

20.4 SECONDARY PLAN

FINAL DRAFT - SEPTEMBER 2011



LONDON PSYCHIATRIC HOSPITAL LANDS

CITY OF LONDON



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London Psychiatric Hospital Secondary Plan

September 2011

Introduction

20.4.1

20.4.1.1 Introduction

The London Psychiatric Hospital (LPH) Secondary Plan is applied to the area on the east side of Highbury Avenue North between Oxford Street East and Dundas Street and is generally bounded on the east side by a CNR spur line and comprises approximately 77 hectares (180 acres) of land. The majority of the lands were under public ownership and used as a mental health facility since the 1870's. The majority of the lands were owned, at the time of the adoption of this Secondary Plan, by the Government of Ontario with the remainder owned by the Government of Canada, the Salvation Army, the Diocese of London and a private landowner.

20.4.1.2 Purpose and Use

The purpose of the Secondary Plan is to establish a vision, principles and policies for the evolution of the former Provincial mental health facility property and adjacent lands to a vibrant residential community which incorporates elements of sustainability, mixed use development, heritage conservation, walkability and high quality urban design. This Secondary Plan provides a greater level of detail than the general policies in the Official Plan. The Secondary Plan serves as a basis for the review of planning applications and constitutes Official Plan policy which will be used in conjunction with the other policies of the Official Plan.

The goals, objectives, policies and schedules of the City's Official Plan shall apply to all lands within the study area, except in instances where more detailed or alternative direction is provided in the Secondary Plan, in which case the Secondary Plan shall prevail.

The text and schedules of the London Psychiatric Hospital Secondary Plan constitutes Section 20.4 of the City of London Official Plan. The Schedules form part of the Secondary Plan and have policy status whereas other maps, graphs, tables, illustrations and photographs included in this Secondary Plan are provided for graphic reference, illustration and information.



Vision

The redevelopment of the London Psychiatric Hospital Lands shall reflect the historic significance of the lands to create an urban village with a strong pedestrian focus enhanced by ample public green spaces that link places to live, work and play. The community will embrace sustainable development principles that place priority on multi-modal transportation choices integrated with a diverse range of land uses, lifestyle and cultural opportunities.



20.4.1.3 Principles of the Secondary Plan

The preparation of this Secondary Plan has been guided by a series of principles that are described below. Any amendments to this Secondary Plan shall be consistent with these principles.

20.4.1.4 Retention of Cultural Heritage

i) Principle

Retain as much of the identified cultural and heritage resources of the area as possible.

ii) Objectives

- a) Celebrate the area's built and cultural heritage.
- b) Create a distinct urban community that builds upon the heritage significance of the property.
- c) Create a strong sense of place that relates to the heritage character of the property.
- d) Conserve heritage designated buildings and landscapes.
- e) Conserve the cultural heritage landscape.
- f) Encourage sustainable re-use of heritage buildings.

20.4.1.5 Creation of a Distinct Community

i) Principle

Create inclusive and diverse residential neighbourhoods which have a mix of uses and a high level of urban design.

ii) Objectives

- a) Provide for a range of land uses including residential, open space, public uses, local commercial uses, office uses, mixed use and regional educational uses.
- b) Foster a sense of community interaction by connecting neighbourhoods with public uses and the open space system.
- c) Provide for the extension of necessary public services and facilities to support the community.
- d) Enhance recreational opportunities throughout the neighbourhoods.
- e) Identify opportunities for places and activities that foster community identity.
- f) Ensure the Treed Allée remains a focal point for the Community.
- g) Enhance the public realm, including streetscapes, public spaces and infrastructure.
- h) Integrate the new community with surrounding neighbourhoods.
- i) Focus the design of roads and future development around significant built features.
- j) Ensure the design of roads and buildings is appropriately scaled to and in character with significant built features.
- k) Design the community street pattern to create or enhance view corridors and vistas of public spaces and significant built features.

20.4.1.6 Providing for a Range of Housing Choices

i) Principle

Provide for a mix of housing types and designs.

ii) Objectives

- a) Provide for a range and mix of housing types in order to achieve a balanced residential community.
- b) Encourage housing developments and designs that achieve compact residential development.
- c) Achieve an urban form which makes effective use of land, services, community facilities and related infrastructure.
- d) Ensure that the community caters to the needs of all ages, stages of life and income groups.
- e) Provide opportunities for live-work opportunities to reduce the need for commuting and improve community safety.
- f) Provide housing opportunities to accommodate people with special needs, students and seniors.
- g) Provide affordable housing opportunities.



20.4.1.7 Environmental Sustainability

i) Principle

Achieve high standards of environmental sustainability.

ii) Objectives

- a) Seek out opportunities for sustainability.
- b) Integrate the Community into the greater city through walking/biking trails to reduce the need to use private vehicles.
- c) Create an interconnected open space system.
- d) Enhance livable neighbourhood ideals using public green spaces as a significant design feature and by fostering walkable neighbourhoods.
- e) Encourage LEED approved developments.
- f) Encourage the planting of vegetation to improve air quality.
- g) Encourage community wide recycling.
- h) Protect species at risk and rare species.



20.4.1.8 Transportation System

i) Principle

Walking and bicycling should be the primary means of transportation within the community, while providing for broader connections to the City-wide transportation network.

ii) Objectives

- a) Provide a traditional grid street network to encourage walkability within the community.
- b) Give priority to pedestrian movement.
- c) Establish a high degree of connectivity within the Community and between existing and new neighbourhoods.



20.4.1.9 Financial Viability/Sustainable Development

i) Principle

The Community shall be developed in logical phases to be efficient and financially responsible.

ii) Objectives

- a) Extend infrastructure in a logical and cost-effective manner.
- b) Establish key heritage and community features through the earliest phases of development.
- c) Ensure a Stewardship Plan is in place to identify how the Provincially significant features are to be maintained and identify suitable sources of funding.





Community Structure Plan

20.4.2

20.4.2.1 Community Structure Plan

The Community Structure Plan is illustrated in Schedule 1 of this Plan and sets out the overall structural elements for the LPH Secondary Plan area. The elements identified in this section of the Plan are intended to assist with implementing the vision for the built form, public realm and neighbourhood street pattern. Development proposed through planning applications for the LPH lands shall implement the following community structure objectives:

- i) The designated heritage buildings, the Treed Allée and the cultural heritage landscape and its significant features shall be conserved;
- ii) Development patterns shall generally reflect an urban grid street network with a high level of connectivity. The central north-south axis shall serve as a key organizing element;
- iii) The Treed Allée shall be closed to vehicular traffic and used as public open space;
- iv) A landscaped pedestrian corridor shall be provided between the Allée and the Infirmary building;
- v) Each heritage building shall be provided with its own landscaped setting. A larger setting shall be established around the Horse Stable to provide agricultural context and maintain open views of the building;
- vi) Remnants of the historic circulation pattern, including the traffic circle and portions of the ring road, shall be integrated with the new street networks. Where possible, the existing mature trees that flank these roads shall be retained;
- vii) Five primary gateways to the lands shall be created, located centrally along each of Oxford Street East, Highbury Avenue North and Dundas Street. The Dundas Street gateway will be pedestrian only and should help anchor the Treed Allée. The central Oxford Street East gateway will be pedestrian only and will maintain the vista into the heart of the Community;
- viii) Street connections shall be provided to Howland Avenue, Rushland Avenue and Spanner Street in the neighbourhood to the east so that new roads may extend east-west through the lands;
- ix) Possible future street connections shall be accommodated for access to the lands east of the LPH Secondary Plan area;
- x) Pedestrian and cycling routes shall link the central node to transit hubs and parkland;
- xi) A central mixed-use activity node will be located around the centre of the lands, incorporating the existing heritage buildings;
- xii) The intersections of Oxford/Highbury and Highbury/Dundas shall be developed as intensive, transit-oriented mixed-use nodes;
- xiii) Development shall generally be most intensive along Oxford Street East, Dundas Street and Highbury Avenue North.

20.4.2.2 Cultural Heritage Landscape

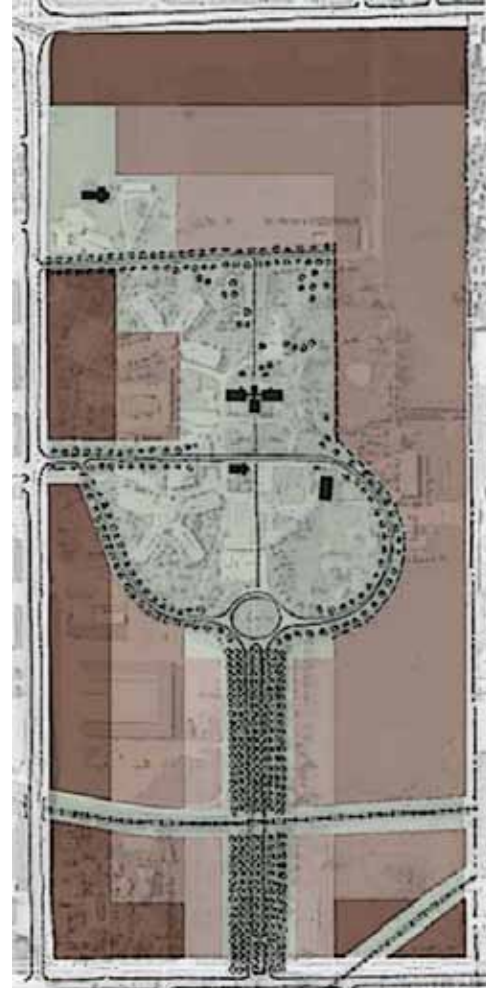
Schedule 7 of this Secondary Plan identifies the cultural heritage landscape. This landscape has played an important role in the preparation of this plan and will be conserved through the development of the LPH Secondary Plan. Originally known as the London Asylum, the 19th Century London Psychiatric Hospital complex may be one of the most significant sites in the history of mental health in Canada. Part of this significance derives from its association with Dr. Richard Bucke (superintendent, 1877-1902), who championed 'moral treatment' in the care of mental health patients.

Based on the Conservation Plan prepared for this Secondary Plan more intensive development shall be directed to property around the perimeter of the lands, particularly along Highbury Avenue North and Oxford Street East, with a shallower landscaped "bowl" in the middle of the lands. Within this central area, the Infirmary Building, the Chapel, and the entrance avenue retain their heritage character and rich landscape setting. The access and circulation routes shall build on the historic road patterns which will also maximize opportunities for the retention of mature trees.

Development proposed through planning applications for the London Psychiatric Hospital Secondary Plan area will need not only to conserve the significant heritage buildings, but also the unique cultural heritage landscape. Maintaining the integrity of the significant cultural heritage landscape will be a priority in the review of land use planning applications. The therapeutic landscape setting and its physical and visual relationships to historic buildings must be conserved to allow for meaningful interpretation of the cultural heritage resources.

Other landscape features to be considered in the review of all planning applications include:

- i) The large traffic circle, with its landscaped island, located at the terminus of the Treed Allée;
- ii) Remnants of the historic ring road circulation pattern which encircled the original hospital complex, between the Allée and the Infirmary building;
- iii) The westerly access to the lands which extends inward from Highbury Avenue North, connecting the internal circular patterns and the external grid;
- iv) The row of mature trees which line the southern edge of the central ring road;
- v) The lesser Allée which extends northward from the rear of the Infirmary building, continuing the central north-south axis as an ordering element for the lands; and
- vi) Mature specimen trees which line historic circulation routes and frame the "pleasure grounds".



20.4.2.3 Heritage Landmarks

The majority of the London Psychiatric Hospital lands have been recognized as a cultural heritage landscape of Provincial significance. Further, several features on the lands, including the Central Treed Allée, the Infirmary Building, the Recreation Hall, the Chapel of Hope, and the Horse Stable, are all designated by the City of London under Part IV of the *Ontario Heritage Act*. These cultural heritage resources shall be conserved. Specific policies relating to development within and adjacent to the cultural heritage landscape and its associated significant features are outlined throughout this Secondary Plan. Significant cultural heritage resources to be conserved within the landscape include:



Central Treed Allée: an entry avenue consisting of two one-way roads and a wide median containing a pedestrian walk is lined with several parallel rows of trees. While originally planted with elms, the Allée today consists of a variety of tree species, both coniferous and deciduous. The Allée forms a magnificent vista north from Dundas Street into the lands and terminating at the Infirmary building.



Infirmary Building (1902): this symmetrical, three storey Victorian yellow brick building is aligned on axis with the entrance avenue. A central surgical block, complete with a rare surviving operating room, is connected by two passageways to east and west patient wings. Large skylights provided light for the surgical suite on the third floor. Each patient wing features large sun rooms along the side building flankages.

Chapel of Hope (1884): built by patients as an interdenominational chapel, it is one of the only free-standing chapel buildings within a psychiatric hospital site in Ontario. The chapel is a one-and-a-half storey brick structure with a gable roof, built in the Gothic Revival style.

Horse Stable (1894): built of buff-coloured brick with a slate roof, the Horse Stable is the last of three original agricultural buildings. While the building was functional, the picturesque effect produced by its classical proportions and ventilation cupolas also make it a handsome landmark building. The stable is a meaningful symbol of the hospital's significant agricultural past, recalling the importance of farm work to patient therapy and community self-sufficiency.



Recreation Hall (1920): this two storey brown brick building was used to host recreational activities for patients, including a basement swimming pool (now filled in) and a stage with a balcony. The auditorium space features large tall windows on each side, and a double door centre entrance which faces north.

Schedule 7 of this Plan identifies the heritage features designated under the *Ontario Heritage Act*.

20.4.2.4 Edges and Interfaces

Major arterial roads (Oxford Street East, Dundas Street and Highbury Avenue North) clearly define the community's edges on three sides, to the north, south and west. New development should build upon and integrate established patterns found in the abutting neighbourhood to the east. This adjacent neighbourhood should transition into the study area and function as an extension of the new community. An industrial cluster is also located immediately to the east, separated from the community by a rail spur. There may be opportunities to integrate these lands in the long term if they transition from industrial to residential uses over time. Necessary future connections to allow for this potential shall be protected.



20.4.2.5 Nodes

A central node will be located in the area between the terminus of the Allée, the Infirmary building, and Highbury Avenue North. This node will function as the centrepiece of the community and should accommodate a range of uses including convenience commercial establishments that cater to the daily needs of residents or serve as community gathering places. The central node will be pedestrian-oriented and include public amenity space.



Two mixed-use nodes are to be established around the intersections of Oxford/Highbury to the north, and Highbury/Dundas to the south. Both of these nodes should continue to serve as minor transit hubs, and shall be pedestrian-oriented with increased densities to support this role.

20.4.2.6 Linkages and Transportation System

The overall transportation system of a community is an integral component in creating a strong sense of place. This type of system is known as a place-based transportation system, which promotes connectivity through the community and to the larger city. It also promotes opportunities for healthy movement to various activities with a priority on providing a high quality pedestrian experience.

Pedestrian, cycling and vehicular access will be provided through new streets extending from both Highbury Avenue North and Oxford Street East. New streets will extend east-west throughout the lands supporting an urban street grid pattern. Specifically, Howland Avenue, Rushland Avenue and Spanner Street are to be connected and extended through the lands.

Two possible future street connections to the east, between Rushland Avenue and Spanner Street are also illustrated, to recognize that the industrial cluster may evolve over time and that long-term connection opportunities should be identified and planned for.





The existing Treed Allée driveways will be closed to vehicular traffic and serve only as pedestrian corridors. Other elements of the historic circulation pattern such as the traffic circle and portions of the ring road will be integrated with new street networks. Retaining elements of this historic pattern will reinforce the significance of the central node. These routes focus on moving people between the central node and transit hubs. The Allée/linear park will serve as the backbone of this circulation system and will provide a pedestrian/cycling connection across the Canadian Pacific Rail line. This central pedestrian spine will extend through the centre of the lands, from Dundas Street to Oxford Street East. Secondary routes will extend to the northeast and northwest from the central node, towards the Fanshawe College and the Oxford/Highbury transit nodes respectively. A pedestrian/cycling corridor is also shown along the Canadian Pacific Rail line which will contribute to establishing an east-west connection between the area's park systems, including Kiwanis Park to the south and McCormick/Carling Heights parks to the west.

Transit service is expected to continue along the three flanking arterial roads. Three existing transit nodes are illustrated on the Community Structure Plan, as well as a potential new transit node on Highbury Avenue North. Transit service, internal to the lands, is not anticipated as established bus routes already travel around the perimeter of the lands and are within comfortable walking distance to and from all parts of the Secondary Plan Area.

20.4.2.7 Building Height Plan

Development is envisioned to be of a generally high-rise form along the flanking arterial roads. Mid-rise forms are envisioned within and approaching the central node. Development patterns on the balance of the lands are expected to be predominantly low and/or mid-rise. More precise height ranges are identified in Schedule 4 of this Plan.

20.4.2.8 Urban Design Priorities

Schedule 8 of this Plan shows urban design priorities. The Plan supports a well-connected urban street grid pattern, with a clearly defined neighbourhood centre. The intent is to create an urban village that reflects London's traditional urban development patterns, focusing on the lands' significant built heritage and cultural landscape features.

Historically, the psychiatric hospital campus was organized around a central north-south axis, extending up the Allée and through the symmetrical hospital buildings. This north-south axis shall remain a key organizing element for future road patterns.



Character Area Land Use Designations

20.4.3

20.4.3.1 Character Areas – Secondary Plan Land Use Designations

The following character areas form land use designations unique to the LPH Secondary Plan as shown on Schedule 2. These areas have separate identified character elements, which are defined in the character area policies. The character areas and general policies of this Plan work together to form the overall structure of the Secondary Plan.

i) Village Core

The Village Core is to form the main street, mixed-use “heart” of the LPH community and is to be pedestrian-oriented.

ii) Transit Oriented Corridor Area

This area is to support the transit functions along the assigned corridors with intense mixed-use development. The character in this area is to be pedestrian-oriented.

iii) Academic Area

The Academic Area is to facilitate the expansion of post-secondary academic institutions, both in terms of built form and open space in an orderly fashion. This will help to build an urban streetscape along Oxford Street East.

iv) Residential Area

The Residential Area designation is applied to areas of this plan that will contain a variety of residential building types.

v) Heritage Area

The Heritage Area designation applies to areas that contain heritage buildings and cultural heritage resources and will focus on protection and conservation of these resources.

vi) Open Space Area

The Open Space Area is to provide for passive and active recreational opportunities within the community. These areas will also serve as a buffer for the industrial uses that exist to the east of the Secondary Plan area.



