

## Report to Planning and Environment Committee

**To:** Chair and Members  
Planning and Environment Committee

**From:** Scott Mathers, MPA, P.Eng.  
Deputy City Manager, Planning and Economic Development

**Subject:** 2323225 Ontario Inc.  
1900-1902 Trafalgar Street & 159 Clarke Road  
File Number: Z-9604, Ward 02  
Public Participation Meeting

**Date:** July 17, 2023

## Recommendation

That, on the recommendation of the Director, Planning and Development, the following actions be taken with respect to the application of 2323225 Ontario Inc. c/o Candevcon Limited relating to the property located at 1900-1902 Trafalgar Street & 159 Clarke Road:

- (a) The proposed by-law attached hereto as Appendix "A" **BE INTRODUCED** at the Municipal Council meeting July 25, 2023 to amend Zoning By-law No. Z.-1, in conformity with the Official Plan, The London Plan, to change the zoning of the subject property **FROM** a Residential R3 (R3-2) and Convenience Commercial (CC3) Zone, **TO** a Special Provision Neighbourhood Shopping Area (NSA3(\_)) Zone;
- (b) The Site Plan Approval Authority **BE REQUESTED** to consider the following items through the site plan process:
  - I. Fencing and/or landscaping be provided along the perimeter of the site to ensure adequate buffering maintained between the subject lands and adjacent residential properties;
  - II. Reduce parking to provide space for outdoor amenity areas at the rear of the building, as well to incorporate landscape islands, drive aisles and to facilitate on-site maneuvering.

**IT BEING NOTED**, that the above noted amendment is being recommended for the following reasons:

- i) The recommended amendment is consistent with the PPS 2020;
- ii) The recommended amendment conforms to The London Plan, including, but not limited to the Neighbourhoods Place Type and Key Directions; and
- iii) The recommended amendment facilitates the development of an underutilized site within the Built Area Boundary and Primary Transit Area with an appropriate form of infill development that provides choice and diversity in housing options.

## Executive Summary

### Summary of Request

The applicant has requested an amendment to the Zoning By-law Z.-1 to rezone the property from a Residential R3 (R3-2) and Convenience Commercial (CC3) Zone to a Neighbourhood Shopping Area (NSA3) Zone.

### Purpose and the Effect of Recommended Action

The recommended action will permit a mixed-use form of development, with 9 commercial units on the first floor and 9 residential units on the second floor.

Staff are also recommending approval with special provisions that will facilitate an interior side yard parking setback of 3.0 metres to the north and west property lines, a lot frontage of 39 metres, a minimum front yard and exterior side yard setback of 1.0 metres from Clarke Road & Trafalgar Street, a minimum ground floor height of 4.5 metres for commercial uses and a minimum landscaped open space to provide for a larger parkette on the site.

## Linkage to the Corporate Strategic Plan

This recommendation supports the following Strategic Areas of Focus:

- Housing and Homelessness - London's growth and development is wellplanned and considers use, intensity, and form.
  - Direct growth and intensification to strategic locations in a way that maximizes existing assets and resources.

## Analysis

### 1.0 Background Information

#### 1.1 Previous Reports Related to this Matter.

None.

#### 1.2 Planning History

Minor Variance: A.143/15 – To construct a commercial building.

#### 1.3 Property Description and Location

1900-1902 Trafalgar Street and 159 Clarke Road are located north-west corner of the Clarke Road and Trafalgar Street intersection, in the Argyle Planning District. The lands currently contain two single detached dwellings at 1900 and 1902 Trafalgar Street and a vacant property at 159 Clarke Road. The surrounding neighbourhood consists primarily of low-density development to the north and west of 1900 and 1902 Trafalgar Street, with a smaller strip plaza being located to the north of 159 Clarke Road. To the south of all three parcels is a Community Shopping Area consisting of Metro Grocery Store and other stores that support the surrounding neighbourhood. To the east of 159 Clarke Road is a gas station.

#### Site Statistics:

- Current Land Use: Single detached dwellings and vacant lands
- Frontage: 39.6 metres (Clarke Road) 41.1 metres (Trafalgar Street)
- Depth: 65.6 metres (215.2 feet)
- Area: 0.42 hectares (1.05 acres)
- Shape: irregular
- Located within the Built Area Boundary: Yes
- Located within the Primary Transit Area: Yes

#### Surrounding Land Uses:

- North: Low-Density Residential (Trafalgar Street) Neighbourhood Shopping Area (Clarke Road)
- East: Gas Station
- South: Community Shopping Area (Metro Grocery Store)
- West: Low-Density Residential

**Existing Planning Information:**

- Existing London Plan Place Type: Neighbourhoods Place Type fronting two Neighbourhood Connectors
- Existing Special Policies: N/A
- Existing Zoning: Residential R3 (R3-2) & Convenience Commercial (CC3)

Additional site information and context is provided in Appendix "B/C".

