

Agenda Including Addeds

London Advisory Committee on Heritage

The 2nd Meeting of the London Advisory Committee on Heritage

January 8, 2020, 5:30 PM

Committee Rooms #1 and #2

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Pages

- 1. Call to Order**
 - 1.1 Disclosures of Pecuniary Interest

- 2. Scheduled Items**
 - 2.1 5:30 PM M. Stone, Accessibility Specialist – Accessibility for Ontarians with Disabilities Act Training

 - 2.2 *5:50 PM M. Greguol, Heritage Planner – Demolition Request for Heritage Listed Property at 247 Halls Mill Road by J. McLeod* 2

- 3. Consent**
 - 3.1 1st Report of the London Advisory Committee on Heritage 43

 - 3.2 Letter of Resignation - J. Monk 47

- 4. Sub-Committees and Working Groups**

- 5. Items for Discussion**
 - 5.1 Proposed Amendments to the Ontario Heritage Act - B. Wells 48

 - 5.2 Notice of Planning Application - Official Plan and Zoning By-law Amendments - 435-451 Ridout Street North 49

 - 5.3 Notice of Planning Application - Zoning By-law Amendment - 725-735 Dundas Street, 389-393 Hewitt Street, a Portion of 700 King Street and Other Properties 221

 - 5.4 LACH 2020 Work Plan 321

 - 5.5 Heritage Planners' Report

(Note: A copy of the Heritage Planners' Report will be available at the meeting.)

- 6. Adjournment**

Next Meeting Date: February 12, 2020

Report to London Advisory Committee on Heritage

To: Chair and Members
London Advisory Committee on Heritage
From: John M. Fleming
Managing Director, City Planning and City Planner
Subject: Demolition Request for Heritage Listed Property at 247 Halls
Mill Road by John McLeod
Meeting on: January 8, 2020

Recommendation

That, on the recommendation of the Managing Director, Planning & City Planning, with the advice of the Heritage Planner, with respect to the demolition request for the accessory building on the heritage listed property at 247 Halls Mill Road, that:

- a) Notice **BE GIVEN** under the provisions of Section 29(3) of the *Ontario Heritage Act*, R.S.O. 1990, C.O. 18, of Municipal Council's intention to designate the property at 247 Halls Mill Road to be cultural heritage value or interest for the reasons outlined in Appendix E of this report

Executive Summary

A demolition request was received for the accessory building ("the Red Barn") on the heritage listed property at 247 Halls Mill Road. The subject property is listed on the City of London's Register of Cultural Heritage Resources. When a demolition request is received for a building or structure on a heritage listed property, a formal review process is triggered pursuant to the requirements of the *Ontario Heritage Act* and the Council Policy Manual.

As a part of the review, staff evaluated the property including the accessory building and dwelling using the criteria of *Ontario Regulation 9/06, Criteria for Determining Cultural Heritage Value or Interest*. Therefore, the property is a significant cultural heritage resource. The evaluation found that the property met the criteria for designation under the *Ontario Heritage Act*. The dwelling and the accessory building on the property have been identified as heritage attributes and described in the Statement of Cultural Heritage Value prepared for the property (Appendix F).

The property should be designated under Part IV of the *Ontario Heritage Act* to protect the heritage attributes of the property, including the dwelling and "the Red Barn", and to recognize the significant cultural heritage value of the property.

Analysis

1.0 Background

1.1 Property Location

The property at 247 Halls Mill Road is located on the west side of Halls Mill Road, north of Commissioners Road West (Appendix A). The property is located in the former Westminster Township, and the village of Byron, annexed by the City of London in 1961.

1.2 Cultural Heritage Status

The property at 247 Halls Mill Road is a heritage listed property. The property is considered to be of potential cultural heritage value. The listing of the property on the Register of Cultural Heritage Resources came into force and effect on March 26, 2007.

1.3 Description

The buildings on the property (a dwelling and accessory building) are situated on an elevated portion of the property that rises above the existing grade of the road. Steps have been built into the sloping earthen embankment to provide access to the front of the dwelling. Similarly, a curved and sloping gravel driveway provides vehicular access to the property, leading to the accessory building.

The subject property is approximately one acre in size.

The Queen Anne Revival dwelling located at 247 Halls Mill Road is a one-and-a-half storey, hipped roof, with cross gables, buff brick side hall plan cottage (Appendix B). The front entrance is located on the east elevation of the dwelling and consists of a panelled wood door and stained glass transom. Side entrances are located on the north and south elevations. The east elevation includes a projecting front gable which includes a concentration of decorative wood details including carved wood brackets, scalloped shingle imbrication, and decorative detailing within the bargeboard and gable. Comparable applied details are continued on the gable located on the south façade of the dwelling, as well as the north gable and a gable dormer that extends out from the roof line on the north façade of the dwelling. Buff brick quoins are located on the north and south sides of the dwelling, along the west side of the dwelling. The quoins however, are not continued on the east side of the dwelling. The dwelling originally had Queen Anne style wood windows that were located in the front and side gables, however, they have since been replaced with vinyl windows that mimic the light pattern of the former windows. The dwelling previously had a slate roof that was removed between 1999 and 2019.

The dwelling includes two porches on the north and south facades. The north porch includes a shed style roof that is supported by decorative turned posts. The top of the posts include carved wood brackets, and decorative spandrels extend along the entire porch. The porch skirt includes a wood lattice design. Comparatively, the south porch is larger and includes a shed-style roof with a gable built out above the entrance. The south porch includes seven decorative turned posts, and two engaged posts that directly abut the south wall of the dwelling. Carved brackets and wood spandrels are also present on the south porch. The design of the porch skirt on the south porch consists of wooden boards with a pattern of circular holes.

Additions have been made to the rear (west) façade of the house including a single storey addition, and a garage. The majority of the additions have been clad with a buff brick material that is similar in colour to the buff brick of the dwelling.

The accessory building on the property consists of a two storey timber frame structure that has been used for various functions. The building is colloquially identified today as a “barn” structure, however, various published accounts of the property refer to the structure’s historic function as a “coach house” as well as a “warehouse” for the Griffith Bros. woollen mill which was active in the Hall’s Mill area from the 1860s to the 1890s.

The accessory building was constructed utilizing a timber frame construction method with the use of 10” x 10” posts, connected through the use of mortise and tenon joinery, a traditional timber framing technique. The exterior board-and-batten cladding consists of chestnut and is painted red. The composition and detailing of the structure is highlighted and summarized in Nancy Tausky’s *Historical Sketches of London From Site to City* (1993).

Tausky writes:

The elaborate treatment of the barn’s centre section makes it the focal point of the long front: the round-headed window is recessed within a projecting gable. Below, a further projection containing the main doors (originally solid) is covered with a hipped roof that echoes the dimensions of the gable above. The ventilator at the ridge of the barn completes the central complex. Some concern for symmetry in the rest of the façade is indicated by the two ground floor windows, which are equidistant from the main door. But other openings, possibly later

alterations, have obscured any formal balance that may once have existed. Griffith's interest in style and workmanship is indicated by the fact that, when he made a Queen Anne house out of his 1840s cottage, he imported Georgia pine from the southern United States to use for the interior trim (Tausky 1993, 80).

1.4 Property History

The subject property at 247 Halls Mill Road is located on what was historically known as Lot 45, Concession B in the Broken Front in Westminster Township. The original Crown grant for the lot (approximately 120 acres) was given to Archibald McMillan in 1819. By 1827, McMillan began selling off portions of the property and sold 5 ¼ acres to Anson Simons and John Preffer who built a carding and fulling mill in the northeast corner of the lot. The milling operations that Simons and Preffer initiated was the beginning of an extensive 19th and early-20th century milling history in the Halls Mills area. (Land Registry Records, Burnell, 14).

In 1831, Simons and Preffer sold their milling operation to Burleigh Hunt, who went into partnership with Cyrenius Hall in 1835. One year later, Hall purchased the entirety of the business from Hunt and began expanding his milling operations.

Cyrenius Hall is perhaps the most well-known milling figure associated with the early history of Byron as the namesake of Hall's Mills. He emigrated to Upper Canada around 1810 from New Hampshire, and worked primarily as a merchant in Fort Erie. In the 1830s he moved to Westminster Township where he partnered with Hunt before acquiring the milling operations entirely and a sizeable portion of the lands in the area. Shortly after acquiring the mills in the area, he added a distillery and tannery to the milling complex. Hall and his family members operated most of the milling operations in the area, and under him the area prospered (Burnell, 14-15; Grainger 2002, 288-290). Cyrenius Hall also began selling portions of the lot into the mid-19th century, including various portions that were sold to Lawrence Lawrason. A merchant, MLA, and land-speculator, Lawrason held a number of prominent positions in early London, including an appointment as London's first deputy postmaster. He partnered with George Jarvis Goodhue in opening a general store, dry goods business, real estate office, and post office in London (Brock, 2003; Armstrong, 1986, 74-107). Like many of London's early merchants, Lawrason became heavily involved in land speculation in the area, explaining his acquisition of property in Hall's Mills (Brock, 2003).

The Halls Mills area was captured in 1846, in *Smith's Gazetteer*. The area was described as having 200 inhabitants as well as a grist mill, a distillery, a carding machine and cloth factory, a tannery, a tavern, a store, a fanning-mill maker, a blacksmith, a wagon maker, a shoemaker, and a tailor (Smith 1846, 218).

The Griffith brothers, for whom the subject property is most notably associated, first began acquiring property in the area in 1861. John Griffith, the eldest of three brothers who partnered in business in the area acquired three acres from Lawrence Lawrason. John, along with his brothers William and Eli, continued to acquire property in the area throughout the 19th century, and by the mid-1860s began a woollen mill operation in Hall's Mills (Tausky, 1993, 80; Armstrong 1986, 74-107; Grainger, 2002, 290-294).

The three Griffith brothers were the sons of Eli Griffith and Alexandria McAdam, early settlers in Westminster Township, in the Byron area. Eli (the father) emigrated from Vermont to Westminster Township along with several other Griffith family members who are considered as some of the earliest Euro-Canadian settlers of the Byron area. Eli and Alexandria, and their nine children settled on Lot 35, Concession B, Westminster. John, William, and Eli, three of their seven sons went into business together operating a woollen mill in Hall's Mill, and a second operation in present-day Springbank Park (Westminster Historical Society, 238-239). William Griffith eventually became the owner of the lands that included the property at 247 Halls Mill Road in 1867. The woollen mill that he operated with his brothers was located across Halls Mills Road (formerly Centre Street) on the City-owned lot that is now known as Halls Mills Park (Land Registry Records).

Census records from the 1870s to the 1890s refer to the three brothers as “Clothiers” and “Woollen Manufacturers”, separate from their immediate family who continued to be identified as farmers. Prior to the 1870s, the brothers are noted as living within the residence of their parents, Eli and Alexandria on Lot 35.

The Griffith Bros. woollen mill prospered. By 1868, after a few years in operation, the City and County Directory noted two woollen mills in the Byron area. In the description of Byron, it was further noted that “Griffith Bros. factory is of frame, two stories, 64 x 45 feet, in which water power is used, and about 12 hands are employed in the manufacture of tweeds, fulled clothes and flannels. Their machinery is considered superior to any other in the Province” (City and County Directory, 1868). The 1871-72 Directory includes an advertisement for the “Byron Woollen Mills, Griffith Bros., Proprietors, Manufacturers of tweeds, full clothes, plain and fancy flannels of all kinds” (City and County Directory, 1871-72) (Appendix C).

As noted by Tausky in *From Site to City*, it is unclear on when the accessory building on the subject property was constructed, however, the structure has been identified in various sources as being used as a coach house, a barn, as well as a warehouse/storage facility for the Griffith Bros. woollen mill. The mill was in operation by the Griffiths between the 1860s and 1890s, so it is believed that the structure was constructed within this timeframe.

The Griffiths Bros. mill was closed by the 1890s, and by 1897 an agreement was made between William Griffith and the City of London for the City to purchase the mill property under the *London Water Works Act, 1873*. The purchase of the property would not take place for another ten years, however, the agreement stated that the City was at liberty to lay a 1” pipe from the stream or pond on the property for the purposes of drawing water, and that William Griffith be permitted to draw water for domestic purpose for his cottage, situated northwesterly across the road, at what is now 247 Halls Mill Road. The agreement also noted that the City was at liberty to remove the mill building and all machinery on the premises at any time. In 1900, the mill was disassembled and sold for lumber (Kerr, 1983).

William Griffith owned the property at 247 Halls Mill Road until he passed away in 1926. The Hall’s Mills area has developed north and south of the subject property over the last century, but the Queen Anne Revival cottage and the accessory building have remained in situ and continue to be associated with the early milling history of Hall’s Mills and Byron.

The Hall’s Mills area continues to be associated with the history of Westminster Township and the village of Byron. In *Heritage Places 2.0*, the area is noted as being generally characterized by the collection of early to mid-19th century properties along Halls Mill Road and Commissioners Road West. The property at 247 Halls Mill Road is prominently noted in this Guideline Document including photographs of both the accessory building and dwelling, as well as a reference to the property’s contribution to the concentration of cultural heritage resources in the area that are listed on the City of London’s *Register of Cultural Heritage Resources (Heritage Places 2.0, 2019, 41.)* The Hall’s Mills area is identified within *Heritage Places 2.0* as being a future potential heritage conservation district, worthy of study.

2.0 Legislative and Policy Framework

2.1 Provincial Policy Statement

Section 2.6.1 of the *Provincial Policy Statement (2014)* directs that “significant built heritage resources and significant cultural heritage landscapes shall be conserved.” “Significant” is defined in the *Provincial Policy Statement (2014)* as, in regards to cultural heritage and archaeology, “resources that have been determined to have cultural heritage value or interest for the important contribution they make to our understanding of the history of a place, and event, or a people.”

2.2 Ontario Heritage Act

Section 27 of the *Ontario Heritage Act* requires that a register kept by the clerk shall list all properties that have been designated under the *Ontario Heritage Act*. Section 27(1.2) of the *Ontario Heritage Act* also enables Municipal Council to add properties that have not been designated, but that Municipal Council “believes to be of cultural heritage value or interest” on the Register.

The only cultural heritage protection afforded to heritage listed properties is a 60-day delay in the issuance of a demolition permit. During this time, Council Policy directs that the London Advisory Committee on Heritage (LACH) is consulted, and a public participation meeting is held at the Planning & Environment Committee.

Section 29 of the *Ontario Heritage Act* enables municipalities to designate properties to be of cultural heritage value or interest. Section 29 of the *Ontario Heritage Act* also establishes consultation, notification, and process requirements, as well as a process to appeal the designation of a property. Appeals to the Notice of Intent to Designate a property pursuant to Section 29 of the *Ontario Heritage Act* are referred to the Conservation Review Board (CRB), however the final decision rests with Municipal Council until changes to the *Ontario Heritage Act* arising from Bill 108 come into force and effect.

2.3 The London Plan

The Cultural Heritage chapter of *The London Plan* recognizes that our cultural heritage resources define our City’s unique identity and contribute to its continuing prosperity. It notes, “The quality and diversity of these resources are important in distinguishing London from other cities and make London a place that is more attractive for people to visit, live or invest in.” Policies 572_ and 573_ of *The London Plan* enable the designation of individual properties under Part IV of the *Ontario Heritage Act*, as well as the criteria by which individual properties will be evaluated.

Policies 575_ and 576_ of *The London Plan* also enable City Council to designate areas of the City under Part V of the *Ontario Heritage Act* as Heritage Conservation Districts. These policies include a set of criteria in the evaluation of an area. *Heritage Places 2.0* is a guideline document as a part of the Cultural Heritage Guidelines. The document describes potential heritage conservation districts and assigns a priority to these districts for consideration as heritage conservation districts.

2.5 Register of Cultural Heritage Resources

Municipal Council may include properties on the Register of Cultural Heritage Resources that it “believes to be of cultural heritage value or interest.” These properties are not designated, but are considered to have potential cultural heritage value or interest.

The Register of Cultural Heritage Resources states that further research is required to determine the cultural heritage value or interest of heritage listed properties. The subject property is included on the Register of Cultural Heritage Resources.

3.0 Request for Designation

A request for the designation of the property at 247 Halls Mill Road under Part IV of the *Ontario Heritage Act* was received by members of the community in November 2019. At its meeting on November 13, 2019, the LACH referred the Stewardship Sub-Committee for the evaluation of the property using O.Reg. 9/06.

4.0 Demolition Request

In 2008, the accessory building on the property at 247 Halls Mill Road was the subject of a demolition request. At the time, Municipal Council’s approved the demolition request. In 2009, the Chief Building Official revoked the demolition permit due to non-action. The Solicitor for the City of London had confirmed that a new demolition request

for the property would require the demolition process for a heritage listed property to re-start.

In September 2019 the Heritage Planner received complaints from community members about the demolition of the accessory building. A Municipal Law Enforcement Officer investigated, and advised the property owner that a demolition permit would be required for the accessory building on the property. At the time, the property owner indicates that portions of the roof sheathing had blown off, and that he had no intentions of removing the building at that time.

In November 2019, the Heritage Planner received complaints from community members that the property owner was continuing to demolish the structure without the necessary permits. In addition, in November 2019, a request from the community members to designate the property pursuant to the *Ontario Heritage Act* was received (See Section 3.0).

On November 28, 2019, the property owner submitted a Required Clearances for Demolition Permit form to the City Planning office. The Heritage Planner followed up on the same day, informing the property owner of the review process for heritage listed properties. The property owner was informed that written intention to demolish a structure must be received in order to initiate the 60-day review period.

On December 3, 2019, the Heritage Planner visited the property at 247 Halls Mill Road with the property in to view the property and buildings. The property owner provided access to the interior of the accessory building. On December 6, 2019, the Heritage Planner followed up on the property site visit, reminding the property owner of the demolition process for heritage listed properties, and that written intent to demolish a structure or building on a heritage listed property must be received to initiate the process and review period.

On the morning of December 11, 2019, the Heritage Planner was contacted by community members advising that at approximately 7:45pm on December 10, 2019, the roof of the accessory building had collapsed. The community members also indicated that the property owner had continued to remove exterior boards from the structure during the day on December 10, 2019.

The property owner was issued as “Unsafe Building – Order to Make Safe” pursuant to the *Building Code Act* December 13, 2019. Written notice of intention to demolish the accessory building on the property located at 247 Halls Mill Road was submitted by the property owner on December 13, 2019.

Municipal Council must respond to a notice of intention to demolish a heritage listed property within 60 days, or the request is deemed consented. During this 60-day period, the London Advisory Committee on Heritage (LACH) is consulted and, pursuant to Council Policy, a public participation meeting is held at the Planning & Environment Committee (PEC).

The 60-day period for the demolition request for the accessory building on the property at 247 Halls Mill Road expires on February 11, 2020.

5.0 Cultural Heritage Evaluation

5.1 Criteria for Determining Cultural Heritage Value or Interest

The criteria of *Ontario Heritage Act* Regulation 9/06 establishes criteria for determining the cultural heritage value or interest of individual properties. These criteria are:

1. Physical or design value:
 - i. Is a rare, unique, representative or early example of a style, type, expression, material or construction method;
 - ii. Displays a high degree of craftsmanship or artistic merit; or,
 - iii. Demonstrates a high degree of technical or scientific achievement.
2. Historical or associative value:
 - i. Has direct associations with a theme, event, belief, person, activity,

- organization or institution that is significant to a community;
 - ii. Yields, or has the potential to yield, information that contributes to an understanding of a community or culture; or,
 - iii. Demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community.
3. Contextual value:
- i. Is important in defining, maintaining or supporting the character of an area;
 - ii. Is physically, functionally, visually or historically linked to its surroundings; or,
 - iii. Is a landmark.

A property is required to meet one or more of the abovementioned criteria to merit protection under Section 29 of the *Ontario Heritage Act*. Should the property not meet the criteria for designation, the demolition request should be granted and the property removed from the *Inventory of Heritage Resources* (Register).

The evaluation of the property using the criteria of *Ontario Heritage Act* Regulation 9/06 can be found below.

5.2 Evaluation

The property at 247 Halls Mill Road was evaluated using the criteria of O.Reg. 9/06 (see Section 5.1, above). A summary of the evaluation is included below.