20.4.3.2

Village Core Designation**i) Function and Purpose**

The Village Core is located half-way between Dundas Street and Oxford Street East within the western portion of the secondary planning area. This area is to be the main focal point for neighbourhood level services within a comfortable walking distance for most residents. The Village Core will serve to transition from the high-rise built form along Highbury Avenue North to the low-rise built form internal to the neighbourhood. This area will provide for a mix of uses and civic functions, such as public/private gathering spaces. The Village Core is made up of three sub-areas, as follows:

- a) Main Street/Live-Work;
- b) Mixed-Use Office; and,
- c) Mixed-Use Residential.

ii) Character

The Village Core is to be a walkable urban mixed-use “mainstreet” with a pedestrian scale. The built form will be primarily street oriented on all public rights-of-way within this area. Sites internal to the neighbourhood (east of the lots on the Highbury Avenue North frontage), will be of a mid-rise height. Structures along Highbury Avenue North will be street oriented and of a high-rise height. Schedule 4 shows the heights to be achieved within the sub-areas of the Village Core. Public rights-of-way in the Village Core Area will be of an urban character, primarily designed to support walking and street oriented retail. Boulevards will consist entirely of hard surface treatment and provide opportunities for landscaping, such as street trees and furniture, to create a vibrant urban main street context.

iii) Applications To Expand, Add or Modify

Applications to add or to expand the Village Core Designation, will be evaluated based on the following criteria, in addition to all other policies included in this Secondary Plan:

- a) A demonstrated demand/need to extend or add to the designation, considering the supply of land within the designation that is not currently developed;
- b) A location that is contiguous with the existing Village Core Policy Area 1 designation; and,
- c) A location that will benefit the Cultural Heritage Landscape as the major focal point for the community.
- d) Where applicable, Planning Impact Analysis Policies in Section 5.4 of the Official Plan shall apply.



20.4.3.2.1 Village Core Policy Area 1 – Main Street/Live-Work

i) Intent

The intent of the Village Core Policy Area 1 designation is to allow for the development of a live-work residential area within a pedestrian-oriented main street environment. The Village Core Policy Area 1 is located along the major east-west entrance into the community and abuts the central open space, providing for easy access to all residents and generating activity that will support this central community focal point.

ii) Permitted Uses

Permitted uses in the Village Core Policy Area 1 designation shall include those residential uses permitted in the “Multi-Family, Medium Density Residential” designation, with the exception of single-detached, semi-detached and duplex dwellings. In addition, non-residential uses may be located on the ground floor of any residential unit within a live-work built form. The permitted non-residential uses may include those uses which are appropriate in a residential environment, providing a service to its residents, and which do not create significant impacts on neighbouring residential uses. These include, but are not necessarily limited to:

- a) professional and services offices;
- b) medical dental offices;
- c) personal service establishments;
- d) retail stores;
- e) convenience stores;
- f) studios and galleries;
- g) day care centres;
- h) small-scale restaurants;
- i) specialty food stores; and,
- j) fitness and wellness establishments.

Non-residential uses will be restricted to the ground floor and stand-alone non-residential uses shall not be permitted.

iii) Built Form and Intensity

- a) Building floorplates shall be designed to accommodate retail or commercial uses at grade with residential uses located at, or above, grade.
- b) Residential development shall not exceed a net density of 75 units per hectare.
- c) The built form shall be of a low-rise height. Schedule 4 identifies the height limits within this area. The proposed development is to provide a transition between the heritage area surrounding the Infirmary building and the higher-rise built form along Highbury Avenue North.





- d) The ground floor of the residential units within the Village Core Policy Area 1 designation shall be designed and constructed in a manner which ensures flexibility and adaptability over time. In no instance shall the entire building be used exclusively for a non-residential use; however, the entire building may be used for residential purposes.
- e) Large windows, patio space and canopies are encouraged to be incorporated into the building's ground level. Rooftop patios and balconies are encouraged and shall be considered as amenity areas for residents within mixed use buildings.
- f) Buildings should be designed with defined spaces for signage that respects the building's scale, architectural features and the established streetscape design objectives.

iv) Transportation

- a) It is intended that the primary mode of transportation within the Community will be by walking or cycling. Parking shall not be allowed within the front yard of any buildings within the Village Core Policy Area 1 designation. A limited amount of parking may be provided in the rear yard of live-work uses for the associated residential component of these uses. Business parking will be directed to on-street locations.

20.4.3.2.2 Village Core Policy Area 2 – Mixed Use Office

i) Intent

The Village Core Policy Area 2 is intended to allow for office uses, within a stand-alone or mixed use form. A street-oriented built form will be required to support the Village Core character and to act as a gateway into the community. The following policies also apply.

ii) Permitted Uses

Permitted uses in the Village Core Policy Area 2 designation shall include those uses permitted in the Office/Residential designation of the Official Plan. Stand-alone office uses may be permitted at a medium scale. All secondary uses shall be restricted to ground level locations.

iii) Built Form and Intensity

- a) A maximum of 5 000m² of total office gross floor area is permitted in the Village Core Policy Area 2 designation. This may be included in a single building or in more than one building.
- b) The built form shall be of a mid-rise to high-rise height, as shown in Schedule 4.
- c) Net residential densities will normally be less than 150 units per hectare.

- d) A residential density exceeding 150 units per hectare may be permitted through a site specific zoning by-law amendment and site plan application, and the associated urban design review. Additional criteria for increasing density are specified in Section 3.4.3 ii) b-e) - Scale of Development, in the Official Plan.
- e) The frontage of buildings located on the main street, at the major entryway to the community from Highbury Avenue North, shall be designed to accommodate secondary uses at grade.
- f) Corner sites or areas connecting to parking facilities are encouraged to incorporate forecourts or mid-block connections that may be private, but provide for public access and amenity.
- g) Large windows, patio space and canopies are encouraged to be incorporated into the building's ground level. Rooftop patios and balconies are encouraged and shall be considered as amenity areas for residents within mixed use buildings.
- h) Buildings within the designated area shall be designed to form a well defined and continuous street edge with high quality architectural features.

iv) Transportation

- a) It is anticipated that the office uses within this designation will employ residents of the area to reduce the need for commuting and the need for at-grade parking.



20.4.3.2.3 Village Core Policy Area 3 – Mixed Use Residential

i) Intent

This policy area is intended to allow for a transition from Highbury Avenue North to the internal portions of the community. Mixed use buildings will be encouraged, but stand-alone residential uses will be permitted. A street-oriented building form will be required to support the Village Core character and to act as a gateway into the community.

ii) Permitted Uses

Permitted uses in the Village Core Area 3 shall include mid-rise to high-rise apartment buildings, apartment hotels, nursing homes, and seniors residences. In addition, small scale office uses with a maximum total floor area of 2,000 m² or less within each building, will be permitted. Secondary uses will also be permitted only on the ground floor of residential buildings, including, but not limited to: personal services, retail stores, financial institutions, convenience stores, day care centres, pharmacies, studios and galleries, specialty food stores and fitness and wellness establishments.

iii) Built Form and Intensity

- a) Net residential densities will normally be less than 150 units per hectare.
- b) A residential density exceeding 150 units per hectare may be permitted through a site specific zoning by-law amendment and site plan application, and the associated urban design review. Additional criteria for increasing density are specified in Section 3.4.3 ii) b-e) - Scale of Development, in the Official Plan.
- c) The frontage of buildings located on the main street, at the major entryway to the community from Highbury Avenue North, shall be designed to accommodate secondary uses at grade.
- d) The built form shall be of a mid-rise to high-rise height as specified in Schedule 4 of this Plan.
- e) Corner sites or areas connecting to parking facilities are encouraged to incorporate forecourts or mid-block connections that may be private, but provide for public access and amenity.
- f) Large windows, patio space and canopies are encouraged to be incorporated into the building's ground level. Rooftop patios and balconies are encouraged and shall be considered as amenity areas for residents within mixed use buildings.
- g) Buildings should be designed with defined spaces to accommodate signage that respects the building's scale, architectural features and the established streetscape design objectives.



20.4.3.3 Transit-Orientated Corridor Designation

i) Function and Purpose

The purpose of the Transit-Oriented Corridor designation is to focus residential and commercial uses along transit routes consistent with the Province of Ontario's "Transit Supportive Guidelines". It is also consistent with the emphasis on walking and bicycling for this Community. Transit-Oriented Corridors are intended to allow for the creation of a band of residential and mixed use development at medium and high densities to support transit along Highbury Avenue North, Oxford Street East and Dundas Street. More substantial amounts of retail development, at grade, will be directed to arterial road intersections. Stand-alone commercial uses will not be permitted.

The Transit-oriented Corridor designation is made up of three sub-areas, as follows:

- a) Mixed Use;
- b) High-rise Residential; and,
- c) Mid-rise Mixed-Use.

ii) Character

The Transit Oriented Corridor is to be a walkable urban mixed-use "mainstreet". The built form will be primarily street-oriented on all public rights-of-way within this area. This area will be characterized by higher intensity built form to support transit. Public rights-of-ways in the Transit-Oriented Corridor Area will be of an urban character, primarily designed to support walking and street oriented retail. Boulevards will consist entirely of hard surface treatment and provide opportunities for landscaping, such as street trees and furniture, to create a vibrant urban main street context.

iv) Applications To Expand, Add or Modify

Applications to expand the Transit-Oriented Corridor Designation will be evaluated using all of the policies of this Secondary Plan. It is not intended that this designation will be applied within the internal portions of the community and any expansions or additions to this designation shall front onto an arterial road.

20.4.3.3.1 Transit- Oriented Corridor Policy Area 1 – Mixed Use

i) Intent

This designation is applied to the major intersections of Highbury Avenue North at both Oxford Street East and Dundas Street and will support the greatest level of commercial use intensity in the Plan.





These pictures provide examples of the design and density for development envisioned at the Highbury Avenue North and Oxford Street East intersection.



ii) Permitted Uses

Permitted uses in the Transit-Oriented Corridor Policy Area 1 designation shall include mid-rise to high-rise apartment buildings, apartment hotels, nursing homes, and seniors residences. In addition, small scale office uses with a maximum total floor area of 2,000 m² or less within each building, will be permitted. Secondary uses will also be permitted only on the ground floor of those buildings fronting onto Dundas Street, Highbury Avenue North, or Oxford Street East, including, but not limited to: personal services, food stores, retail stores, financial institutions, convenience stores, day care centres, pharmacies, studios and galleries, specialty food stores and fitness and wellness establishments.

iii) Built Form and Intensity

- a) Net residential densities will normally be less than 150 units per hectare.
- b) A residential density exceeding 150 units per hectare may be permitted through a site specific zoning by-law amendment and site plan application, and the associated urban design review. Additional criteria for increasing density are specified in Section 3.4.3 ii) b - e) - Scale of Development, in the Official Plan.
- c) The frontage of buildings located on Dundas Street, Highbury Avenue North and Oxford Street East, shall be designed to accommodate secondary uses at grade.
- d) The built form shall be of a mid-rise to high-rise height as shown in Schedule 4 of this Plan.
- e) Corner sites or areas connecting to parking facilities are encouraged to incorporate forecourts or mid-block connections that may be private, but provide for public access and amenity.
- f) Large windows, patio space and canopies are encouraged to be incorporated into the building's ground level. Rooftop patios and balconies are encouraged and shall be considered as amenity areas for residents within mixed use buildings.
- g) Buildings should be designed with defined spaces to accommodate signage that respects the building's scale, architectural features and the established streetscape design objectives.

20.4.3.3.2 Transit-Oriented Corridor Policy Area 2 – High-rise Residential

i) Intent

This designation is applied to Highbury Avenue North to provide for transit-oriented, mid to high-rise, residential development that is not mixed use in nature.

ii) Permitted Uses

Permitted uses in the Transit-Oriented Corridor Policy Area 2 designation shall include mid-rise to high-rise apartment buildings, apartment hotels, nursing homes, and seniors residences.

iii) Built Form and Intensity

- a) Net residential densities will normally be less than 150 units per hectare.
- b) A residential density exceeding 150 units per hectare may be permitted through a site specific zoning by-law amendment and site plan application, and the associated urban design review. Additional criteria for increasing density are specified in Section 3.4.3 ii) b-e) - Scale of Development, in the Official Plan.
- c) Corner sites or areas connecting to parking facilities are encouraged to incorporate forecourts or mid-block connections that may be private, but provide for public access and amenity.

20.4.3.3.3 Transit- Oriented Corridor Policy Area 3 – Mid-rise Mixed Use

i) Intent

This designation is applied to the north side of Dundas Street to provide for transit-oriented mid-rise residential development that is mixed use in nature. Adjacency to the Treed Allée is a primary consideration in the review of all planning applications.

ii) Permitted Uses

Permitted uses in the Transit-Oriented Corridor Policy Area 3 include those primary uses permitted in the Multi-family, Medium Density Residential designation of the Official Plan. This Policy Area is divided by the Treed Allée with specific policies for each side.

West of the Treed Allée secondary uses will also be permitted only on the ground floor of those residential buildings fronting onto Dundas Street, including, but not limited to: personal services, food stores, retail stores, financial institutions, convenience stores, day care centres, pharmacies, studios and galleries, specialty food stores, fitness and wellness establishments and small scale office uses with a maximum total gross floor area for the site of 2,000 m².

East of the Treed Allée the secondary uses noted above (for west of the Allée) may be permitted in a stand-alone commercial building. Multiple stand-alone commercial buildings shall not be permitted. A lawn bowling facility, and community gardens (as shown in Schedule 2), may also be permitted on the rear portion of this designation.

iii) Built Form and Intensity

- a) Net residential densities will normally be less than 75 units per hectare.





b) A greater building height will be supported on the west side of the Allée as shown on Schedule 4 of this Plan. A residential density exceeding 75 units per hectare may be permitted through a site specific zoning by-law amendment and site plan application, and the associated urban design review. A request for an increase in density shall also be subject to the following criteria:

- The development is to be designed and occupied for seniors housing, or shall include provision for unique attributes and/or amenities that may not normally be provided for in lower density projects having a public benefit; such as, but not limited to, enhanced open space and recreational facilities, innovative forms of housing and architectural design features;
- Parking facilities shall be designed to minimize the visual impact off-site and provide for enhanced amenity and recreation areas for the residents of the development;
- Conformity with the policies of Section 11.1 of the Official Plan and this Secondary Plan shall be demonstrated through the preparation of a concept plan of the site that exceed the prevailing standards for the planning area; and,
- The final approval of zoning shall be withheld pending a public participation meeting on the site plan and the enactment of a satisfactory agreement with the City.

c) Development adjacent to the Allée, shall be oriented to the Allée.

d) Corner sites or areas connecting to parking facilities are encouraged to incorporate forecourts or mid-block connections that may be private, but provide for public access and amenity.

e) The frontage of buildings located on Dundas Street shall be designed to accommodate secondary uses at grade and oriented to Dundas Street.

f) Large windows, patio space and canopies are encouraged to be incorporated into the building's ground level. Rooftop patios and balconies are encouraged and shall be considered as amenity areas for residents within mixed use buildings.

g) Buildings should be designed with defined spaces to accommodate signage that respects the buildings scale, architectural features and the established streetscape design objectives.

iv) Transportation

a) At the Site Plan stage, arrangements for shared private driveway access from Dundas Street shall be required, to minimize the number of driveways and to ensure properties adjacent to the Canadian Pacific Railway are not landlocked.

20.4.3.4 Academic Area Designation

i) Function and Purpose

The Academic Area designation is applied to lands in the northeast corner of the Secondary Plan area, southwest of the main Fanshawe College campus and provides a long term opportunity for an academic institution to expand their facilities and programs to a site which is in close proximity. This additional land will provide space for offices, classrooms, recreational activities and residential uses for students. It is intended that the Academic Area will provide opportunities for an academic institution to develop purpose built residential housing that can be appropriately designed and operated to avoid potential problems that can be associated with near-campus neighbourhoods. The Academic Area designation consists of three sub-areas, which are as follows:

- a) Private Recreation;
- b) Academic Classrooms and Offices; and,
- c) Satellite Campus Residences.

ii) Character

This area will be developed as a pedestrian-oriented urban campus with a street network that integrates these uses with the surrounding community. Built form is to be street-oriented on all public rights-of-way, with buildings located at or near the property line and front entrances oriented to the street. This area will be characterized by relatively high density built form to support transit. Purpose-built student residences will take the form of mid-rise to high-rise development. Public rights-of-way in the Academic Area will be of an urban character, primarily designed to support walking and street oriented educational, office and retail facilities.





iii) Applications To Expand, Add or Modify

It is not anticipated that the Academic Area will expand substantially. However, minor expansions or modifications may be permitted. Applications to expand the Academic Area designation or modify the policy area boundaries will be evaluated on all of the policies of this Secondary Plan. In addition, the following criteria will be considered:

- a) A demonstrated need for the expansion, addition or modification of this designation, given undeveloped lands that are already designated for these uses;
- b) The Academic Area and applicable policy area boundaries shall not be permitted to expand to a degree which undermines the intended residential character of the adjacent Residential Character Area Designation - North Residential Area;
- c) The expansion of the designation or policy area boundaries shall not have a negative impact on adjacent or nearby properties;
- d) Where applicable, Planning Impact Analysis policies in Chapter 6 of the Official Plan will be utilized.

iv) Applications For Land Use Change

It is anticipated that the lands within the Academic Area will be used for educational purposes. Applications to change the permitted uses shall be subject to an Official Plan Amendment and shall be evaluated on the following criteria:

- a) The applicant shall demonstrate that there is no demand for academic uses at this location;
- b) A demonstrated need for the change of land use, given undeveloped lands that are already designated for these uses;
- c) Compatibility of the proposed use with the residential character of the adjacent residential area designation - North Residential Area;
- d) The change in land use will not have a negative impact on adjacent or nearby properties or municipal objectives to provide recreational programming;
- e) Hospitals, large religious institutions, military establishments and correctional or detention centres shall be discouraged; and,
- f) Where applicable, Planning Impact Analysis policies in Chapter 3 of the Official Plan will be utilized.