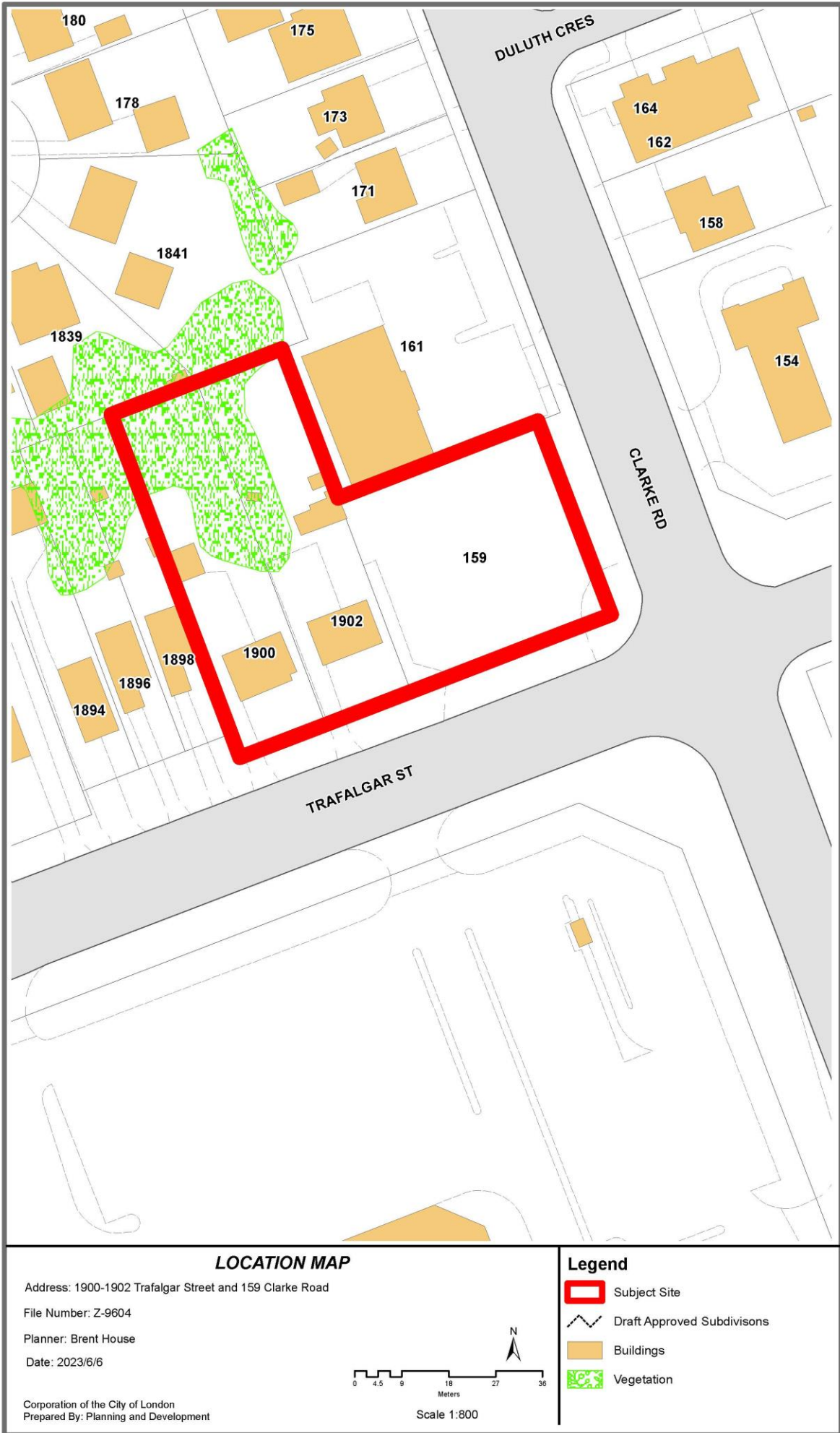


Figure 1- Aerial Photo of 1900-1902 Trafalgar Street and 159 Clarke Road.





Figure 2 - Streetview of 1900-1902 Trafalgar Street (view looking north)



Figure 3 – Streetview of 159 Clarke Road (view from Trafalgar Street and Clarke Road Intersection)

## 2.0 Discussion and Considerations

### 2.1 Development Proposal

The proposed developments of the subject lands consist of a two-storey mixed-use apartment complex.

The proposed development includes the following features:

- Land use: Mixed-use Residential and Commercial Development
- Form: 2 storey mixed use building with commercial on the first floor and residential on second floor
- Height: 2 storeys (8 metres)
- Residential units: 9 units
- Commercial units: 9 units
- Density: 21 Units per Hectare
- Gross floor area: 1740.6 m<sup>2</sup>
- Building coverage: 20%
- Parking spaces: 12 residential parking spaces and 46 commercial retail spaces
- Bicycle parking spaces: 8 residential Spaces and 4 commercial spaces
- Landscape open space: 20%



- Functional amenity space: 50m<sup>2</sup>

Additional information on the development proposal is provided in Appendix “B/C”.

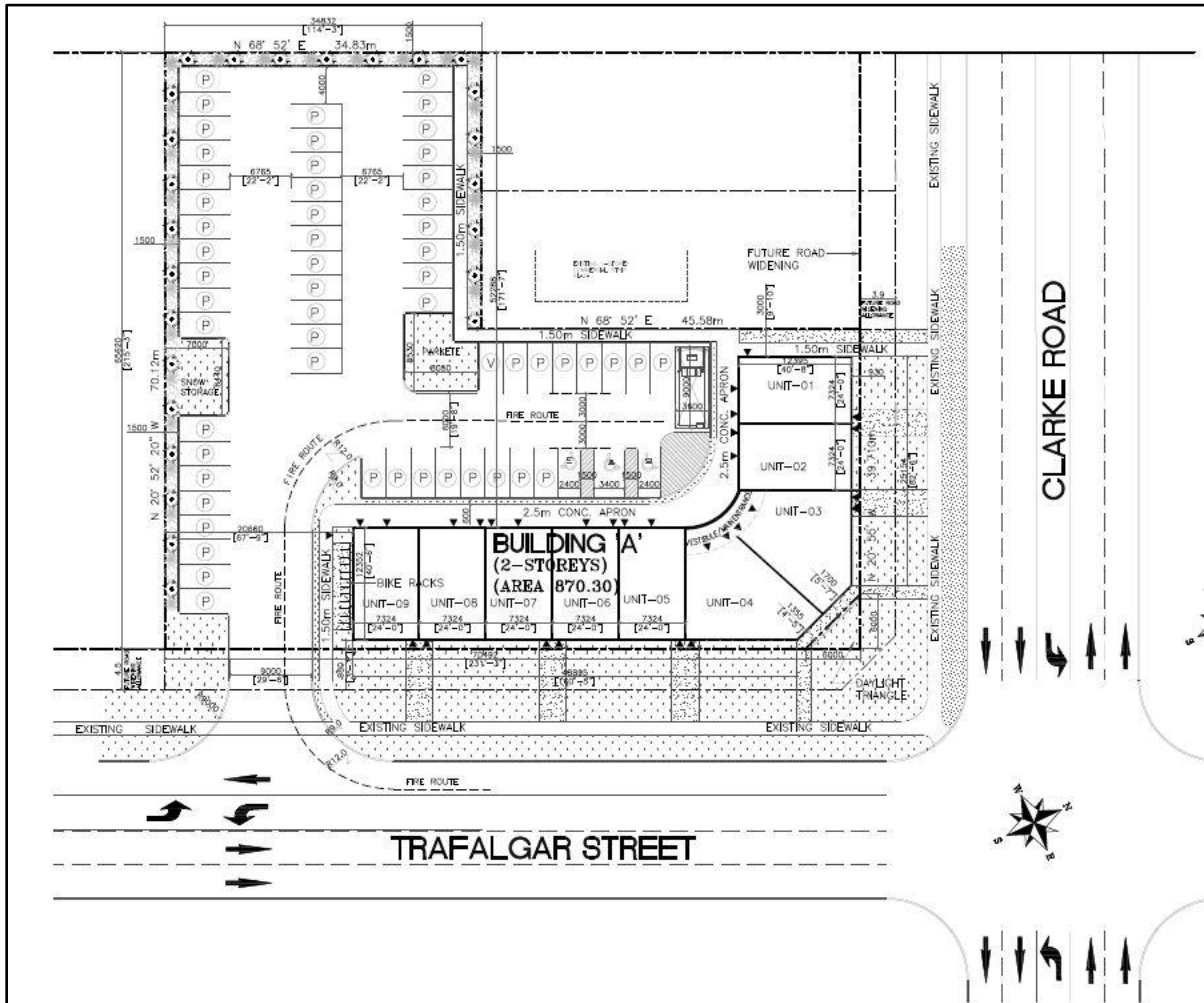


Figure 3 - Conceptual Site Plan (Received November 18, 2022)

### Elevations



Figure 4 – Elevations (Received November 2022)

Additional plans and drawings of the development proposal are provided in Appendix “C/D”.