Cultural Heritage Value	Criteria	Evaluation
The property has design value or physical value because it,	Is a rare, unique, representative or early type, expression, material, or construction method	<p>The property at 247 Halls Mill Road includes a representative example of a Queen Anne Revival side hall plan cottage. The cottage is believed to have been constructed in the 1840s and was altered in the 1890s century when a number of its decorative elements were added, making it representative of Queen Anne Revival style architecture.</p> <p>The accessory building on the property is a unique example of a timber frame accessory structure that has been used for various purposes over the course of its existence. Known locally at “the Red Barn”, the structure has been reportedly used as a barn, coach house and warehouse for the Griffith Bros. woollen mills. The structure has been designed to include a series of stylistic embellishments that elevates the appearance of the structure beyond typical barn construction. Its siding, projecting gable, window treatments, and central ventilator all contribute to it being a unique example of a timber frame accessory building. The accessory building can be considered rare within its context in Halls Mills, as well as within London.</p>
	Displays a high degree of craftsmanship or artistic merit	The concentration of decorative wood detailing on the cottage’s gable and bargeboard elements as well as its decorative verandah posts contribute to the expression of its style as a Queen Anne Revival cottage. As a result, the property displays a high degree of craftsmanship.
	Demonstrates a high degree of technical or scientific achievement	The property was reportedly used for storage for the Griffith Bros. woollen mill, located across the road. Although the property is associated with early milling activities in the area, the dwelling and

		accessory building do not demonstrate a high degree of technical or scientific achievement.
The property has historical value or associative value because it,	Has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community	The property is directly associated with William Griffith, one of three Griffith brothers who owned and operated the Griffith Bros. mill. The Griffith Bros. woollen mill was located directly across the road from the property at 247 Halls Mill Road, on the property now known as Halls Mills Park. The Griffith Bros. mill operated between the 1860s and 1890s. The cottage on the property at 247 Halls Mill Road was the home of William Griffith and the accessory building functioned as a coach house and storage warehouse for the woollen products produced by the Griffith Brothers.
	Yields, or has the potential to yield information that contributes to an understanding of a community or culture	The property at 247 Halls Mill Road has the potential to yield information related to the history of the Halls Mills area. As a property historically associated with the Griffiths Bros. and 19 th century milling in Byron, the property has potential to yield information that contributes to the understanding of the history of industry, development and growth of the Halls Mills area and early Byron.
	Demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community	The property at 247 Halls Mill Road is not known to demonstrate or reflect the work or ideas of an architect, artist, builder, designer, or theorist who is significant to a community.
The property has contextual value because it,	Is important in defining, maintaining, or supporting the character of an area	<p>The property at 247 Halls Mill Road is important in defining the character of the Halls Mills area. The Hall's Mills area is characterized by a geographical context near the Thames River and its topography, along with the collection of early and mid-19th century buildings located along Halls Mills Road and Commissioners Road West.</p> <p>As the property includes an 1840s dwelling and 19th century accessory building, the property is a part of the concentration of cultural heritage resources in the Halls Mill area that contribute to its character and have led to its identification as a potential heritage conservation district.</p>
	Is physically, functionally, visually, or historically linked to its surroundings	The property is historically linked to the property now known as Halls Mill Park, on the east side of Halls Mill Road. The Halls Mill Park property was the site of the Griffith Bros. woollen mill which was operated by William Griffith, owner of 247 Halls Mill Road, in partnership with his brothers. As the milling site for their Byron operation, the properties are historically linked.
	Is a landmark	The property at 247 Halls Mill Road is considered to be a local landmark within the Halls Mill area. Specifically, "the Red Barn" is known locally amongst community members as a landmark in the area.

5.3 Comparative Analysis

Comparative analyses were undertaken from the perspective of cultural heritage resources within London with other one-and-a-half storey, buff brick, side hall plan cottages with Queen Anne Revival style influences (Appendix D).

The comparative analysis supported the identification of the dwelling at 247 Halls Mill Road as a representative a Queen Anne Revival style, side hall plan cottage.

When compared to other accessory buildings, the identification of accessory building at 247 Halls Mill Road is supported as a unique and rare example of a timber frame accessory building.

5.4 Integrity

Integrity is not a measure of originality, but a measure of whether the surviving physical features (heritage attributes) continue to represent or support the cultural heritage value or interest of the property. Likewise, the physical condition of a cultural heritage resource is not a measure of its cultural heritage value. Cultural heritage resources can be found in a deteriorated state, but may still maintain all or part of their cultural heritage value or interest (MTC, 2006).

The dwelling at 247 Halls Mill Road has undergone some alterations, however the majority of the alterations include additions at the rear of the dwelling, and the majority of the surviving physical features, or heritage attributes, have been retained and continue to support the cultural heritage value of the property. This includes the buff brick exterior, the decorative woodwork in the gables, the decorative turned posts of the verandahs, and the surviving stained glass windows on the dwelling.

Although the accessory building on the property at 247 Halls Mill Road has recently deteriorated which has resulted in a change in its physical condition, the building still retains its cultural heritage value. A careful restoration of the structure would retain the structure's cultural heritage value as a unique example of a timber frame accessory building. Sufficient documents exists to direct the appropriate restoration of the accessory building.

5.5 Consultation

Pursuant to the Council Policy for the demolition of heritage listed properties, notification of the demolition request was sent to property owners within 120m of the subject property on December 20, 2019, as well as community groups including the Architectural Conservation Ontario – London Region, London & Middlesex Historical Society, and the Urban League of London. Notice was also published in *The Londoner*.

6.0 Conclusion

The evaluation of the property at 247 Halls Mill Road found that the property met the criteria for designation under the *Ontario Heritage Act* (See Statement of Cultural Heritage Value or Interest in Appendix E). Despite changes in conditions to the accessory building on the property, the property's cultural heritage resources including the dwelling and the accessory building continue to demonstrate the cultural heritage value of the property. The property should be designated under Part IV of the *Ontario Heritage Act* to preserve the loss of this significant cultural heritage resource.

Prepared by:	Michael Greguol Heritage Planner
Submitted by:	Gregg Barrett, AICP Manager, Long Range Planning and Research
Recommended by:	John M. Fleming, MCIP, RPP Managing Director, City Planning and City Planner

MG/

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Appendix A Property Location

Appendix B Images

Appendix C Historical Documentation and Research Materials

Appendix D Comparative Analysis

Appendix E Statement of Cultural Heritage Value or Interest – 247 Halls Mill Road

Sources

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Appendix A – Property Location

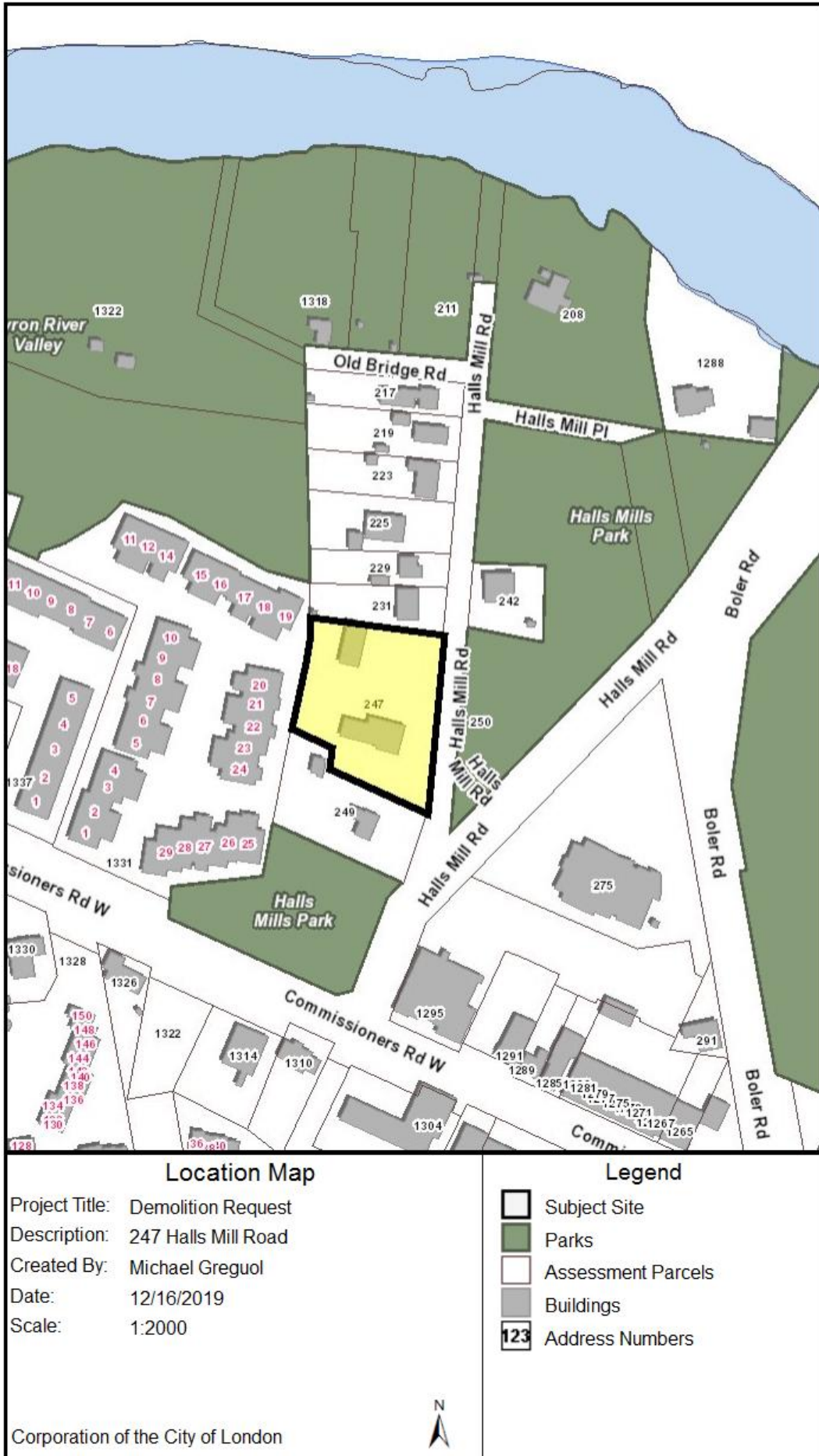


Figure 1: Location of the subject property at 247 Halls Mill Road. The accessory building is located along the north boundary line of the property.

Appendix B – Images



Image 1: Photograph of the north façade of the dwelling at 247 Halls Mill Road, 1999.



Image 2: Photograph of the south façade of the dwelling at 247 Halls Mill Road, 1999.



Image 3: Photograph of the north and west facades of the dwelling at 247 Halls Mill Road, 1999.



Image 4: Photograph of the east façade of the dwelling at 247 Halls Mill Road, 1999.



Image 5: Photograph of the east side of the accessory building, 1999.



Image 6: Photograph of the west side of the accessory building, 1999.



Image 7: Photograph of the west side of the accessory building, 1999.



Image 8: Interior photograph of the accessory building, showing the interior of the west wall of the building, 1999.



Image 9: Photograph of the south side of the accessory building, showing board and batten exterior cladding and window details, 1999.



Image 10: Photograph showing the accessory building in September 2019



Image 11: Photograph of the accessory building as viewed from Halls Mill Road, September 2019.



Image 12: Photograph of the east side of the accessory building, September 2019.



Image 13: Photograph showing the east façade of the dwelling at 247 Halls Mill Road, December 2019



Image 14: Photograph showing the north façade of the dwelling in December 2019. Note, additions have been constructed onto the rear (west) side of the dwelling.



Image 15: Detail showing decorative woodwork in gable on the east elevation of the dwelling, December 2019.



Image 16: Detail showing window/doorway alterations on the east elevation to 247 Halls Mill Road, December 2019.



Image 17: Photograph showing the south elevation of the dwelling at 247 Halls Mill Road, showing decorative elements and south porch



Image 18: Photograph showing the south elevation of the dwelling at 247 Halls Mill Road, showing rear addition at left, December 2019.



Image 19: Photograph showing the accessory building as viewed from Halls Mill Road, December 3, 2019.



Image 20: Photograph showing the accessory building as viewed from Halls Mill Road, December 3, 2019.



Image 21: Photograph of the east side of the accessory building, December 3, 2019.



Image 22: Photograph of the south side of the accessory building. Note the portions of the exterior cladding and roof had been removed by the property owner the in the fall of 2019, December 3, 2019.



Image 23: Photograph of the west side of the accessory building, showing the removed roof sheathing, December 3, 2019.



Image 24: Photograph of the foundation on the west side of the accessory building showing a mix of buff brick and field stone materials, December 3, 2019.



Image 25: Detail of the central projecting gable peak, round-headed window, and hipped roof covering the bay entrance to the accessory building. The ventilator can be seen rising above the centre of the peak, December 3, 2019.



Image 26: Interior view of the loft in the accessory building, approximately one week prior to its collapse, December 3, 2019.



Image 27: Interior detail of the mortise and tenon timber frame construction of the accessory building, December 3, 2019.



Image 28: Interior detail of the mortise and tenon timber frame construction and roof rafters on the east side of the accessory building, December 3, 2019.



Image 29: Interior detail showing traditional mortise and tenon timber frame connections used throughout the accessory building, December 3, 2019.



Image 30: Photograph showing the east side of the accessory building as viewed from Halls Mill Road, showing the collapse of the roof framing, December 11, 2019.



Image 31: Photograph showing the east side of the accessory building as viewed from Halls Mill Road, showing the collapse of the roof framing, December 11, 2019



Image 32: Photograph showing the east side of the accessory building as viewed from Halls Mill Road, showing the collapse of the roof framing, December 11, 2019.

Appendix C – Historical Documentation and Research Materials

Westminster Lot No 45				Concession B.			
Patent	Feb 16 th 1819		The Crown	Archibald McMillan			Ally
944	1 st and 2 nd S	Apr 5 th 1819	Apr 5 th 1819	Sam'l Seaman	Archibald McMillan	3/4	W & P
944	"	" 5 th 1819	" 5 th 1819	Archibald McMillan	John M. Ferlicke	1/4	"
1372	"	Apr 14 th 1830	June 7 th 1831	Archibald McMillan	Burlingh Hunt	3/4	W & P
1416	"	Feb 8 th 1831	Aug 16 th 1831	Amos Seaman et al.	William Olds	"	W & P
1472	Since Jan 9 th 1831	Aug 8 th 1831	Jan 9 th 1832	Burlingh Hunt	William Olds	1/4	"
1473	1 st and 2 nd S	" 5 th 1831	June 3 rd 1832	Burlingh Hunt	Burlingh Hunt	1/4	"
1612	"	Apr 6 th 1832	Apr 26 th 1832	John M. Ferlicke	Burlingh Hunt	1/4	W & P
1871	"	March 13 th 1833	" 26 th 1833	William Olds	Burlingh Hunt	"	W & P
1882	1 st and 2 nd S	Apr 15 th 1833	" 24 th 1833	William Olds	William Olds	1/4	"
1928	1 st and 2 nd S	March 15 th 1833	June 14 th 1833	Alfred E. McMillan et al.	Alfred E. McMillan et al.	9/16	W & P
1970	"	Apr 10 th 1833	Aug 26 th 1833	Archibald McMillan	Amos Seaman	1/4	"
2188	"	Jan 28 th 1834	Dec 5 th 1834	William Olds	Burlingh Hunt	2	"
2792	"	Aug 26 th 1834	Dec 26 th 1835	Archibald McMillan	Sam'l Taylor	1/4	"
2932	"	Nov 5 th 1835	March 26 th 1836	Archibald McMillan	"	"	"

Figure 2: Land Registry records for Lot 45, Concession B in Westminster Township. The highlighted entry notes the Crown grant for all lands within the lot to Archibald McMillan in February 1819. Shortly afterwards McMillan began subdividing the lot into various parcels, which came to be used for milling purposes in the early/mid-19th century.

Westminster Lot No 45				Concession B.			
2928	1 st and 2 nd S	March 11 th 1839	March 11 th 1839	George S. Brown & wife	William S. Lawrence	1/4	"
2948	1 st and 2 nd S	Apr 11 th 1839	Apr 11 th 1839	Amos Seaman	Amos Seaman	1/4	"
2950	1 st and 2 nd S	Apr 11 th 1839	Apr 11 th 1839	Amos Seaman	Amos Seaman	1/4	"
2958	"	March 27 th 1839	" 27 th 1839	Amos Seaman & wife	Amos Seaman	1/4	"
2960	1 st and 2 nd S	Apr 12 th 1839	" 13 th 1839	Amos Seaman & wife	Amos Seaman	1/4	"
3060	1 st and 2 nd S	Apr 5 th 1839	Apr 5 th 1839	William S. Lawrence	William S. Lawrence	1/4	"
3193	1 st and 2 nd S	Apr 11 th 1839	Apr 11 th 1839	Amos Seaman	Amos Seaman	1/4	"
3299	1 st and 2 nd S	June 25 th 1860	June 25 th 1860	Thomas Woodruff et al.	Thomas Woodruff et al.	1/4	"
3432	1 st and 2 nd S	Apr 1 st 1861	July 5 th 1861	Amos Seaman	Amos Seaman	1/4	"
3539	1 st and 2 nd S	June 8 th 1861	June 16 th 1861	Amos Seaman	John Griffith	1/4	"
3540	1 st and 2 nd S	June 10 th 1861	June 10 th 1861	John Griffith	John Griffith	1/4	"
3541	1 st and 2 nd S	June 11 th 1861	June 12 th 1861	John Griffith	John Griffith	1/4	"
3562	1 st and 2 nd S	June 27 th 1861	July 11 th 1861	Amos Seaman	Amos Seaman	1/4	"

Figure 3: Land Registry Records for Lot 45, Concession B in Westminster Township. The highlighted entry is a Deed of land between Lawrence Lawrason and John Griffith in 1861, noted in Instrument No. 3539. This is the first land transaction between a member of the Griffith family on Lot 45. Throughout the late-19th century John Griffith and his brothers William and Eli would continue to acquire portions of the lot on which they would eventually operate their woollen mill, and William would eventually reside on the a portion of the lot that is now known as 247 Halls Mill Road.

NUMBER	INSTRUMENT	DATE OF INSTRUMENT	DATE OF REGISTRATION	GRANTOR	GRANTEE	AREA	CONSIDERATION	REMARKS
17729	Conveyance	Apr. 24/91	May 7, 1891	William H. Finch	The Trustees of Byron Methodist Church	1200 sq. ft.		Part of lot
17811	Deed	May 27/61	Sep. 12/91	Wm. Jaynes & wife	David Lecky			Part with Judges Order
17812	Deed	June 23/91	Sep. 12/91	David Lecky, etux	Corp'n City of London			Part of lot
17813	Q.C. deed	Aug. 20/91	Sep. 12/91	Elizabeth Lecky	Corp'n City of London			Part of lot
17822	Q.C. Deed	Sep. 10/80	Oct 3, 1891	Alfred Harrington, etal	Ariel Harrington			Part of lot
17823	Deed	Apr. 11/85	Oct. 3, 1891	Carrie & James McLaren	Ariel Harrington			Part of lot
17829	Assign. of Mortgage	Sep. 28/91	Oct. 12/91	A. G. Chisholm	Matthew Hooley			Parts of lot
17848	Deed	Oct. 27/91	Nov. 2, 1891	William H. Finch	Charlotte Elson	5 ac.		Part of lot
17850	Mortgage	Sep. 21/91	Nov. 2, 1891	W.S. Chisholm	Andrew G. Chisholm	a.o.l.	1530 66	Part, except
17882	Deed	Dec. 7, 1891	Dec. 7, 1891	Thomas Sumner, etux	City of London	1 ac.		Part of lot
18218	Deed	Apr. 8, 1893	Apr. 8, 1893	Byron Kilborn, etux	John Duval	9 r		Part of E part
18231	Deed	Apr. 18/93	Apr. 20/93	Thos. McCormick, etux	Wm. J. Johnston, etal	1/5		Part
18268	Deed	May 30/93	June 22/93	W.S. Chisholm	Corp'n Cmp. of Meas'r			Part. Subject 2 Mtges.
18549	Deed	Dec. 26/94	Jan. 18/95	William J. Johnston, etal	Jane Maria Frank	1/5		Part of lot
18725	Deed Und. P of Sale	Apr. 6, 1895	Apr. 9, 1895	Agric'l Sav. & Loan Co.	Wm. T. Orman	15		Part of lot
19015	Deed Und. P of Sale	May 14/96	May 23/96	Agric'l Sav. & Loan Co.	Walter & Wm. T. Orman			Pt. as in 15580 except
19016	Deed	May 15/96	May 23/96	Walter & Wm. T. Orman	Natilda M. & Catherine Orman			Pt. as in 15580, except
19034	Deed	June 18/96	June 23/96	William Hy Finch	James E. Griffith, etal Trustees for Order of Chosen Friends #197			Part of lot
19291	Deed	Aug. 25/96	May 25/97	John Duval & wife	Margaret McLaren	1 ac		Part of lot (lot 69, Plan 563)
19349	Deed	Aug. 2, 1897	Aug. 4, 1897	Samuel Sissons & wife & Robert Sissons & wife	John Wellington Crouse	1 1/2 ac.		Part of lot W of Centre St. & on river
19435	Deed	Nov. 6, 1897	Nov. 30/97	William Henry Finch	Timothy Adair	12 ac.		Part of S pt. of W 1/2 S of Comm. Rd. except 5 1/2 acs.
19464	Agreement	Dec. 11/97	Dec. 30/97	William Griffith & Mabel Ann Griffith	Water Commissioners & Corp'n City of London	3 ac.		Part of lot. Subject
19684	Deed	Oct. 26/98	Oct. 29/98	Elizabeth Jane Wells	Samuel Thomas Gelders	1 ac.		Part of 2 1/2 known as village lot 3 (lot 104, Plan 553)
19710	B & Sale	Apr. 16/98	Dec. 2, 1898	Cornelia Duval, widow of John Duval & Admin. of Estate; John A. Duval, etal	George Rudston Garthorne	2800	00	SE pt. as desc. all claims released
19808	Release	Feb. 20/99	Mar. 6, 1899	John W. Crouse & wife etal	Corp'n City of London Water Commissioners of City	a.o.l.	150 00	Part
20006	B & Sale	Nov. 11/99	Nov. 11/99	William Truman Orman & wife	Corp'n City of London	200	00	Pt. as desc. (lot 113, Plan 563)
20794	B & Sale	Apr. 7, 1902	Apr. 7, 1902	William Hy Finch	Joseph K. Montague	900	00	Part NW pt. of E 1/2 lying S of Comm. Rd. as desc. except pt. to Order of Chosen Friends
20877	Will	Dec. 17/85	July 10/02	Ephraim Ellwood	Hannah B. Ellwood, Devisee	1/2 ac.		Pt. S of Main St.
				"Sarah A. Ellwood & William Hutchinson, Ex'ors."	Samuel Ellwood, Devisee			Part of lot on which the house stands N of Main St.
					Wesley Ellwood, Devisee			Part of lot, N of Main St.
21215	B & Sale	Apr. 24/03	Sep. 8, 1903	John Wellington Crouse etux	Hell & Angus Galbraith	1/2 ac.	2800 00	Part of lot (a.o.l.) with Mill. Machinery, barn & mill privileges. (Lots 122 & 123, Plan 563)
21221	Q.C. Deed	Jan. 6, 1903	Sep. 26/03	Catherine C. Summers	Mary Summers	1	00	Part released. W of Centre St. & E of Comm. Rd.

