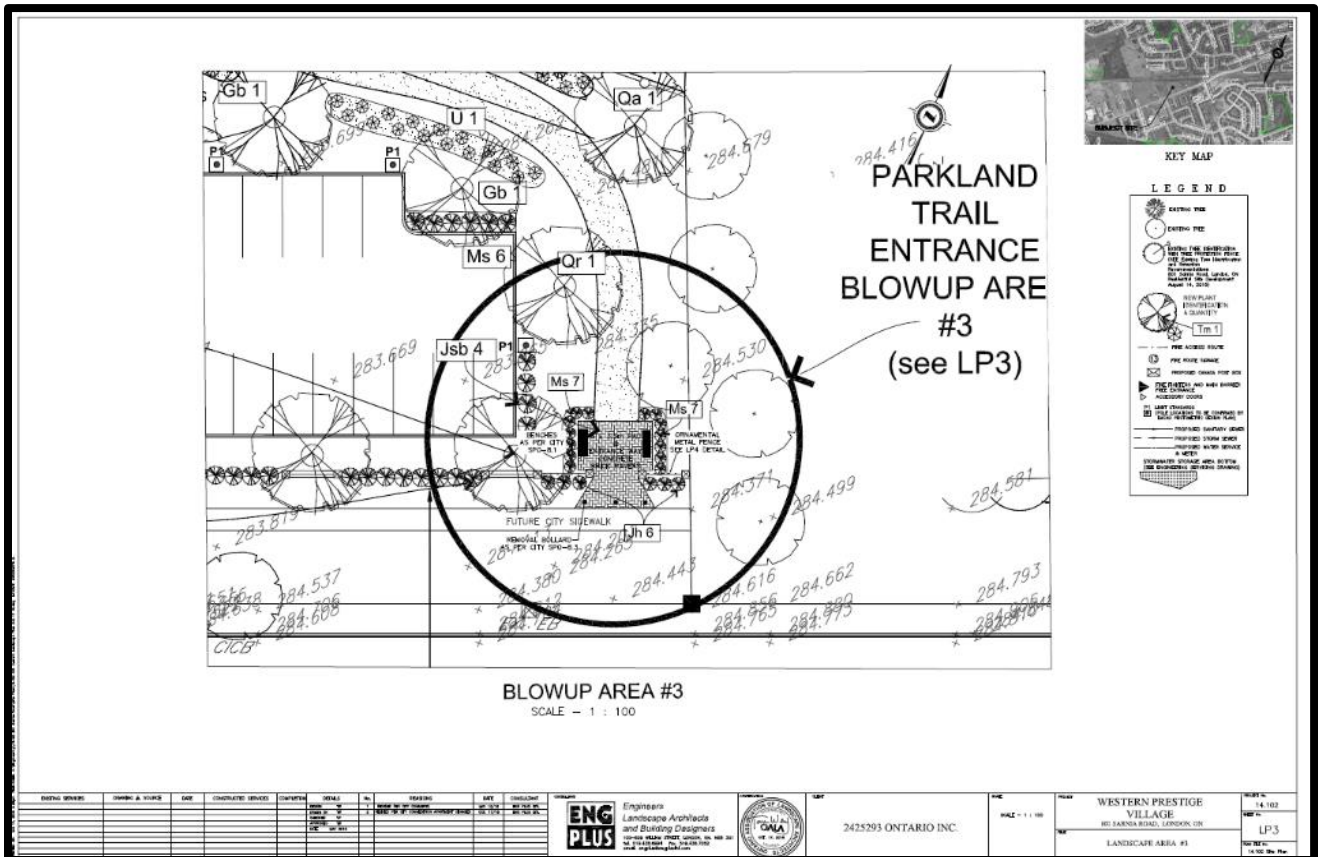
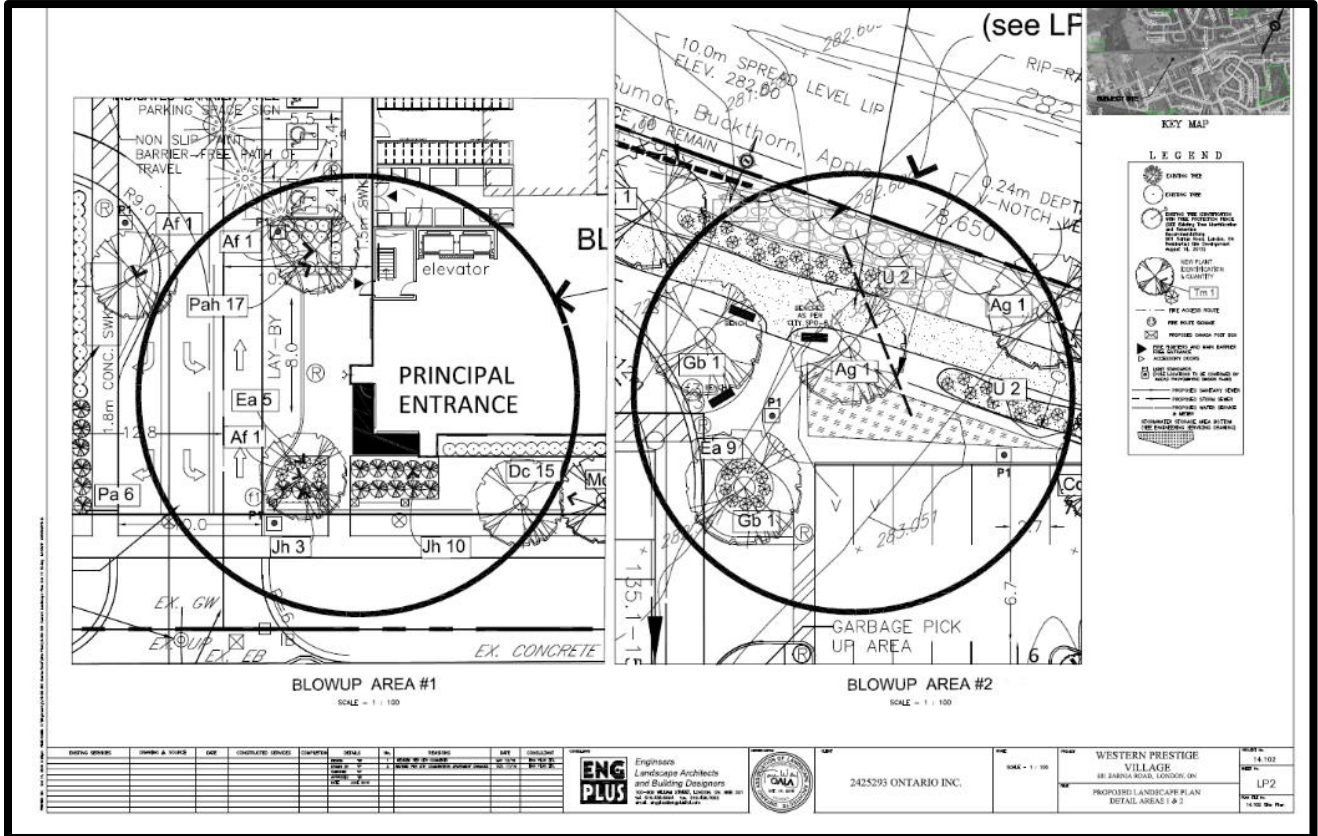
Schedule 1- Site Concept and Elevations