## 2.2 Requested Amendment(s)

The applicant has requested an amendment to the Zoning Bylaw Z.-1 to rezone the property from a Residential R3 (R3-2) and Convenience Commercial (CC3) Zone to a Neighbourhood Shopping Area (NSA3) Zone.

The following table summarizes the special provisions that have been proposed by the applicant and those that are being recommended by staff.

Regulation (Zone)	Required	Proposed	Staff Recommendation
Frontage (minimum)	40 metres	39 metres	<b>39 metres</b>
Interior Side Yard Parking Setback (minimum)	No requirement	1.5 metres	<b>3.0 metres (North and West Property Line)</b>
Front Yard Setback (minimum)	0 metres	0.93 metres	<b>1.0 metres</b>
Exterior Side Yard Setback (minimum)	0 metres	3 metres	<b>1.0 metres</b>
Landscaped Open Space (minimum)	15%	20.15%	<b>25%</b>

## 2.3 Internal and Agency Comments

The application and associated materials were circulated for internal comments and public agencies to review. Comments received were considered in the review of this application and are addressed in Section 4.0 of this report.

Key issues identified by staff and agencies included:

- Parking configuration
- Landscaped Open Space
- Interior side yard setbacks for north and west lot lines

Detailed internal and agency comments are included in Appendix “D/E” of this report.

## 2.4 Public Engagement

On April 20, 2023, a Notice of Application was sent to 63 property owners and residents in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on April 27, 2023. A “Planning Application” sign was also placed on the site.

There were 2 responses received during the public consultation period. Comments received were considered in the review of this application and are addressed in Section 4.0 of this report.

Concerns expressed by the public relate to:

- Privacy
- Noise
- Elevations and grading

Detailed public comments are included in Appendix “E/F” of this report.

## 2.5 Policy Context

### ***The Planning Act and the Provincial Policy Statement, 2020***

The Provincial planning policy framework is established through the *Planning Act* (Section 3) and the *Provincial Policy Statement, 2020 (PPS)*. The *Planning Act* requires that all municipal land use decisions affecting planning matters shall be consistent with the *PPS*.

The mechanism for implementing Provincial policies is through the Official Plan, *The London Plan*. Through the preparation, adoption and subsequent Ontario Land Tribunal (OLT) approval of *The London Plan*, the City of London has established the local policy framework for the implementation of the Provincial planning policy framework. As such, matters of provincial interest are reviewed and discussed in *The London Plan* analysis below.

As the application for a Zoning By-law amendment complies with *The London Plan*, it is staff's opinion that the application is consistent with the *Planning Act* and the *PPS*.

### ***The London Plan, 2016***

*The London Plan* (TLP) includes evaluation criteria for all planning and development applications with respect to use, intensity and form, as well as with consideration of the following (TLP 1577-1579):

1. Consistency with the Provincial Policy Statement and all applicable legislation.
2. Conformity with the Our City, Our Strategy, City Building, and Environmental policies.
3. Conformity with the Place Type policies.
4. Consideration of applicable guideline documents.
5. The availability of municipal services.
6. Potential impacts on adjacent and nearby properties in the area and the degree to which such impacts can be managed and mitigated.
7. The degree to which the proposal fits within its existing and planned context.

Staff are of the opinion that all the above criteria have been satisfied.

## **3.0 Financial Impact/Considerations**

None.

## **4.0 Key Issues and Considerations**

### **4.1 Land Use**

The proposed mixed-use commercial and residential development is supported by the policies of the *Provincial Policy Statement* and is contemplated in the Neighbourhoods Place Type in *The London Plan* (TLP Table, 10). The site is located at the intersection of two Neighborhood Connectors on Map 3, Street Classification, within The London Plan, which, as noted above contemplates mixed-use buildings with commercial uses at grade but does not permit stand alone commercial uses. The requested NSA3 zone would permit stand alone commercial uses, as such, a special provision is being recommended to ensure any permitted commercial use would be located within the first or second floor of an apartment building.

### **4.2 Intensity**

The proposed intensity is consistent with the policies of the PPS that encourage residential intensification (PPS 1.1.3.3 and 1.4.3), an efficient use of land (PPS 1.1.3.2) and a range and mix of housing options (PPS 1.4.3). The proposed intensity conforms with the Neighbourhoods Place Type in *The London Plan* which requires a minimum 2-storey built form while helping contribute to the intensification target within the Primary Transit Area and Built Area Boundary (TLP Table 11). Servicing is available for the proposed number of units.

### **4.3 Form**

The proposed built form is street oriented and in conformity with design guidelines within the City of London's City Design Guidelines. The building is oriented to both Trafalgar Street and Clarke Road, which provides a street wall along both Neighbourhood Connectors. The special provisions requested by City Staff relate to



interior side yard setbacks, lot frontage and front and exterior side yard setbacks and an increase in landscaped open space to facilitate site design and building layout.

The following form-based issues raised through the review of the Zoning By-law Amendment application can be addressed as part of the subsequent Site Plan Application and are included as recommended considerations to the Site Plan Approval Authority:

- Parking configuration/Number of parking spaces
- Driveway Access
- Fencing, landscaping, boundary trees

The applicant has provided parking at a rate of 1.25 spaces per unit for the residential units (12 spaces) and at a rate of 1 per 15 square metres for commercial spaces (46 spaces), which is based on previous parking standards outlined in the By-law (Section 4.19). As the parking requirements have changed, the updated rate of 0.5 spaces per unit for residential spaces (5 spaces) and at an updated rate of 1 parking space per 50 square metres of commercial gross floor area (18 spaces) will allow for less overall surface parking on the subject lands and provide more room for on site amenities such as a larger parkette for the built form and surrounding neighbourhood.

Interior side yard setbacks facilitate buffers between properties for privacy and maintenance purposes, while also protecting boundary trees and critical root zones. The applicant has provided a 1.5 metre buffer from the northern interior side yard and the western interior side yard. City staff are recommending these setbacks be increased to 3 metres in order to provide protection to the boundary trees located on the lands. The 3-metre setback is more appropriate to provide additional buffering and landscaping between the proposed parking and abutting residential lots. As the interior side yard to the north has two different neighbouring properties, staff are recommending the 3-metre setback to be included for the residential parcel to the north, and the 1.5 metre setback can be kept where the property borders the NSA1 Zone.