Figure 4: Land Registry Records for Lot 45, Concession B in Westminster Township. The highlighted entry shows the agreement between William and Mabel Ann Griffith to sell a portion of their property to the City of London in 1897. This was the parcel of land that the Griffith Brothers woollen mill was located on, and is now the property located across the road from 247 Halls Mill Road, known as Halls Mills Park.

Province of Ontario District No. 1 C. R. Middlesex Sub-District 2 Westminster Concession Division 1

Schedule No. 1—Nominal Return of the Living.

No.	Name	Sex	Age	Place of Birth	Religion	Origin	Profession, Occupation or Trade	Married with wife	Married with second wife	Education				Date of Operation as Elector
										Reading	Writing	Arithmetic	Other	
134	David Charles	M	28	England	Unit	England	Labourer							
	William	M	23	"	"	"	Labourer							
	David	M	21	"	"	"	Labourer							
	Eli	M	14	"	"	"								
134 1/2	James John	M	60	England	Methodist	England	Labourer							
	Sarah	F	53	Ireland	"	Unit								
	William	M	23	"	"	England	Clothier							
	Louisa	F	20	"	"	"								
	Greenous	M	16	"	"	"	Labourer							
	David	M	12	"	"	"								
137 1/2	Ormond Michael	M	74	Ireland	Roman Catholic	Unit	Blind							
	Ann	F	57	"	"	"								
	James	M	22	"	"	"	Labourer							
138 1/2	Griffith John	M	32	Windsor	Unit	Windsor	Clothier							
	Mary	F	26	Windsor	Unit	Windsor								
	Carrie	F	5	"	"	"								
	Sarah	F	3	"	"	"								
139 1/2	Griffith William	M	29	"	Unit	Windsor	Clothier							
	Mabel Ann	F	26	"	"	Windsor								
140 1/2	Griffith E.	M	27	"	Unit	Windsor	Clothier							

Figure 5: Excerpt from the 1871 Census of Upper Canada. The highlighted entries show John, William, and Eli Griffith, all noted as "Clothiers" as a result of their woollen mill in Byron.

BYRON.

A post village situated on the river Thames, in the township of Westminster, $5\frac{1}{4}$ miles from London and two miles from a station on the Great Western Railway. It contains a Church of England, and the New Connexion Methodists hold services in the School House. There are two Woollen Mills. Messrs. Griffith Bros. factory is of frame, two stories, 64×45 feet, in which water power is used, and about 12 hands are employed in the manufacture of tweeds, fulls cloths and flannels. Their machinery is considered superior to any other in the Province. Messrs. Dufton's mill is located in the neighborhood. This firm is also noted for the class of goods turned out. Messrs. J. Sissons & Sons established a hame factory here in 1862. They use both steam and water power, and employ nine hands, turning out a considerable number of hames during the year. A flouring mill was established about thirty years ago. It is now carried on by Mr. Robert Summers, Jr. It contains two run of stone, which are propelled by water power. The capacity is about 200 bushels per day. Tri-weekly mail. Population 200.

Caldwell, Charles, farmer.
 Caldwell, James, farmer.
 Church, George, blacksmith.
COOMBS, CHARLES, flour and
 grist mills.
 Dufton, E. T., J.P.
 Dufton, John, (J. & J. Dufton &
 Co.)
 Dufton, Joshua, (J. & J. Dufton
 & Co.)
DUFTON, J. & J. & CO., woollen
 mill proprietors.
 GLOVER, J., prop Byron Hotel.
GRIFFITH BROS., (John, Wm.
 and Eli,) woollen mill props.

Harrington, A., general merchant.
 Hollywood, Anson, farmer.
 McCallum, Archibald, (J. & J.
 Dufton & Co.)
 Martin, Wm., boot and shoe maker.
 Montague, John, farmer.
SADLER, ROBERT, postmaster
 and general merchant.
**SISSONS, JOHN, ROBERT &
 SAMUEL**, hame manufacturers
 Stephens, John, farmer.
SUMMERS, ROBERT, Jr., flour-
 ing and grist mill proprietor.
 Wells, Bartholomew, farmer.
 Wells, J. B., laborer.

Figure 6: Excerpt from the 1868 City and County Directory, showing the description of Byron, including a brief description of the Griffith Bros. woollen mill.

BYRON WOOLLEN MILLS,
GRIFFITH BROS., Proprietors,
 MANUFACTURERS OF
TWEEDS, FULL CLOTHS, PLAIN & FANCY FLANNELS
 OF ALL KINDS.

Custom Carding, Spinning, Weaving, Dying and Cloth Dressing attended to
 with dispatch
 The highest price paid for wool, or taken in exchange for cloth.

ALL WORK WARRANTED.

Figure 7: Advertisement for the Griffith Bros. woollen mills as shown in the 1871-71 City and County Directory.

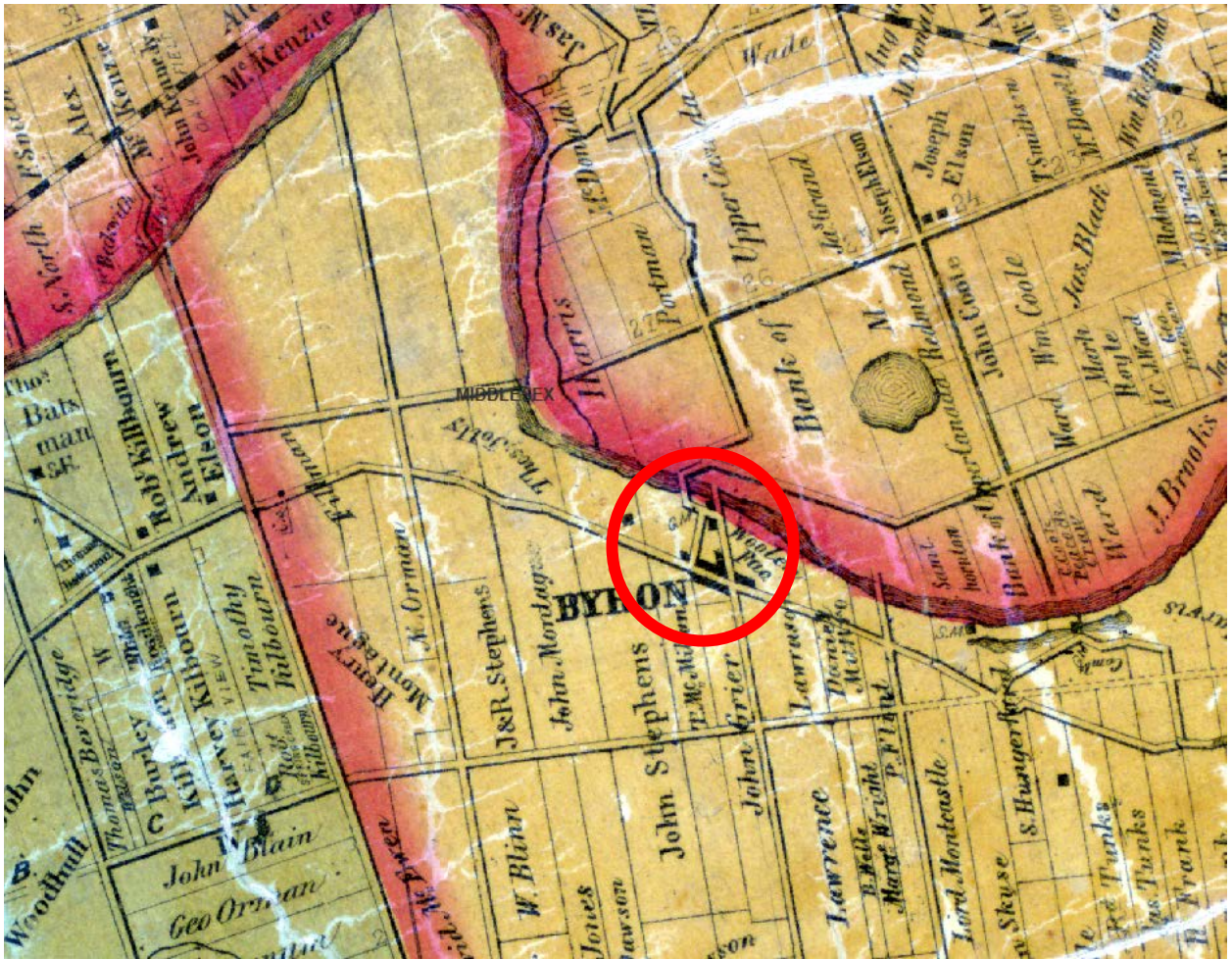


Figure 8: Excerpt from Tremaine's Map of the County of Middlesex (1862). Note, the Byron area is shown as mostly built up and individual property owners are not depicted. "G.M." noted along the Thames River refers to the presence of a gristmill in the area.



Figure 9: Excerpt from the Illustrated Historical Atlas of Middlesex County (1878). Note, the Byron area is shown as mostly built up and individual property owners are not depicted.

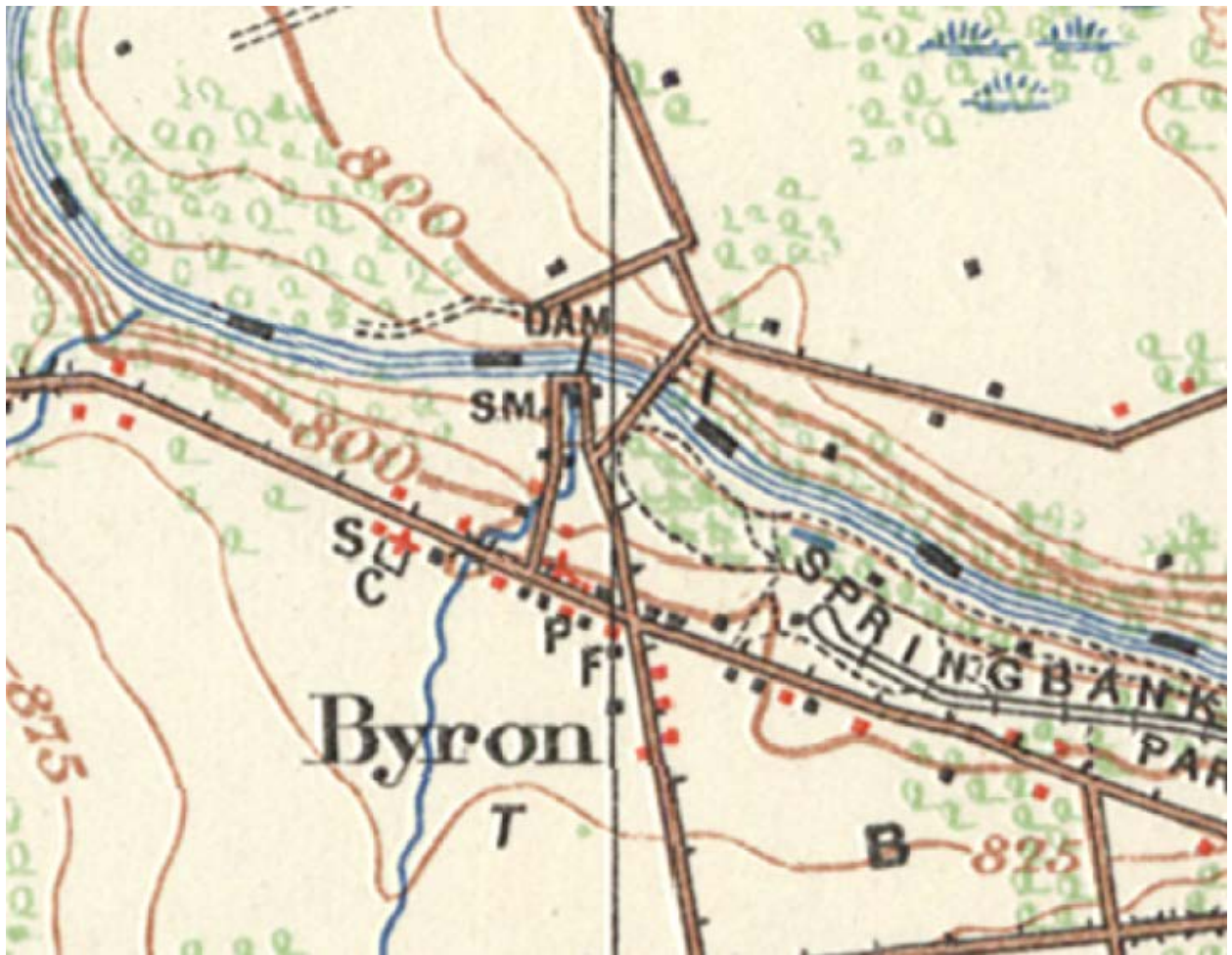


Figure 10: Excerpt from 1913 National Topographic Series map, showing the Byron area (1913). The area is depicted with various wood and masonry buildings. A sawmill (SM) is shown in the Hall's Mill area and a dam is still constructed across the Thames River.

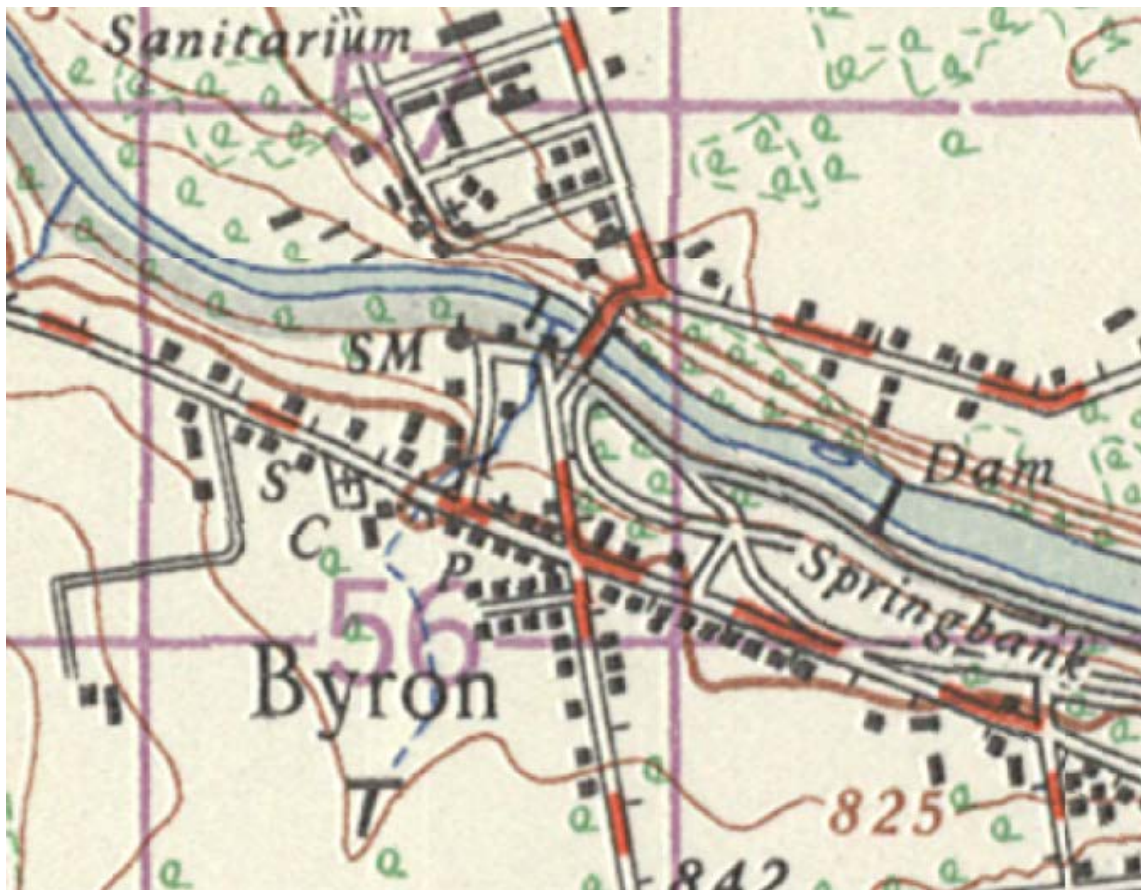


Figure 11: Excerpt from 1948 National Topographic Series map, showing the Byron area (1948). The area is depicted with buildings. A sawmill (SM) is still shown in the Hall's Mill area, however, the dam is no longer present, as it was washed away in the Flood of 1937.

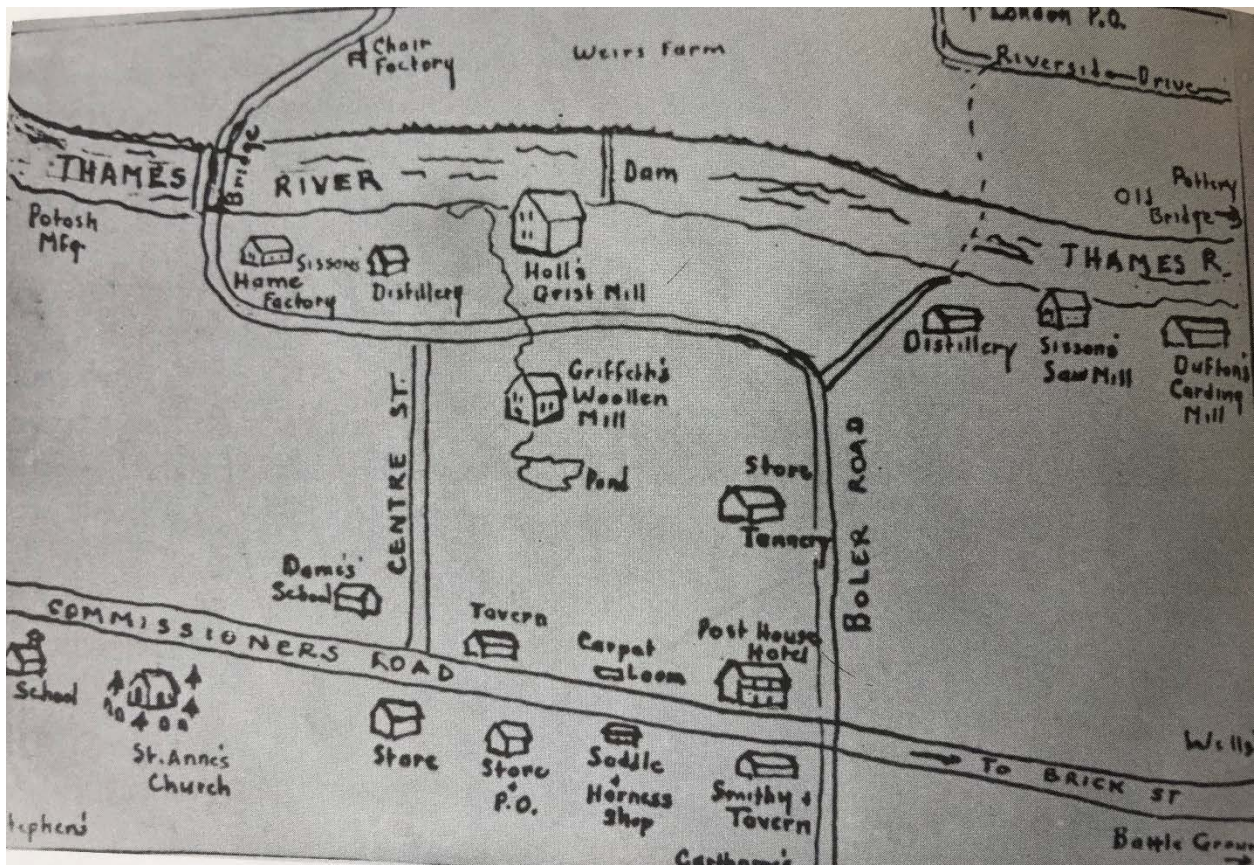


Figure 12: Hand-drawn map depicting various commercial buildings in Hall's Mills/Byron c. 1870, as shown in Roy Kerr's 160 Years of Westminster, Halls Mills, Byron.