20.4.3.4.1 Academic Policy Area 1 – Private Recreation

i) Intent

This area is intended to provide recreation space for staff and students of an academic institution. This space, combined with the public open space block to the south, will also form a buffer between the older industrial area to the east and the developing residential community to the west.

ii) Permitted Uses

These lands will be utilized for recreational purposes related to academic uses, including such uses as active parkland, playing fields, athletic facilities, training facilities, and sports and recreational buildings. Banquet facilities and assembly halls shall not be permitted.

Should this policy area not be required for academically related recreational purposes, the area shall be maintained for recreational uses in either public or private ownership. Partnerships for the shared use of this land are encouraged.

iii) Built Form and Intensity

- a) The total built form coverage shall not exceed 50% of the entire Academic Policy Area 1 designation.
- b) Individual buildings shall not exceed a gross floor area (GFA) of 5,000 m².
- c) Built form is to be of a low to mid-rise height as shown on Schedule 4 of this Plan.
- d) Built form, including massing and scale, shall be sensitive to the residential community to the east of the Academic Policy Area 1 designation. Consideration shall be given to mitigating the impact of height by stepping buildings down, ensuring that massing is not obtrusive, and using building materials that mitigate mass and bulk.
- e) Further to general policies relating to the primary orientation of built form, the secondary orientation of buildings shall be to the parklands, playing fields and open space to the south.
- f) Corner sites or areas connecting to parking facilities are encouraged to incorporate forecourts or mid-block connections that may be private, but provide for public access and amenity.

iv) Transportation

- a) Adequate parking shall be required, on site, to accommodate all uses within this designation, such that parking does not occur throughout the residential community to the east, west and south. Where surface parking is located in the interior side yard the owner shall demonstrate that there is sufficient opportunity to ensure the transition of the parking area to built form over time.





20.4.3.4.2 Academic Policy Area 2 – Academic Classrooms and Offices

i) Intent

The Academic Policy Area 2 designation is intended to support a range of academic buildings, including classrooms, administrative offices and a range of secondary uses to provide services to the academic population.

ii) Permitted Uses

Permitted uses include classrooms, laboratory and research facilities, and administrative office buildings associated with an academic institution. Secondary uses, which are to serve the students, faculty and administration of the academic use shall be permitted only along the Oxford Street East frontage within buildings used for the primary permitted uses. These secondary uses include a range of service offices, personal services, retail stores and other uses intended to service students and staff. Restaurants, taverns, clubs and bars shall not be permitted within this designation, but shall be directed to the intersection of Oxford Street East and Highbury Avenue North and other commercial designations within the vicinity.

iii) Built Form and Intensity

- a) As shown in Schedule 4, structures within this designation shall be of a mid-rise height, with the exception of the lands at the northeast corner of the secondary plan which are to range from 4 to 10 storeys in height.
- b) Built form, including massing and scale, shall be sensitive to the residential community to the east of the Academic Policy Area 2 designation. Consideration shall be given to mitigating the impact of height by stepping buildings down, ensuring that massing is not obtrusive, and using building materials that mitigate mass and bulk.
- c) Corner sites or areas connecting to parking facilities are encouraged to incorporate forecourts or mid-block connections that may be private, but provide for public access and amenity.
- d) Large windows, patio space and canopies are encouraged to be incorporated into the building's ground level.

iv) Transportation

- a) Adequate parking shall be required, on site, to accommodate all uses within this designation, such that parking does not occur throughout the residential community to the east, west and south. Where surface parking is located in the interior side yard the owner shall demonstrate that there is sufficient opportunity to ensure the transition of the parking area to built form over time.

20.4.3.4.3 Academic Policy Area 3 – Satellite Campus Residences

i) Intent

The Academic Policy Area 3 designation allows for the establishment of higher density housing related to academic institutions. Purpose-designed housing can mitigate the problems that can sometimes occur within near-campus neighbourhoods.

ii) Permitted Uses

Permitted uses include purpose-designed residences related to academic uses.

iii) Built Form and Intensity

- a) The built form shall be of a mid-rise to high-rise height as shown in Schedule 4 of this Plan.
- b) Corner sites or areas connecting to parking facilities are encouraged to incorporate forecourts or mid-block connections that may be private, but provide for public access and amenity.
- c) Further to the general urban design policy 20.4.4.10 (v) - Heritage, the built form fronting Oxford Street East surrounding the Heritage designation is envisioned to develop simultaneously with the heritage area. With this intent, the built form is permitted to bridge across the heritage area. The design of the built form must continue to provide the full view corridor to the Infirmary Building as identified in Schedule 8 of this Plan.
- d) Balconies and rooftop patios shall not be permitted.

iv) Transportation

- a) Given the proximity to the main campus, the presence of group parking lots and the emphasis placed on walking, bicycling and transit, parking standards for this area may be reduced based on a parking study. Where surface parking is located in the interior side yard the owner shall demonstrate that there is sufficient opportunity to ensure the transition of the parking area to built form over time.





20.4.3.5 Residential Area Designation

i) Function and Purpose

It is intended that this designation will support an urban housing stock, with height and intensity generally increasing with greater distance from the central cultural heritage landscape. Residential areas are to accommodate a diversity of dwelling types, building forms, heights and densities, in order to use land efficiently, provide for a variety of housing prices and to allow members of the community to “age-in-place”. These residential areas will accommodate a significant population density which will help to support the services offered in the Village Core and the provision of transit along the Transit-Oriented Corridors. Planning applications to significantly reduce the residential height and intensity of these areas would undermine these objectives and should not be supported. The Residential Area designation is made up of two sub areas:

- a) North Residential Neighbourhood; and,
- b) South Residential Neighbourhood.

ii) Character

This designation will have a variety of setbacks, depending on the built form intensity. Generally, the area will be urban in nature but will not allow for the mix of uses located in other strategic locations within the community. Buildings are to be street-oriented with the principle entrance facing the street. Public rights-of-way will be of an urban character, primarily designed to support walking for both utility and recreation.



20.4.3.5.1 Residential Policy Area 1 – North Residential Neighbourhood

i) Intent

The Residential Policy Area 1 designation will provide for higher-intensity residential uses than the Residential Policy Area 2 designation. The North Residential Neighbourhood is in close proximity to Fanshawe College and the Academic Area designation. Accordingly, certain policies have been established to avoid the potential for near-campus neighbourhood issues.

ii) Permitted Uses

The primary permitted uses in the Multi-family, Medium Density Residential designation of the Official Plan will be permitted, with the exception of: single detached dwellings, duplexes, semi-detached dwellings, rooming and boarding houses and cluster townhouses. Convenience commercial uses and secondary permitted uses allowed in the Multi-family, Medium Density Residential designation shall not be permitted within these areas. Accessory dwelling units in single and semi-detached dwellings shall not be permitted.

iii) Built Form and Intensity

- a) Residential building heights will vary according to the limits shown in Schedule 4 of this Plan. Development shall not be permitted at a residential density of less than 30 units per hectare and shall generally not be permitted to exceed 75 units per hectare. A residential density exceeding 75 units per hectare may be permitted through a site specific zoning by-law amendment and site plan application, and the associated urban design review. A request for an increase in density shall also be subject to the following criteria:
- The development is to be designed and occupied for seniors housing, or shall include provision for unique attributes and/or amenities that may not normally be provided for in lower density projects having a public benefit; such as, but not limited to, enhanced open space and recreational facilities, innovative forms of housing and architectural design features;
 - Parking facilities shall be designed to minimize the visual impact off-site and provide for enhanced amenity and recreation areas for the residents of the development;
 - Conformity with the policies of Section 11.1 of the Official Plan and this Secondary Plan shall be demonstrated through the preparation of a concept plan of the site that exceed the prevailing standards for the planning area; and,
 - The final approval of zoning shall be withheld pending a public participation meeting on the site plan and the enactment of a satisfactory agreement with the City.
- b) No dwelling unit shall contain more than 3 bedrooms.
- c) Plans of subdivision shall accommodate a diversity of building types. A variety of townhouse forms including 2 storey townhouses, 3 storey townhouses and stacked townhouses is encouraged. Row houses shall be limited to a maximum of 6 attached units to ensure breaks in the street wall. Developments proposed through site plan applications will provide variety and interest by varying façade designs, building materials, fenestration, and colour from townhouse block to townhouse block.
- d) Garages on townhouses shall not project beyond the front wall of the dwelling (Front porches do not constitute the front wall).





- e) In areas where higher/more intense built form is to be located near lower-rise forms, the built form with greater height/intensity is to be designed with massing and articulation that provides for a transition between the lower-rise form and the higher-rise form.
- f) Townhouses and ground level apartment units are encouraged to provide design elements that support activity in the front setback. These may include, but are not limited to, front porches.
- g) Built form that is 3 storeys or greater and is not a townhouse unit located at a corner site shall provide for a building entrance, massing, articulation and height that pronounces the corner.
- h) Townhouses located at corner sites are to incorporate design features that assist with signifying its location at a corner site. These features may include, but are not limited to, wrap around front porches and height elements.
- i) Balconies above the third storey and rooftop patios shall not be permitted.
- j) Built form located at the termination of vistas/view corridors, as identified on the Schedule 8, are to incorporate architectural design elements and massing that enhances the terminal view. Garages shall not terminate a vista/view corridor.
- k) Built form located adjacent to those areas identified in Schedule 7 as part of the cultural heritage landscape or as heritage buildings are to incorporate architectural elements and massing that is compatible with the heritage context.
- l) Corner sites or areas connecting to parking facilities are encouraged to incorporate forecourts (at the corner) or mid-block connections that may be private, but provide for public space amenity.

20.4.3.5.2 Residential Policy Area 2 – South Neighbourhood

i) Intent

The Residential Policy Area 2 designation will provide for slightly lower-intensity residential uses than the Residential Policy Area 1 designation.

ii) Permitted Uses

The primary permitted uses in the Multi-family, Medium Density Residential designation of the Official Plan will be permitted, with the exception of rooming and boarding houses and cluster townhouses. Convenience commercial uses and secondary permitted uses allowed in the Multi-family, Medium Density Residential designation will not be permitted within these areas. Accessory dwelling units in single and semi-detached dwellings shall not be permitted.

iii) Built Form and Intensity

- a) Residential building heights will vary according to the limits shown in Schedule 4 of this Plan. Development shall not be permitted at a residential density of less than 15 units per hectare and shall generally not be permitted to exceed 75 units per hectare. A residential density exceeding 75 units per hectare may be permitted through a site specific zoning by-law amendment and site plan application, and the associated urban design review.
- b) No dwelling unit shall contain more than 3 bedrooms.
- c) Plans of subdivision shall accommodate a diversity of building types. A variety of townhouse forms including 2 storey townhouses, 3 storey townhouses and stacked townhouses is encouraged. Row houses shall be limited to a maximum of 6 attached units to ensure breaks in the street wall. Developments proposed through site plan applications will provide variety and interest by varying façade designs, building materials fenestration, and colour from townhouse block to townhouse block.
- d) Built form adjacent to the Treed Allée within the Heritage Area, shall be oriented towards the Allée in applicable locations.
- e) Garages on detached dwellings and townhouses shall not project beyond the front wall of the dwelling.
- f) In areas where higher intensity built form is to be located near single family dwellings the built form with greater height/intensity is to be designed with massing and articulation that transitions between the lower-rise form and the higher-rise form.
- g) Single detached dwellings and townhouses are encouraged to provide design elements that support activity in the front yard setback. These may include, but are not limited to, front porches.
- h) Single detached dwellings and townhouses located at corner sites shall incorporate design features that assist with signifying their location at a corner site. These may include, but are not limited to, wrap around front porches, windows and height elements.
- i) Built form that is 3 storeys or greater and is not a townhouse unit located at a corner site shall provide for a building entrance, massing, articulation and height that pronounces the corner.
- j) Townhouses located at corner sites shall incorporate design features that assist with signifying its location at a corner site. These may include, but are not limited to, wrap-around front porches, windows and height elements.
- k) Built form located at the termination of vistas/view corridors, as identified on Schedule 8, shall incorporate architectural design elements and massing that enhances the terminal view. Garages on all building types are not to terminate a vista/view corridor.



- l) Built form located adjacent to those areas identified in Schedule 7 as part of the cultural heritage landscape or as heritage buildings shall incorporate architectural elements and massing that is compatible with the heritage context.
- m) Consideration may be given to permitting additional bedrooms in accordance with Municipal By-laws, subject to a Zoning By-law amendment. Applications to amend the by-law to permit additional bedrooms shall be subject to the following criteria:
 - The additional bedrooms shall not have a negative impact on adjacent lands;
 - Sufficient on and off-site parking can be provided;
 - There are sufficient on and off-site amenities;
 - Where applicable, Planning Impact Analysis Policies in Chapter 3 of the Official Plan shall be utilized.

iv) Transportation

- a) In the portions of the South Neighbourhood that abut the “Cultural Heritage” designation, driveway access to dwelling units may be limited. Schedule 8 of this Plan illustrates areas in which limited driveway access is permitted, and areas in which no driveway access is permitted. In limited access areas a shared driveway to multi-unit development may be permitted; however individual driveways to lower density units shall not be permitted.
- b) Parking for the single-detached, semi-detached, and duplex residential units should be located in an enclosed garage on the front within the building envelope or to the rear in an enclosed attached or detached garage or surface space.

v) Development next to Railway

The following policies will apply related to noise mitigation measures:

- a) Low density housing forms such as, single detached, semi-detached and duplex dwellings should be oriented so that outdoor amenity space is located away from and protected from the rail line.
- b) Buffers/noise walls shall be landscaped and/or treated in creative and artistic ways that result in attractive mitigation solutions.
- c) Where possible, non-habitable portions of buildings, such as stairwells and service areas, are to be oriented towards the rail line. Where non-habitable portions of the building may be visible from existing or future recreational opportunities, screening of these areas is to integrate with the building’s architecture.
- d) All noise policies within the general policies of this Secondary Plan and the City of London Official Plan.





20.4.3.6 Heritage Area Designation

i) Function and Purpose

The Heritage Area designation includes the cultural heritage landscape as well as the individual heritage buildings and their landscape setting that exist on the LPH lands. These buildings, and the heritage landscape, will be conserved. Conservation allows for alterations to a property and buildings, if it can be demonstrated that the significant heritage attributes of the heritage resource are not negatively impacted by the change. The conservation and re-use of the potting shed, vegetable shorting shed and central heating plant is encouraged but not required.

The following policies apply to areas identified on Schedule 7 - Cultural Heritage Framework of this Plan. The buildings and features to be conserved include:

- a) Treed Allée;
- b) Recreation Hall;
- c) Chapel of Hope;
- d) Infirmary Building;
- e) Horse Stable;
- f) Cultural Heritage Landscape.

ii) Character

The areas identified within the Heritage Area designation are to be conserved and wholly integrated into the design of the neighbourhood. The Heritage Area designation includes cultural open space, which is part of the cultural heritage landscape. This includes the historic Allée and the planned 'Village Green' which provide a major pedestrian corridor and opportunities for programmable events.

As these elements and/or features form part of the public realm, the surrounding character of the area will respond, in architectural design, to these features and/or elements. All development adjacent to the Heritage Area designation will be developed with sensitivity to the cultural heritage landscape and its component parts. Important views and vistas, as shown on Schedule 8, will be conserved and will remain unobstructed by development. Permitted building heights will be lowest adjacent to the cultural heritage landscape and greatest in locations further from the cultural heritage landscape.





iii) Permitted Uses

The restoration and sensitive adaptation of significant heritage buildings for contemporary urban uses is encouraged. The Infirmary Building should be considered for office and/or institutional uses, which may include an interpretive centre. The continued use of the Chapel of Hope, as a place of worship, and the Recreation Hall for community uses is preferred. The stable should be adapted for food or farming-related uses such as a market, restaurant and/or educational centre. The possible use of the stable for horticultural purposes associated with an established educational facility is encouraged. The Cultural Heritage Landscape is intended to be used for passive recreational uses and programmable events. In the area surrounding the Horse Stable, educational facilities related to horticulture or agriculture and/or community gardens, as shown on Schedule 2, may also be permitted.

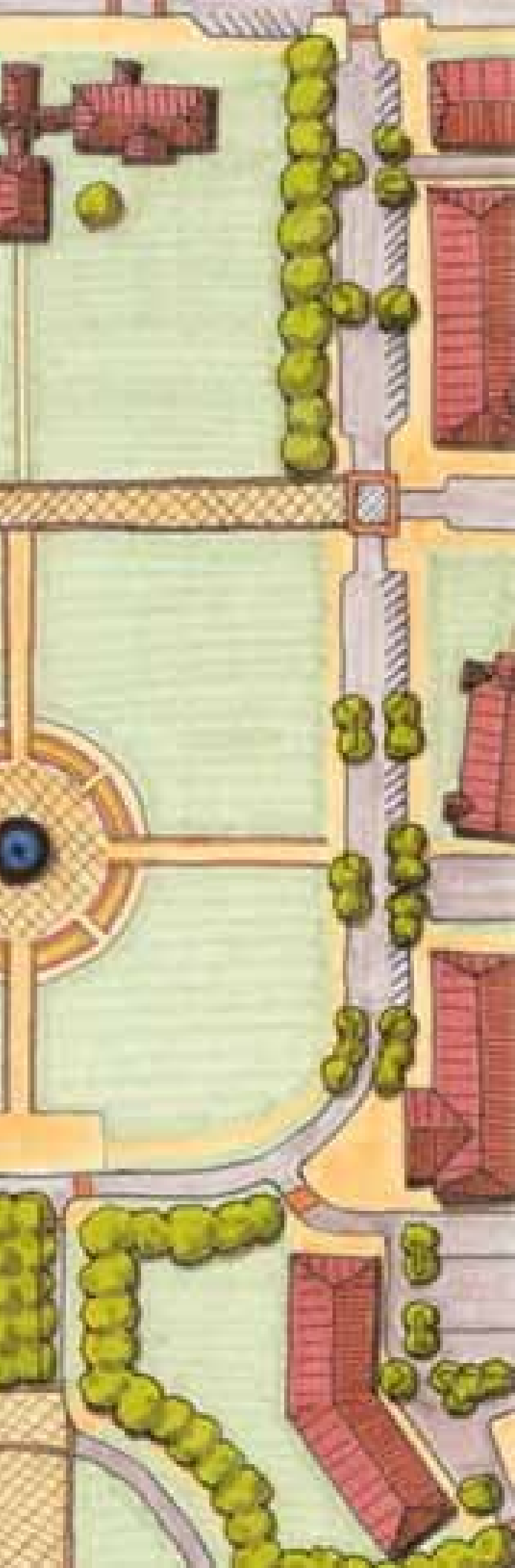
iv) Public Realm

- a) The Heritage Area, and the associated cultural heritage landscape is to form part of the public realm. Developments adjacent to the areas identified as Heritage Areas are to orient the built form towards these features and/or the public right-of-way that bounds them. Specific urban design policies for the interface between heritage areas and new development are found in Section 20.4.4.10 Urban Design, of this plan.
- b) Within heritage open space areas a tree management and planting strategy shall be established in order to conserve and sustain the significant landscape setting.
- c) Vegetation and greenspace contribute significantly to the cultural heritage landscape and provide a setting for its significant features. The following landscape features shall be established and/or conserved:
 - The historic central Treed Allée including its parallel row of trees;
 - An open greenspace extending from the Allée to the Infirmary Building;
 - An open space buffer to the north, south and west of the Stable of sufficient size to retain the building's agricultural setting;
 - Manicured lawns with specimen trees adjacent to the Infirmary Building, Chapel and Recreation Hall.