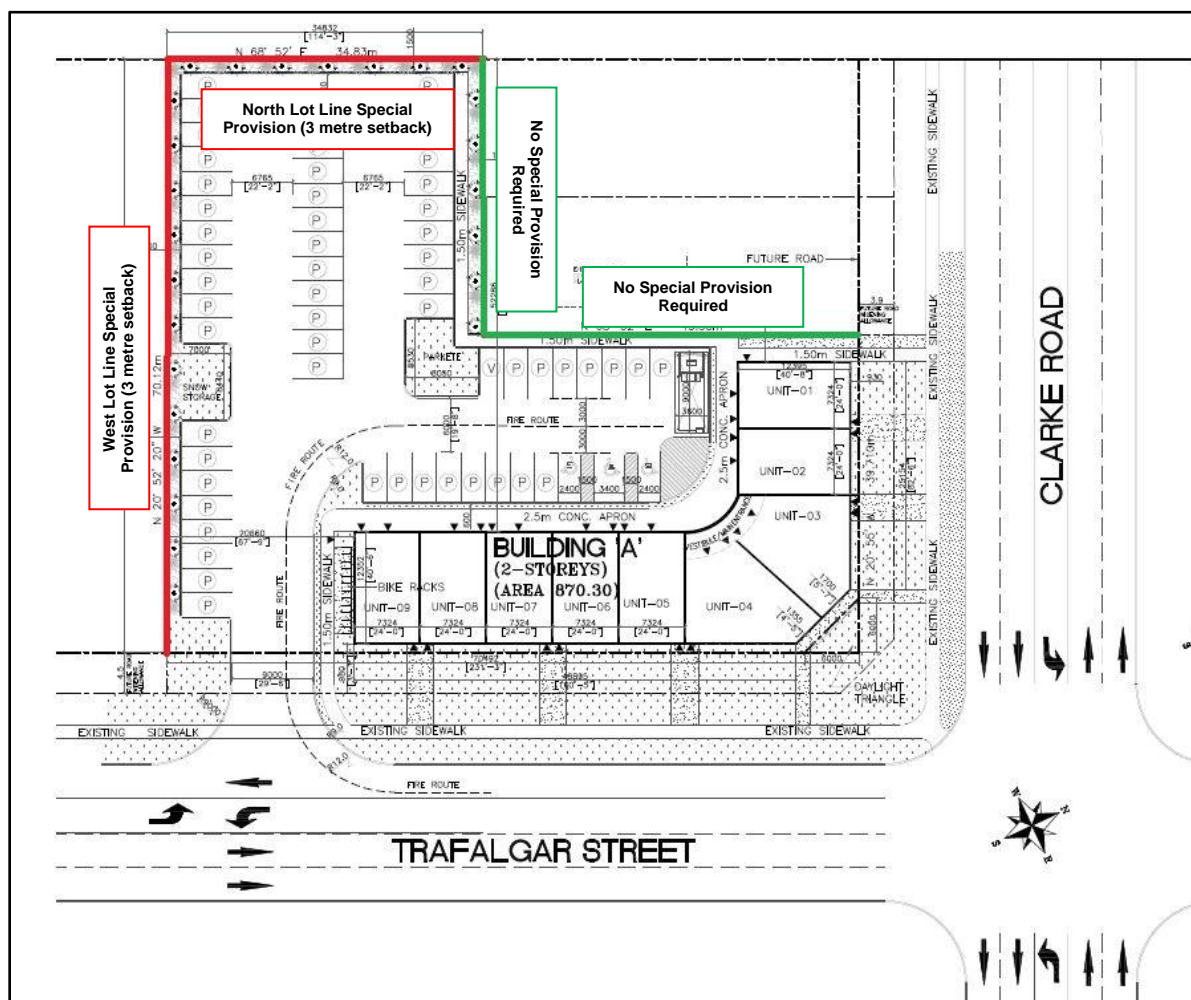


Figure 4: Depiction of the Special Provisions required for the interior side yard parking setbacks.

In terms of lot frontage, the applicant is providing a lot frontage of 39 metres, whereas 40 metres is the minimum required. As the frontage is a reduction of 1 metre, City Staff have no concerns with the special provision for the reduction, as all site amenities can be accommodated for. The minimum front yard setback that staff are requesting is a 1.0 metre setback, to promote a street-oriented development that caters to the commercial component of the building.

Staff are satisfied the proposed form is consistent with the Neighbourhoods Place Type policies and the City Design Policies and that the above noted form issues can be sufficiently addressed through a future Site Plan Application.

## **Conclusion**

The applicant has requested an amendment to the Zoning By-law Z.-1 to rezone the property from a Residential R3 (R3-2) & Convenience Commercial (CC3) Zone to a Neighbourhood Shopping Area (NSA3) Zone. Staff are recommending approval of the requested Zoning Bylaw amendment with special provisions.

The recommended action is consistent with the PPS 2020, conforms to The London Plan and will permit a mixed-use building with 9 commercial units on the ground floor and 9 residential units on the second floor.

**Prepared by:** Brent House, Planner I

**Reviewed by:** Mike Corby, MCIP, RPP  
Manager, Planning Implementation

**Recommended by:** Heather McNeely, MCIP, RPP  
Director, Planning and Development

**Submitted by:** Scott Mathers, MPA, P.Eng.  
Deputy City Manager, Planning and Economic  
Development

Copy: Britt O'Hagan, Manager, Current Development  
Michael Pease, Manager, Site Plans  
Ismail Abushehada, Manager, Development Engineering

## Appendix A – Zoning Bylaw Amendment

Bill No. (number to be inserted by Clerk's Office)  
2023

By-law No. Z.-1-

A by-law to amend By-law No. Z.-1 to rezone an area of land located at 1900 & 1902 Trafalgar Street & 159 Clarke Road

WHEREAS this rezoning conforms to the Official Plan;

THEREFORE the Municipal Council of The Corporation of the City of London enacts as follows:

1. Schedule "A" to By-law No. Z.-1 is amended by changing the zoning applicable to lands located at 1900-1902 Trafalgar Street & 159 Clarke Road, as shown on the attached map comprising part of Key Map No. A108, **FROM** a Residential R3 (R3-2) and a Convenience Commercial (CC3) Zone **TO** a Neighbourhood Shopping Area (NSA3) Zone.
2. Section Number 23.4 of the Neighbourhood Shopping Area (NSA3) Zone is amended by adding the following Special Provisions:

NSA3( ) 1900-1902 Trafalgar Street & 159 Clarke Road

a. Permitted Uses:

- i) An apartment building with any or all of the other permitted uses on the first and/or second floor in the NSA1, NSA2 and NSA5 Zone variations.

b. Regulations

i) Landscaped Open Space (minimum)	25%
ii) Exterior Side Yard Setback (Trafalgar Street) (minimum)	1.0 metres
iii) Front Yard Setback (Clarke Road) (minimum)	1.0 metres
iv) Lot Frontage (minimum)	39 metres
v) West Interior Side Yard Parking Setback (minimum)	3.0 metres
vi) North Interior Side Yard Parking Setback (minimum)	3.0 metres

The inclusion in this By-law of imperial measure along with metric measure is for the purpose of convenience only and the metric measure governs in case of any discrepancy between the two measures.