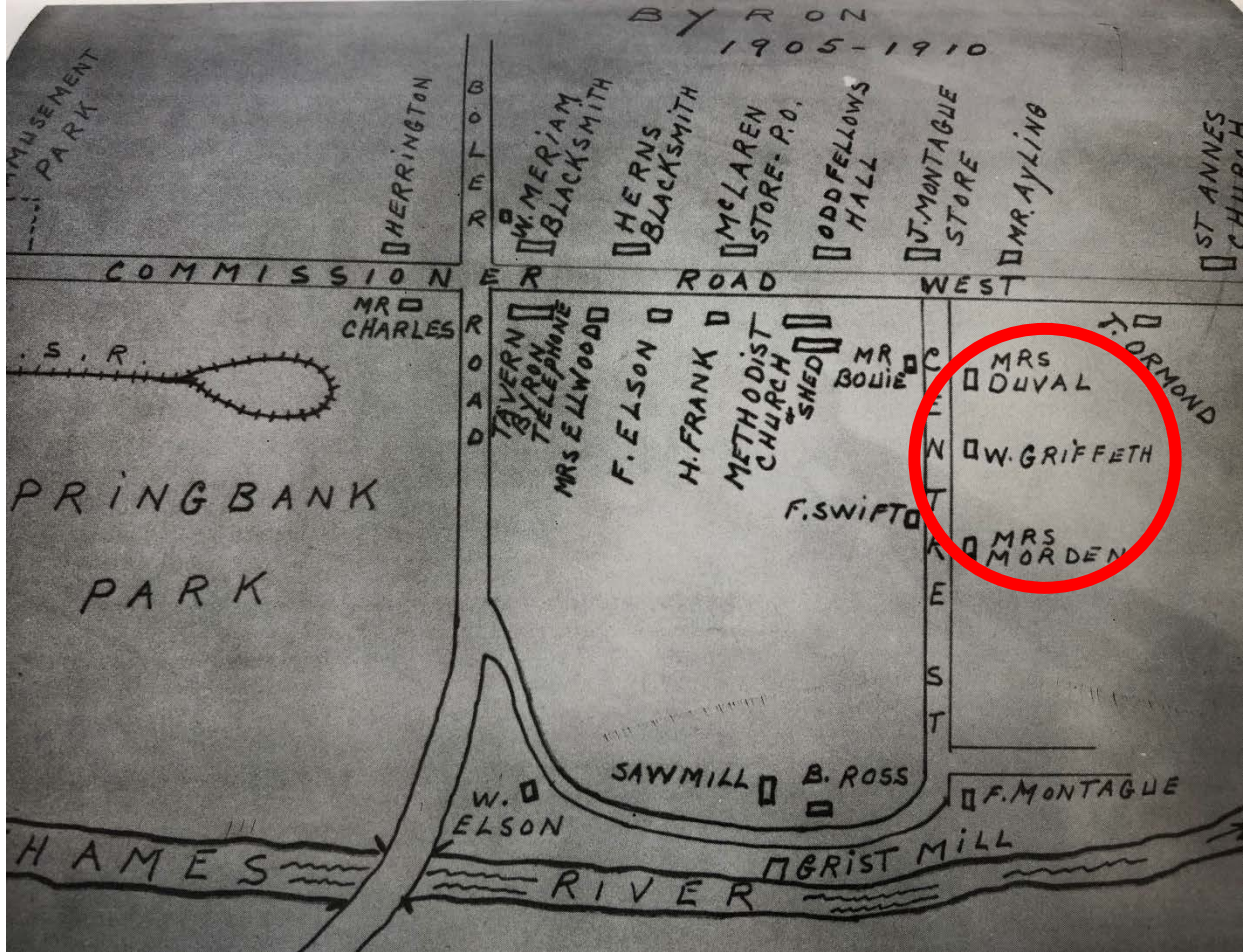


Figure 13: Hand-drawn map depicting various commercial and residential buildings in Hall's Mills/Byron c. 1905-1910, as shown in Roy Kerr's 160 Years of Westminster, Hall's Mill, Byron.

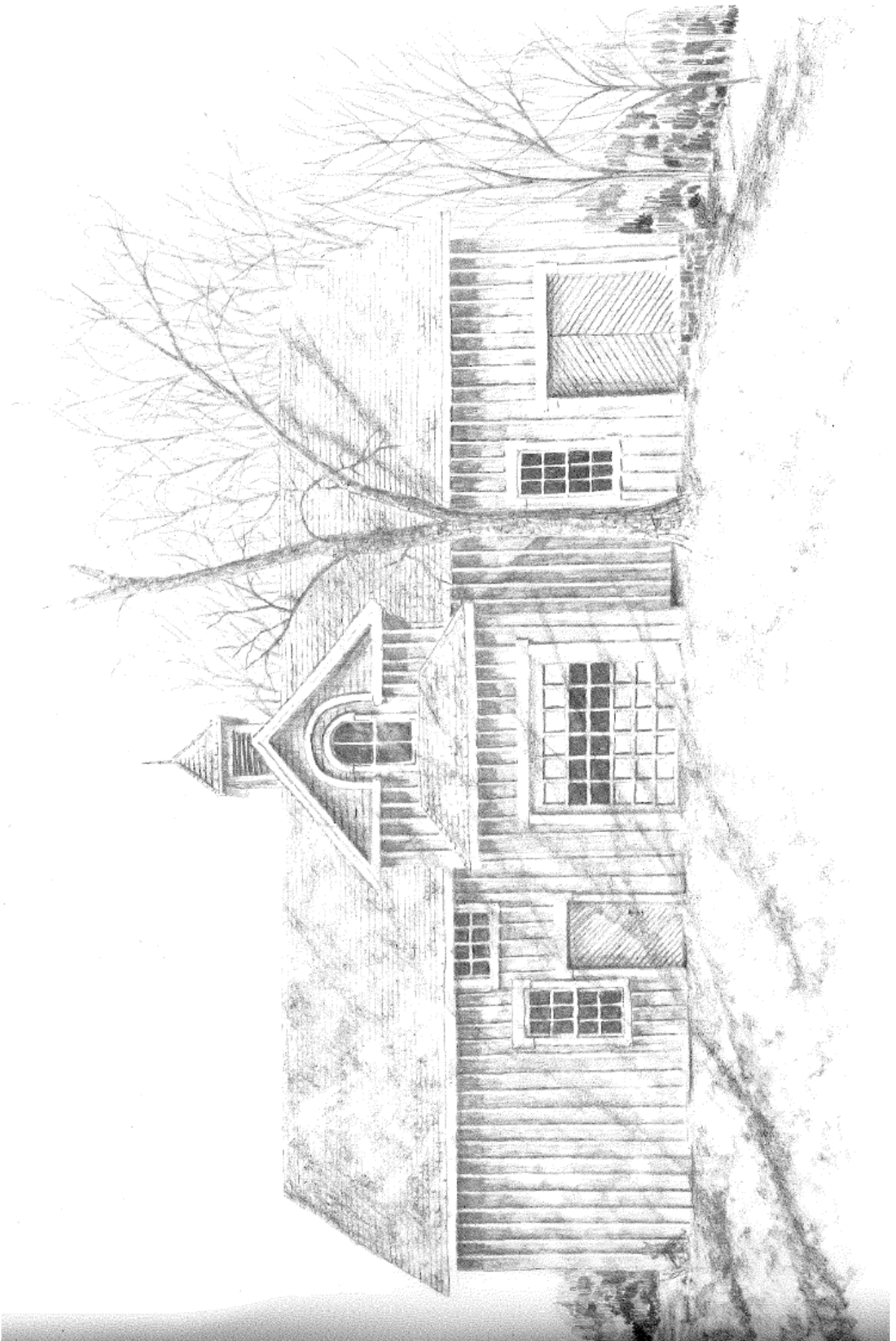


Figure 14: Sketch of the accessory building as depicted in Nancy Tausky's book *Historical Sketches of London from Site to City* (1993). Tausky refers to the building as "one of the most impressive coach houses left in London". In comparison, Tausky refers to the coach houses at Buchan House (566 Dundas Street) and Thornwood (393 St. George Street) as other examples in the City.

Appendix D – Comparative Analysis

Comparative analyses were undertaken from the perspective of cultural heritage resources within London with other one-and-a-half storey, buff brick, side hall plan cottages with Queen Anne Revival style influences.

The following properties were identified as comparison properties (some are pictured below):

- 77 Byron Avenue East (Wortley Village-Old South Heritage Conservation District);
- 86 Askin Street (Part IV designated, and Wortley Village-Old South Heritage Conservation District);
- 105 Bruce Street (Wortley Village-Old South Heritage Conservation District);
- 933 Dufferin Avenue (Old East Heritage Conservation District);
- 928 Dufferin Avenue (Old East Heritage Conservation District);
- 43 Byron Avenue East (Wortley Village-Old South Heritage Conservation District);
- 71 Byron Avenue East (Wortley Village-Old South Heritage Conservation District);
- 76 Colborne Street (Part IV designated);
- 477 Elizabeth Street (Old East Heritage Conservation District).

When compared to other one-and-a-half storey, buff brick, side hall plan cottages in London, the identification of the dwelling at 247 Halls Mill Road is supported as a representative example of the Queen Anne style of this form.

Comparative analyses were also undertaken from the prospective of cultural heritage resources within London with other “accessory buildings”. The following properties were identified as comparison properties:

- 335 St. George Street [Thornwood Coach House] – (Part IV designated);
- 556 Dundas Street [Buchan House Coach House];
- 660 Sunningdale Road East – (Part IV designated).
- 1017 Western Road [Grosvenor Lodge Carriage House] – Part IV designated).

When compared to other accessory buildings, the identification of accessory building at 247 Halls Mill Road is supported as a unique and rare example of a timber frame accessory building.



Image 33: Property at 86 Askin Street, Part IV designated and Wortley Village-Old South Heritage Conservation District



Image 34: Property at 105 Bruce Street, Wortley Village-Old South Heritage Conservation District



Image 35: Property at 933 Dufferin, Old East Heritage Conservation District



Image 36: Property at 76 Colborne Street, part IV designated.



Image 37: Property at 335 St George Street, part IV designated



Image 38: Property at 556 Dundas Street



Image 39: Property at 660 Sunningdale Road East, part IV designated



Image 40: Property at 1017 Western Road, part IV designated

Appendix E – Statement of Cultural Heritage Value or Interest

Legal Description

Part of Lot 115, RCO 563, as in 755312 London

Description of Property

The property at 247 Halls Mill Road is located on west side of Halls Mill Road, north of Commissioners Road West. The property includes a dwelling located to the southern portion of the property, and an accessory building located to the north of the property.

Statement of Cultural Heritage Value

The property at 247 Halls Mill Road is of significant cultural heritage value or interest because of its physical/design value, its historical/associative value, and its contextual value.

The property at 247 Halls Mill Road includes a representative example of a Queen Anne Revival style, side hall plan cottage, with a buff brick exterior. The cottage is believed to have been constructed in the 1840s and evolved in the 1890s when a number of its decorative elements were added, making it a representative example of the Queen Anne Revival style applied to a side hall plan cottage in London.

The accessory building on the property is a unique and rare example of a timber frame accessory structure that has been used for various purposes of the course of its existence. Known locally as “the Red Barn”, the structure has been reportedly used as a barn, coach house, and warehouse for the Griffith Bros. woollen mills. The structure has been designed to include a series of stylistic embellishments that elevates the appearance of the structure beyond that of a typical barn. Its chestnut board-and-batten siding, projecting gable, window treatments, and central ventilator all contribute to its being a unique example of a timber frame accessory building.

The concentration of decorative wood detailing on the cottage’s gable and bargeboard elements as well as its decorative verandah posts contribute to the expression of its style as a Queen Anne Revival cottage. As a result, the property displays a high degree of craftsmanship.

The property is directly associated with William Griffith, one of the three Griffith brother who owned and operated the Griffith Bros. woollen mill in Byron between the 1860s and 1890s. The mill was located directly across the road from the property at 247 Halls Mill Road, on the property now known as Halls Mills Park. The cottage on the subject property was the home of William Griffith and the accessory building on the property was reportedly used as a coach house and warehouse for the woollen products produced at the Griffith Bros. mill.

The property at 247 Halls Mill Road has the potential to yield information related to the history of the Halls Mills area. As a property historically associated with the Griffith Bros., and 19th century milling in Byron, the property has potential to yield information that contributes to the understanding of the Halls Mills area.

The property at 247 Halls Mill Road is important in defining the character of the Halls Mills area. The Hall’s Mills area is characterized by a geographical context near the Thames River and its topography, along with the collection of early and mid-19th century buildings located along Halls Mills Road and Commissioners Road West.

As the property includes an 1840s dwelling and 19th century accessory building, the property is a part of the concentration of cultural heritage resources in the Halls Mill area that contribute to its character and have led to its identification as a potential heritage conservation district.

The property is historically linked to the property now known as Halls Mill Park, on the east side of Halls Mill Road. The Halls Mill Park property was the site of the Griffith

Bros. woollen mill which was operated by William Griffith, owner of 247 Halls Mill Road, in partnership with his brothers. As the milling site for their Byron operation, the properties are historically linked.

The property at 247 Halls Mill Road is considered to be a local landmark within the Halls Mills area. Specifically, “the Red Barn” is known locally recognized as a landmark in the area.

Heritage Attributes

Heritage attributes which support and contribute to the cultural heritage value or interest of this property include:

- The siting of the dwelling a grade above road level, on the south side of the property, accessed from steps from the public road allowance;
- Form, scale, and massing of the one-and-a-half storey dwelling and details including;
 - Field stone foundation;
 - Buff brick exterior cladding, with voussoirs above the window and door openings on the facades and quoins on the north and south elevations;
 - Gables located on the north, east, and south facades;
 - Decorated north, east and south gables, and gable dormer on the north side of the house, including wood details:
 - Bargeboard with decorative linear and medallion elements, corbels, and dentils;
 - Scalloped wood shingle imbrication on gables and dormer;
 - “Alisée Pattée” cross motif along the frieze of the gables;
 - Circular feature including “Alisée Pattée” cross design and medallions;
 - Dentil course above the gable windows;
 - Wood corbels at the base of the gable
 - Decorated north and south porches including wood details:
 - Turned posts;
 - Decorative wood spandrels;
 - “Alisée Pattée” cross designs and medallion designs in the peak of the gable on the south porch;
 - Stained glass semi-circular windows on the north and east façades and the transom;
 - South paired wood door;
 - East panelled wood door with glazing;
 - North panelled wood door with glazing;
 - Hipped roof with cross gables;
 - Buff brick chimney on the south elevation of the dwelling;
- Form, scale, and massing of the timber frame accessory building and details including;
 - Red-painted, exterior chestnut board-and-batten cladding;
 - Buff brick and field stone foundation;
 - Gable roof form of the building;
 - Projecting front bay on the east elevation of the structure including gable roof peak above the round headed window, and hipped roof above the main bay door;
 - Wood windows including;
 - Six-over-six divided light windows on the east, west, and north sides of the structure;
 - Three-over-three divided light windows on the south side of the structure;
 - Divided light window panel in the gable of the north side of the structure;
 - Exterior wood door and window surrounds;
 - Central hipped-roof ventilator located on the ridge of the gable;
- Spatial relationships between the dwelling and the accessory building.

London Advisory Committee on Heritage

Report

The 1st Meeting of the London Advisory Committee on Heritage
December 11, 2019
Committee Rooms #1 and #2

Attendance PRESENT: D. Dudek (Chair), M. Bloxam, J. Dent, S. Gibson, T. Jenkins, J. Manness, E. Rath, M. Rice, K. Waud and M. Whalley and J. Bunn (Committee Clerk)

ABSENT: S. Bergman, L. Fischer and S. Jory

ALSO PRESENT: K. Gonyou, M. Greguol, L. Jones, M. Knieriem, M. Morris and A. Rammeloo

The meeting was called to order at 5:30 PM.

1. Call to Order

1.1 Disclosures of Pecuniary Interest

T. Jenkins discloses a pecuniary interest in Item 2.4 of the 1st Report of the London Advisory Committee on Heritage, having to do with a Notice of Planning Application and Notice of Public Meeting with respect to Official Plan and Zoning By-law Amendments related to the Hamilton Road Corridor Planning Study, by indicating that her employer is involved in this matter.

1.2 Election of Chair and Vice Chair for the term Ending November 30, 2020

That it BE NOTED that the London Advisory Committee on Heritage elected D. Dudek and M. Whalley as the Chair and Vice-Chair, respectively, for the term ending November 30, 2020.

2. Scheduled Items

2.1 Demolition Request and Heritage Alteration Permit Application by Distinctive Homes London Ltd. at 88 Blackfriars Street, Blackfriars/Petersville Heritage Conservation District

That, on the recommendation of the Managing Director, City Planning and City Planner, with the advice of the Heritage Planner, the application under Section 42 of the Ontario Heritage Act seeking approval for the demolition of the existing building and approval for a proposed building, as described in Appendix D of the staff report dated December 11, 2019, on the property at 88 Blackfriars Street, within the Blackfriars/Petersville Heritage Conservation District, BE PERMITTED with the following terms and conditions:

- buff brick be used for the exterior cladding of the proposed building;
- a painted wood front door be used for the proposed building;
- parking be limited to a driveway to the west of the proposed building with front yard parking prohibited;
- the Heritage Planner be circulated on the Building Permit application drawings to verify compliance with this Heritage Alteration Permit prior to issuance of the Building Permit; and,
- the Heritage Alteration Permit be displayed in a location visible from the street until the work is completed;

it being noted that the condition of the above-noted building constitutes another regrettable example of demolition by neglect and the London Advisory Committee on Heritage implores stronger enforcement of the Property Standards By-law to avoid future demolition by neglect of London's cultural heritage resources;

it being further noted that the ~~attached~~ presentation from K. Gonyou, Heritage Planner, with respect to this matter, was received.

2.2 Heritage Alteration Permit Application by Brian Allen at 906 Lorne Avenue, Old East Heritage Conservation District

That, on the recommendation of the Managing Director, City Planning and City Planner, with the advice of the Heritage Planner, the application under Section 42 of the Ontario Heritage Act seeking approval for alterations to the property at 906 Lorne Avenue, within the Old East Heritage Conservation District, BE PERMITTED with the terms and conditions:

- all exposed wood be painted;
- a wood lattice porch skirt set in a frame to be added where missing;
- the top rail be constructed no higher than 30" to maintain the proportions of the porch;
- the railings and guards on the steps be replaced to be consistent with the railings and guards on the entirety of the porch;
- a new base around the northwest column be installed; and,
- the Heritage Alteration Permit be displayed in a location visible from the street until the work is completed;

it being noted that the ~~attached~~ presentation from M. Greguol, Heritage Planner, with respect to this matter, was received.

2.3 Cultural Heritage Evaluation Reports (CHERs) for the Properties Located at 90, 92 and 102 Wellington Road

That the following actions be taken with respect to the Cultural Heritage Evaluation Reports (CHERs) for the properties located at 90, 92 and 102 Wellington Road:

- a) the Civic Administration BE ADVISED that the London Advisory Committee on Heritage (LACH) concurs with the findings of the above-noted CHERs, as appended to the agenda; it being noted that the ~~attached~~ presentation from M. Morris, Major Projects, with respect to this matter, was received; and,
- b) the ~~attached~~ Stewardship Sub-Committee Report, from its meeting held on November 26, 2019, BE FORWARDED to the Civic Administration for consideration.

2.4 Notice of Planning Application and Notice of Public Meeting - Official Plan and Zoning By-law Amendments - Hamilton Road Corridor Planning Study

That it BE NOTED that the ~~attached~~ presentation from M. Knieriem, Planner II, with respect to a Notice of Planning Application and Notice of Public Meeting related to Official Plan and Zoning By-law Amendments for the Hamilton Road Corridor Planning Study, was received.

3. Consent

3.1 11th Report of the London Advisory Committee on Heritage

That it BE NOTED that the 11th Report of the London Advisory Committee on Heritage, from its meeting held on November 13, 2019, was received.

3.2 Municipal Council Resolution - 11th Report of the London Advisory Committee on Heritage

That it BE NOTED that the Municipal Council resolution, from its meeting held on November 26, 2019, with respect to the 11th Report of the London Advisory Committee on Heritage, was received.

3.3 Public Meeting Notice - Zoning By-law Amendment - 1018-1028 Gainsborough Road

That it BE NOTED that the Public Meeting Notice, dated November 13, 2019, from L. Mottram, Senior Planner, with respect to a Zoning By-law Amendment for the properties located at 1018-1028 Gainsborough Road, was received.

4. Sub-Committees and Working Groups

4.1 Stewardship Sub-Committee Report

That it BE NOTED that the Stewardship Sub-Committee Report, from the meeting held on November 26, 2019, was received.

4.2 556 Wellington Street Heritage Impact Statement Working Group Report

That the attached 556 Wellington Street Heritage Impact Statement Working Group Report, as appended to the agenda, BE FORWARDED to the Civic Administration for consideration; it being noted that the London Advisory Committee on Heritage supports and endorses the above-noted Working Group Report.

5. Items for Discussion

5.1 Properties Located at 197, 183 and 179 Ann Street

That the following actions be taken with respect to the requests for delegation from A. Valastro and M. Tovey related to the properties located at 197, 183 and 179 Ann Street:

a) the properties located at 175, 179, 183 and 197 Ann Street and 84 and 86 St. George Street BE REFERRED to the Stewardship Sub-Committee for research and evaluation for a possible heritage designation; it being noted that a verbal delegation by A. Valastro, with respect to this matter, was received; and,

b) the request for delegation by M. Tovey BE APPROVED for the February 2020 meeting of the London Advisory Committee on Heritage.

5.2 Heritage Alteration Permit Application by D. Sagar and K. Corcoran at 430 Dufferin Avenue, West Woodfield Heritage Conservation District

That, on the recommendation of the Managing Director, City Planning and City Planner, with the advice of the Heritage Planner, the application

under Section 42 of the Ontario Heritage Act seeking retroactive approval for alterations to the porch of the property located at 430 Dufferin Avenue, within the West Woodfield Heritage Conservation District, BE PERMITTED; it being noted that the attached presentation from K. Gonyou, Heritage Planner, with respect to this matter, was received.

5.3 Demolition Request for Heritage Listed Property at 2325 Sunningdale Road East by Lafarge Canada Inc.

That, on the recommendation of the Managing Director, Planning and City Planning, with the advice of the Heritage Planner, the following actions be taken with respect to the demolition request for the existing dwelling on the heritage listed property at 2325 Sunningdale Road East:

- a) the Chief Building Official BE ADVISED that Municipal Council consents to the demolition of the dwelling on this property, and;
- b) the property at 2325 Sunningdale Road East BE REMOVED from the Register of Cultural Heritage Resources;

it being noted that the attached presentation from M. Greguol, Heritage Planner, with respect to this matter, was received.