- Where possible, priority trees to be conserved include the ring of trees which surround the traffic circle, the row of trees which line the southern edge of the historic ring road, the two parallel rows of trees that extend northward from the rear of the Infirmary and the rows of trees which line both sides of the road that extends east-west through the site, south of the Horse Stable, as shown on Schedule 5.
 - Existing trees will also be retained where they flank street alignments. These trees are a key defining element of the cultural landscape and must be managed. New buildings and streets must provide appropriate drip line setbacks.
- d) The therapeutic landscape setting and its physical and visual relationships to the historic buildings shall be conserved and monitored to allow for meaningful interpretation of the cultural heritage resources. The following measures shall be taken to facilitate interpretation of the site:
- The establishment of an interpretive centre to tell the story of the site and of mental health care in Canada. A possible location for such a use is the administrative wing of the Infirmary building;
 - The creation of an interpretive walk, which would tell the story of the site and explain the function of the therapeutic landscape as people move through it;
 - A prominent street within the property should be named after Dr. Richard Bucke (superintendent, 1877-1902), if possible;
 - Interpretive signage, public art, way finding strategies and other techniques may be considered.
- e) As trees mature and require replacement, new trees should be planted close to the original position. Within the Allée, the replacement trees must be added in the same north/south alignment in order to maintain the existing definitive rows. Replanting of trees shall be based on the variety of species historically planted on the site, with the exception of ash trees. There should continue to be a variety of larger native and non-native trees, deciduous and coniferous species, that will create scale, provide shade and frame views.
- f) New development on the west and east sides of the Allée shall be set back a minimum of 5 metres from the limit of the root zone (drip line). The design for new infrastructure on the site including new streets and utilities shall be planned to minimize excavation or filling within the root zones of the major vegetation features. This may require the adoption of alternative road design standards along streets to be lined by existing trees. A detailed tree preservation plan showing tree protection measures shall be required for any development applications on lands abutting the Allée or the Priority Tree Retention Areas as shown on Schedule 5. For clarity, “development” includes roads and driveways.





g) Archaeological assessments will be required in accordance with applicable Provincial policy prior to site redevelopment, to the satisfaction of the Ministry of Tourism and Culture. Of particular interest on the LPH lands, is the possibility of unmarked patient burial grounds associated with the asylum.

v) Built Form and Intensity

- a) Significant alterations or additions to the heritage buildings affecting their form and massing or diminishing their cultural value shall not be permitted. Minor additions or alterations, which are visually distinguishable from and subordinate to the historic structure may be permitted. Permission is subject to approval by the City of London and/or the Province of Ontario in accordance with Provincial policy and procedures.
- b) As shown on Schedule 8, visual access shall be maintained between the Infirmary Building and the Allée, as well as between the Infirmary Building and the Chapel of Hope. The Infirmary will continue to form the view terminus from the southern extent of the community. Height restrictions are shown on Schedule 4 of this Plan.

vi) Transportation

- a) The Allée shall be closed to vehicular traffic and will be used for linear park space, orientated to pedestrian leisure, cycling and passive recreation.
- b) Remnants of the historic ring road alignment should be retained and form the basis of circulation patterns around the centre of the site. Priority shall be given to retaining the historic alignment of the southern half of the ring road which will also facilitate retention of many of the trees which line this portion of the road. A large traffic circle shall be retained at the terminus of the Allée.
- c) Large surface parking lots shall not be permitted within this designation. On-street parking in close proximity to these areas is encouraged.

vii) Stewardship and Sustainability

Prior to the disposition of lands or structures designated as Provincially Significant in accordance with the *Final Conservation Plan*, December 2008, prepared by Julian Smith and Associates, a stewardship plan shall be completed in accordance with Provincial policies and procedures. The stewardship plan shall identify how these Provincially Significant features are to be maintained, the costs associated with the maintenance and identify sources of funding to cover the maintenance costs.

20.4.3.7 Open Space Designation

i) Function and Purpose

The Open Space designation will apply to open space areas of the Secondary Plan that are intended for active and passive recreation. The Open Space designation is made up of three sub-areas:

- a) Public parkland;
- b) Natural heritage/environment;
- c) Pedestrian corridors; and
- d) Stormwater management.

ii) Character

There are two distinct character types for the public parkland open space areas identified in this plan:

- a) Open Space Adjacent to Railway Spur - This area will have an active recreation character. The primary design focus will be to accommodate recreation sporting events. Ancillary opportunities for passive recreation are also to be integrated into the open space's character.
- b) Open Space Adjacent to Canadian Pacific Main line - This open space will have an urban park type character. It will allow for trails/paths and opportunities for passive recreation. The space is to integrate, through design, with the railway corridor that exists in the centre of the Secondary Plan area. Opportunities exist for public art and more extensive hardscaping.

iii) Public Realm

- a) Public rights-of-way in the Open Space Designation will be primarily designed to support walking for both utility and recreation.

Design of an urban park interface with the railway spur line.



20.4.3.7.1 Open Space Policy Area 1 – Public Parkland

i) Intent

This policy applies to the lands on the eastern edge of the Secondary Plan which are intended for active recreation uses including sporting fields. In addition, this open space serves as a separation buffer between residential uses in the Plan and the industrial uses currently located to the east of the LPH lands.

ii) Permitted Uses

Permitted uses include active parkland, athletic fields, and other outdoor recreational fields.

iii) Built Form and Intensity

- a) Buildings in this designation will generally consist of out-buildings or built form that serves the users of the open space.
- b) Buildings shall be designed to be compatible with the surrounding architectural character of the cultural heritage landscape and heritage buildings, subject to Section 20.4.4.10 (v) of this Plan.

iv) Transportation

Parking is to be generally supplied on-street. Small surface parking areas may be developed within the open space designation that support the permitted uses. The design of these parking facilities shall incorporate the use of sustainable materials and will minimize stormwater run-off.



20.4.3.7.2 Open Space Policy Area 2 – Natural Heritage/Environmental

i) Intent

This policy applies to lands in the southeast corner of the Secondary Plan area between the two railway lines, to protect the existing wetland and provide adequate buffers between this environmental feature and development. Any changes proposed for the wetland shall be subject to the Natural Heritage policies contained in Chapter 15 Environmental Policies, of the Official Plan.

ii) Permitted Uses

It is intended that these environmental features shall be protected and conserved. Community gardens may be permitted as shown on Schedule 2, provided that the locations will not have a negative impact on the cultural heritage landscape, surrounding natural features and or ecological functions of the area.



iii) Special Policies

No development shall occur within a 30 metre buffer around the wetland until an Environmental Impact Study (EIS) is submitted and required development setbacks are determined. In addition, a study may be required to confirm any requirement for the protection of locally significant plant species in the vicinity of the wetland.

20.4.3.7.3 Open Space Policy Area 3 – Stormwater Management

i) Intent

This policy area is intended to serve as the stormwater management area for the entire Community. Should detailed stormwater management design studies, at the functional design stage, determine that a larger area is required, modifications to the adjacent road network and block pattern may be required. An Official Plan Amendment may be required subject to the Interpretation Policies as set out in Section 20.4.5.11 of this Plan.

Conversely, if it is determined that a smaller area is needed, the surplus lands may be developed for multi-family medium density residential purposes. This is provided that sufficient developable area is available to accommodate the aforementioned use. In addition, an Official Plan amendment shall be required.

ii) Permitted Uses

These lands will be used for a stormwater management facility.

iii) Special Policies

- a) The stormwater management facility shall be designed and developed in accordance with an approved Stormwater Management Class Environmental Assessment.
- b) The stormwater management facility located on these lands shall be integrated with the Community park network. If through further study, it is determined that the stormwater management facility should discharge to the wetland feature to the south, an Environmental Impact Study shall be undertaken to address lands surrounding the stormwater management facility.





20.4.4

20.4.4.1 Heritage and Archaeology

- i) Prior to disposition of the London Psychiatric Hospital property or the demolition of any of the buildings or structures on the property, the approval of the Province of Ontario in accordance with Provincial policies and guidelines may be required.
- ii) The entire Secondary Plan area is identified as containing possible archaeological resources. Archaeological assessment reports will be required for all development within the Secondary Plan area.

20.4.4.2 Housing**i) Affordable Housing**

The LPH lands provide an opportunity to contribute to the supply of affordable housing and may assist the City in meeting its target for provision of affordable housing. The following policies shall also apply to the LPH lands:

- a) 35% of the new housing units on the LPH lands will be in forms other than single detached dwellings.
- b) Where appropriate, density bonusing will be considered for proposals that have an affordable housing component above 25% of the total dwelling count in any one development.
- c) Opportunities for affordable housing shall be integrated into neighbourhoods and developments that also provide for regular market housing.
- d) A wide range of unit sizes within multiple-unit buildings will be encouraged in discussions with development proponents.

ii) Seniors and Special Populations Housing

The City may pre-zone specific areas of the Multi-Family, Medium Density Residential designation to permit small scale nursing homes, homes for the aged, rest homes, and continuum of care facilities. These zones should be in close proximity to the "Village Core" designation. Additional permitted uses may be restricted to ensure development of such facilities within the LPH lands.





iii) Providing for Positive Near Campus Housing

- a) To provide for positive forms of near campus housing and to mitigate the potential for issues that can occur in near-campus neighbourhoods the following policies will apply:
- Provision will be made for purpose-built dormitories within the Satellite Campus Residential Policy Area, in close proximity to Fanshawe College.
 - Special policies in the Multi-Family, Medium Density Residential Designation limit the number of bedrooms per unit and limit single-detached and semi-detached dwellings to the south neighbourhood of the Plan. This will be implemented through the City's zoning by-law.

20.4.4.3

Noise/Land Use Compatibility

i) Rail Noise

The Secondary Plan area is bisected by two rail lines. Rail noise shall be addressed subject to policies of the Official Plan as well as the following:

- a) Buffers, berms and/or noise walls shall be landscaped and/or treated in creative and artistic ways that result in attractive mitigation solutions.
- b) The design of buildings should orient non-habitable portions of the building towards the rail line. Where non-habitable portions of the building may be visible from existing or future recreational opportunities, screening of these areas is to integrate with the building's architecture.
- c) Low density housing forms such as, single detached, semi-detached and duplex dwellings should be oriented so that outdoor amenity space is located away from and protected from the rail line.
- d) Prior to final approval, planning applications will require completion of noise studies to confirm compliance with provincial regulations. A noise study shall be required as part of a complete application.
- e) All noise policies within the general policies of this Secondary Plan and the City of London Official Plan.

ii) Noise from Industrial Uses

To mitigate the potential for noise conflicts between the proposed community and the industrial area east of the LPH lands, the Secondary Plan's Land Use Schedule identifies an 'Open Space' area adjacent to the abutting industrial lands. The Open Space designation acts, in part, as a noise mitigation measure. Any residential development that is contemplated in proximity to this industrial area shall have regard for the Ministry of Environment (MOE) Land Use Compatibility Guidelines. To address potential noise impacts from the adjacent industrial area, the following additional policies apply:

- a) On lands within a 300 metre area of influence measured from the west property lines of 535 and 539 Commercial Crescent, and in the presence of a Class II or Class III industrial use at 539 Commercial Crescent and the associated use of the railway siding at 535 Commercial Crescent, sensitive land uses shall be prohibited unless a "Feasibility Analysis" which meets Ministry of Environment guidelines has been completed and the development proposal meets all of the recommendations of the analysis for setbacks and mitigation. At a minimum the "Feasibility Analysis" shall address the issues of point source and/or fugitive noise emissions for the entire 300m area of influence, and ground borne vibration within 75 metres of the west property line of 535 and 539 Commercial Crescent. Sensitive land uses may include any building or associated amenity area (i.e. may be indoor or outdoor space) which is not directly associated with the industrial use, where humans or the natural environment may be adversely affected by emissions generated by the operation of a nearby industrial facility. Such uses include, but are not limited to residences, senior citizen homes, schools, day care facilities, hospitals, churches and other similar institutional uses. For draft plans of subdivision, and draft plans of condominium, the Feasibility Analysis shall be requested and submitted as part of a complete application.
- b) Where, as a result of the completion of a Feasibility Analysis, there are irreconcilable noise and vibration incompatibilities, the development of sensitive land uses shall be prohibited until such time as the Class II or Class III industrial use ceases to exist. Should that occur policies in Section 20.4.4.3 (ii) will no longer apply.
- c) Reduction of the 300 metre area of influence will be supported only through the submission of a study which addresses the entire area of influence and all study methodology, conclusions and recommendations are acceptable to the City of London.
- d) Noise studies, where required, will form part of a complete application and any recommendations from those studies shall be implemented.





20.4.4.4 Sustainable/"Green" Development

i) Principles

The LPH Secondary Plan is based on a conceptual design which maximizes the potential for sustainable development throughout. This is achieved through such features as mixed use development, a modified grid road system, enhanced connectivity to transit and the cultural heritage landscape and open space system.

Sustainable design elements shall be incorporated into municipal facilities located within the LPH Secondary planning area. Through planning applications, proponents should design development to:

- a) Reduce the consumption of energy, land and other non-renewable resources;
- b) Minimize the waste of materials, water and other limited resources;
- c) Create livable, healthy and inclusive environments;
- d) Reduce greenhouse gases.

ii) Policies

As part of a complete application for development within the LPH Secondary Plan a report shall be submitted explaining how the proposed development has incorporated the following goals and objectives:

- a) New buildings should strive to achieve LEED certification.
- b) Subdivisions plans should endeavor to achieve LEED Neighbourhood Development certification.
- c) Alternative energy sources are encouraged including solar and appropriately sized rooftop mounted wind collectors. Such technologies should be sensitively incorporated into buildings and community design.
- d) Where appropriate, buildings should be oriented to maximize opportunities for passive solar gain.
- e) A range of residential dwelling types are to be provided that support life-cycle housing and provide opportunities to age-in-place. This may include seniors housing.
- f) Landscaped areas will be maximized and trees will be planted on the right-of-way, on development sites, and on buildings and structures to reduce the urban heat-island effect, improve air quality, moderate sun and wind, and improve ground water infiltration.
- g) Opportunities will be explored for the integration of urban agriculture into parks, buildings and landscapes.
- h) New development will incorporate existing mature trees into landscape schemes and appropriate measures should be taken to keep these trees healthy.



- i) Alternative roadside drainage techniques, the use of pervious paving methods, enhanced use of organic cover, and/or the adoption of “road diets” to reduce impervious coverage are to be accommodated where possible.
- j) Quality materials that are durable and have high levels of energy conservation will be chosen for buildings.
- k) The employment of building technologies such as “greenroofs” is encouraged. Alternately, the use of reflective roof surface materials with high solar and thermal reflectivity to reduce the “heat island” effect is also desired.
- l) The incorporation of food production opportunities is encouraged throughout the site. This includes but is not limited to community gardens, private gardens, greenhouses, roof-top gardens and edible landscaping programs.

20.4.4.5 **Community Parkland**

Active parkland, cultural parkland and natural parkland are the three components which are recognized by the Open Space designation in this Community Plan. Active parkland includes such uses as sports fields. Cultural parkland includes the central Allée corridor and the Horse Stables. Natural parkland includes the wetland in the southeast corner of the Community. Parkland may be either under public or private ownership. It is anticipated that there will be approximately 6000 people living in this community at full build-out. Open space is a necessary component of a thriving community and this Secondary Plan provides ample open space particularly in the central and eastern portions of the Plan area.

i) **Active Parkland**

Chapter 16 of the Official Plan contains the policies which address active parkland. The following additional policies will also apply:

- a) Trail development should be focused along the central corridor through the community extending from Dundas Street to Oxford Street East (and beyond) which will include the Grand Allée, Infirmary and central median area.
- b) The pathway network shown on Schedule 6 may be treated as pedestrian walkways for the purposes of municipal land dedication under the Planning Act. Lands included in the pathway network in excess of 5m in width will be recognized as parkland.
- c) Additional public urban squares and/or parkettes will be acquired at the time of development through outstanding parkland dedication or in accordance with a Community Parkland Implementation Plan. Urban squares and parkettes will generally have a minimum size of 2500m². Generally, one public square and/or parkette shall be provided in each quadrant of the Secondary Plan.



ii) Cultural Parkland

The 'village green', surrounding the Infirmary Building, and the Allée provide an opportunity for beautiful and unique parkland within the City of London, similar to Victoria and Springbank Parks. These spaces could include an amphitheatre for community concerts, or a large public square ideal for music festivals, community picnics and 'art in the park' events. They would also provide a major pedestrian and cycling linkage within the overall Community parkland network.