Landscape Plans



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File: 39CD-15516/Z-8549/SPC15-036
 Planner: C. Smith

Elevations



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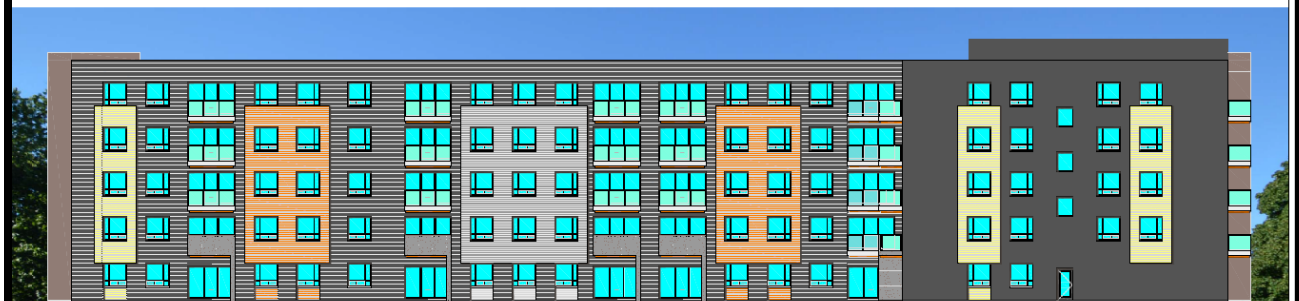
File: 39CD-15516/Z-8549/SPC15-036
 Planner: C. Smith



OVERALL WEST ELEVATION



OVERALL EAST ELEVATION



OVERALL NORTH ELEVATION



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File: 39CD-15516/Z-8549/SPC15-036
 Planner: C. Smith

ELEVATION MATERIAL TYPES

- 1 Corrugated metal horizontal siding; Vlcwest 7/8" Rounded Corrugation. Colour: Charcoal or Dark Brown.
- 2 EIFS; Outsulation system. Colour: med concrete gray / antique gray. Texture: sandblast or sandpebble fine finish.
- 3a a/b/c: Hardl Board prefin composite concrete siding system. Colours vary from yellow, orange, white, mahogany.
- 4 Prefin white aluminum clear glazed windows, c/w 1 sliding door & 2 fixed sidelight door panels &/or window vent system (depends on location).
4a: Prefin white aluminum clear glazed storefront type entry system.
- 5 Prefin white aluminum clear glazed windows. Vented with slider, c/w extended and 4" dropped prefin white metal flashing w/ drip.
- 6 Prefin white aluminum clear glazed balcony/patio entry system. C/w 1 sliding door & 2 fixed sidelight door panels, all raised 6" above floor level.
- 7 Prefin white aluminum clear glazed balcony guard. Balcony level c/w prefin white metal flashing surround.
- 8 Decorative lighting. To be determined.
- 9 Building numbering & Street Identification
- 10 Prefin metal siding screen to roof top mech units. Type, location and size to be determined.

801 SARNIA ROAD

3b) Sliding on inside wall & soffit. Same each balcony above.

4" dropped pre-fin soffit.

<p>4 BUILD FOR</p> <p>DATE: 2014.12.11</p> <p>PROJECT: PROPOSED 5 STOREY APARTMENT BUILDING</p> <p>801 SARNIA RD., LONDON, ON</p> <p>WESTERN PRESTIGE VILLAGE</p>	<p>ARCHITECT: WOODS BAYD PROTECT ARCHITECT</p> <p>PRELIMINARY NOT FOR CONSTRUCTION</p>	<p>DRAWING DESCRIPTION: CONTEMPORARY VRSN- SCHEMATIC DESIGN SOUTH ELEVATION DETAIL - WEST SIDE MATERIAL SCHEDULE</p> <p>PROJECT NO: 14-001</p> <p>DATE: 2014.12.11</p> <p>SCALE: 1/8" = 1'-0"</p> <p>REV: 0</p>
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A4.4

<p>4 BUILD FOR</p> <p>DATE: 2014.12.11</p> <p>PROJECT: PROPOSED 5 STOREY APARTMENT BUILDING</p> <p>801 SARNIA RD., LONDON, ON</p> <p>WESTERN PRESTIGE VILLAGE</p>	<p>ARCHITECT: WOODS BAYD PROTECT ARCHITECT</p> <p>PRELIMINARY NOT FOR CONSTRUCTION</p>	<p>DRAWING DESCRIPTION: CONTEMPORARY VRSN- SCHEMATIC DESIGN SOUTH WEST ENTRY SKETCH</p> <p>PROJECT NO: 14-001</p> <p>DATE: 2014.12.11</p> <p>SCALE: 1/8" = 1'-0"</p> <p>REV: 0</p>
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