5.4 Community Heritage Ontario 2020 Membership Renewal

That the London Advisory Committee on Heritage 2020 membership with the Community Heritage Ontario BE APPROVED; it being noted that the CHOnews newsletter for Autumn 2019, was received.

5.5 2020 LACH Work Plan

That it BE NOTED that the London Advisory Committee on Heritage (LACH) held a general discussion with respect to the 2020 LACH Work Plan.

5.6 Heritage Planners' Report

That it BE NOTED that the attached submission from K. Gonyou, L. Dent and M. Greguol, Heritage Planners, with respect to various updates and events, was received.

5.7 (ADDED) Court House at 399 Ridout Street North

That the Heritage Planner BE REQUESTED to forward copies of the Heritage Designating By-laws for the Court House on 399 Ridout Street North to the Stewardship Sub-Committee for review and a report back at a future meeting of the London Advisory Committee on Heritage.

6. Adjournment

The meeting adjourned at 7:32 PM.

Good morning Jerri,

I hope that you're well.

I must inform you that I will need to step down from the LACH. Relating to our previous discussion about school conflicts, it appears as though I will have a conflict once again next semester, unfortunately.

Thank you again for being accommodating this semester.

Kindly,

Josh

For the next meeting of the London Advisory Committee on Heritage

To help avoid another fiasco as has occurred with our national historic site at the old courthouse and gaol in London, an amendment to the *Ontario Heritage Act* is in order to protect the public interest.

An amendment whereby any municipally owned (including upper-tier municipalities such as Middlesex County) designated heritage properties cannot be sold without first consulting with the local heritage advisory committee (such as London's Advisory Committee on Heritage) and three (3) widely advertised notices for a public participation meeting being placed in daily or weekly newspapers, both online and in print.

The old courthouse was designated a National Historic Site under the federal *Historical Sites and Monuments Act* in 1955, under Part IV of the *Ontario Heritage Act* in 1980 and the old gaol was designated under Part IV of the *Ontario Heritage Act* in 1986.

A heritage easement was registered on title of 399 Ridout Street North in 1981 by the Ontario Heritage Trust.

Despite all this statutory protection, our national historic site at the river forks can still be compromised or worse, without constant monitoring in the hands of a private land developer. We've all seen it several times before.

Barry Wells

London ON

* Named to the Mayor's New Years' Honours List for Heritage in 2006

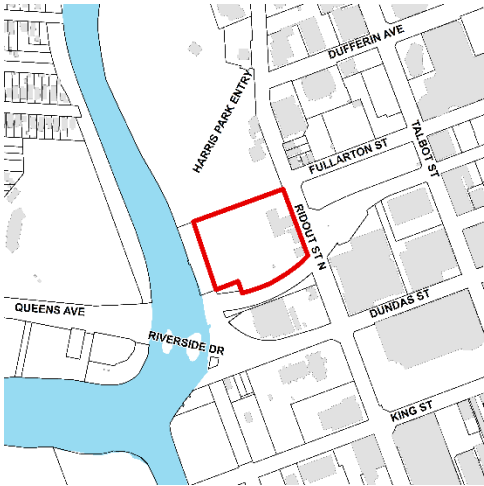
* Member, London Advisory Committee on Heritage, 1995-1998

* Board member, London Heritage Foundation, 1995-1998.

NOTICE OF PLANNING APPLICATION

Official Plan and Zoning By-law Amendments

435-451 Ridout Street North



File: OZ-9157

Applicant: Farhi Holdings Corporation

What is Proposed?

Official Plan and Zoning Amendments to allow:

- Adaptive reuse of the existing heritage buildings
- A 40-storey mixed-use building containing 280 residential units and 6,308 sq.m of office/commercial space, incorporated with the existing heritage building at 451 Ridout Street North
- A maximum density of 500 units per hectare
- A maximum building height of 125 metres
- A setback of 17.9 metres to the residential component of the building

LEARN MORE & PROVIDE INPUT

Please provide any comments by **January 22, 2020**

Catherine Lowery

clowery@london.ca

519-661-CITY (2489) ext. 5074

Development Services, City of London, 300 Dufferin Avenue, 6th Floor,
London ON PO BOX 5035 N6A 4L9

File: OZ-9157

london.ca/planapps

You may also discuss any concerns you have with your Ward Councillor:

Councillor Arielle Kayabaga

akayabaga@london.ca

519-661-CITY (2489) ext. 4013

**If you are a landlord, please post a copy of this notice where your tenants can see it.
We want to make sure they have a chance to take part.**

Application Details

Commonly Used Planning Terms are available at london.ca/planapps.

Requested Amendment to the Current Official Plan

To add a Chapter 10 specific policy to permit a 40-storey mixed-use building containing 280 residential units and 6,308 square metres of office/commercial space, in addition to 1,627 square metres of office/commercial space in the existing heritage buildings.

Requested Amendment to The London Plan (New Official Plan)

To add a specific policy to the Downtown Place Type to permit a maximum of building height of 40-storeys.

Requested Zoning By-law Amendment

To change the zoning from a Downtown Area Special Provision (DA2(3)*D350) Zone, a Heritage/Regional Facility (HER/RF) Zone, and an Open Space (OS4) Zone to a Downtown Area Special Provision (DA2(_)*D500*H125) Zone and an Open Space (OS4) Zone. Changes to the currently permitted land uses and development regulations are summarized below. The complete Zoning By-law is available at london.ca/planapps.

Current Zoning

Zone: Downtown Area Special Provision (DA2(3)*D350) Zone, a Heritage/Regional Facility (HER/RF) Zone, and an Open Space (OS4) Zone

Permitted Uses: Retail stores; supermarkets; amusement game establishments; apartment buildings; apartment hotel; art galleries; assembly halls; bake shops; clinics; commercial parking structures; commercial recreation establishments; convenience stores; day care centres; dry cleaning and laundry depots; duplicating shops; dwelling units; emergency care establishments; film processing depots; financial institutions; funeral homes; group home type 2; hotels; institutions; laboratories; laundromats; libraries; medical/dental offices; museums; offices; patent testing centre laboratories; personal service establishments; places of worship; printing establishments; private clubs; repair and rental establishments; restaurants; restaurants, outdoor patio; schools; senior citizen apartment buildings; service and repair establishments; service trades; studios; taverns; theatres and cinemas; video rental establishments; lodging house class 2; place of entertainment; artisan workshop; craft brewery; adult secondary schools; ancillary residential and/or hostels and accommodations, together with permitted uses in the RF Zone; commercial schools; community colleges; elementary schools; hospitals; private schools; recreational buildings; secondary schools; stadia; supervised residences; universities; conservation lands; conservation works; golf courses without structures; private parks without structures; public parks without structures; recreational golf courses without structures; cultivation or use of land for agricultural/horticultural purposes; sports fields without structures

Special Provisions: Permitted uses only in existing buildings and height as existing on the date of the passing of By-law No. Z.-1.

Density: 350 units per hectare.

Requested Zoning

Zone: Downtown Area Special Provision (DA2(_)*D500*H125) Zone and an Open Space (OS4) Zone

Permitted Uses: Retail stores; supermarkets; amusement game establishments; apartment buildings; apartment hotel; art galleries; assembly halls; bake shops; clinics; commercial parking structures; commercial recreation establishments; convenience stores; day care centres; dry cleaning and laundry depots; duplicating shops; dwelling units; emergency care establishments; film processing depots; financial institutions; funeral homes; group home type 2; hotels; institutions; laboratories; laundromats; libraries; medical/dental offices; museums; offices; patent testing centre laboratories; personal service establishments; places of worship; printing establishments; private clubs; repair and rental establishments; restaurants; restaurants, outdoor patio; schools; senior citizen apartment buildings; service and repair establishments; service trades; studios; taverns; theatres and cinemas; video rental establishments; lodging house class 2; place of entertainment; artisan workshop; craft brewery; conservation lands; conservation works; golf courses without structures; private parks without structures; public parks without structures; recreational golf courses without structures; cultivation or use of land for agricultural/horticultural purposes; sports fields without structures.

Special Provision: A reduced setback for the residential component of the building of 17.9 metres, whereas 44.4 metres is required.

Density: 500 units per hectare.

Height: 125 metres.

Bonus Zone: A bonus zone may be requested to permit the proposed density, height, and setback in return for eligible facilities, services, and matters outlined in Section 19.4.4 of the 1989 Official Plan and policies 1638_ to 1655_ of The London Plan.

The City may also consider the use of additional special provisions.

An Environmental Impact Study has been prepared to assist in the evaluation of this application.

Planning Policies

Any change to the Zoning By-law must conform to the policies of the Official Plan, London's long-range planning document. These lands are currently designated as Downtown and Open Space in the Official Plan, which permits a broad range of retail, service, office, institutional, entertainment, cultural, high density residential, transportation, recreational, and open space uses as the main uses.

The subject lands are in the Downtown Place Type and the Green Space Place Type in *The London Plan*, permitting a range of residential, retail, service, office, cultural, institutional, hospitality, entertainment, recreational, and other related uses. Permitted uses in the Green Space Place Type include agriculture, woodlot management, horticulture, conservation, and recreational uses.

How Can You Participate in the Planning Process?

You have received this Notice because someone has applied to change the Official Plan designation and the zoning of land located within 120 metres of a property you own, or your landlord has posted the notice of application in your building. The City reviews and makes decisions on such planning applications in accordance with the requirements of the *Planning Act*. The ways you can participate in the City's planning review and decision making process are summarized below. For more detailed information about the public process, go to the [Participating in the Planning Process](#) page at [london.ca](#).

See More Information

You can review additional information and material about this application by:

- visiting Development Services at 300 Dufferin Ave, 6th floor, Monday to Friday between 8:30am and 4:30pm;
- contacting the City's Planner listed on the first page of this Notice; or
- viewing the application-specific page at [london.ca/planapps](#).

Reply to this Notice of Application

We are inviting your comments on the requested changes at this time so that we can consider them as we review the application and prepare a report that will include Development Services staff's recommendation to the City's Planning and Environment Committee. Planning considerations usually include such matters as land use, development intensity, and form of development.

Attend a Community Information Meeting

A community information meeting will be held in your neighbourhood to present this proposal and obtain input from interested members of the public. The meeting has not yet been scheduled, but will be in advance of the Future Public Meeting described below. You will receive a separate notice inviting you to this meeting. The Community Information Meeting is not the public meeting required by the Planning Act and attendance at this meeting does not create a right to appeal the decision of Council to the Local Planning Appeal Tribunal.

Attend a Future Public Participation Meeting

The Planning and Environment Committee will consider the requested Official Plan and zoning changes on a date that has not yet been scheduled. The City will send you another notice inviting you to attend this meeting, which is required by the *Planning Act*. You will also be invited to provide your comments at this public participation meeting. The Planning and Environment Committee will make a recommendation to Council, which will make its decision at a future Council meeting.

What Are Your Legal Rights?

Notification of Council Decision

If you wish to be notified of the decision of the City of London on the proposed official plan amendment and zoning by-law amendment, you must make a written request to the City Clerk, 300 Dufferin Ave., P.O. Box 5035, London, ON, N6A 4L9, or at docservices@london.ca. You

will also be notified if you speak to the Planning and Environment Committee at the public meeting about this application and leave your name and address with the Secretary of the Committee.

Right to Appeal to the Local Planning Appeal Tribunal

If a person or public body would otherwise have an ability to appeal the decision of the Council of the Corporation of the City of London to the Local Planning Appeal Tribunal but the person or public body does not make oral submissions at a public meeting or make written submissions to the City of London before the proposed official plan amendment is adopted, the person or public body is not entitled to appeal the decision.

If a person or public body does not make oral submissions at a public meeting or make written submissions to the City of London before the proposed official plan amendment is adopted, the person or public body may not be added as a party to the hearing of an appeal before the Local Planning Appeal Tribunal unless, in the opinion of the Tribunal, there are reasonable grounds to add the person or public body as a party.

If a person or public body would otherwise have an ability to appeal the decision of the Council of the Corporation of the City of London to the Local Planning Appeal Tribunal but the person or public body does not make oral submissions at a public meeting or make written submissions to the City of London before the by-law is passed, the person or public body is not entitled to appeal the decision.

If a person or public body does not make oral submissions at a public meeting or make written submissions to the City of London before the by-law is passed, the person or public body may not be added as a party to the hearing of an appeal before the Local Planning Appeal Tribunal unless, in the opinion of the Tribunal, there are reasonable grounds to do so.

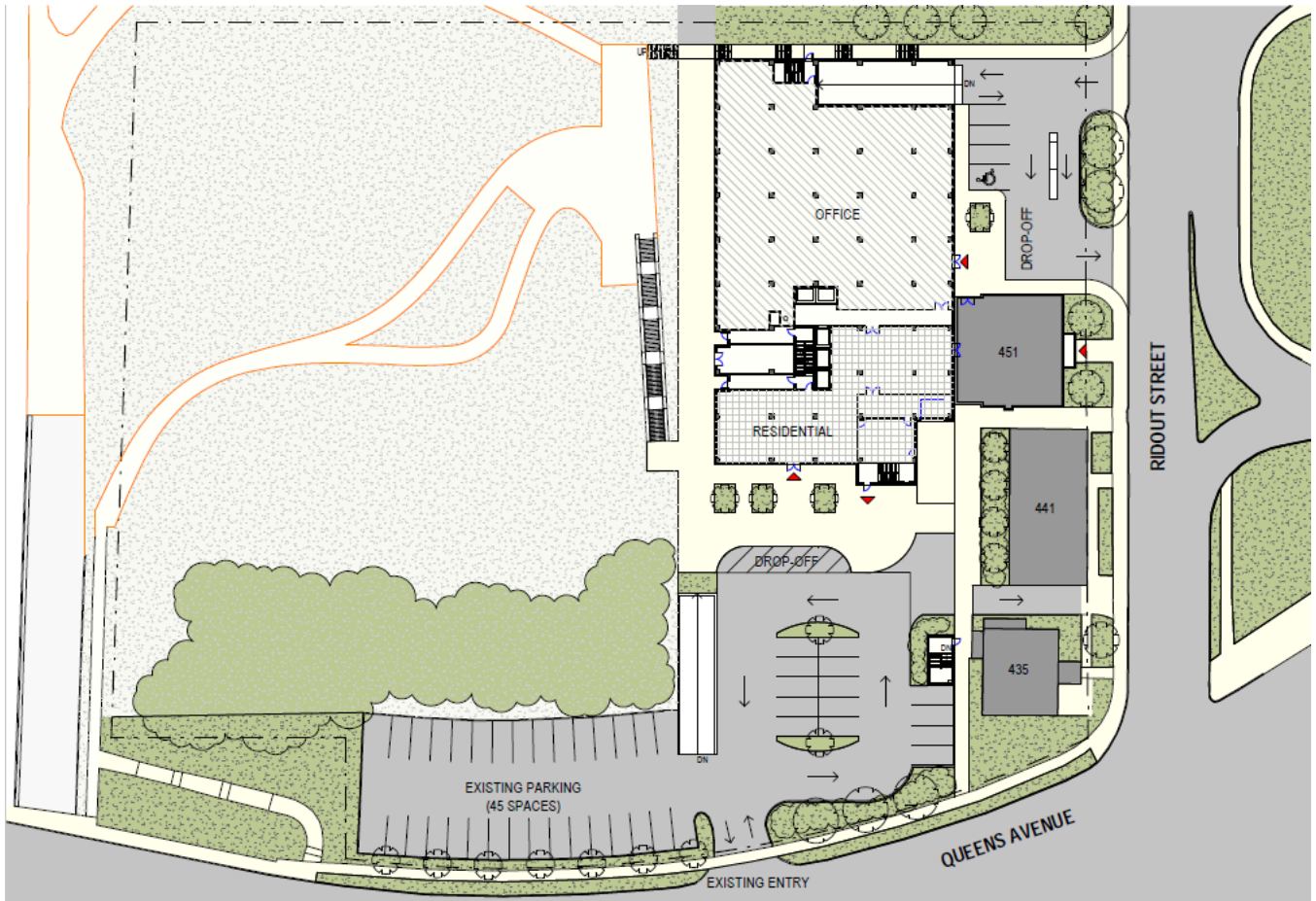
For more information go to <http://elto.gov.on.ca/tribunals/lpat/about-lpat/>.

Notice of Collection of Personal Information

Personal information collected and recorded at the Public Participation Meeting, or through written submissions on this subject, is collected under the authority of the *Municipal Act*, 2001, as amended, and the *Planning Act*, 1990 R.S.O. 1990, c.P.13 and will be used by Members of Council and City of London staff in their consideration of this matter. The written submissions, including names and contact information and the associated reports arising from the public participation process, will be made available to the public, including publishing on the City's website. Video recordings of the Public Participation Meeting may also be posted to the City of London's website. Questions about this collection should be referred to Cathy Saunders, City Clerk, 519-661-CITY(2489) ext. 4937.

Accessibility – Alternative accessible formats or communication supports are available upon request. Please contact accessibility@london.ca or 519-661-CITY(2489) extension 2425 for more information.

Site Concept



Site Concept Plan

Building Renderings



Conceptual Renderings

The above images represent the applicant's proposal as submitted and may change.

Farhi Holdings Corp.

**Heritage Impact Assessment
435-451 Ridout Street North
London, Ontario**

Prepared by:

AECOM

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Statement of Qualifications and Limitations

The attached Report (the “Report”) has been prepared by AECOM Canada Ltd. (“AECOM”) for the benefit of the Client (“Client”) in accordance with the agreement between AECOM and Client, including the scope of work detailed therein (the “Agreement”).

The information, data, recommendations and conclusions contained in the Report (collectively, the “Information”):

- is subject to the scope, schedule, and other constraints and limitations in the Agreement and the qualifications contained in the Report (the “Limitations”);
- represents AECOM’s professional judgement in light of the Limitations and industry standards for the preparation of similar reports;
- may be based on information provided to AECOM which has not been independently verified;
- has not been updated since the date of issuance of the Report and its accuracy is limited to the time period and circumstances in which it was collected, processed, made or issued;
- must be read as a whole and sections thereof should not be read out of such context;
- was prepared for the specific purposes described in the Report and the Agreement; and
- in the case of subsurface, environmental or geotechnical conditions, may be based on limited testing and on the assumption that such conditions are uniform and not variable either geographically or over time.

AECOM shall be entitled to rely upon the accuracy and completeness of information that was provided to it and has no obligation to update such information. AECOM accepts no responsibility for any events or circumstances that may have occurred since the date on which the Report was prepared and, in the case of subsurface, environmental or geotechnical conditions, is not responsible for any variability in such conditions, geographically or over time.

AECOM agrees that the Report represents its professional judgement as described above and that the Information has been prepared for the specific purpose and use described in the Report and the Agreement, but AECOM makes no other representations, or any guarantees or warranties whatsoever, whether express or implied, with respect to the Report, the Information or any part thereof.

Without in any way limiting the generality of the foregoing, any estimates or opinions regarding probable construction costs or construction schedule provided by AECOM represent AECOM’s professional judgement in light of its experience and the knowledge and information available to it at the time of preparation. Since AECOM has no control over market or economic conditions, prices for construction labour, equipment or materials or bidding procedures, AECOM, its directors, officers and employees are not able to, nor do they, make any representations, warranties or guarantees whatsoever, whether express or implied, with respect to such estimates or opinions, or their variance from actual construction costs or schedules, and accept no responsibility for any loss or damage arising therefrom or in any way related thereto. Persons relying on such estimates or opinions do so at their own risk.

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This Statement of Qualifications and Limitations is attached to and forms part of the Report and any use of the Report is subject to the terms hereof.

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Signatures

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Revision History

Revision #	Date	Revised By:	Revision Description
0	Dec 21, 2018	M. Greguol	Draft Submission to Farhi Holdings Corp.
1	Oct 31, 2019	L. Smythe	Revisions per city's request
2	Nov. 28, 2019	A Grant	Revisions per city's request

Executive Summary

AECOM Canada Ltd. (AECOM) was contracted by Farhi Holdings Corp. to conduct a Heritage Impact Assessment (HIA) for a proposed development located at 435-451 Ridout Street North, in the City of London, Ontario. The study area consists of a single property that is comprised of several existing buildings and parking lots, as well as areas of manicured lawn located on historic Lot 16, Concession 1, in the Geographic Township of London, Middlesex County, now the City of London, Ontario (Figures 1 and 2). The property is now situated within its downtown context, adjacent to the Thames River.

The proposed development consists of a 4-storey parking structure along Queens Avenue, and a 40-storey apartment building with 5 floors of hotel to the north of the site along Ridout Street North, as one comprehensive development proposal. The proposed development would maintain the 3 existing heritage structures located on the property. The buildings are all located within a singular property, but for descriptive purposes, are described within this report as 435, 441, and 451 Ridout Street North. The property at 435-451 Ridout Street North includes various levels of heritage recognition and protection related to the 3 existing structures on the property (See Section 3 of this report). As a result, and as an outcome of the Pre-Application Consultation process, the City of London requires completion of an HIA as a part of the proposed development application.

The history of the subject properties is thoroughly documented in each of the respective designations and commemorations for the site. In addition, given that the buildings included on this property are some of the city's earliest landmarks, their role in London's history has been well-documented in published and un-published local histories. In order to avoid redundancy in reporting, the site histories described in the relevant designations and commemorations below were considered sufficient historic context for understanding for site's history as it relates to this report.