20.4.4.6 Parkland Dedication

The City of London Policy Manual provides Council direction regarding parkland dedication. In addition to the Parkland Conveyance & Levy By-Law, parkland dedication will be achieved through the City's parkland dedication policies in Section 16.3.2 of the Official Plan and following additional policies:

- i) The components of the Community Parkland identified in Section 20.4.4.5 and shown as Open Space and Heritage on Schedule 3 of this Secondary Plan shall be dedicated to the City for public park purposes. Some components of Cultural Parkland may serve other public uses, such as institutional and/or academic, in which case the land may be conveyed to the public proponent of said use.
- ii) A more detailed Community Parkland Implementation Plan shall be prepared to address matters pertaining to parkland dedication that would otherwise be achieved through a typical subdivision agreement, including the range of uses that are permitted within these parkland components, the anticipated maintenance protocols required, the determination of dedication ratios for the cultural parkland and natural parkland components, the phasing of dedication, brownfields remediation, access and servicing. This Implementation Plan shall be completed by the owner and adopted by City Council in accordance with Section 19.2.2 of the Official Plan, prior to the disposition of the community parkland components. Should the City and Province both agree, one or more components of the parkland may be transferred to the City or another public proponent in advance of the preparation of the Community Parkland Implementation Plan.
- iii) In association with the Implementation Plan, an analysis of funding sources shall be identified to address the ongoing maintenance of these parkland components including such measures as cash-in-lieu payments, development charge payments, condominium 'common element' fees and other suitable mechanisms to ensure a sustainable source of funding.



20.4.4.7

Natural Heritage

Section 15.2 of the Official Plan includes the City's natural heritage policies. A Phase 2 Natural Heritage Study and Environmental Management Plan was completed as part of the Secondary Plan process. The recommendations of that study are implemented through the following special policies:

i) Species at Risk – Chimney Swift

- a) The use and/or redevelopment of the Infirmary will be permitted provided the chimneys are maintained and disturbance to species and the habitat are avoided;
- b) Further consultation with the Ministry of Natural Resources and the Canadian Wildlife Service will occur during subsequent phases of development to identify, refine and assess the significance of any foraging habitat within the Secondary Plan Area and to determine appropriate measures to mitigate impacts on this habitat;
- c) Additional monitoring of Chimney Swift activity within the Secondary Plan Area will be undertaken through consultation with the MNR and Canadian Wildlife Service, with possible participation by Bird Studies Canada, to monitor Chimney Swift activity and determine whether additional structures or habitats are being used by Chimney Swift (roosting, nesting, foraging) within the Secondary Plan Area;
- d) No development, grading, construction or other disturbance occur within 50 metres of the Infirmary during the breeding bird season when the Chimney Swift species is present;
- e) Any tree and vegetation removal, or any disturbance to any bird nest on the Infirmary building will be avoided during the breeding bird window of between May 1st and July 31st in accordance with the Migratory Bird Convention Act; and,
- f) An Environmental Impact Study shall be prepared in support of any development within 120 metres of the Infirmary that includes monitoring surveys to determine the location and significance of Chimney Swift foraging habitat and whether additional structures are being used by Chimney Swifts for roosting or nesting, and which recommends appropriate mitigation measures to avoid disturbance to the existing Chimney Swift populations as a result of land use activities within or adjacent to the Infirmary.

ii) Tree Protection

- a) A Tree Preservation Plan shall be prepared for all development applications to identify trees to be retained and removed, as well as measures to protect individual species during construction and grading activities against inadvertent damage.



- b) Pruning or removal of key specimen trees within the cultural heritage landscape shall be limited to removal of dead, diseased or hazardous trees or where retention of specific trees is not feasible due to future development locations or grading requirements.
- c) Grading and construction activities adjacent to any key specimen trees within the cultural heritage landscape shall maintain appropriate setbacks to avoid damage to the limbs and/or root zone, including those trees located within the Treed Allée. Specific setbacks will be determined through the required Tree Preservation Plan.
- d) Prior to development adjacent to the Treed Allée a woodland management plan shall be prepared to ensure the long term conservation of this cultural woodland community, with consideration for removal of damaged or unhealthy trees, replacement with suitable native species and strategic replanting/management of this feature to maintain the woodland community in the absence of natural regeneration and successional growth.
- e) Protection of existing trees, where possible, shall be a priority in the implementation of the new Community road network.

iii) Wetland

- a) The wetland buffer and surrounding open space area north of the existing unevaluated wetland, identified on Schedule B1 - Natural Heritage Features, of the City of London Official Plan will be naturalized. Exceptions may be permitted for a community garden.
- b) Invasive and exotic tree and shrub species will be removed and managed across the LPH lands, but specifically within the wetland and surrounding area.
- c) Local rare plant species impacted by a proposed development will be transplanted to suitable habitat adjacent to the wetland, buffers or open space areas on the lands that are to be retained.
- d) An Environmental Impact Statement (EIS) will be prepared in support of any future development within 30 metres of the wetland. The EIS will include a fall botanical survey, summer odonata and herpetile survey, delineation of wetland boundaries per the Ontario Wetland Evaluation System protocol and corresponding evaluation of wetland significance.
- e) A water balance study will be completed in association with the EIS for any development adjacent to the wetland to determine the existing hydrological conditions supporting this wetland feature (surface water/groundwater) and to confirm that any proposed grading, construction or stormwater management (SWM) will maintain the hydrological conditions necessary to support the wetland feature and its associated ecological/hydrological functions.



20.4.4.8 Stormwater Management

The optimal stormwater management solution for the London Psychiatric Hospital Lands to meet future development needs, satisfy all applicable design constraints, and maintain or improve the condition of Pottersburg Creek, was included in the London Psychiatric Hospital Lands Municipal Environmental Assessment Study (Stantec, September 14, 2011). As part of the assessment, special meetings were held to notify the stakeholders of the commencement of the project, identify the alternative solutions, present the preferred alternative and to receive public and agency input. Opportunities were reviewed to optimize the location of the storm/drainage and stormwater management servicing solution, while maintaining engineering design standards, and considering the social/cultural, natural environment, technical, planning, and economic implications. The solution accepted by Council is incorporated into the Official Plan as part of this Secondary Plan.

Any recommendations arising from the integrated London Psychiatric Hospital Lands Municipal Environmental Assessment Study shall be implemented. Any development of the LPH lands shall also be consistent with Official Plan policies in Section 17.6.

20.4.4.9 Transportation

Proposed streets within the Plan consist of both Secondary Collector and Local Streets that establish the community structure, provide connections to two flanking arterial roads and provide new connections to the adjacent neighbourhood. Use of the transit network, cycling and walking are to be supported through design. Street development shall be in accordance with the Transportation policies specific to individual land use designations, applicable urban design policies in Section 20.4.4.10 of this plan and the following policies:

- i) Enhanced design streets as shown on Schedule 5, shall be designed to support pedestrian-oriented development patterns, with strong relationships to the cultural heritage landscape;
- ii) On street frontages identified as "Restricted Driveway Access" on Schedule 8, no driveways shall be permitted;
- iii) On street frontage identified as "Limited Driveway Access" on Schedule 8, only driveways or laneways providing access to common parking areas or parking structures located to the rear of buildings shall be permitted;
- iv) The use of Common Elements Condominiums should be considered for the ownership, use and maintenance of common laneways and driveways;





- v) In areas shown as “Priority Tree Retention Areas” on Schedule 5, alternative road cross sections, utility placement and construction standards may need to be considered to protect the long term health of trees. Within these areas, in addition to the Tree Preservation Plan required by Section 20.4.4.7 (ii), a tree management and planting strategy shall be established in order to conserve and sustain the significant landscape setting;
- vi) Special design treatments shall be implemented in areas shown as “Potential Traffic Calming” to slow or restrict traffic movements and place a priority on pedestrian movements.
- vii) Traffic controls, including the provision of signalized intersections and turning movement restrictions shall be implemented as shown on Schedule 5;
- viii) At the subdivision and/or site plan application stage, consideration should be given to the provision for the conveyance and construction of “Possible Future Streets” as shown on Schedule 5 to create future opportunities for connectivity between neighbourhoods should the industrial area evolve to other uses;
- ix) At the subdivision and/or site plan application stage, Rapid Transit corridors including “transit only” lanes along Oxford Street East and Highbury Avenue North shall be protected in accordance with City requirements;
- x) At the subdivision and/or site plan application stage, any land required to accommodate additional through lanes or turn lanes as determined by the Transportation Master Plan, subsequent studies recommended by the Transportation Master Plan or a detailed transportation impact analysis shall be protected in accordance with City requirements.
- xi) Design guidelines, including right-of-way cross sections, urban street infrastructure and facilities, plantings, surface treatments, parking and service and utility placement, may be prepared to provide further guidance for the development of public rights-of-way to support pedestrian and cyclist friendly environments for using the public streets, public transit, public parking, cycling and pedestrian networks.
- xii) The utilization of on-street parking facilities, both angled and parallel, shall be incorporated into the design of the public right-of-way surrounding the central green to protect this cultural heritage feature from large surface parking areas.
- xiii) Long stretches of on-street parking shall be broken-up with landscaped “bump-outs”.

20.4.4.10 Urban Design

The following policies will apply to all designations within the Secondary Plan area:

- i) Gateways
 - a) Buildings located adjacent to areas identified as “Gateways” on Schedule 8 shall incorporate corner massing elements and the building’s main entry at the corner, so as to signify the entrance to the community.
- ii) Built Form
 - a) Where built form is situated on more than one public right-of-way, the building’s main entrance shall be oriented to the highest order public right-of-way, as identified in Schedule 5. Arterial roads will serve as the highest order right-of-way. The built form shall incorporate articulation and massing in a coherent architectural manner adjacent to all public rights-of-way. This policy does not apply where the built form is adjacent to an area identified as a gateway on Schedule 8. The Gateways policy (20.4.4.10 (i)) shall apply in this situation.
 - b) Built form shall be street-oriented on all public rights-of-way, with buildings located at or near the property line and front entrances oriented to the street.
 - c) Buildings shall be designed to form a well defined and continuous street edge with high quality architectural features.
 - d) Articulation and massing in a coherent architectural manner shall be applied to distinguish the built form’s base, middle and top.
 - e) A diversity of material types, with texture, applied to the base, middle and top of the building(s) is encouraged.
 - f) Buildings shall be oriented so that their amenity spaces do not require sound attenuation walls and that noise impacts on adjacent buildings are minimized.
 - g) The built form shall avoid long expanses of pitched roofs.
 - h) Buildings with a height over 3 storeys shall incorporate architectural massing that avoids the use of pitched roofs.





- i) Buildings with a height of 10 storeys or less shall provide articulation and massing in a coherent architectural manner to the built form's base, middle and top.
 - j) Buildings with a height of 11 storeys or greater shall incorporate a podium at the base of 3 to 4 stories in height. The floorplate size for any tower above the top floor of the podium shall have a maximum floorplate area of 1200 m².
 - k) For east-west streets, where possible, ensure adequate sunlight is provided for sidewalks by building within a 45 degree angular plane from the opposite sidewalk.
 - l) Long expanses of flat and blank facades are to be avoided.
 - m) Built form identified as Priority View Terminus, on Schedule 8 Urban Design Priorities, of this Plan, shall act as a focal point for the view corridor. This can be achieved through the siting, massing, materiality and articulation of the building.
 - n) Built form adjacent to views and vistas identified in Schedule 8 Urban Design Priorities of this Plan, shall enhance and frame the view corridor through the massing and articulation of the building.
- iii) Public Realm
- a) On-street parking will be provided along all streets within this Secondary Plan.
 - b) Provision is to be made for street trees and an appropriate planting environment along all streets within this plan.
 - c) Where permitted, utilities shall co-locate under the sidewalk to provide optimal growing space for trees. In addition, above-grade utility boxes/features are to be minimized and/or clustered. The preference is for these services to be placed below grade.
 - d) Public Realm Design Guidelines, including right-of-way cross sections, may be prepared to provide further guidance for the development of public rights-of-ways in all designations of this plan.
 - e) Landscape walls for the purposes of identifying or delineating community/subdivision entrances shall not be permitted.
 - f) Decorative street and pedestrian level light standards are to be used to differentiate the Village Core Area. These light standards, will reflect and complement the historic features of the Community and will be similar to those used throughout the entire Secondary Plan area, but with enhanced elements.

- iv) Transportation
- a) Driveway access shall be restricted and/or limited in certain locations as identified in Schedule 8 of this Plan and defined in 20.4.4.9 of this Plan.
 - b) Subject to more specific or contradictory transportation policies in the applicable Character Area policies, off-street parking for all uses other than single detached, semi-detached, duplex and townhouse dwellings, shall be located underground or to the rear of the site (behind the building), in a structure or surface parking field. If these options are not possible, then surface parking can be considered in the interior side yard, provided design measures are used to mitigate the parking field from the public realm. Parking is not permitted between the building line and the public right-of-way.
 - c) Off-street parking for single detached, semi-detached, duplex and townhouse dwellings, may include the following
 - As set out in b) above, with the exception of interior side yard parking; or,
 - In an enclosed attached garage located at the front of the dwelling or to the rear of the dwelling in an enclosed attached or detached garage or surface space.Garages located at the front of the dwelling are not to project beyond the front wall of the dwelling. (Front porches do not constitute the front wall).
 - d) Appropriate buffering shall be provided between parking areas and “back of building” functions, and adjacent land uses.





- v) Heritage
- Where all non-heritage designations interface with the Heritage Designation of this Plan the following policies shall apply:
- a) Utilities located within and/or adjacent to lands in the Heritage Designation shall be placed below-grade so as not to interfere with the cultural heritage landscape.
 - b) Where no public right-of-way exists, buildings shall be located at or near the property line adjacent to the cultural heritage landscape area in order to frame the space.
 - c) Buildings shall be aligned parallel to the cultural heritage landscape area with the building's main entrances addressing the Heritage Area Designation.
 - d) On-site surface or structured parking, is not permitted between the building line and the property line adjacent to the cultural heritage landscape area.
 - e) Building heights adjacent to the Heritage Area designation shall be in accordance with Schedule 4 Building Height Plan, of this Plan.
 - f) Built form adjacent to the Heritage Area designation shall be designed to be compatible with the character (articulation, massing, landscaping and materials) of the cultural heritage landscape and heritage buildings within the heritage designation of this Plan.
 - g) New construction adjacent to heritage buildings should be compatible with, but visually distinguishable from and subordinate to the heritage resource. The heritage building must be identifiable as a landmark, with new construction forming the background.
 - h) New development should incorporate materials and finishes that are predominant in the remaining and already demolished site buildings. Colours and materials should be selected that enhance or harmonize with the historic buildings.
 - i) The use of salvaged building materials in landscaping, public art and/or new building construction is encouraged should any existing structures be demolished.

20.4.5.1 Implementation of the Plan

The London Psychiatric Hospital Secondary Plan shall be implemented utilizing the following implementation mechanisms:

- i) All municipal works shall be consistent with the policies of this Plan;
- ii) Heritage preservation and conservation shall be consistent with this Plan; and,
- iii) All planning applications shall be consistent with the policies of this Plan.

20.4.5.2 Municipal Works

Municipal works shall be consistent with the policies of this Plan. Such works include:

- i) Road development;
- ii) Sewer, water and wastewater infrastructure;
- iii) Stormwater management facilities in accordance with the recommendations arising from the London Psychiatric Hospital Lands Municipal Environmental Assessment Study (Stantec, September 14, 2011);
- iv) Parks; and,
- v) Public facilities.

20.4.5.3 Official Plan Amendments

- i) Any amendment to the text or Schedules of this Secondary Plan represents an Official Plan amendment. Furthermore, amendments to the Schedules of this Plan may require amendments to the associated schedules of the Official Plan - Schedules "A" - Land Use, "B1" - Natural Heritage Features, "B2" - Natural Resources and Natural Hazards, "C" - Transportation Corridors and "D" - Planning Areas.
- ii) Any applications to amend this Secondary Plan shall be subject to all of the applicable policies of this Secondary Plan, as well as all of the applicable policies of the City of London Official Plan.

20.4.5.4 Zoning

- i) Any applications for amendment to the City of London Zoning By-law shall be subject to the policies of this Secondary Plan and applicable policies of the City of London Official Plan.
- ii) Consideration of other land uses through a Zoning By-law amendment shall be subject to a Planning Impact Analysis as described in the applicable designation of the Official Plan. The Zoning By-law may restrict the size of some uses.

20.4.5.5 Plans of Subdivision/Plans of Condominium/ Consents to Sever

Any applications for subdivision, condominium, or consent to sever, shall be subject to the policies of this Secondary Plan and applicable policies of the City of London Official Plan.

20.4.5.6 Site Plan Approval

Any applications for site plan approval shall be subject to the policies of this Secondary Plan and applicable policies of the City of London Official Plan.

20.4.5.7 Urban Design Policies

All development within the London Psychiatric Hospital Secondary Plan boundaries shall be subject to the urban design policies contained in this Plan, in addition to applicable policies in the Official Plan.

20.4.5.8 Guideline Documents

Guideline documents may be adopted by Council to provide greater detail and guidance for development and the public realm elements of the Secondary Plan area.

20.4.5.9 Phasing, Financing and Monitoring

The London Psychiatric Hospital lands shall be developed with the following objectives:

- i) It is desirable for the Village Core to be developed, at least in part, as early as possible in the development phasing process, such that the vision for the Community can be established;
- ii) The logical and efficient extension of servicing will be the primary driver for the overall phasing of community development.

20.4.5.10 Height Plan

The height plan, as shown on Schedule 4, is a fundamental component of the overall vision for the Secondary Plan. While an Official Plan amendment may be sought to amend this height plan, changes, including applications for bonus zoning, will only be made where:

- i) The objectives of the community structure plan are preserved;
- ii) The cultural heritage landscape is not negatively impacted;
- iii) Land use conflicts with adjacent uses are not created due to the proposed increase in heights;
- iv) Important views and vistas are not obstructed or inappropriately impacted.