This By-law shall come into force and be deemed to come into force in accordance with Section 34 of the *Planning Act, R.S.O. 1990, c. P13*, either upon the date of the passage of this by-law or as otherwise provided by the said section.

PASSED in Open Council on July 25, 2023

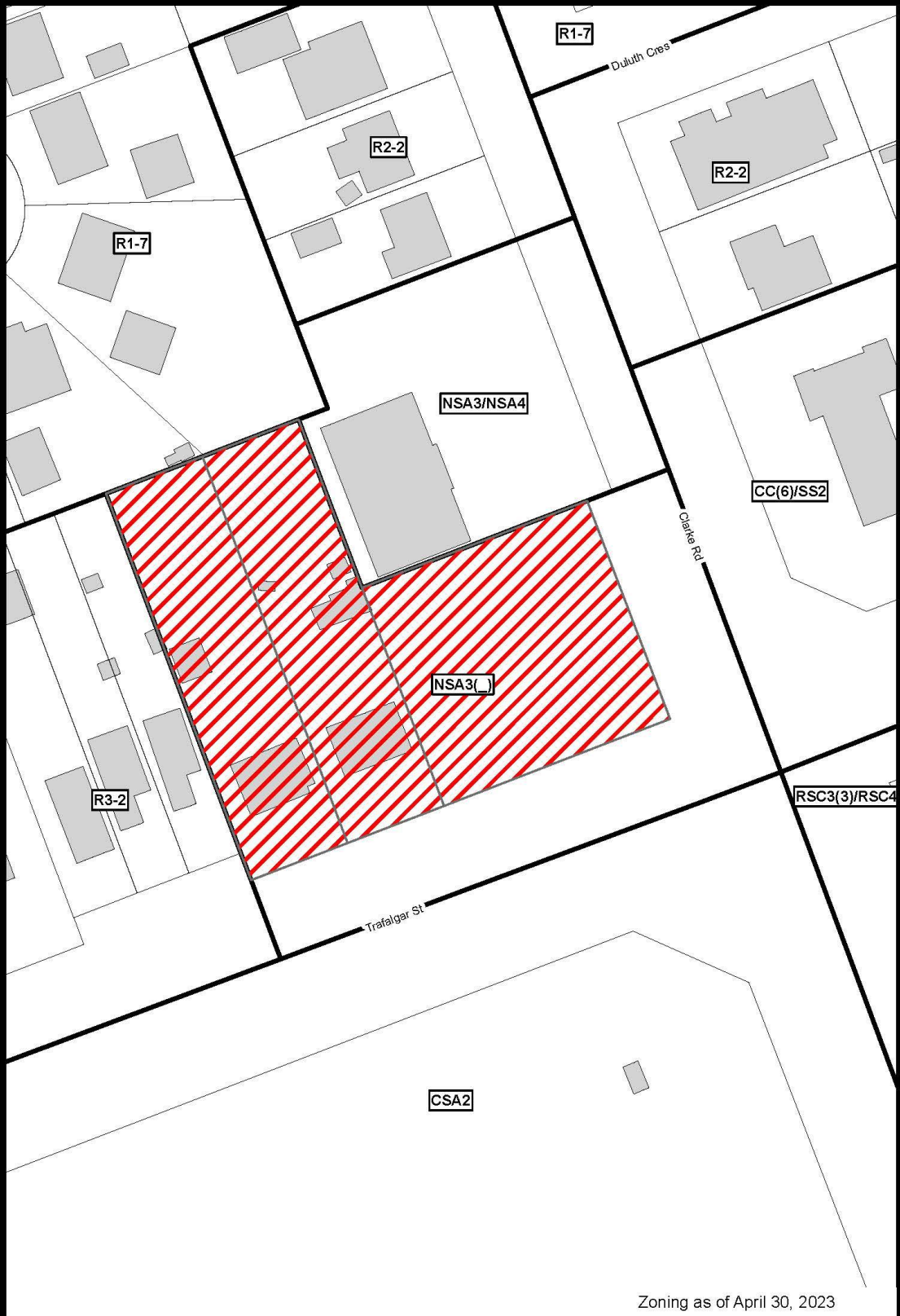
Josh Morgan  
Mayor




Michael Schulthess  
City Clerk

First Reading – July 25, 2023  
Second Reading – July 25, 2023  
Third Reading – July 25, 2023



AMENDMENT TO SCHEDULE "A" (BY-LAW NO. Z.-1)



<p><b>File Number:</b> Z-9604 <b>Planner:</b> BH <b>Date Prepared:</b> 2023/6/6 <b>Technician:</b> JI <b>By-Law No:</b> Z.-1-</p>	<p><b>SUBJECT SITE</b> </p> <p><b>1:800</b></p> <p>0 4 8 16 24 32 Meters </p> <p></p>
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## Appendix B - Site and Development Summary

### A. Site Information and Context

#### Site Statistics

Current Land Use	Single detached dwellings / Vacant lands
Frontage	39 metres (Clarke Road) 41.1 metres (Trafalgar Street)
Depth	65.6 metres (215.2 feet)
Area	0.42 hectares (1.05 acres)
Shape	Regular (rectangle) / irregular
Within Built Area Boundary	Yes
Within Primary Transit Area	Yes

#### Surrounding Land Uses

North	Low-Density Residential
East	Gas Station
South	Community Shopping Area (Metro Grocery Store)
West	Low-Density Residential

#### Proximity to Nearest Amenities

Major Intersection	Trafalgar and Clarke Road
Dedicated cycling infrastructure	Clarke Road, 350 metres
London Transit stop	Trafalgar Street at Clarke Road EB #1791, 41 metres
Public open space	Pottersburg ESA Park, ~730 metres
Commercial area/use	155 Clarke Road - Multiple commercial uses, 32 metres
Food store	Metro Grocery Store, 180 metres
Primary school	Princess Anne French Immersion Public School, ~750 metres
Community/recreation amenity	East Community Centre, ~950 metres

### B. Planning Information and Request

#### Current Planning Information

Current Place Type	Neighbourhoods Place Type fronting two Civic Boulevards
Current Special Policies	N/A
Current Zoning	Residential R3-2 & Convenience Commercial CC3 Zone

#### Requested Designation and Zone

Requested Place Type	Neighbourhoods Place Type fronting two Civic Boulevards
Requested Special Policies	Lot Frontage, North and West Interior Side Yard Setbacks, Exterior Side Yard Setbacks, Front Yard Setbacks, and Landscaped Open Space
Requested Zoning	Special Provision Neighbourhood Shopping Area (NSA3(_))

#### Requested Special Provisions

Regulation (NSA3(_))	Required	Proposed
Lot Frontage (Clarke Road)	40 metres	<b>39 metres</b>

<b>Interior Side Yard Parking Setback (West)</b>	0 metres	<b>3.0 metres</b>
<b>Interior Side Yard Parking Setback (North)</b>	0 metres	<b>3.0 metres</b>
<b>Exterior Side Yard Setback (Trafalgar Street)</b>	0 metres	<b>1.0 metres</b>
<b>Front Yard Setback (Clarke Road)</b>	0 metres	<b>1.0 metres</b>
<b>Landscaped Open Space</b>	<b>15%</b>	<b>25%</b>

## C. Development Proposal Summary

The building is a two-storey apartment/condominium complex containing a total GFA of 1740.6 square metres comprised of first floor commercial space and second floor residential units.