The subject property is designated under Part IV of the *Ontario Heritage Act*, and is also designated under Part V of the *Ontario Heritage Act* as a part of the Downtown London Heritage Conservation District. Lastly, the property is identified as a National Historic Site of Canada.

In order to mitigate the potential direct and indirect impacts to identified cultural heritage value described within this report, the mitigation strategies described in this report should be considered in further project refinements and approaches. Details related to the exterior design, the streetscape character, and the future re-use of the heritage structures should be considered in depth as a part of the proposed project in order to mitigate impacts, and conserve the cultural heritage value of the property.

Upon further design refinement, this HIA should be updated in order to capture any design alterations or changes that have been made to the proposed site plan or tower design to reflect the heritage conservation efforts as a part of the proposed development. Continued consultation with City of London Heritage Planning staff is encouraged as a follow-up step in order to ensure that the significant heritage attributes and cultural heritage value of the property will be conserved as a part of the proposed development. Due to the extensive cultural heritage value of this property, collaboration with Heritage Planning staff will ensure that the mitigation strategies will be appropriately identified and undertaken in order to preserve and enhance the heritage value of the site.

Table of Contents

	Page
1. Introduction	1
1.1 Study Purpose	1
1.2 Study Method.....	1
1.3 Planning Act and Provincial Policy Statement.....	2
1.4 Ontario Heritage Act	2
1.5 City of London Policies	2
1.5.1 The London Plan.....	2
1.5.2 Inventory/Register	3
1.5.3 Downtown Heritage Conservation District	3
Blackfriars-Petersville Heritage Conservation District	4
1.5.4 Canadian Register of Historic Places	4
1.5.5 National Historic Sites	4
1.5.6 Canadian Heritage Rivers System.....	4
2. Historical Research, Site Analysis, and Evaluation.....	5
2.1 Existing Conditions	5
2.2 Site History	6
2.2.1 Ridout Street Complex	6
2.2.2 Eldon House and Harris Park	6
2.2.3 Thames River	6
2.3 Ridout Street Complex.....	7
2.3.1 City of London – Part IV Designation.....	7
Historical and Architectural Reasons.....	7
435 Ridout Street – Georgian (c.1838).....	7
441-447 Ridout Street – Georgian (c.1847)	8
451 Ridout Street – Victorian Eclectic (c.1850).....	8
2.3.2 National Historic Site of Canada – Statement of Significance.....	9
Description of Historic Place.....	9
Heritage Value	9
Character-Defining Elements.....	9
2.4 Eldon House – 481 Ridout St. North	10
Description of Historic Place.....	10
Heritage Value	11
Character-Defining Elements.....	11
2.5 Register of Cultural Heritage Resources.....	11
2.6 Downtown Heritage Conservation District.....	13
2.6.1 Blackfriars-Petersville Heritage Conservation District.....	14
2.7 Thames River	14
2.8 Heritage Integrity	15
3. Proposed Undertaking.....	24
3.1 Project Description.....	24
3.2 Approach	25

3.3	Planning Justification	25
4.	Measurement of Development or Site Alteration Impact.....	30
4.1	Heritage Policy Evaluation (PPS, London Official Plan, and Downtown Heritage Conservation District Plan).....	30
4.2	Potential Impacts on Heritage Attributes	35
4.2.1	Ridout Street Complex	35
4.2.2	Eldon House.....	37
4.2.3	Downtown Heritage Conservation District	37
4.3	Shadow Impacts	39
4.4	Views and Vistas	42
4.5	Impacts on the Thames River Corridor and “The Forks”	42
4.6	Construction Related Impacts	44
4.7	Record of Consultation	45
5.	Consideration of Alternatives, Mitigation and Conservation Methods	45
5.1	Additional Studies Required.....	49
5.2	Mitigation Strategies	50
5.2.1	Vibration.....	50
5.2.2	Mature Vegetation.....	50
5.2.3	Existing Ridout Block Buildings	51
5.3	Heritage Impact Assessment Update.....	51
6.	Summary Statement of Conservation Recommendations	51
7.	Closure.....	52
8.	Sources.....	53
9.	Figures.....	55
10.	Resumes.....	61

List of Figures

Figure 1: Study Area	56
Figure 2: Aerial.....	57
Figure 3: Heritage Districts	58
Figure 4: Heritage Features	59
Figure 5: Architects Tillman Ruth Robinson Site Plan.....	60

List of Tables

Table 1: Adjacent Heritage Properties	12
Table 2: Downtown HCD Classification	13

Table 3: Evaluation of Policies Against Development 30

Table 4: Potential Direct Impacts and Relevance to the Project 35

Table 5: Potential Indirect Impacts and Relevance to the Project..... 36

Table 6: Potential indirect impacts and their relevance to the project 37

Table 7: Potential Direct Impacts to the Downtown HCD 37

Table 8: Potential Indirect Impacts to the Downtown HCD..... 38

Table 9: Shadow Impacts 39

Table 10: Impacts to the Thames River Corridor and Forks..... 43

Table 11: Mitigation Measures 46

Appendices

Appendix A. Planning Justification Report 435, 441 and 451Ridout Street North, July 2019.

1. Introduction

1.1 Study Purpose

AECOM Canada Ltd. (AECOM) was contracted by Farhi Holdings Corp. to conduct a Heritage Impact Assessment (HIA) for a proposed development located at 435-451 Ridout Street North, in the City of London, Ontario. The study area consists of a single property that is comprised of several existing buildings and parking lots, as well as areas of manicured lawn located on historic Lot 16, Concession 1, in the Geographic Township of London, Middlesex County, now the City of London, Ontario (Figures 1 and 2). The property is now situated within its downtown context, adjacent to the Thames River.

The proposed development consists of a 40-storey apartment building with 5 floors of hotel to the north of the site along Ridout Street North, as one comprehensive development proposal. The proposed development would maintain the 3 existing heritage structures located on the property. The buildings are all located within a singular property, but for descriptive purposes, are described within this report as 435, 441, and 451 Ridout Street North. The property at 435-451 Ridout Street North includes various levels of heritage recognition and protection related to the 3 existing structures on the property (See Section 3 of this report). As a result, and as an outcome of the Pre-Application Consultation process, the City of London requires completion of an HIA as a part of the proposed development application.

1.2 Study Method

The City of London does not have a specified Terms of Reference for the preparation of HIAs. As a result, the general tasks and processes identified in relevant Ministry of Tourism, Culture, and Sport (MTCS), and Provincial Policy Statement (PPS) guidance documents have been utilized in the preparation of this report.

This HIA was prepared according to the guidelines set out in the MTCS *Heritage Resources in the Land Use Planning Process* document included as a part of the *Ontario Heritage Toolkit*. For the purposes of this report, AECOM undertook the following tasks:

- 1) Review of appropriate legislative and planning framework;
- 2) Preparation of a historical context overview for the project area, including a review of the Downtown Heritage Conservation District Study and Plan, and review of the designating by-laws and recognitions for the properties described within this report;
- 3) A site investigation, undertaken on December 13, 2018, to document the existing conditions of the properties and their heritage attributes;
- 4) Identification and description of the proposed undertaking;
- 5) Assessment of impacts to identified cultural heritage value and heritage attributes;
- 6) Identification of potential mitigation strategies and preparation of recommendations to ensure the conservation of identified cultural heritage value.

1.3 Planning Act and Provincial Policy Statement

The *Planning Act* (1990) and the associated *Provincial Policy Statement* (2014) provide a legislative framework for land use planning in Ontario. Both documents identify matters of provincial interest, which include the conservation of significant features of architectural, cultural, historical, archaeological, or scientific interest. The *Planning Act* requires that all decisions affecting land use planning matters “shall be consistent with” the Provincial Policy Statement (PPS). In general, the PPS recognizes that Ontario’s long-term prosperity, environmental health, and social well-being depend on protecting natural heritage, water, agricultural, mineral, cultural heritage, and archaeological resources for their economic, environmental, and social benefits.

Section 2 of the *Planning Act* makes a series of provisions regarding cultural heritage. Section 2 of the *Planning Act* identifies various provincial interests that must be considered by the relevant authorities during the planning process. Specific to cultural heritage, Subsection 2(d) of the *Planning Act* states that, “The Minister, the council of a municipality, a local board, a planning board and the Municipal Board, in carrying out their responsibilities under this Act, shall have regard to, among other matters, matter of provincial interest such as...the conservation of features of significant architectural, cultural, historical, archaeological or scientific interest.”

As one of 18 interests to be considered, cultural heritage resources are to be considered within the framework of varying provincial interests throughout the land use planning process.

Pursuant to Section 3 of the *Planning Act* the PPS 2014, Policy 2.6.1 states, “*Significant built heritage resources and significant cultural heritage landscapes shall be conserved.*”

1.4 Ontario Heritage Act

The *Ontario Heritage Act* enables municipalities and the province to designate individual properties and/or districts as being of cultural heritage value or interest. The province or municipality may also “list” a property, or include a property on a municipal register, that has not been designated but is believed to be of cultural heritage value or interest. *Ontario Regulation 9/06, Criteria for Determining Cultural Heritage Value or Interest* (O. Reg. 9/06) under the *Ontario Heritage Act* provides criteria for determining cultural heritage value or interest. If a property meets one or more of the following criteria it may be designated under Section 29 of the *Ontario Heritage Act*.

1.5 City of London Policies

1.5.1 The London Plan

The *London Plan* is the City’s new Official Plan. The *London Plan* sets out a new approach for planning in London which emphasizes growing inward and upward, so that the City can reduce the costs of growth, create walkable communities, revitalize urban neighbourhoods and business areas, protect farmlands, and reduce greenhouse gases and energy consumption. The plan sets out to conserve the City’s cultural heritage and protect environmental areas, hazard lands, and natural resources. The plan has been approved by the Ontario Ministry of Municipal Affairs.

Specifically related to heritage conservation, the *London Plan* outlines a number of policies related to the conservation of cultural heritage resources within the city. The General Cultural Heritage Policies related to Design note:

New development, redevelopment, and all civic works and projects on and adjacent to heritage designated properties and properties listed on the Register will be designed to protect the heritage attributes and character of those resources, to minimize visual and physical impact on these resources. A heritage impact assessment will be required for new development on and adjacent to heritage designated properties and properties listed on the Register to assess potential impacts, and explore alternative development approaches and mitigation measures to address any impact to the cultural heritage resource and its heritage attributes.

1.5.2 Inventory/Register

The City of London's Inventory of Heritage Resources (the Register) (2006) was adopted as the Register pursuant to Section 27 of the Ontario Heritage Act by Municipal Council on March 26, 2007. It includes information related to the listing of properties in London of recognized or potential cultural heritage value or interest. The Inventory (Register) includes a priority level system for identifying properties of greater priority and/or significance for heritage recognition. In addition, properties designated under the Ontario Heritage Act are maintained on the City's Inventory (the Register). The Inventory (Register) is a living document subject to changes and approvals by Council, advised by the London Advisory Committee on Heritage (LACH).

All three of the buildings are listed on the Register; however are also individually designated under Part IV of the *Ontario Heritage Act*. The properties are partially described within designating by-law L.S.P.-3330-152, registered July 5, 2001 as:

- 435 Ridout Street North; c.1836, style, Georgian; earliest commercial building in the City of London; Bank of Upper Canada-all restored elements including portico and fanlight over entryway;
- 441 Ridout Street North; c.1847; style, Georgian; Labatt Restoration-all restored elements including door and carriageway;
- 451 Ridout Street North; c.1855; style, Eclectic; Anderson House-rebuilt structure, all restored.

1.5.3 Downtown Heritage Conservation District

Heritage Conservation Districts are designated under Part V of the *Ontario Heritage Act* for the purpose of conserving cultural heritage values. The emphasis in a Heritage Conservation District is on the collective character of the overall area, as defined by its historic context, architecture, streets, landscape and other physical and visual features. HCDs are established to facilitate the preservation of a defined area with the intent of retaining critical functional and visual attributes that convey or have a historical relationship to the area in which they are situated. This includes buildings, natural and cultural landscapes, streetscapes, hardscape elements, and other features that contribute to an area's recognizable character.¹

The subject property is located within the boundaries of the Downtown Heritage Conservation District. The buildings which comprise the Downtown HCD each relate to one of five stages through which the Downtown evolved from its founding to the recent past. In the nineteenth and twentieth centuries the Downtown was the region's commercial, industrial, and service centre. There are approximately 400 properties in the Downtown HCD. The Downtown HCD was prepared by Stantec, in association with SJMA Architecture Inc., Michael Baker, and Sylvia Behr in March 2012,

¹ *Downtown London Heritage Conservation District Plan, 1.3.*

and was adopted by Municipal Council on April 11, 2012, and designated under Part V of the *Ontario Heritage Act* on June 18, 2013.

Blackfriars-Petersville Heritage Conservation District

The subject lands are also located in the vicinity of the Blackfriars-Petersville Heritage Conservation District, which can be viewed from the subject lands directly across the Thames River.

The Blackfriars-Petersville HCD was prepared by Golder Associates in association with the GSP Group, Tausky Heritage Consultants, and IBI Group and was adopted by Council on May 6, 2014 and designated under Part V of the Ontario Heritage Act on May 15, 2015.

1.5.4 Canadian Register of Historic Places

The Canadian Register of Historic Places (CRHP) is a list that formally recognizes the heritage value of historic properties by local, provincial, territorial and/or federal authorities. Federal, Provincial, and Territorial governments recognize the contribution historic places make to Canadian communities. The CRHP provides a single source of information for historic places recognized for their heritage value at the local, provincial, territorial, and national levels throughout Canada. The three buildings that are the subject of this HIA are included on the CRHP, listed as the "Ridout Street Complex National Historic Site of Canada".

1.5.5 National Historic Sites

National Historic Sites are places of profound importance to Canada. Through various listings, designations, and programs, Parks Canada maintains a list of sites throughout Canada that have been identified by the Historic Sites and Monuments Board of Canada (HSMBC) for their significance to Canadian history. The HSMBC advises the Government of Canada on the identification and commemoration of nationally significant aspects of Canada's history. Following thorough evaluations of a site, the Board and Minister of the Environment can declare a site, event, or person of national historic significance.

The subject property has been recognized as a National Historic Site of Canada, known as the Ridout Street Complex.

1.5.6 Canadian Heritage Rivers System

Located to the west of the subject property, the Thames River has been designated as a Canadian Heritage River. The Canadian Heritage Rivers System (CHRS) is a federal-provincial-territorial government program which gives national recognition to outstanding Canadian rivers, encourages their long-term management to conserve their natural, cultural, and recreational values. The CHRS program works with community-level river stewardship groups to promote and conserve these values. All conservation efforts on Canadian Heritage Rivers are voluntary, and dependant on existing laws and regulations.

Proponents of river nominations work directly with their provincial and territorial governments to research and prepare documents that are part of the nomination and designation process. The final step in the designation process is the approval of the relevant provincial or territorial Minister, and the approval of the Minister responsible for Parks Canada (the Minister of Environment and Climate Change). Founded in 1984, the CHRS currently has thirty-nine designated rivers, and one nominated river totalling over 10,000 kilometres of waterways.

2. Historical Research, Site Analysis, and Evaluation

2.1 Existing Conditions

The subject property consists of an irregularly shaped lot, approximately 3.3 acres in size, and is located on the west side of Ridout Street North just north of its intersection with Queens Avenue. The property includes the three heritage buildings described above, as well as existing surface parking lots located on the north, south, and west sides of the property. Immediately to the front of the heritage structures, Ridout Street North is a paved two-lane road with a left turning lane approaching Queens Avenue. Concrete sidewalks are located along the roadway, and concrete paving tiles are arranged immediately in front of the structures. Plantings, gardens, street trees, and heritage-influenced street lighting have also been installed in front of the structures.

The curvature of Queens Avenue immediately east and south of the subject property is a result of a significant road realignment that took place in the mid-1970s. Prior to then, Queens Avenue terminated at Ridout Street. As a part of a much larger plan to ease traffic in downtown London, beginning in 1949, the realignment of Queens Avenue was eventually approved by the City's traffic committee in 1970 after over twenty years of debate and property negotiations. Subsequently, the road was extended south of the subject property and carried over the Thames River on The Queen's Bridge, which was constructed in 1973.²

The subject property can be accessed from Ridout Street North, and from the surface parking lot accessed on the north side of Queens Avenue, as well as from the surface parking lot located west of the buildings. This parking lot can only be accessed by Harris Park Gate (which is further accessed by Dundas Street), or by Harris Park Entrance. A substantial topographical grade change is located between the east end of the property, where the buildings are located, and the west parking lot to the rear of the property. The west parking lot also includes a rear entrance to 451 Ridout Street North, through a much more modern three-storey addition that has been built onto the rear of the heritage structure. This addition has been constructed primarily along the topographically lower portion of the property and remains relatively hidden from street view along Ridout Street North, preserving an unobstructed view of the three heritage structures.

The subject property is also surrounded by properties and buildings of varying use and age. Immediately to the north, the property shares a boundary with 481 Ridout Street North, otherwise known as Eldon House, which includes a c.1834 residential dwelling, one of the oldest houses in London. Eldon House is surrounded by a naturalized landscape. Further north and west, the floodplains of the Thames River also include Harris Park, a public space, and the paved Thames Valley Parkway pedestrian/cyclist trail. South of the subject property, the Museum London building (421 Ridout Street North) is located between Queens Avenue and Dundas Street, and the Ontario Court of Justice building is located at 80 Dundas Street, on the southeast corner of Queens Avenue and Ridout Street North. Together, the Museum London property and the Ontario Court of Justice property provide a stark juxtaposition between the historic subject property and the much newer buildings on these properties. Within a broader context, the Middlesex Courthouse and Gaol, located at 399 Ridout Street North, is located immediately south of Museum London and includes more significant heritage structures. Collectively, the property and its surrounding context consist of a variety of historic and more modern forms of architecture.

² Footnote from LFP articles – see Queens Avenue article

2.2 Site History

2.2.1 Ridout Street Complex

The original town plot for London was laid out in 1826, and over time the town plot and surrounding downtown core have become a densely built-up area, containing structures and streetscapes which date back to the first half of the nineteenth-century. Prior to the 1850s, Ridout Street served as London's main street. Most traffic entering what is now the downtown core, did so from the north and west, by means of the Blackfriars and Westminster bridges. Between the 1830s and 1850s, the area around Ridout Street established itself as London's first financial district. Lured by the large amounts of money being spent by the soldiers of the local Garrison, many banks headquartered in Toronto and Hamilton set up branch offices in London. The building located at 435 Ridout Street is an example of one of these early bank buildings, having been constructed to house the Bank of Upper Canada circa 1838.

2.2.2 Eldon House and Harris Park

Eldon house is the oldest residence in the City of London. Now operated as a house museum, Eldon House was constructed in 1834 by Captain John Harris of the Royal navy, and named for Lord Eldon, the Lord Chancellor of England during the nineteenth century. Captain Harris was an admirer of Eldon and considered Eldon and himself to be "self-made" men. Captain Harris and his wife Amelia raised their family of ten at Eldon House; John Harris passed away in 1850. By the 1860s, only Captain Harris's son Edward resided in the house with his mother, and wife Sophia. Edward and Sophia decided to build a large addition to the house in the late 1870s; this included a formal dining room, a new kitchen, and more space for servants. A decade later, Edward faced financial difficulties and sold the property to his brother George. The house remained in the family for another two generations. George's wife Lucy received a large inheritance from her family in England, and the couple were able to make a number of improvements to the house and the grounds. These included the rebuilding of the terrace, and the introduction of electric lighting in 1896. The last family member to reside in the house was Amelia "Milly" Harris, who passed away in December of 1959. In January 1960, the house and property were donated to the City of London to be used as a museum by Milly's children. The house was opened as a museum in 1961.

Originally, the Harris property extended all the way to the banks of the Thames River. When the property was donated to the City of London, the flat land at the bottom of the hill beside the river was landscaped and became Harris Park, named for its original landowners.

2.2.3 Thames River

The Thames flows west 273 kilometres (170 mi)^[3] through southwestern Ontario, from the Town of Tavistock westward through the cities of Woodstock, London and Chatham to Lighthouse Cove on Lake St. Clair. Its drainage basin is 5,825 square kilometres (2,249 sq mi).^[3]

Known as Deshkan Ziibi ("Antler River") in Anishnaabemowin, the Ojibwe language spoken by Anishnaabe people, who together with the Iroquoian Neutrals have lived in the area since before Europeans arrived. In 1793, the river was renamed after the River Thames in England by Lieutenant Governor John Graves Simcoe.^[5]

Ontario's Thames River, is one of the most southern Canadian water courses, and has exerted a strong influence on both the natural features and the settled landscape within its watershed.

The Thames is a relatively small river on a Canadian scale but has unique natural heritage features. The Thames was one of the first rivers to form following the retreat of the Wisconsin Glacier from Ontario and the upper reaches still flow through these ancient spillways. The lower river reach, with its shallower gradient, emerged after thousands of years as a glacial lake.

With respect to its human heritage, the Thames River has provided the setting for 11,000 years of significant Indigenous and European settlement. The Thames, with its abundant fish and game, provided a focus for each group in the sequence of Indigenous peoples, including those who were the first to practise agriculture in Canada between 500 and 1650 A.D. In the 1700s, the river attracted French fur traders and European settlers, while still being used by Indigenous groups. During his visit to the area in 1793 Lieutenant Governor John Graves Simcoe designated the forks of the Thames River as the future site of the capital of Upper Canada. Although the initial plan to have London become the capital of Upper Canada did not come to fruition, it was chosen to become the seat of Upper Canada's London District.