20.4.5.11 Interpretation

Section 19.1 of the Official Plan provides for the interpretation of Official Plan designation boundaries and these policies apply to this Secondary Plan.

Schedule 5 shows the street hierarchy plan. This plan establishes a road pattern that represents the foundation for the Community. It establishes the framework for the layout of land uses, the preservation of the cultural heritage landscape and the establishment of the village core.

As Schedule 5 of the Secondary Plan specifically identifies the alignment of roads, and recognizing that these roads may need to be slightly shifted to address constraints and opportunities identified through future subdivision process, minor changes in these road alignments can be made without amendment to the Plan. Substantive changes to any road alignments will require an Official Plan amendment and shall only be allowed where the underlying principles of the Community Structure Plan and the Street Hierarchy Plan are not undermined.





20.4.6

Schedule 1

Community Structure Plan

Schedule 2

Character Area Land Use Designations

Schedule 3

Sub Area Designations

Schedule 4

Building Height Plan

Schedule 5

Street Hierarchy Plan

Schedule 6

Pedestrian and Cycling Network

Schedule 7

Cultural Heritage Framework

Schedule 8

Urban Design Priorities

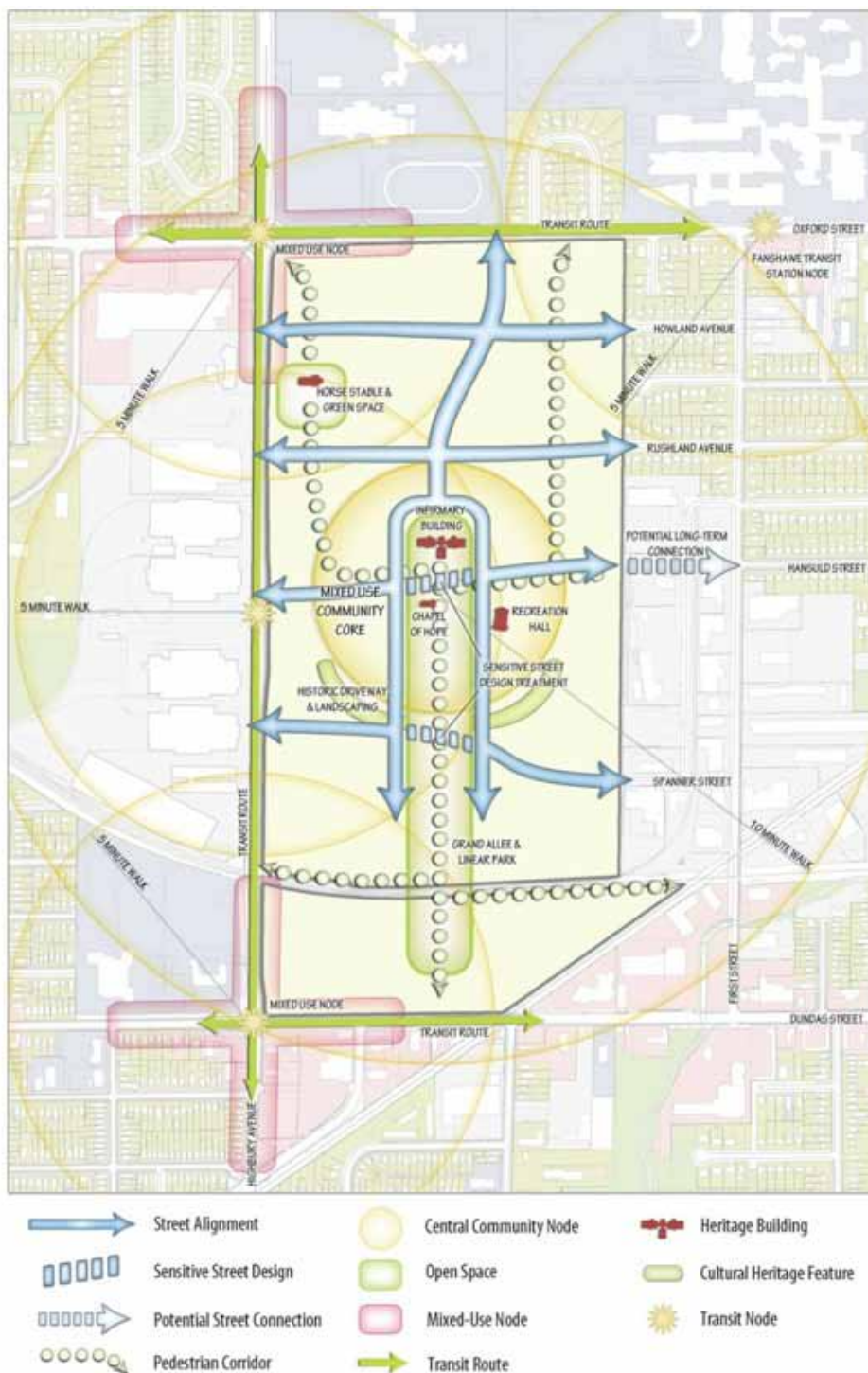
Schedule 9

Potential Noise and Vibration Impact Area

Schedule 10

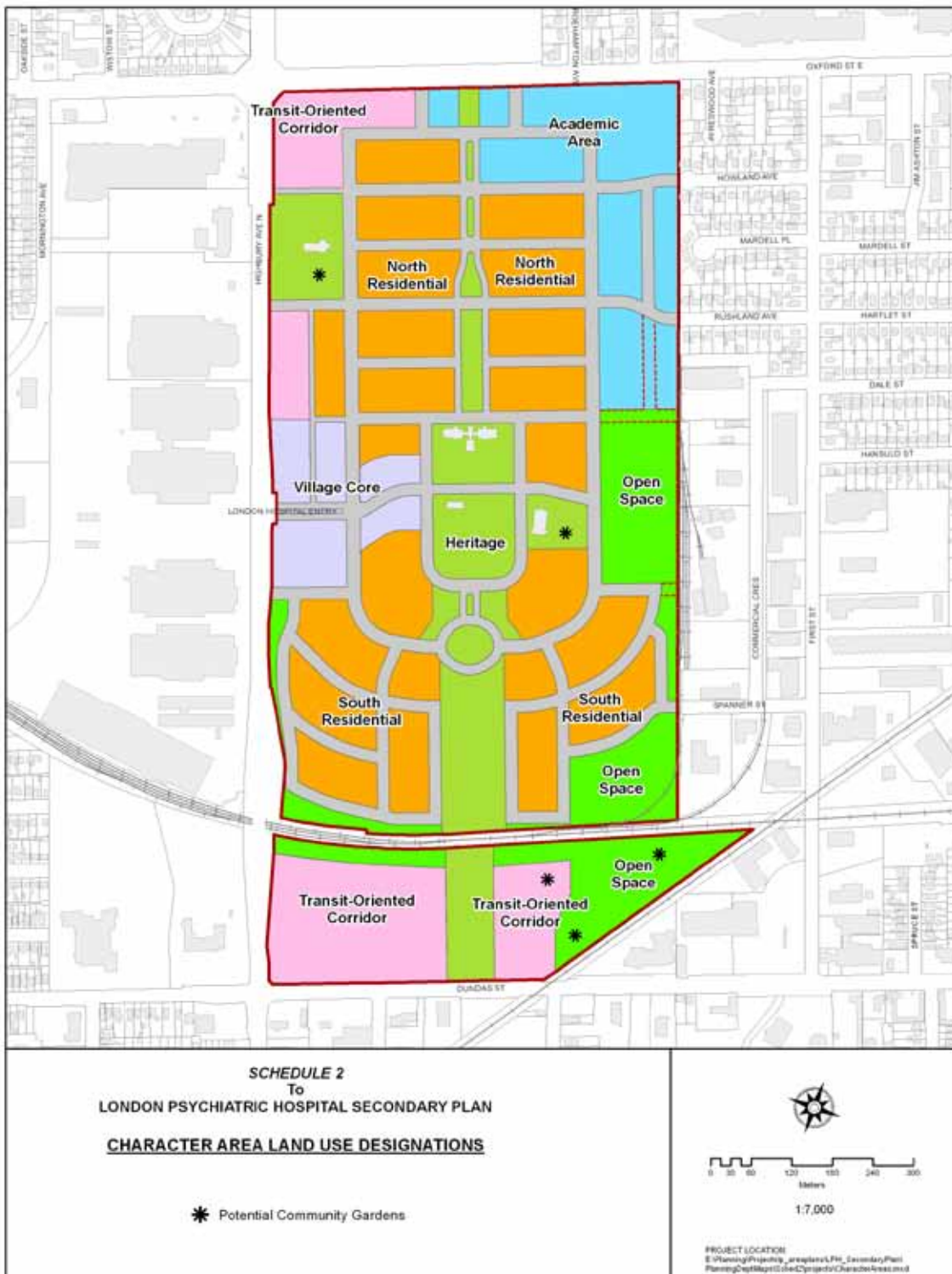
Preferred Storm/Drainage and SWM Servicing Works Solution