### Proposal Statistics

Land use	Mixed-use Residential and Commercial Development
Form	2 storey mixed-use with commercial on first floor and residential on second floor
Height	2 storeys (8 metres)
Residential units	9
Density	21 units per hectare
Gross floor area	1,740.6 m <sup>2</sup>
Building coverage	20%
Landscape open space	20%
Functional amenity space	50 m <sup>2</sup>
New use being added to the local community	Yes

### Mobility

Parking spaces	12 Residential Spaces and 46 Commercial Spaces
Vehicle parking ratio	1.25 per residential unit 1/15 m <sup>2</sup> per commercial unit
New electric vehicles charging stations	N/A
Secured bike parking spaces	8 Residential spaces 4 commercial spaces
Secured bike parking ratio	0.75 spaces per residential unit 0.44 spaces per commercial unit
Completes gaps in the public sidewalk	Yes
Connection from the site to a public sidewalk	Yes
Connection from the site to a multi-use path	NA

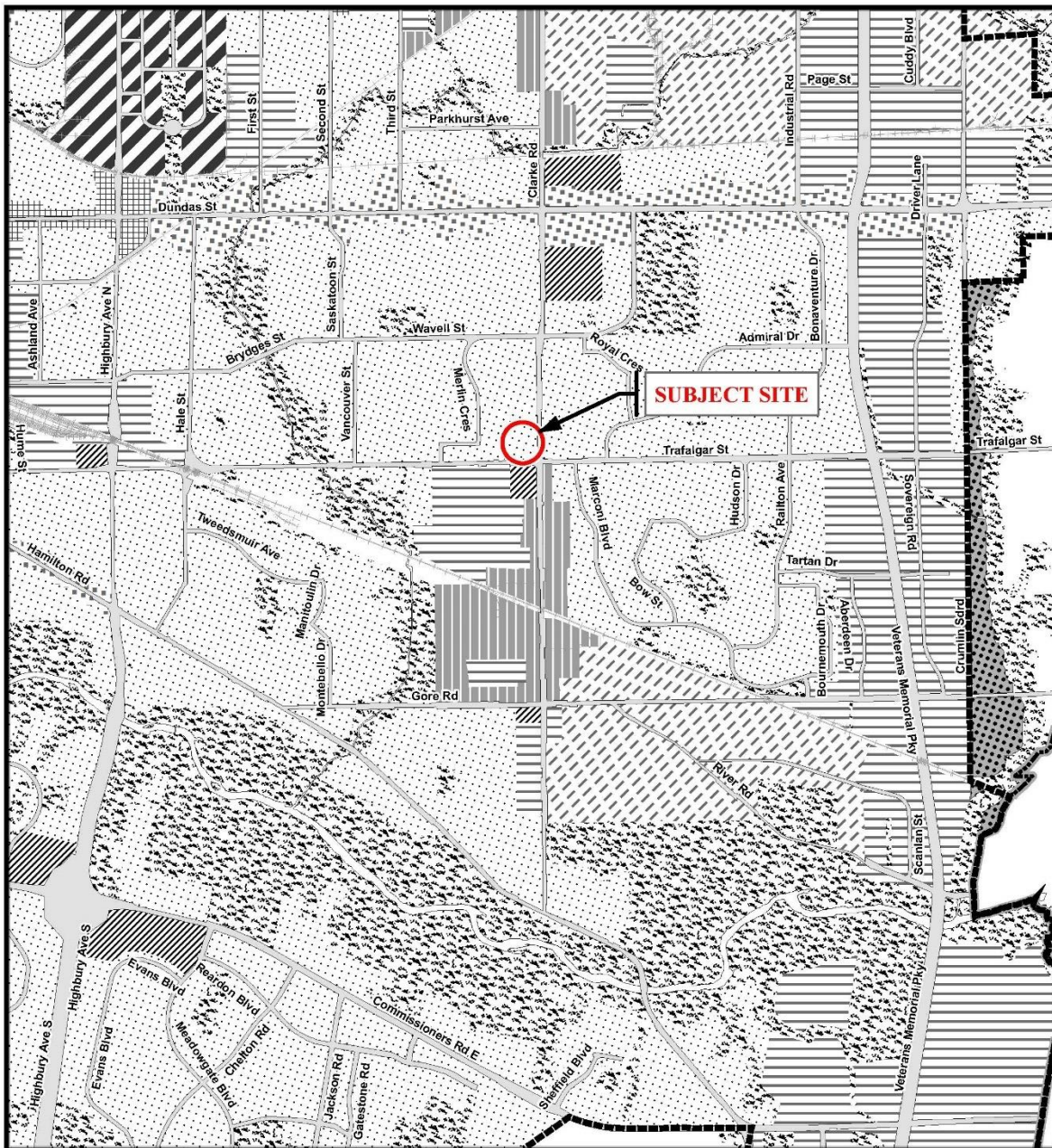
### Environmental Impact

Tree removals	26 trees
Tree plantings	N/A
Tree Protection Area	No
Loss of natural heritage features	N/A
Species at Risk Habitat loss	No
Minimum Environmental Management Guideline buffer met	Yes

Existing structures repurposed or reused	No
Green building features	Unknown



# Appendix C – Additional Plans and Drawings



### Legend

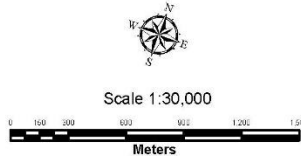
- |                        |                          |   |
|------------------------|--------------------------|---|
| Downtown               | Future Community Growth  | Environmental Review                    |
| Transit Village        | Heavy Industrial         | Farmland                                |
| Shopping Area          | Light Industrial         | Rural Neighbourhood                     |
| Rapid Transit Corridor | Future Industrial Growth | Waste Management Resource Recovery Area |
| Urban Corridor         | Commercial Industrial    | Urban Growth Boundary                   |
| Main Street            | Institutional            |   |
| Neighbourhood          | Green Space              |   |

This is an excerpt from Planning & Development's working consolidation of Map 1 - Place Types of the London Plan, with added notations.