Following the role of the Thames valley as a major theatre in the War of 1812, which included many battles and Tecumseh's death in the Battle of the Thames at Moraviantown, pioneer settlement within the watershed developed into Canada's first successful commercial agrarian society based on wheat. In turn, many of the numerous watermill sites provided the basis for industrial and urban development, including the major riverine cities, notably the early river port and ship building centre of Chatham, as well as London, Stratford and Woodstock, and a number of smaller towns and villages. Agriculture flourished during this time and the watershed has continued as the most prosperous farming landscape in Canada.

2.3 Ridout Street Complex

2.3.1 City of London – Part IV Designation

The subject property is designated under Part IV of the *Ontario Heritage Act*. The reasons for designation are described in By-law No. L.S.P.-3330-152 and are included below to inform this HIA.

Historical and Architectural Reasons

The buildings at 435 Ridout Street and 441-447 Ridout Street are the oldest commercial structures in the City. They are excellent examples of Georgian Commercial architecture. With 451 Ridout Street built for a residence they provide an important link between London's oldest home – Eldon house – and the old Middlesex Court House, now the Middlesex County building, the oldest building in the city.

435 Ridout Street – Georgian (c.1838)

This is the earliest commercial building in the City of London. This two-storey, white brick building rests on a stone foundation. It has parapet gables and an Adamesque doorway with side and upper fan lights and a classical porch.

Built in 1838, this building housed the bank of Upper Canada until its demise in 1866. James Hamilton managed the bank from 1856 to 1865 and then bought the building as a residence where he resided until his death in 1896.

Born in 1810 in London, England, Hamilton was the son of a British army officer. The family came to Canada in 1820. Hamilton started his career as a teller at the Bank of Upper Canada in Toronto before moving to London, Ontario.

James Hamilton was a father of four, a member of the St. Paul's Cathedral, and a Secretary-Treasurer of the Proof Line Road Company. This was the company that built the toll road between London and Lucan. However, Hamilton may be best known as a painter. Although he was more of a "Sunday painter" for much of his life, he devoted considerable time to art during his retirement. His earlier watercolours and oils depict the landscape of Toronto and area. Later works, generally in oil, portray London, Ontario and are an important source of information on London's early period. The J.J. Talman Regional Collection at the D.B. Weldon Library, University of Western Ontario and the London Regional Art and Historical Museums each have several examples of his work in their collections.

In 1970, the John Labatt Company carried out a thorough restoration and renovation of the then very dilapidated buildings. In the interest of structural solidity, the buildings were all placed on new foundations and the old floor structures were replaced by reinforced concrete. These alterations required the removal of the cooking fireplace in the basement of 435 Ridout Street North, though the andirons were retained in the collection of the London Historical Museum. Otherwise, an effort was made to restore both the exteriors and the interiors of the building, retaining the porches and the interior architectural fittings that the buildings had at the time of the restoration. Because the brick work at 451 Ridout Street North was badly deteriorated, the bricks were entirely removed on that building, and the walls rebuilt from the ground up. In keeping with the aim of restoring as much of the original material as possible, however, the outside faces of the building were almost entirely recovered with original bricks, though many were cut in half to achieve this result.

441-447 Ridout Street – Georgian (c.1847)

Actually two buildings, one with a central carriageway, this three storey, white brick structure had corbelled parapet walls. It also has a cornice with dentil work and doors with transoms.

The building was built by Dr. Alexander Anderson in c.1847. It was a home to both the Gore Bank and the Commercial Bank of Canada by the mid 1840's. It was also a popular residence for barristers like Richard Bayly and Thomas Westcott because of its proximity to the court house. The latter's daughters, the Misses Westcott, operated a private school in the structure between 1887 and 1919.

451 Ridout Street – Victorian Eclectic (c.1850)

The mansion, which initially held 23 rooms, has a Georgian arched doorway with side lights and a transom at its centre. On the second floor there is a narrower central entrance to the roof of the doorway porch. The building is three storeys in height, with mullioned windows on the third floor. The foundation is stone and the exterior walls are brick.

Dr. Alexander Anderson also built this building and used it as his own residence. He called it, "Wilmington House". He lived and practised medicine there until his death in 1873. His widow resided there until past the turn of the century.

Dr. Anderson was a surgeon in the Royal Navy before coming to Canada. He opened an office in London in December of 1835. Dr. Anderson was paradoxical mixture of the modern and the old fashioned. He was the first local doctor to be trained in both the arts and sciences, and many prominent local doctors did preliminary training under him. A notable example was John McLeay of Lobo. McLeay was with Sir John Franklin expeditions of 1819 and 1829. It was Dr. Anderson's wife who was convinced Anderson not to join up with ill-fated trip of 1845. Nonetheless, Dr. Anderson was certainly not a progressive in other matters. He reportedly disliked surgery and still employed bleeding as a technique. There is a record of his bleeding a son of Reverend William Proudfoot on November 14, 1836.

Dr. Anderson was equally divided in matters of religion. His father was a Presbyterian and his mother a Roman Catholic. Anderson was brought up Presbyterian but later converted to Catholicism. During his final illness, he had a change of heart when he realized that his Protestant wife would not be able to be buried beside him when she died. This upset the priest attending Dr. Anderson. In the end, a compromise was reached; a mass was said in St. Peter's Cathedral, and the internment followed by a Protestant cemetery. Bells tolled from the Episcopal and Roman Catholic churches, and all business in the City was suspended during the service.

After Mrs. Anderson's death, the house had a variety of uses. The Knights of Columbus renamed it Branden Hall when they acquired it in 1914. In 1917, a monastery was set up by the Sisters of the Precious Blood. Between 1920 and 1952, the Brothers of the Christian Schools of Ontario provided Catholic instruction at the high school level there.

Briefly it was again a single family dwelling. Then it was a Royal Canadian Legion Branch, and later the John Labatt Limited head office.

2.3.2 National Historic Site of Canada – Statement of Significance

The subject property has been recognized as a National Historic Site of Canada, known as the Ridout Street Complex. The following sub-sections include the Statement of Significance that was developed following the HSMBC evaluation of the property as a National Historic Site.

Description of Historic Place

The Ridout Street Complex National Historic Site of Canada is located in downtown London, Ontario, at the confluence of the north and south branches of the River Thames. The complex is comprised of a row of three distinguished, mid-19th-century residential and commercial buildings: the Anderson Residence, the Bank of Upper Canada building and the Gore Bank of Canada building. Their forms, materials, and details provide unity to the group, while their individual treatments and separate structure provide a varied streetscape. The designation refers to three buildings at 435, 44, and 451 Ridout Street North and their footprints as they existed at the time of designation (1966).

Heritage Value

The Ridout Street Complex was designated a National Historic Site of Canada in 1966 because:

- It provides a capsule view of the appearance of mid-19th century Ontario cities; and,
- It includes several of London's earliest residential and commercial building.

The heritage value of this site resides in the grouping of these representative examples of mid-19th-century urban architecture in southwestern Ontario. Ridout Street North was London's first financial district, where leaders in the fields of law, medicine, and finance established offices and homes. The street came to be known as "Banker's Row" after the establishment of the head offices of five banks, which were later turned into residences or business premises. The conservative classicized forms and the use of locally made buff-coloured brick are typical of the buildings in this area during the late-19th century.

Character-Defining Elements

Key elements contributing to the heritage value of the site include:

- Its location in downtown London, Ontario near the River Thames;
- The spatial relationship of the individual buildings, close to one another and to the street;
- The classically-inspired design evident in the refined proportions, rectangular massing, symmetry and restrained detailing of all three buildings;
- The brick and timber construction;
- The consistent exterior use of local buff-coloured brick;
- The symmetrically arranged facades with regular placement of multi-paned, double-hung windows; and
- Viewscapes on the complex as a whole from surrounding streets.

Key elements contributing to the heritage value of the Anderson Residence (No. 451) in the Ridout Complex include:

- Its monumental rectangular three-storey massing under a flat roof with a slight pitch to the rear and four end chimneys;
- Its Tudor-arched central doorway with sidelights, transom and paired double-hung windows framed by a brick surround, under a projecting portico;
- Its range of segmental headed windows on the third floor exterior surmounted by a decorative brick frieze;
- Any evidence of its original interior plan;
- Any surviving original interior detailing, furnishing and fittings, including: shutters, door and window trim, ceiling mouldings, cornices, mantelpieces, fireplaces, columns and brackets.

Key elements contributing to heritage value of the Gore Bank (No. 441) in the Ridout Complex include:

- Its three-storey massing as one integral structure split along the line of a carriageway approximately one third of the distance from the south end;
- The semi-elliptical carriageway entrance and the double hung windows corresponding with the openings adjacent to the carriageway;
- The placement and integrity of the chimneys, set above the bold cornice;
- Any surviving original interior features, including: mouldings, pilasters, panelled doors, balustrades, railings.

Key elements contributing to the heritage value of The Bank of Upper Canada (No. 435) in the Ridout Complex include:

- Its two-storey rectangular massing under a low-pitched gable roof with two end chimneys, plainly detailed eaves and side facades forming parapets;
- Its six-panelled central doorway with sidelights and an elaborate semi-elliptical fanlight; and,
- Any surviving original interior features that are associated with the heritage value of the complex, including quirk trim and corner blocks, mantelpieces, staircases, and fireplaces.

2.4 Eldon House – 481 Ridout St. North

Located at 481 Ridout Street (L.S.P.-2329-578; L.S.P.-3419-124;) and known as the Eldon House, the house was built-in 1834 and is designated under Section 29, Part IV of the Ontario Heritage Act, as well as listed on the Canadian Register of Historic Places.

Description of Historic Place

Eldon House is located at 481 Ridout Street North. This home is situated on the west side of Ridout Street and northwest of the termination of Fullarton Street. It is also on the southeast portion of Harris Park in the City of London. The property consists of a two-storey wood sided main house, constructed in 1834, as well as a coach house, a greenhouse and beautifully-landscaped grounds. The property also includes a one-storey wood clad interpretive centre built in 2003 that is not included in the designation.

The property was designated by the City of London in 1977 for its historic and/or architectural value or interest under Part IV of the Ontario Heritage Act (By-law L.S.P. – 2329-578).

Heritage Value

The beautifully landscaped Eldon House located on the southeast portion of Harris Park, is strategically situated within the area of the Forks of the Thames and illustrative of a grand estate.

Considered to be the most important heritage property in London, the first owner of Eldon House, John Harris, was the Treasurer of London District and a leading political figure in the local Family Compact. After Harris' death, his wife Amelia Ryerse, who was from the same prominent Ontario family as the Reverend Egerton Ryerson, the province's first Minister of Education, continued to occupy the home. Other significant owners were George Becher Harris, an important London lawyer, followed by his daughter, Amelia Archange Harris, who was the final inhabitant of the home before it was donated to the City of London in 1961. The residents who lived in Eldon House contributed significantly to its history through their individual personal and public achievements. The interior of the house retains original furnishings and artifacts used by the Harris family.

The Eldon House property is comprised of four buildings – a main house, a coach house, a greenhouse and a recently constructed Interpretive Centre. The main house was constructed in 1834 with additions made in 1878 and a series of extensive renovations followed. The Regency style main house is representative of the wealth and prominence of its owners the Harris family. Notable features of this style include the structure's overall symmetry, the enclosed verandah that spans the facade, the four brick chimneys and the bevelled wood cladding.

Sources: City of London, By-law L.S.P. – 2329-578;
Reasons for Designation, Heritage Property Manual, 1977.

Character-Defining Elements

Character defining elements that contribute to the heritage value include the:

- bevelled wood siding construction
- verandah which spans the façade
- three shuttered windows on the second-storey of the façade
- enclosed brick chimneys, including the three over the main building block and the one above the north wing
- 6 over 6 window in garage gable end
- round windows in the garage
- greenhouse, coach house
- siting of the property on the southeast corner of Harris Park
- grandeur of the estate, emphasized by landscaping and landscape features which include an arbour and fencing³

2.5 Register of Cultural Heritage Resources

The London Register of Cultural Heritage Resources evolved out of the first inventory of heritage resources which was adopted by Council in 1991. The inventory was officially adopted its entirety as a Register pursuant to Section 27 of the Ontario Heritage Act on March 26, 2007. Since that time Council has removed and added properties to the Register by resolution.

³ National Register of Historic Places: 481 Rideout Street, City of London, Ontario, N6A, Canada
<https://www.historicplaces.ca/en/rep-reg/place-lieu.aspx?id=8075>

Section 27 of the Ontario Heritage Act requires the clerk of a municipality that has been designated by the municipality or by the Minister under Part IV and shall contain a specified description with respect to each property. The act indicates that a municipality may also include property that is not designated on the municipal heritage register. The main implication of being included as a non-designated property on the municipal heritage register is that it enables the municipality to withhold issuance of a demolition permit application for 60 days.

The properties which are subject to this development (435 – 45) are designated under the Ontario Heritage Act and are therefore included on the Municipal Heritage Register as designated.

The following individual properties are located adjacent to the proposed development and are included on the municipal heritage register. Other properties of note in the vicinity are also referenced.

Table 1: Adjacent Heritage Properties

Property	Name	Location	Status	Potential Impact
399 Ridout Street North	Middlesex County Court House Museum	Prominent building in the vicinity of the development	Designated Part V – Downtown Heritage District	View from the Courthouse to the City facing east
421 Ridout Street North	London Museum	Adjacent on south side of Riverside Drive	Designated Part V – Downtown Heritage District	View from the London Museum to the City facing north.
466 Ridout Street North		Adjacent on the east side of Rideout Street	Register	View of the sky from the east side of the street and increased shadowing.
468 Ridout Street North		Adjacent on the east side of Rideout Street	Register	View of the sky from the east side of the street and increased shadowing
470 Ridout Street North		Adjacent on the east side of Rideout Street	Register	View of the sky from the east side of the street and increased shadowing
472 Ridout Street North		Adjacent on the east side of Rideout Street	Register	View of the sky from the east side of the street and increased shadowing
471 Ridout Street North	Eldon House Museum	Adjacent to the north	Designated, Part IV	View of the sky from the south side of the house, increased shadowing, potential impact on the health of mature and ornamental vegetation on the property
25 Wilson Avenue	Labatt Park	Opposite across River and Harris Park	Designated, Designated Part V	Impact of the view of the London Skyline from the park which already contains high rise buildings.
531 Ridout Street North	Harris Park	Public Park including the Canada 150 Pavilion, Mature trees on the east side of the park, and a greensward on the west side the park to the river's edge, intersected by the Thames Valley parkway	Designated Part V, Downtown Heritage Conservation District	View of the sky from the south and east sides of the development, potential for increased shadowing and adverse impact on mature vegetation.

Section 565 of the Official Plan requires the following with respect to registered heritage resources:

A heritage impact assessment will be required for new development on and adjacent to heritage designated properties and properties listed on the Register to assess potential impacts, and explore alternative development approaches and mitigation measures to address any impact to the cultural heritage resource and its heritage attributes

The impact of the development on the subject and adjacent properties included on the municipal heritage register will be considered as part of this report.

2.6 Downtown Heritage Conservation District

The Downtown Heritage Conservation District was adopted by Municipal Council in 2012, and designated under Part V of the *Ontario Heritage Act* in 2013. The subject property is included within the HCD.

As a part of the HCD Plan and Study, the individual properties that comprise the HCD, as well as their streetscape and landscape characters were categorized and organized into a “Downtown London HCD Matrix” to classify each property. The individual properties were classified under the following three categories:

- Assignment – classification of the building by its age and/or proximity to other heritage buildings;
- Rank – the evaluation of a building’s heritage importance and attributes classified as either priority A, B, or C; and,
- Landscape – a building or site’s relevance to the adjoining streetscape and historical land uses.

The subject property (and the individual buildings included within this report) formed Block 10 of the HCD and include the classifications identified in the Table below.

Table 2: Downtown HCD Classification

Address	Assignment	Description	Ranking	Character-Defining Elements	Landscapes
441 Ridout Street North	H	Bank Block, c. 1845	A	All restored elements including door to carriageway; refer to Designation By-Law;	i
443 Ridout Street North	H	Bank Block, c. 1845	A		i
451 Ridout Street North	H	Anderson House, c. 1852	A	Rebuilt structure; all façade elements;	i
435 Ridout Street North	H	Bank of Upper Canada	A	All restored elements including portico and fanlight over entryway; refer to Designating By-law;	i

The following definitions refer to the assignment, ranking, and landscape codes used to classify each of the properties:

- H (Historic) – Structure built within the critical period between the 1830s – 1980s as defined during the Downtown London HCD Study (January 2011). The building’s architectural character is derived from a number of elements which may include: materials, window design and pattern; store fronts and upper facades; signage; and/or roof type. It may also be associated with other historical attributes such as architect, owners, use. Its importance as part of the streetscape and the District as a whole is reflected in its ranking. It is imperative that

buildings with an H assignment are recognized as falling under the most stringent guidelines of this document based on the associated ranking.

- A – Structure assessed as currently having any combination of the following attributes; all or most of the building’s façade elements are intact; windows may be replaced but occupy original openings; store front retains tradition shape and some features such as windows or terrazzo pavement; previously designated historical or landmark significance; noted architect; good or very good example of recognizable style; important to streetscape; good restorations.
- i – Residential landscape pattern defined by the plots which were originally laid out to accommodate residential and associated buildings with setbacks from the front and side lot lines, creating a landscape prominence to the street.

Accordingly, the subject property and the entire block within which it resides, are assigned as historic structures and include a ranking of “A”. As a result, the buildings included within the study area are ranked as significantly contributing to the HCD, and the policies and guidelines outlined in HCD apply.

2.6.1 Blackfriars-Petersville Heritage Conservation District

The Blackfriars-Petersville HCD was adopted by Council on May 6, 2014 and designated under Part V of the Ontario Heritage Act on May 15, 2015. The subject property is highly visible from the HCD which is located on the opposite (west) side of the Thames River.

Key policy goals stemming from the District Plan which are relevant to this development include:

- Views along streets terminating at the river;
- Views of the Blackfriars bridge;
- Views to the Thames River and associated dyke.

The proposed development is compatible with the intent of the policies of the District plan in that:

- Views to the Thames River are not impeded;
- Views of the City of London already present a high-rise landscape over downtown;
- Built and cultural attributes of the district are not negatively impacted by the proposed development;
- The purpose of this report is to demonstrate that the heritage attributes of protected heritage properties will be conserved.

2.7 Thames River

The Thames River was formally designated a Canadian Heritage River on August 14, 2000. Ontario’s Thames River, one of the most southern Canadian water courses, has exerted a strong influence on both the natural features and the settled landscape within its watershed.

The Thames is a relatively small river on a Canadian scale but has unique natural heritage features. The Thames was one of the first rivers to form following the retreat of the Wisconsin Glacier from Ontario and the upper reaches still flow through these ancient spillways. The lower river reach, with its shallower gradient, emerged after thousands of years as a glacial lake.

The Thames is the only major river in Canada with the majority of its watershed within the Carolinian Life Zone. This region is recognized as one of the most biologically significant and diverse regions in Canada with more than 2200

species of vascular plants identified including the only two locations of the Wood Poppy in Canada. With its many habitats, nutrient rich waters and connection with the Great Lakes, the Thames also contains the largest diversity of clams, the threatened Eastern Spiny Softshell Turtle and one of the most diverse fish communities in Canada. Despite the long period of human occupation and development in the watershed, there are more species of plants and animals present today than in the past, although, protection and restoration efforts are still required to help preserve this unique and vital area of Canadian natural history.

With respect to its human heritage, the Thames River has provided the setting for 11,000 years of significant Aboriginal and European settlement. The Thames, with its abundant fish and game, provided a focus for each group in the sequence of Aboriginal peoples, including those who were the first to practise agriculture in Canada between 500 and 1650 A.D. In the 1700s, the river attracted French fur traders and European settlers, as well as Aboriginal groups. Following the role of the Thames valley as a major theatre in the War of 1812, which included many battles and Tecumseh's death in the Battle of the Thames at Moraviantown, pioneer settlement within the watershed developed into Canada's first successful commercial agrarian society based on wheat. In turn, many of the numerous watermill sites provided the basis for industrial and urban development, including the major riverine cities, notably the early river port and ship building centre of Chatham, as well as London, Stratford and Woodstock, and a number of smaller towns and villages. Agriculture flourished during this time and the watershed has continued as the most prosperous farming landscape in Canada. During the American Civil War, the Thames served as the final avenue to freedom for blacks fleeing slavery through the Underground Railroad. Today, in remembrance, a tour leads visitors to many significant landmarks in the refugee slave settlements near the Thames River including the Buxton Historic Site and Museum, and the First Baptist Church in Chatham.

From *The Thames River Watershed: A background Study for nomination under the Canadian Heritage Rivers System, 1998, Upper Thames River Conservation Authority for the Thames River Coordinating Committee.*

2.8 Heritage Integrity

According to the Ontario Heritage Toolkit, Heritage Property Evaluation (MTCS 2006), "*Integrity is a question of whether the surviving physical features (heritage attributes) continue to represent or support the cultural heritage value or interest of the property.*" The following discussion of integrity was prepared to consider the ability of the property to represent and retain its cultural heritage value over time. It does not consider the structural integrity of the building. Access to the interior of the building was not available, and observations have been made from the public right-of-way. Structural integrity, should it be identified as a concern, should be determined by way of a qualified heritage engineer, building scientist, or architect.

The subject property contains three heritage structures, designed and constructed in their respective Georgian and Victorian Eclectic styles. As noted above, the heritage buildings were previously in severely dilapidated conditions; however, they were significantly and carefully restored in 1970 by the John Labatt Company. As a result, the buildings and the subject property retain much of the integrity of their original built character. In September 2018, a fire took place within the 435 Ridout Street North building. The integrity of the building was reportedly not compromised; however, firefighters were forced to break windows in order to ventilate the structure. The window openings have since been covered with plywood for security reasons. Nonetheless, the building still retains much of its integrity.