61 Schedule 1

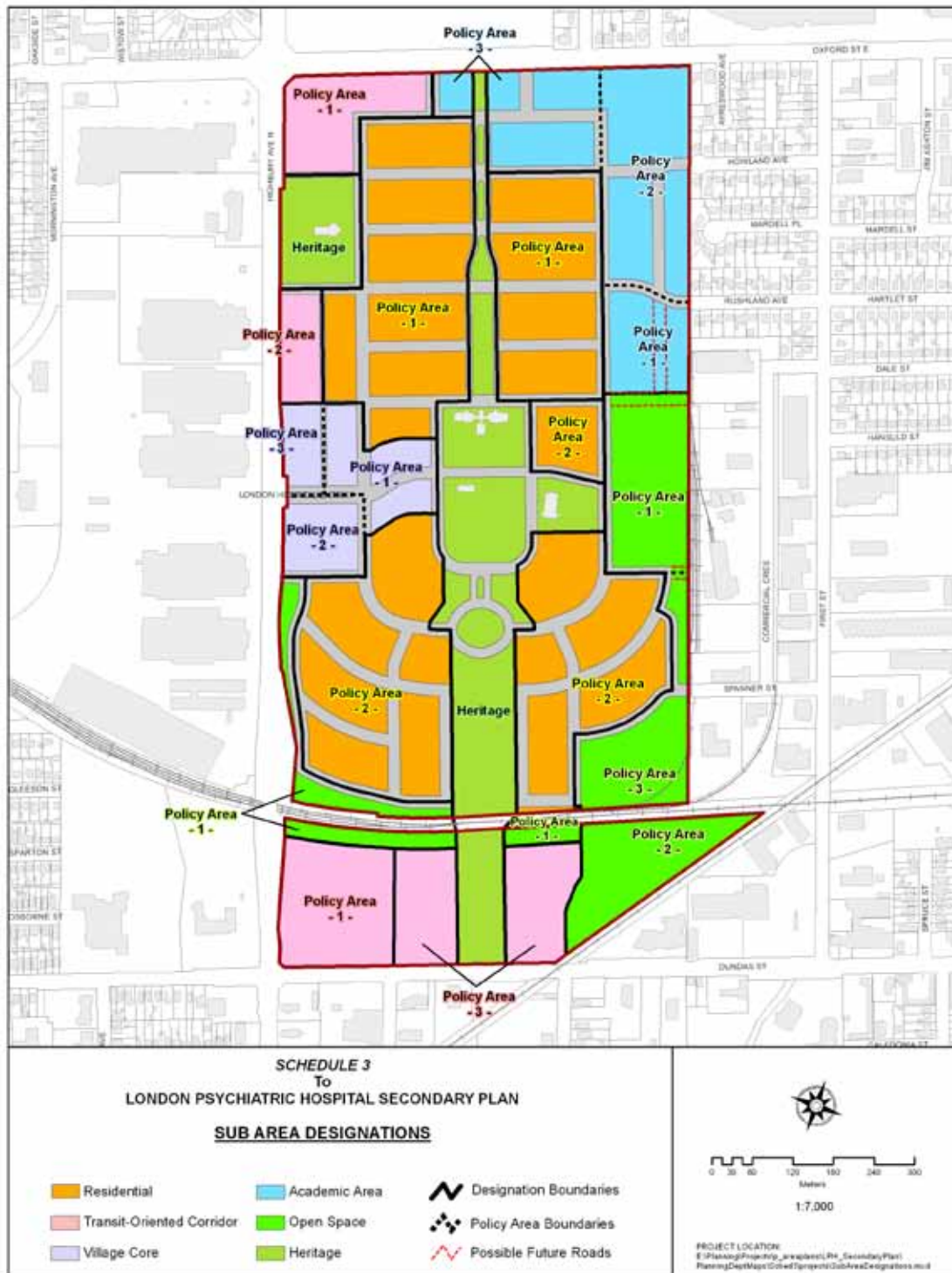


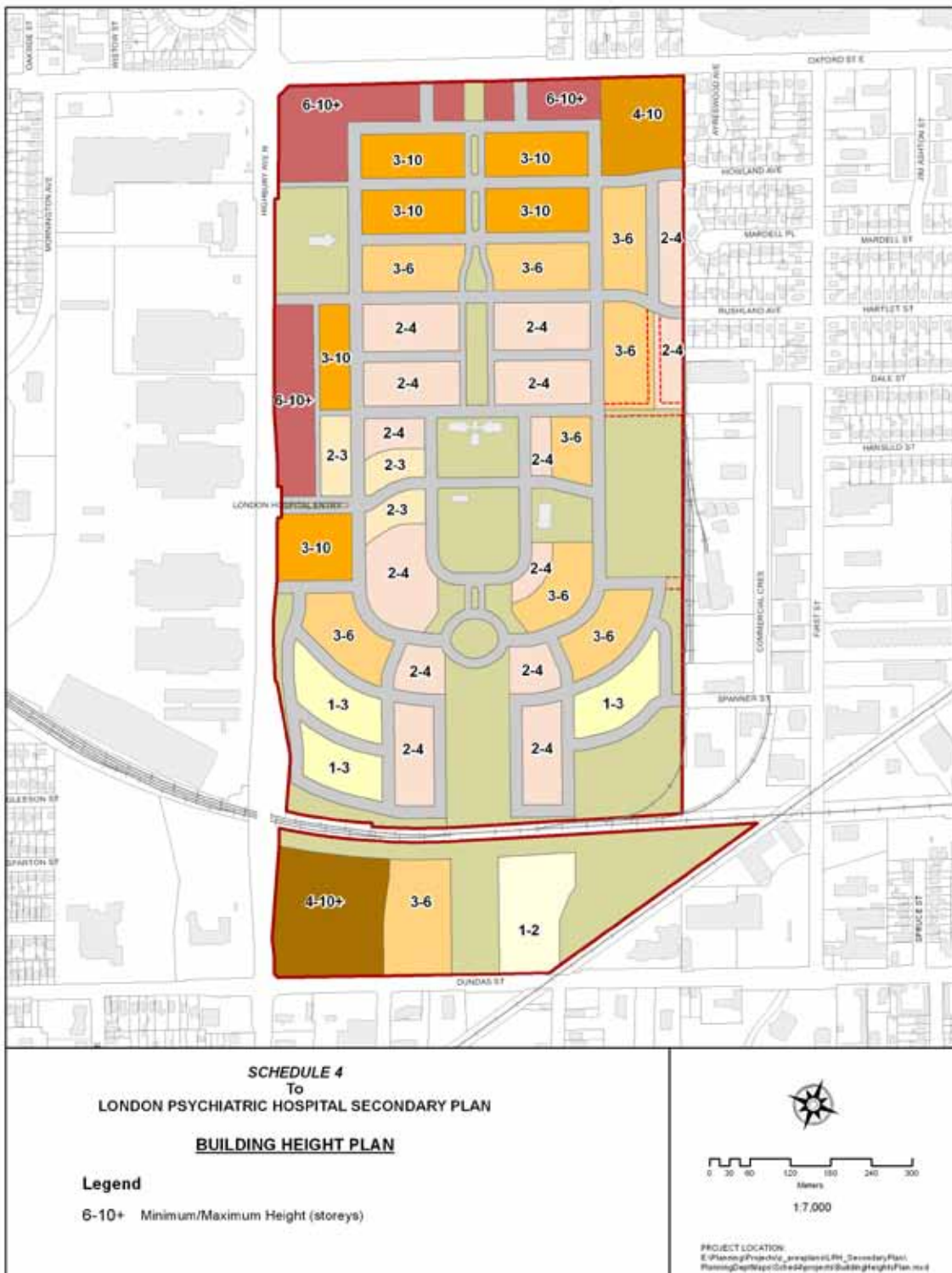
Community Structure Plan

Schedule 2

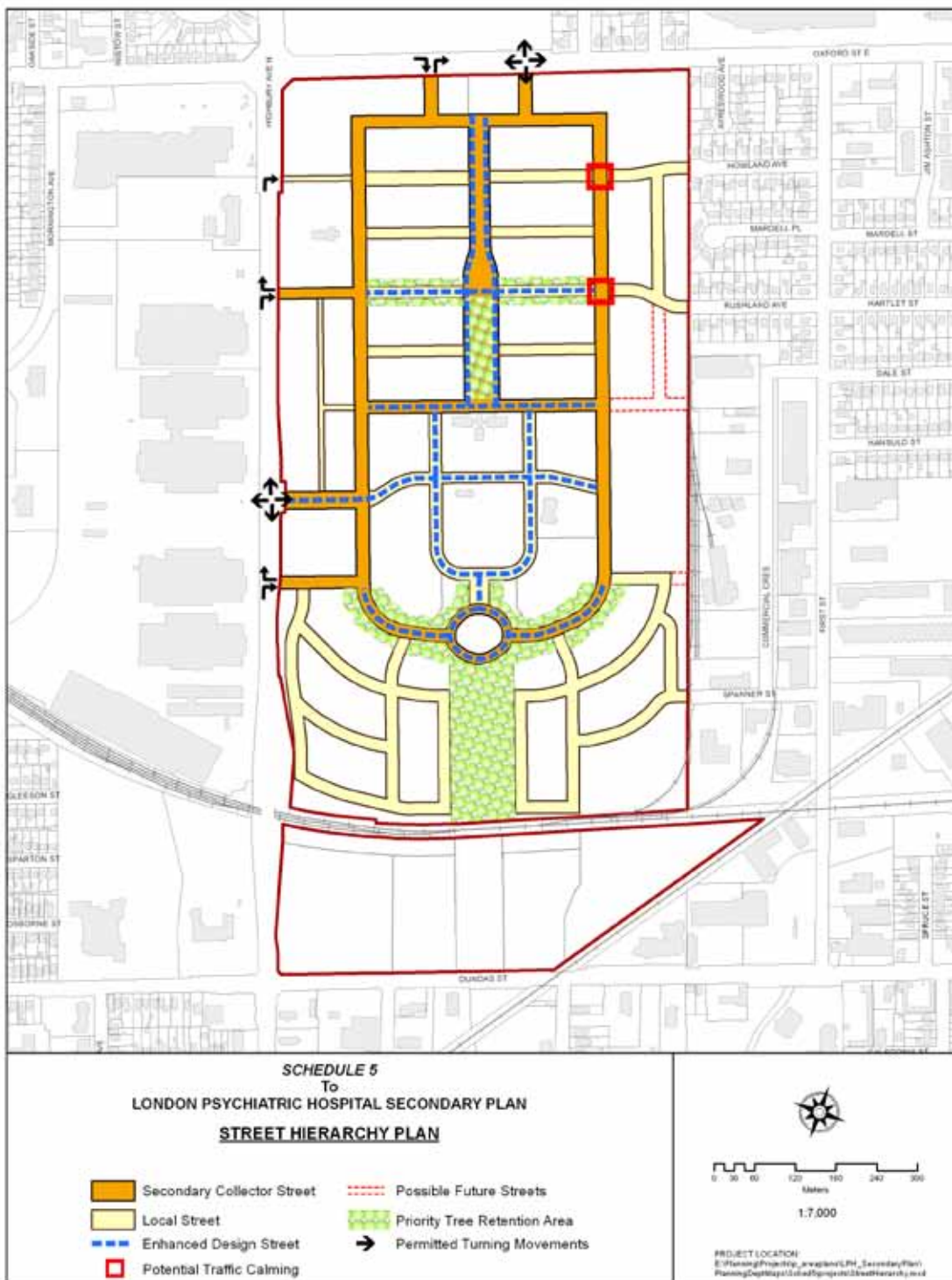


63 Schedule 3

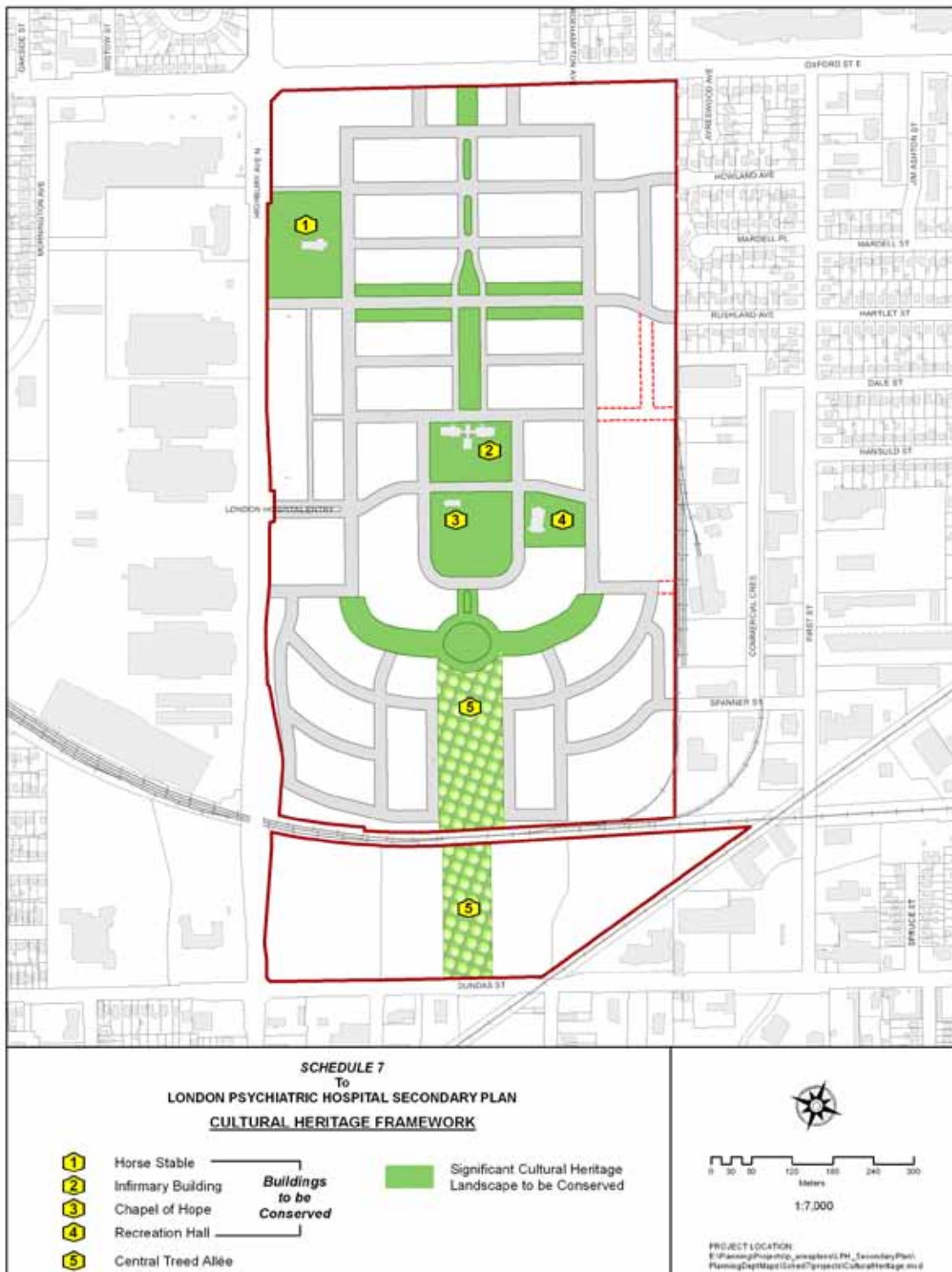


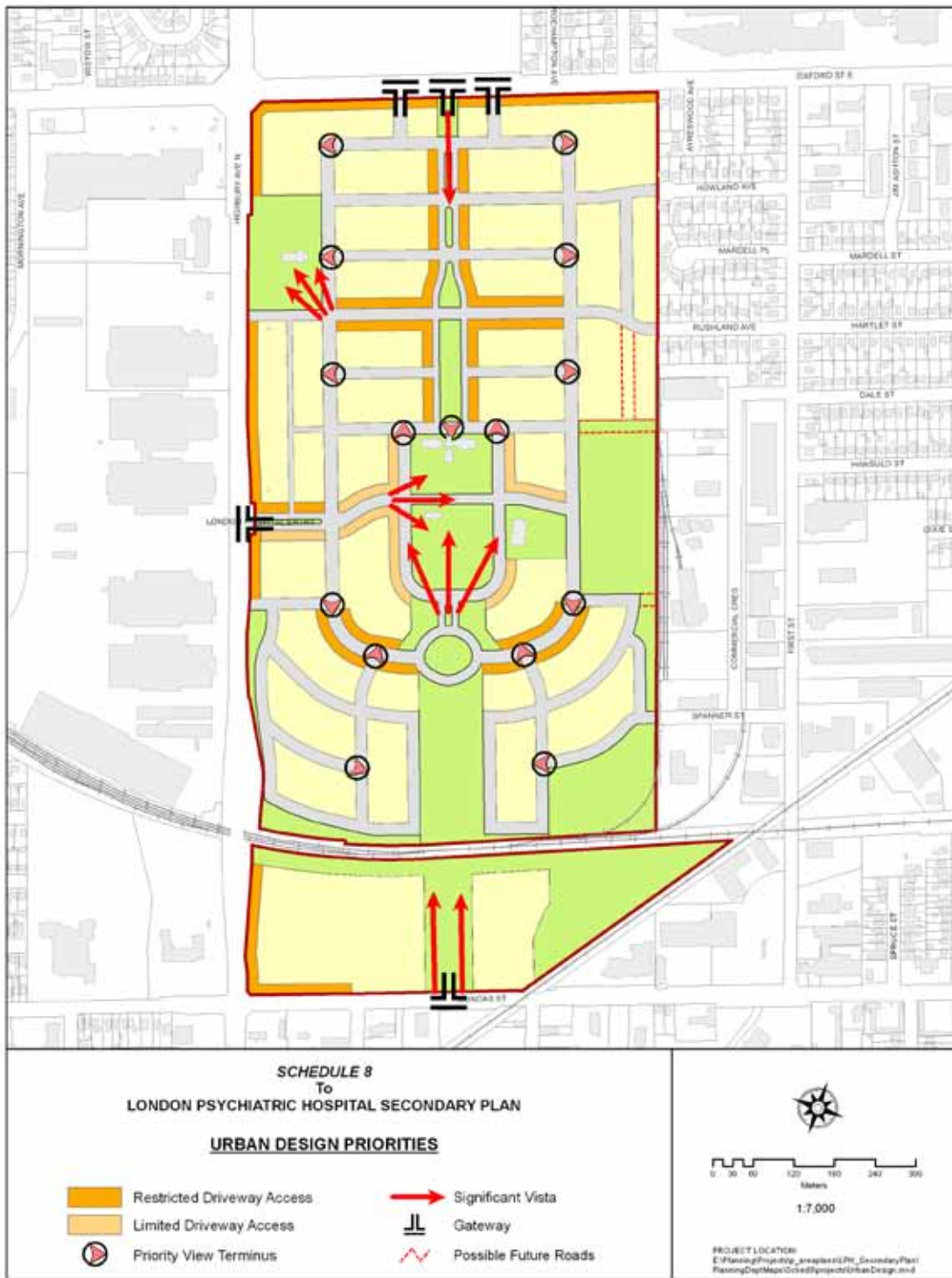


65 Schedule 5

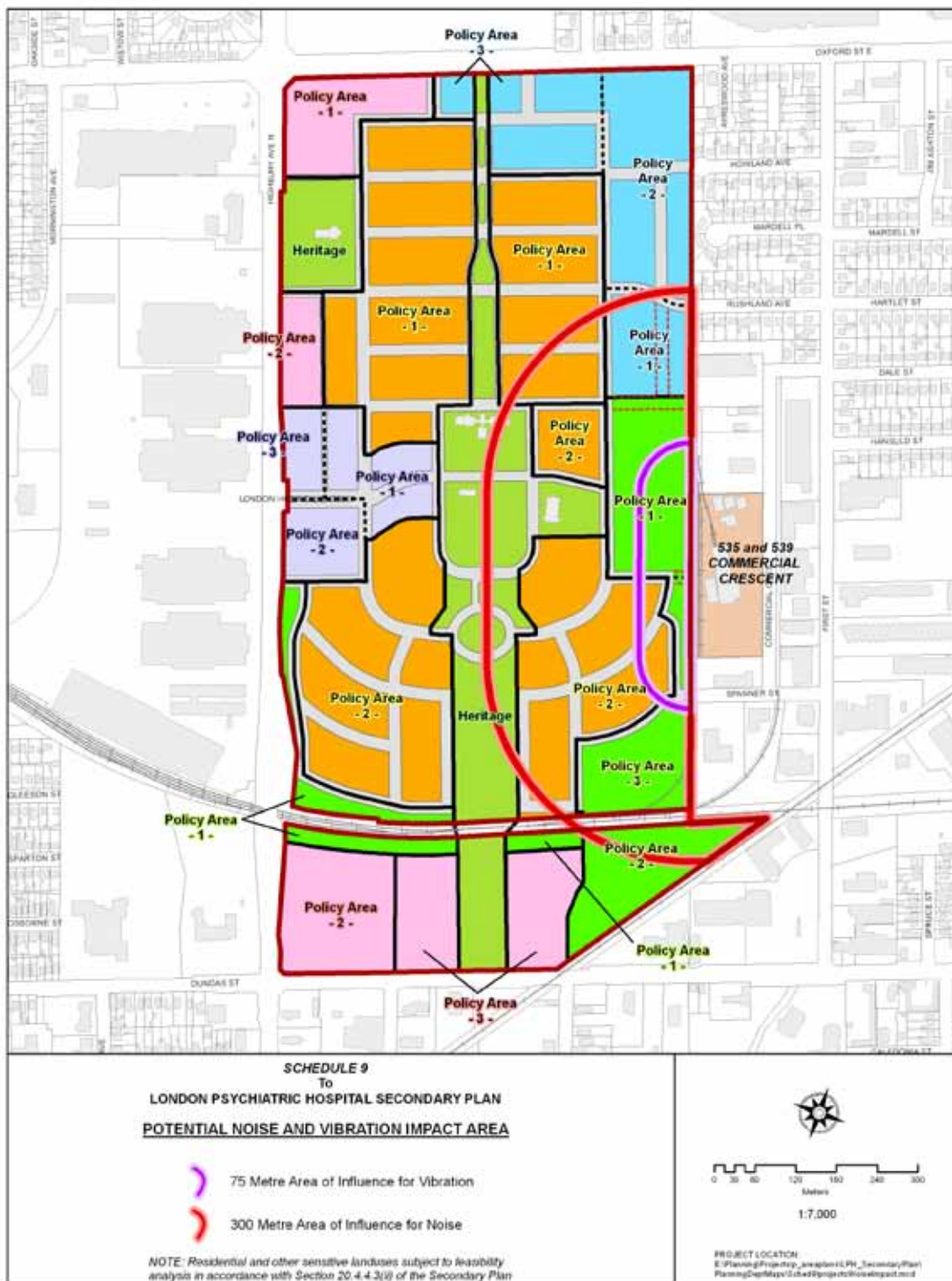


67 Schedule 7





69 Schedule 9





SCHEDULE 10
TO
LONDON PSYCHIATRIC SECONDARY PLAN
PREFERRED STORM/DRAINAGE AND SWM SERVICING WORKS SOLUTION



Schedule A

Land Use

Schedule B1

Natural Heritage Features

Schedule B2

Natural Resource Hazards

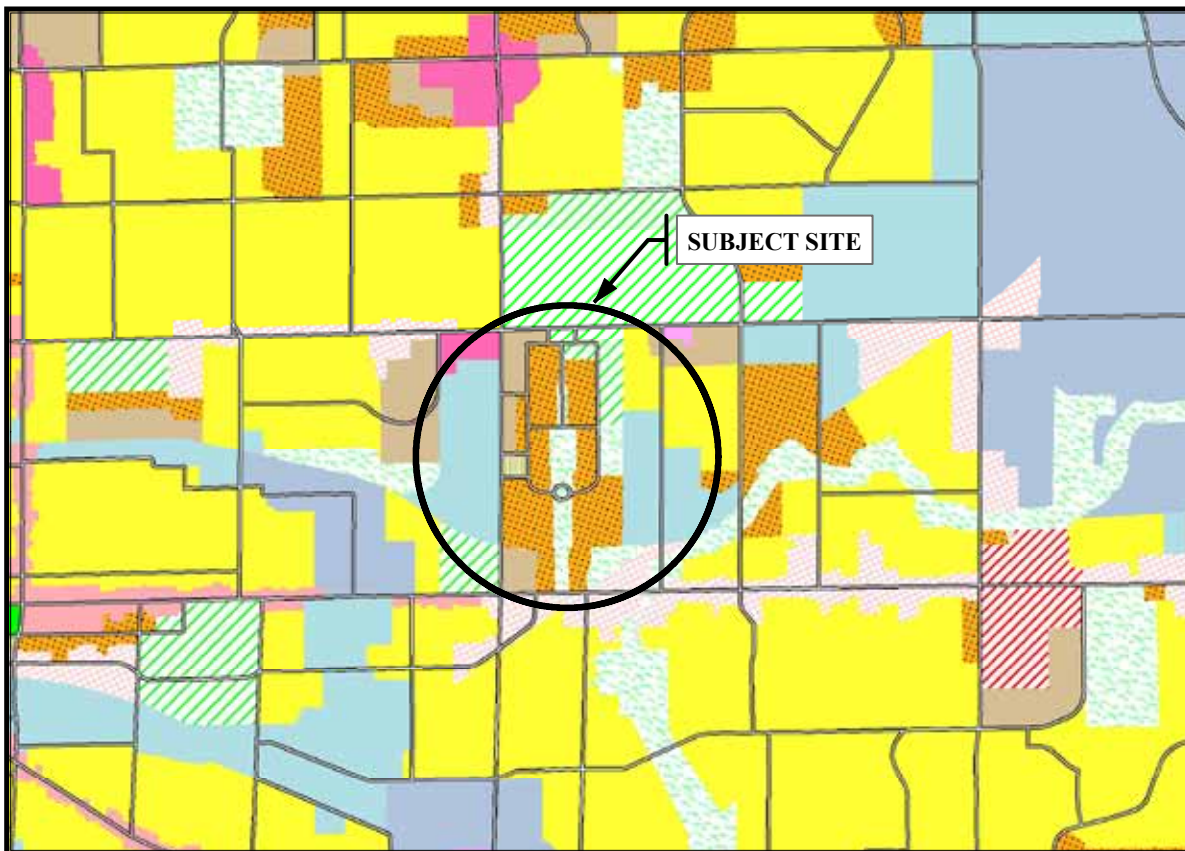
Schedule C

Transportation Corridors

Schedule D

Planning Areas

73 Schedule A



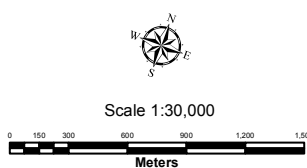
Legend

- | | |
|--|-----------------------------------|
| Downtown | Office Business Park |
| Enclosed Regional Commercial Node | General Industrial |
| New Format Regional Commercial Node | Light Industrial |
| Community Commercial Node | Regional Facility |
| Neighbourhood Commercial Node | Community Facility |
| Main Street Commercial Corridor | Open Space |
| Auto-Oriented Commercial Corridor | Urban Reserve - Community Growth |
| Multi-Family, High Density Residential | Urban Reserve - Industrial Growth |
| Multi-Family, Medium Density Residential | Rural Settlement |
| Low Density Residential | Environmental Review |
| Office Area | Agriculture |
| Office/Residential | Urban Growth Boundary |
| | Areas Under Appeal |

This Official Plan extract is provided for information purposes only. Please refer to Schedule "A" of the Official Plan and any amendments to it for current policy status.