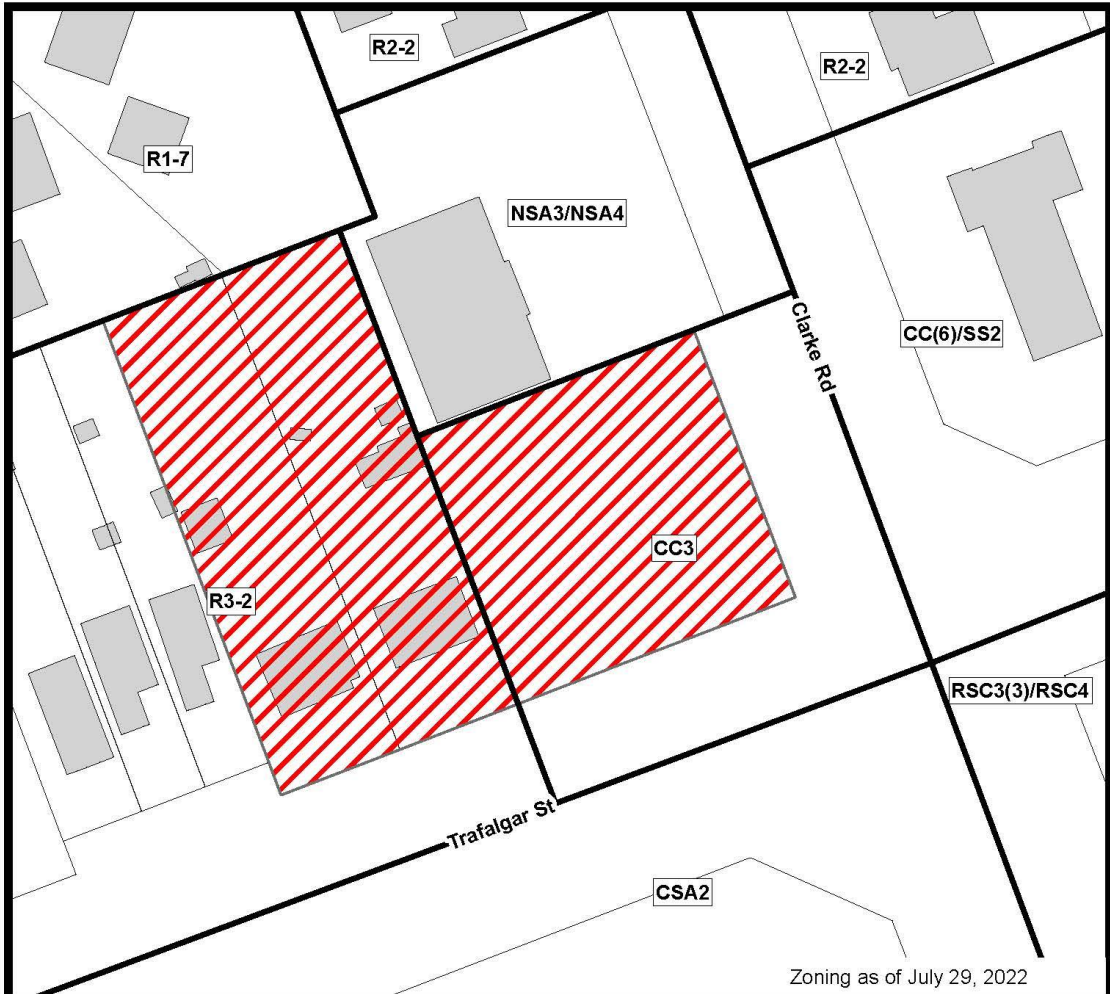
## CITY OF LONDON Official Plan

### LONDON PLAN MAP 1 - PLACE TYPES -

PREPARED BY: Planning & Development



**File Number:** Z-9604  
**Planner:** BH  
**Technician:** JI  
**Date:** 2023/6/6



**COUNCIL APPROVED ZONING FOR THE SUBJECT SITE: R3-2 and CC3**

1) **LEGEND FOR ZONING BY-LAW Z-1**

- R1 - SINGLE DETACHED DWELLINGS
- R2 - SINGLE AND TWO UNIT DWELLINGS
- R3 - SINGLE TO FOUR UNIT DWELLINGS
- R4 - STREET TOWNHOUSE
- R5 - CLUSTER TOWNHOUSE
- R6 - CLUSTER HOUSING ALL FORMS
- R7 - SENIOR'S HOUSING
- R8 - MEDIUM DENSITY/LOW RISE APTS.
- R9 - MEDIUM TO HIGH DENSITY APTS.
- R10 - HIGH DENSITY APARTMENTS
- R11 - LODGING HOUSE

- DA - DOWNTOWN AREA
- RSA - REGIONAL SHOPPING AREA
- CSA - COMMUNITY SHOPPING AREA
- NSA - NEIGHBOURHOOD SHOPPING AREA
- BDC - BUSINESS DISTRICT COMMERCIAL
- AC - ARTERIAL COMMERCIAL
- HS - HIGHWAY SERVICE COMMERCIAL
- RSC - RESTRICTED SERVICE COMMERCIAL
- CC - CONVENIENCE COMMERCIAL
- SS - AUTOMOBILE SERVICE STATION
- ASA - ASSOCIATED SHOPPING AREA COMMERCIAL

- OR - OFFICE/RESIDENTIAL
- OC - OFFICE CONVERSION
- RO - RESTRICTED OFFICE
- OF - OFFICE

- RF - REGIONAL FACILITY
- CF - COMMUNITY FACILITY
- NF - NEIGHBOURHOOD FACILITY
- HER - HERITAGE
- DC - DAY CARE

- OS - OPEN SPACE
- CR - COMMERCIAL RECREATION
- ER - ENVIRONMENTAL REVIEW

- OB - OFFICE BUSINESS PARK
- LI - LIGHT INDUSTRIAL
- GI - GENERAL INDUSTRIAL
- HI - HEAVY INDUSTRIAL
- EX - RESOURCE EXTRACTIVE
- UR - URBAN RESERVE

- AG - AGRICULTURAL
- AGC - AGRICULTURAL COMMERCIAL
- RRC - RURAL SETTLEMENT COMMERCIAL
- TGS - TEMPORARY GARDEN SUITE
- RT - RAIL TRANSPORTATION

- "h" - HOLDING SYMBOL
- "D" - DENSITY SYMBOL
- "H" - HEIGHT SYMBOL
- "B" - BONUS SYMBOL
- "T" - TEMPORARY USE SYMBOL

**CITY OF LONDON**

PLANNING SERVICES / DEVELOPMENT SERVICES

**ZONING  
BY-LAW NO. Z-1  
SCHEDULE A**



THIS MAP IS AN UNOFFICIAL EXTRACT FROM THE ZONING BY-LAW WITH ADDED NOTATIONS

FILE NO:

Z-9604

BH

MAP PREPARED:

2023/06/06

Jl

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Meters



## Appendix D – Internal and Agency Comments

### Site Plan

Please note the following Site Plan Comments for Z-9604:

- The parking rates and bicycle parking rates have been updated, provide 9 long-term (indoor) bicycle parking spaces to meet the minimum, and show on floor plans
- A number of vehicle parking spaces are anticipated to be removed to provide for landscape islands, drive aisles and to facilitate on-site maneuvering, however there are a surplus of parking spaces and a deficiency is not anticipated

At the time of Site Plan:

- Maneuvering for the loading space in and out of the site will need to be shown graphically and further refinement may be required
- Reduce the access width into the site from Trafalgar Street from 9.0m to 6.7m
- No garbage storage is identified on the floor plans and a central garbage storage location will be required, as well as details for collection point and type
- Consider relocating the outdoor amenity area closer to the building to provide a more functional space in a more convenient location
- Privacy perimeter fencing of wood, board on board be anticipated at a height of 1.8m, no fencing will be permitted along the Clarke Road or Trafalgar Street frontages

### Ecology

**Notice of Application (Z-9604) – 1900-1902 Trafalgar Street & 159 Clarke Road**  
Zoning Amendment to allow a two-storey mixed use apartment complex consisting of 9 commercial units on the ground floor and 9 residential units on the second floor.

This e-mail is to confirm that there are currently no ecological planning issues related to this property and/or associated study requirements.

#### Major issues identified

- No Natural Heritage Features on, or adjacent to the site have been identified on Map 5 of the London Plan or based on current aerial photo interpretation.

#### Ecology – complete application requirements

- None.

#### Notes

- None.

### Heritage

- The archaeological requirements for this property and application can be considered addressed.

### Parks

- Parkland dedication is required in the form of cash in lieu, pursuant to By-law CP-25 and will be finalized at the time of site plan approval.

### Urban Design

#### Major Comments (Revised)

The London Plan contemplates mixed use development at the intersection of two Civic Boulevards within the Neighbourhoods Place type. Considering the surrounding neighbourhood, Urban Design is generally supportive of the proposed development.

The Applicant is commended for:

1. Siting the built form close to the intersection establishing a built edge along Trafalgar St and Clarke Rd with primary building entrances, street oriented commercial units and active uses along the frontages.
2. Addressing the corner by incorporating a chamfered facade at the intersection of Trafalgar St and Clarke Rd
3. Providing direct walkways from the commercial units to the city sidewalks on the public streets

The above site and building design features should be carried forward to the site plan application stage.

#### Minor Comments for Zoning

1. Minimum and maximum setbacks from Trafalgar St and Clarke Rd
  - o A minimum of 2.0-3.0m setback from the streets should be provided to avoid encroachment of footings and canopies, and consider the incorporation of patio or forecourt space that spills out into the setback to further activate the space and provide an amenity for tenant businesses
  - o Maximum setbacks should restrict parking between the built forms and the public streets.
2. An adequate landscape buffer shall be provided between the property line and the proposed parking on the north and west, to protect the boundary trees and allow space for additional landscaping. Refer to the London Plan, Policy 224.
3. Include provisions for a minimum ground floor height of 4.5m and a minimum percentage of glazing for commercial frontages to facilitate an active ground floor and a reasonable level of passive surveillance along Trafalgar St and Clarke Rd. Refer to the London Plan, Policy 285, 289\_1, 228
4. Provide a zoning provision for the minimum area of outdoor shared amenity space based on the number of residents anticipated. Refer to the London Plan, Policy 295.

#### Items to be addressed at Site Plan

##### Site Design

1. Limit the amount of surface parking to ensure an adequate amount of amenity area. Refer to The London Plan, Policy 275, 282
2. Maintain the alignment of the 1.5m sidewalk connection from the existing sidewalk on Clarke Rd till the intersection at the parkette and provide a landscape buffer on the north of the sidewalk along the property line
  - o Move the parking to the south so that it does not extend beyond the building façade to avoid being visible from Clarke Rd. Refer to the London Plan, Policy 278
3. Outline the proposed indoor and outdoor amenity spaces (i.e., a common room, balconies, patios, roof terraces, outdoor seating areas etc.) for the development. Refer to the London Plan, Policy 295
4. Clarify the location of garbage storage/pick-up on the site. Ensure it is located in an area that would have minimum negative impact on the users.
  - o Integrate the garbage storage for the apartment within the building
  - o Ensure that the garbage pick-up location is screened with a combination of low masonry walls (max. 0.75m in height) and/or enhanced landscaping (Refer to the London Plan, Policy 266)
5. Include enhanced all-season landscape buffer to screen parking where it is visible from the Trafalgar Street. Refer to the London Plan, Policy 278

##### Building Design

1. Design the building façade fronting Clarke Rd with enhanced detail with store-front design, a similar number of windows as is found on the front elevation, primary entrances and direct walkway connections to Clarke Rd to establish the same relationship with the street as with Trafalgar St. Refer to the London Plan, Policy 290, 289\_1



2. Design the space between the building and the street to have an urban character and an appropriate mix of hardscape and softscape.
  - Differentiate between the residential and commercial entrances. Ensure that the main residential entrance at the rear is distinguishable for wayfinding purposes by providing a prominent canopy and lockable double doors that are universally accessible and provides security for the residential units on the above floor. Refer to the London Plan, Policy, 193\_5, 700
  - Improve pedestrian access by providing weather protection (canopies, overhangs) on all entrances, particularly along the rear side of the building for safety and comfort of the users
  - Ensure that location of recesses, projections, windows and doors are accurately shown on the floor plans match with the site plan and the elevation
3. Submit a full set of dimensioned elevations for all sides of the proposed buildings. Further urban design comments may follow upon receipt of the elevations.

Please note that Urban Design Peer Review Panel (UDPRP) consultation is not required for this application.

## Appendix E – Public Engagement

**Public Responses:** Two (2) written responses were received.

### **Public Comment #1**

There is a drain in the back of 1902 Trafalgar Street which was put in when the strip plaza on Clarke Road (just north of the proposed units) was constructed. This was as a result of a lawsuit initiated because the increased elevation of the strip plaza created flooding in both the properties at 1902 Trafalgar Street and 1841 Whitehall Drive. I believe the flooding also affected 1839 Whitehall Drive.

I see nothing on the submitted application which deals with the drainage. Is it possible to get a better copy of the Site Concept Plan that would show the elevation of the proposed parking area which would back onto my property, as well as any proposed drainage?

### **Public Comment #2**

Just got notice in mail for 1900-1902 Trafalgar when is this happening would like to vote against it , have 2 kids and would not like the noise this is going to cause around here.