CITY OF LONDON
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OFFICIAL PLAN SCHEDULE A
 - LANDUSE -

PREPARED BY: Graphics and Information Services



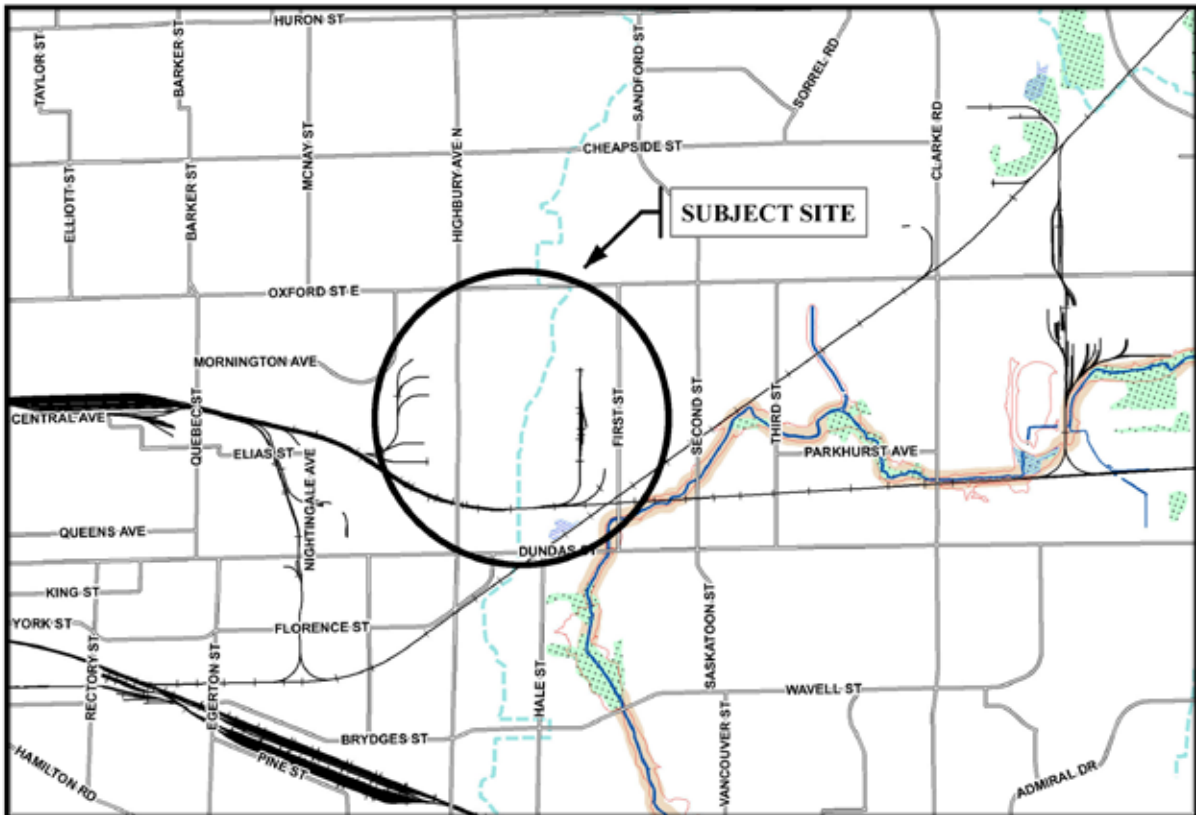
FILE NUMBER: O-7668

PLANNER: BD/CP

TECHNICIAN: MB

DATE: 2011/09/12

PROJECT LOCATION: e:\planning\projects\lp_areaplans\LPH_SecondaryPlan\PlanningDeptMaps\OPA_excerpts\projects\EXCERPTscheduleA_NEW_colour_8x11.mxd



NATURAL HERITAGE SYSTEM

- ESAs
- Potential ESAs
- Significant Woodlands
- Woodlands
- Unevaluated Vegetation Patches
- Significant River, Stream, and Ravine Corridors
- Unevaluated Stream and Ravine Corridors
- Provincially Significant Wetlands
- Locally Significant Wetlands
- Unevaluated Wetlands
- Potential Naturalization Areas
- Potential Upland Corridors
- Ground Water Recharge Areas

NATURAL HAZARDS

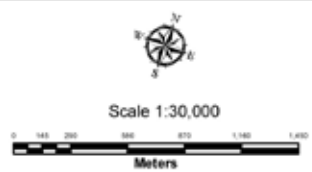
- Maximum Hazard Line
- NOTE 1: Hazard Lines shown on this map are approximate. The precise delineation of hazard line mapping available from the Conservation Authority having jurisdiction.
- NOTE 2: Flood Fringe mapping for certain areas of the city is available from the Upper Thames River Conservation Authority.

Base Map Features

- Railways
- Water Courses/Ponds
- Streets (refer to Schedule "C")
- Conservation Authority Boundary
- Subwatershed Boundary
- Big Picture Meta-Cores and Meta-Corridors
- Areas Under Appeal

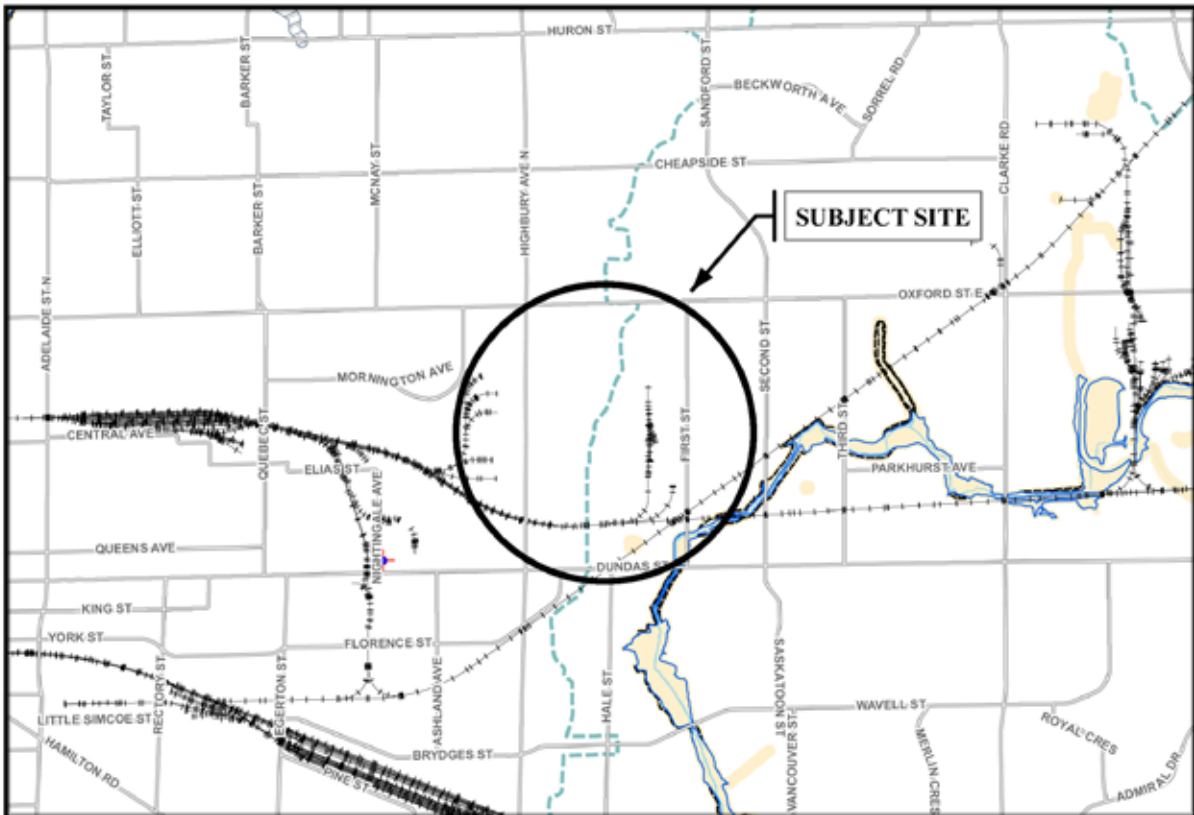
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CITY OF LONDON
 Department of
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OFFICIAL PLAN SCHEDULE B1
- NATURAL HERITAGE FEATURES -
 PREPARED BY: Graphics and Information Services



FILE NUMBER: O-7668
PLANNER: BD
TECHNICIAN: MB
DATE: 2011/09/12

75 Schedule B2



NATURAL RESOURCES

- Aggregate Resource Areas
- Extractive Industrial
- Emergency Municipal Water Wells

Base Map Features

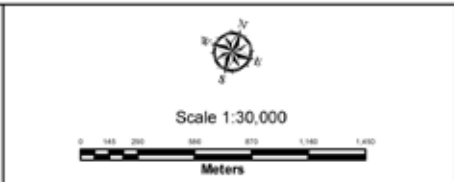
- Railways
- Water Courses/Ponds
- Streets (refer to Schedule "C")
- Conservation Authority Boundary
- Subwatershed Boundary
- Potential Special Policy Areas
- Special Policy Area

NATURAL HAZARDS

- Regulatory Flood Line
NOTE 1: Flood Lines shown on this map are approximate. The precise delineation of flood plain mapping is available from the Conservation Authority having jurisdiction.
NOTE 2: Flood Fringe mapping for certain areas of the city is available from the Upper Thames River Conservation Authority.
- Riverine Erosion Hazard Limit For Confined Systems
- Riverine Erosion Hazard Limit For Unconfined Systems
- Steep Slopes Outside of the Riverine Erosion Hazard Limit
- Abandoned Oil/Gas Wells
- Conservation Authority Regulation Limit

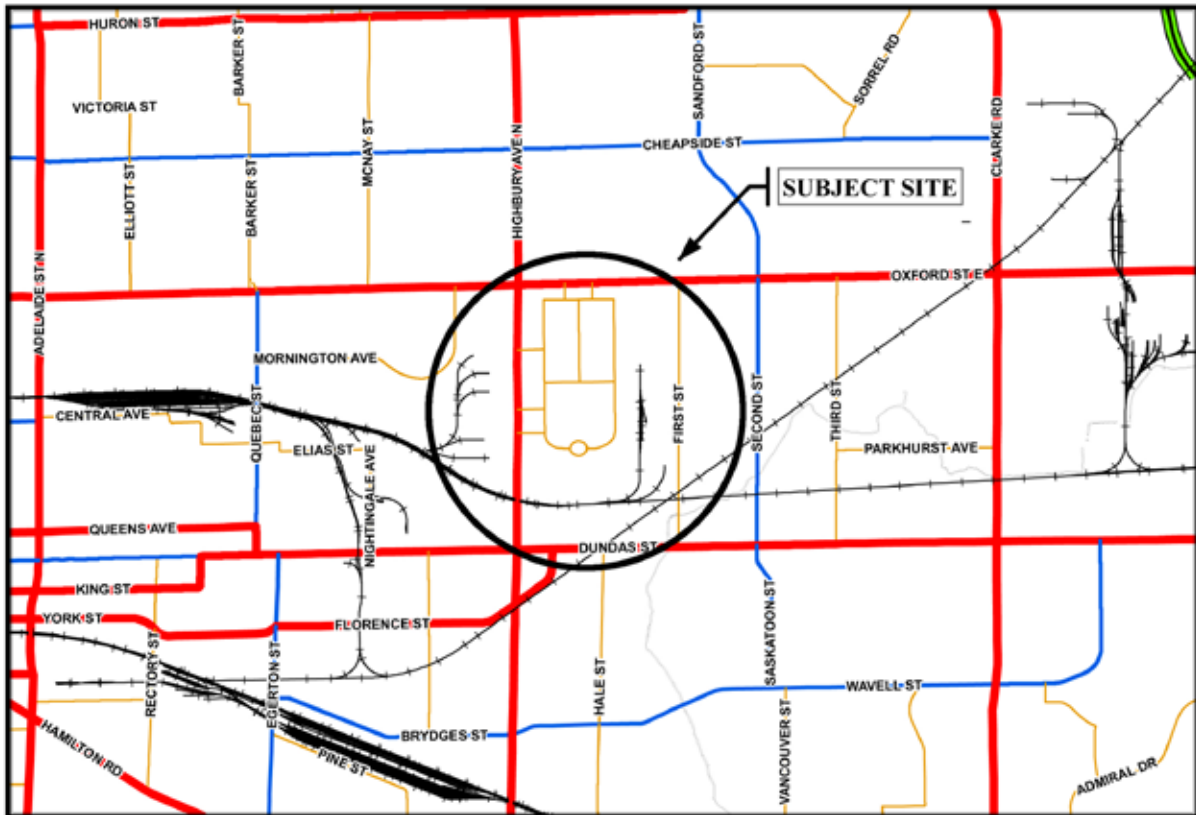
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 Department of
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OFFICIAL PLAN SCHEDULE B2
NATURAL RESOURCES
AND
NATURAL HAZARDS
 PREPARED BY: Graphics and Information Services



FILE NUMBER: O-7668
PLANNER: BD
TECHNICIAN: MB
DATE: 2011/09/12

Schedule C




Legend

ROAD CLASSIFICATION

-  Secondary Collector
-  Primary Collector
-  Arterial
-  Freeway
-  Expressway

PROPOSED ROAD CORRIDOR

-  Proposed Secondary Collector
-  Proposed Primary Collector
-  Proposed Arterial
-  Proposed Freeway
-  Proposed Expressway
-  Proposed Interchange

This Official Plan extract is provided for information purposes only. Please refer to Schedule "C" of the Official Plan and any amendments to it for current policy status.

CITY OF LONDON
Department of
Planning and Development

OFFICIAL PLAN SCHEDULE C
 - TRANSPORTATION CORRIDORS -

PREPARED BY: Graphics and Information Services



Scale 1:30,000



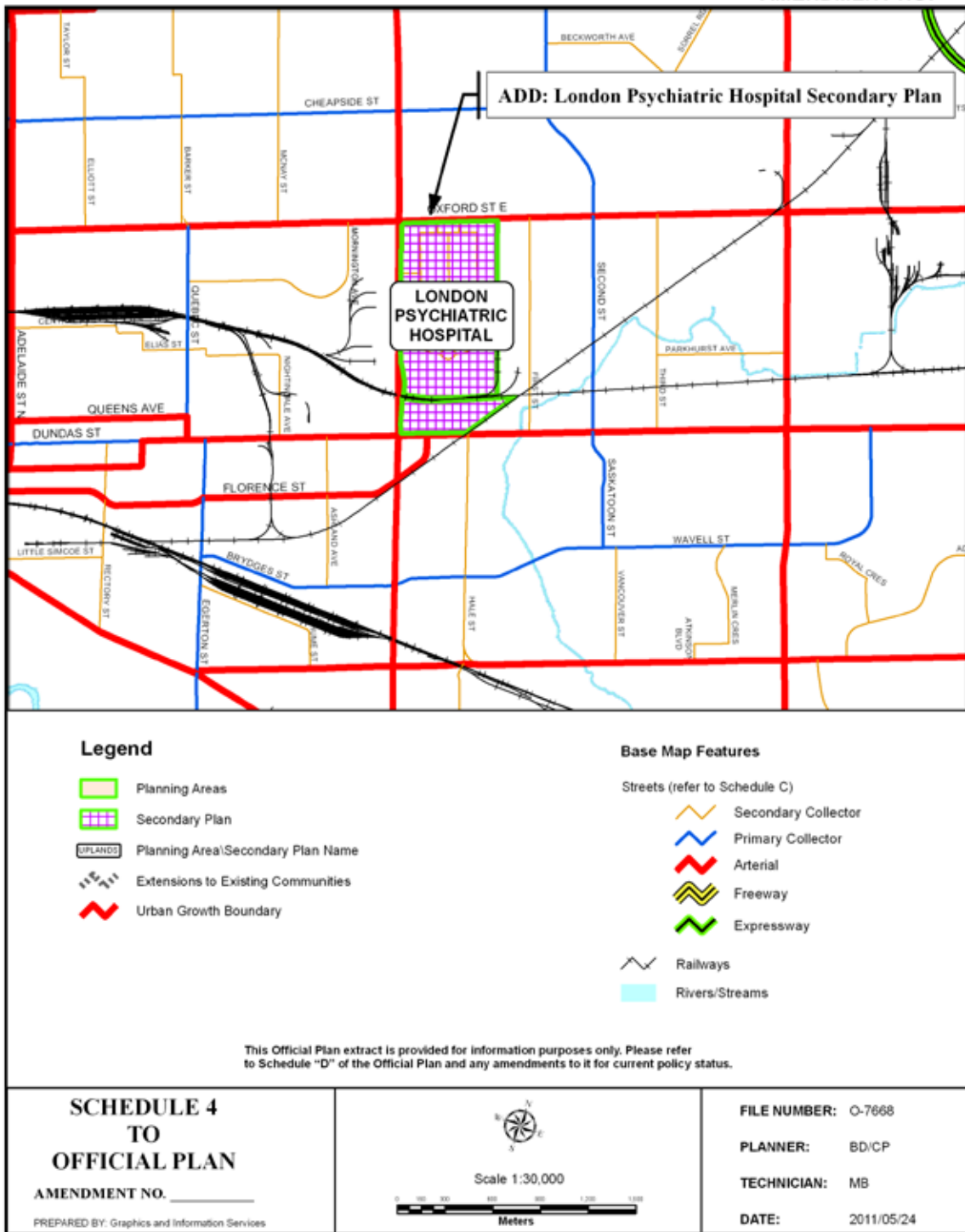
FILE NUMBER: O-7668

PLANNER: BD

TECHNICIAN: MB

DATE: 2011/09/12

77 Schedule D



PROJECT LOCATION: E:\Planning\Projects\p_areaplans\LPH_SecondaryPlan\PlanningDept\Maps\CPA\SCHD\projects\scheduleD_colour_8x11.mxd