Image 1: View showing heritage structure at 435 Ridout Street North, with boarded-up windows due to fire damage from September 2018



Image 2: View showing heritage buildings at 441-447, and 451 Ridout Street North



Image 3: View showing heritage building at 451 Ridout Street North



Image 4: View looking northwest along Ridout Street North showing the three heritage buildings in their street context



Image 5: View showing rear of 435 Ridout Street North from surface parking lot accessed from Queens Avenue



Image 6: View looking east from surface parking lot accessed from Harris Gate. The structure in the foreground is the modern commercial structure built to the rear of 451 Ridout Street North



Image 7: View showing modern commercial structure constructed to the rear of 451 Ridout Street North, built into the embankment



Image 8: View looking west from surface parking lot showing existing lot adjacent to Harris Park



Image 9: View showing rear of the three heritage buildings from the surface parking lot accessed from Queens Avenue



Image 10: View looking south showing the north side of 451 Ridout Street North, and the heritage buildings within their street context



Image 11: View looking west from Queens Avenue, showing the three heritage buildings, and the curvature of Queens Avenue at left



Image 12: View looking north on Ridout Street North, showing the three heritage buildings in their surrounding context



Image 13: View to the Eldon House property, located immediately north of the subject property



Image 14: View looking north showing the three heritage buildings at right, within the larger built environment, located south of the subject property



Image 15: View looking north showing the historic Middlesex County Court House and Gaol at left. The three heritage buildings can be seen far in the centre of the image.

3. Proposed Undertaking

3.1 Project Description

The observations for this HIA described within Section 5.1 and 5.2 were developed based on the proposed project materials prepared in March 2018 for Farhi Holdings Corporation, by Tillman Ruth Robinson Architects.

The proposal is seeking to allow for the development and construction of a 40-storey tower that would include residential, hotel, office, and retail uses (**See Appendix A**). The proposed development has undergone various refinements since 2013 and seeks to maintain all existing heritage buildings on the subject property. Previous iterations of the proposal also sought to construct a new 4-storey parking garage along Queens Avenue. However, as a result of setback requirements for the proposed Bus Rapid Transit project, the parking garage is unfeasible.

The redesigned site plan for the property proposes to construct the tower towards the north end of the property, to the rear of 451 Ridout Street North, replacing the modern structure currently located here that was built in the 1970s. The tower is proposed for this location to minimize impacts on the site, and to minimize the extent of overlap over the flood line. This option also includes the location of the parking garage below the tower, constructed within the east bank of the slope on the property. The parking garage is proposed to be designed to be watertight to the extent of the 250-year flood line. This site plan option would result in the retention of the existing heritage buildings, and their re-purposing for integration into the proposed tower. Driveway and drop-off access would be accessed off of Ridout Street North, within the vicinity of the existing parking lot. A proposed public space located behind the 435 and 441 Ridout Street North buildings would connect the street level with Harris Park, the Thames River, and the trail below the slope.

The following is a description of the proposed development from Tom Tillman, architect with Tillman Ruth Robinson who is designing this project:

- The proposed mixed use development at the Ridout Street complex of buildings is a nexus where London's future will meet and compliment London's past. The three heritage buildings will remain intact and preserved in their entirety in keeping with the heritage designations that protect them. The sitework at street level both along Ridout Street and Queens Avenue will be enhanced with hard and soft scape features that will compliment the significance of these three historic gems. Very special attention and measures will be afforded during the construction to absolutely ensure that the integrity and all architectural features of 435, 441 and 451 Ridout Street remain preserved in their found state. 435 Ridout Street will have its windows re-instated (lost in a fire) in keeping with the architectural style and period of its original construction. When the early 1970's addition is removed from the west face of 451 Ridout Street, it will be replaced with new construction that ensures that 451 Ridout remains intact and fully historically correct on the interior.
- Overall the residential/commercial tower has been placed on axis with 451 Ridout Street and Queens Avenue to reinforce the importance of this site. The existing surface parking lot located down in Harris Park will revert back to parkland in keeping with green landscape in the park and at the Thames River. New pedestrian linkages will be created to allow the public to move more easily between Harris Park below and the Ridout complex of buildings above. The existing trees which border to the north enclosing the Eldon property will remain and be properly protected throughout the construction process. Care has been taken to place the new tower such that it has minimal impact on the Eldon property both from a shadow perspective but also from a growth perspective related to the lawns and gardens.

3.2 Approach

The approach for the proposed development includes a series of visions, goals, and opportunities that are being considered for the project. The vision for the project is to create a landmark development for the City of London that also creates a mixed used function supporting the Downtown Plan, the London Plan, and the Back to the River initiative, while also maintain the existing heritage buildings with an integrated use. A number of goals are proposed for the project as well, including supporting the London Plan, expanding Harris Park to the south (by dedication of land to the City), improving the remnant slope, physically connecting downtown to the river, eliminating existing surface parking along the river, and minimizing flood water displacement.

The following proposed public benefits and opportunities have been identified as a part of the proposal for the development:

- Opportunity to remove non-natural fill materials through excavation of the bank;
- Opportunity to stabilize the existing bank and re-naturalize it through new landscaping;
- Supporting the Downtown Plan by:
 - Engaging the river with publicly accessible lookouts, terraces, and new pathways that connect the street with the Thames River;
 - Bringing people to the river by providing new places to live, work, and play overlooking the Thames River
- Supporting the Back to the River Initiative by:
 - Understanding the importance of the Thames River for the city; the project strives to give Londoners a place to work, to play, and call home.
- Supporting the One River Environmental Assessment by:
 - Improving the natural environment and drawing people to the Thames River.
- Supporting the London Plan by:
 - Planning for exceptional places and spaces, growing inward and upward, giving real and attractive mobility choices (walking, cycling, and transit), and building strong and healthy neighbourhoods for everyone.
- Supporting the Thames Valley Corridor Plan by:
 - Identifying suitable points of access, pathway and trail systems, lookout points and linkages to communities and the Thames Valley Parkway;
- Protecting heritage by:
 - Retaining and re-purposing the existing heritage structures on site;
 - Adding new landscape features to the Eldon House front lawn;
 - Drawing people to the Eldon House property and connecting the south side of the Eldon House property to a new path proposed to link to the river-side circulation routes.

3.3 Planning Justification

The subject application is for Official Plan Amendment and Zoning By-law Amendment Applications for the property located at 435, 441 and 451 Ridout Street North, London, Ontario. The applicant is Farhi Holdings Corporation.

As part of the complete application requirements of the City of London a Planning Justification Report, dated July 2019 was prepared by MacNaughton, Hermson, Britton, Clarkson Planning Limited (MHBC) of London, Ontario.

The subject lands are a single property located at the northwest corner of Queens Avenue and Ridout Street North, immediately east of the Thames River corridor and Harris Park. In total, this project measures 1.4 ha in area and contains three existing parking facilities as well as surface parking facilities. The property is designated under Part IV of the *Ontario Heritage Act*, and is also designated under Part V of the *Ontario Heritage Act* as part of the Downtown London Heritage Conservation District. Further, the property is identified as a National Historic Site of Canada.

The proposed redevelopment concept for the subject property incorporates a 40 storey, mixed-use tower to replace the three-storey office building addition located to the rear/west of the historic structure addressed as 451 Ridout Street. The proposed tower, which would be connected to 451 Ridout Street, would contain approximately 6,308 m² of office/commercial space on lower floors and a total of 280 residential suites on the upper floors. No modifications are proposed to the heritage buildings addressed 435 and 441 Ridout Street. ‘

Key components of the proposed project are as follows:

- A 40-storey tower incorporating an enhanced level of architectural design and containing 6,308 m² of office/commercial space within the first four levels and a maximum of 280 apartment units within levels five (5) to 40. The tower base, which would include the office/commercial space, main lobby space, storage areas and administrative offices, would have a total building area of approximately 1,670 m². The main tower component would incorporate a slender floorplate (approximately 860 m² in area) and would contain a range of suite configurations;
- A variety of indoor amenities within the tower, including a lounge area, fitness facility, party room and outdoor patio space on the fifth floor;
- Outdoor amenity space interspersed throughout the property and visible from the public realm, including landscaped forecourts adjacent to the entrances and vehicle drop-off areas;
- A multi-level parking area integrating four storeys of underground parking and at-grade parking areas, as well as a loading area, bicycle storage and waste/recycling storage. A total of 372 vehicle stalls would be accommodated in this arrangement, with parking (315 stalls) predominately accommodated underground. It is envisioned that at-grade stalls would primarily function as visitor parking;
- Right-in, right-out entrances under existing street configurations providing street access to Queens Avenue (one entrance) and Ridout Street North (two entrances). A right-in entrance from Ridout Street North is also proposed at the northern limit of the Site;
- Access driveways to the 4-storey parking garage provided at-grade from Queens Avenue and Ridout Street North. Additionally, delivery vehicles would access the underground parking facility via the existing private driveway connecting to the Harris Park entrance; and
- Pedestrian connections to improve access to the street frontages, the Thames Valley Parkway and Harris Park in order to support greater active transportation linkages between the Downtown and the Thames River corridor.

Several design elements have been integrated into the proposal in an effort to achieve compatibility with its surroundings:

Building Form and Orientation

- The proposed mixed-use, high rise building would be positioned at the rear of the heritage structure at 451 Ridout Street North, generally at the location of the existing building addition. The tower component of the new building is positioned immediately west of this heritage structure, with the four-storey office/commercial element extending north from the tower base.
- The proposed building arrangement is intended to preserve the unobstructed view of 435, 441 and 451 Ridout Street North from the street frontage and to promote a landmark vista at the western terminus of Queen Street.
- By positioning the tower to the rear of these buildings, the design layout also enhances the prominence of the structure from the Thames River corridor (particularly from the Forks of the Thames activity area). Moreover, the tower location would help to frame Harris Park and the Thames Valley Parkway, while also providing increased surveillance into this area (contributing to a safer pedestrian environment).
- In summary, the building positioning is intended to complement, and be sympathetic to, the character of these heritage buildings and the broader Downtown London setting. In this respect, the form and orientation of the tower would establish an iconic gateway feature, while also helping to accentuate the overall character and vitality of Downtown London.

Building Massing

- The massing of the proposed point tower is designed to create a comfortable and engaging pedestrian environment that is compatible with, and sensitive to, the adjacent streetscapes, the local development context and the broader downtown setting. Further, the building integrates three principal elements above-grade:
 1. 'Base' Element. The 'base' component of the proposed tower generally encompasses the main building entrance, building lobby and office/commercial space. In total, the tower base integrates the first four floors of the development and is designed to both integrate with 451 Ridout Street North and complement nearby heritage buildings. The positioning of the base component also maintains the continuity of the existing street edge along Ridout Street North and helps to frame the development interface with Harris Park.
 2. 'Middle' Element. Residential units are massed in a defined 'middle' feature situated above the office/commercial space and deck parking and extending from the fifth to 29th floors. The middle element includes the core apartment component of the project, as well as amenity space provided on the fifth floor. Apartment suites incorporate individual balconies oriented towards Ridout Street North, Queens Avenue and Harris Park. These protruding balconies provide shadowing into and across the building surface. Further, distinctive patterned design elements extend the length of the middle feature along the front portions of the east and west façades. Collectively, the components of the middle element serve to soften the massing of the building while also accentuating its relationship with the adjacent streetscapes and the Thames Valley corridor.
 3. 'Top' Element. The top of the building incorporates the 30th to 40th floors and has been designed to be distinctive and to positively contribute to the downtown skyline. This tower component is intended to house residential suites having individual balconies consistent in purpose as those planned for the middle element. The rooftop element would integrate a penthouse suite, a large terrace and mechanical equipment.

Façade Treatment

- The tower design incorporates a consistent, contemporary façade style integrating extensive glazing and repetition of lines and windows through horizontal articulations (to help break up the continuous mass of the structure). Most notably, curved elements are integrated the base, middle and top façades to acknowledge and reflect the Thames River corridor interfacing with and inspiring this design. Interruptions (cut-outs) in the balcony design are also proposed to provide unique design components within the tower to further break up the building mass. Moreover, large windows and entrance features in the lower floors would help to promote more active street frontages along Ridout Street North and Queens Avenue, and to further animate the west elevation (viewed from the Thames River corridor). Appendices 'A' and 'B' present several renderings of the proposed development which illustrate the aforementioned design treatments.

Pedestrian Environment/Outdoor Amenity Space

- The office/commercial and residential elements of the proposed tower are to be accessed by a common (main) pedestrian entrance from the Ridout Street North sidewalk to the building lobby. The walkway extends along the north face of 451 Ridout Street North. A secondary building access is also provided along the south building elevation to provide connectivity from the surface parking lot and the Queens Avenue sidewalk. Internal walkways would also provide linkages to surface parking areas, the Thames Valley Parkway and Harris Park.

The planning merits of this proposal are evaluated in detail within the Planning Justification Report and are summarized below:

- The project supports the intensification of lands in the City's downtown that are designated in the City's 1989 Official Plan and The London Plan for high density residential, office, and commercial uses;
- The property is well suited for this development proposal considering its physical size/shape, its location within a mixed-use development area and its proximity to existing offices, retail/service commercial enterprises, government activities, recreational, entertainment and cultural facilities, public transit services and the proposed bus rapid transit (BRT) system. Residents and employees of this new development would therefore have access to, and would support, a range of local businesses, public institutions and transit services;
- Design elements integrated into the proposal promote an attractive and prominent building form that would contribute positively to both the local streetscape and the City's downtown skyline. Additionally, the high-rise building design would create a landmark for the Forks of the Thames activity area and, more broadly, Downtown London;
- Introduces greater housing choice within the downtown core to help broaden the range and mix of housing available to current and future residents of Central London. In effect, this project would help the City accommodate forecasted housing demands and achieve intensification targets; and
- Integrates a mix of higher density residential and complementary office/commercial uses which, collectively, would support efforts to revitalize the downtown core and encourage its continued development as a multi-functional regional centre. The proposal would also help to promote neighbourhood stability in the downtown core by diversifying the mix of housing available in this area (to better meet the changing needs of local residents over the long-term).

In light of these considerations, the Planning Justification Report concludes that this proposal is appropriate for the subject lands and the downtown development context and should not generate significant land use conflicts with adjacent properties.

As part of the planning analysis, the following documents were reviewed:

- Provincial Policy Statement;

- City of London Official Plan (1989);
- City of London Official Plan (The London Plan);
- City of London Zoning By-law Z.-1;
- Our Move Forward (London's Downtown Plan);
- Downtown Heritage Conservation District Plan; and
- Downtown London Community Improvement Project.

All of the above documents have reference to cultural heritage as part of the overall municipal planning policy context.

A 'Pre-Application Consultation' meeting was held on June 13, 2017 to review the submission requirements for the development concept. As set out in the associated 'Record of Pre-Application Consultation', a number of reports have been prepared in support of the proposal and are enclosed with the OPA and ZBA applications: These include the following related to cultural heritage:

- Planning Justification Report, with urban design considerations
- Heritage Impact Statement
- Stage 1-2 Archaeological Assessment

The Planning Justification Report references Heritage Considerations as follows:

The three existing buildings on the Site (435, 441 and 451 Ridout Street North) are listed on the City of London's Inventory of Heritage Resources (Register) as 'Priority 1' structures. Section 4.0 of the Register sets out that Priority I buildings are considered to be London's most important heritage structures and merit designation under Part IV of the Ontario Heritage Act. In this respect, the subject property is designated under Part IV of the Act. The property is also designated under Part V of the Act as a part of the Downtown London Heritage Conservation District, and is identified as a National Historic Site of Canada (referred to as the 'Ridout Street Complex'). Further, the Site is adjacent to other listed heritage properties, including the Eldon House (481 Ridout Street North) and the Middlesex Courthouse and Gaol (399 Ridout Street North).

AECOM Canada Inc. (AECOM) has conducted a Heritage Impact Assessment (HIA) in conjunction with this proposal to evaluate the potential heritage impacts of the proposed redevelopment plan. It is generally concluded in Section 7 of the HIA that, to mitigate the potential direct and indirect impacts to identified cultural heritage value, mitigation strategies described in the report should be considered in conjunction with project refinements.

4. Measurement of Development or Site Alteration Impact

4.1 Heritage Policy Evaluation (PPS, London Official Plan, and Downtown Heritage Conservation District Plan)

The proposed development has been evaluated for compliance with relevant heritage policies in effect in the City of London. These include the Provincial Policy Statement, the London Plan and the Downtown Heritage Conservation District plan. The impact on adjacent properties including the Blackfriars-Petersville Heritage Conservation District has also been assessed. These are as follows:

Table 3: Evaluation of Policies Against Development

Policy	Analysis and Commentary
THE LONDON PLAN (Official Plan)	
<p>565_ New development, redevelopment, and all civic works and projects on and adjacent to heritage designated properties and properties listed on the Register will be designed to protect the heritage attributes and character of those resources, to minimize visual and physical impact on these resources. A heritage impact assessment will be required for new development on and adjacent to heritage designated properties and properties listed on the Register to assess potential impacts, and explore alternative development approaches and mitigation measures to address any impact to the cultural heritage resource and its heritage attributes</p>	<p>The proposed development meets the intent of the Official Plan as follows:</p> <ul style="list-style-type: none"> - The proposed development preserves all existing heritage resources on the site in situ and in doing so also preserves the majority of the human scale streetscape. - The Proposed development is designed to draw the eye to the podium level by utilizing the retention of heritage resources and a combination of compatible materials (brick) and compatible contrast (glazing) to achieve compatibility with the surrounding heritage resources and districts - The London Plan anticipates and has already experienced high-density intensification in this area. There are other high-rise towers in the immediate vicinity which already form a high-rise skyline from such places as Harris Park, Labatt Park, Museum and former Middlesex County Court House complex and the Thames River. - This document satisfies the requirement that a Heritage impact assessment is required for new development on or adjacent to heritage designated properties and properties listed on the register.
<p>568_ Conservation of whole buildings on properties identified on the Register is encouraged and the retention of façades alone is discouraged. The portion of a cultural heritage resource to be conserved should reflect its significant attributes including its mass and volume.</p>	<p>The proposed development retains 3 existing heritage resources on site of confirmed National Heritage Significance and successfully o achieve compatibility with nearby heritage resources.</p>
<p>584_ Building height and densities may be increased, in conformity with the Bonus Zoning policies in the Our Tools part of this Plan, in support of heritage</p>	<p>The proposed development retains 3 existing heritage resources on site of confirmed National Heritage Significance and</p>

Policy	Analysis and Commentary
<p>designation of a property that is of cultural heritage value or interest.</p>	<p>successfully o achieve compatibility with nearby heritage resources.</p>
<p>586_ The City shall not permit development and site alteration on adjacent lands to heritage designated properties or properties listed on the Register except where the proposed development and site alteration has been evaluated and it has been demonstrated that the heritage attributes of the heritage designated properties or properties listed on the Register will be conserved.</p>	<p>The subject development is located adjacent to designated heritage resources (Eldon House) and through design and other measures to be determined (vegetative screen enhancement) demonstrates that the heritage attributes of Eldon House and other part V designated heritage properties are maintained.</p>
<p>589_ A property owner may apply to alter the cultural heritage attributes of a property designated under the Ontario Heritage Act. The City may, pursuant to the Act, issue a permit to alter the structure. In consultation with the London Advisory Committee on Heritage, the municipality may delegate approvals for such permits to an authority</p>	<p>The owner is not proposing alterations to the heritage resources on the subject property, however, if alterations are required it is understood that they would be undertaken in compatibility with the heritage attributes of the site and the applicant would seek a heritage permit as required.</p>
<p>594_ Within heritage conservation districts established in conformity with this chapter, the following policies shall apply:</p> <p>1) The character of the district shall be maintained by encouraging the retention of existing structures and landscapes that contribute to the character of the district.</p>	<p>The proposed development retains all existing structures. Heritage landscapes both adjacent and in the vicinity are already framed by high rise buildings Mitigative measures would need to be implemented (e.g. new plantings) to lessen the impact on existing mature vegetation in the vicinity.</p>
<p>2) The design of new development, either as infilling, redevelopment, or as additions to existing buildings, should complement the prevailing character of the area</p>	<p>The proponent seeks to compliment the prevailing character of the area by retaining all existing heritage resources in situ, by maintaining strong heritage focussed podium that draws the eye to lower levels, by utilizing both compatible materials and compatible contrast materials to improve compatibility of the development with the prevailing character of the area.</p> <p>The subject area has been long planned for intensification and there are multiple high-rise buildings in the vicinity. The subject development would not look out of place; indeed, the tower would complement the overall modern-day City of London skyline.</p>
<p>3) Regard shall be had at all times to the guidelines and intent of the heritage conservation district plan</p>	<p>The following is a consideration of policies relevant to the proposed development within the Downtown London Heritage Conservation District Plan.</p>

Policy	Analysis and Commentary
<p>3.2.2 Social Goals & Objectives</p> <p>Encourage the redevelopment, intensification, and acceptance of the Downtown as the cultural and social focus of the community;</p>	<ul style="list-style-type: none"> ■ <i>The subject proposal supports the goals of Section 3.2.2 by bringing population and businesses downtown, the subject development will complement the economic vitality of downtown and in doing so achieve an environment conducive to the preservation and maintenance of heritage resources</i>
<p>6.1.4 New Construction</p> <p>The Downtown is a vibrant environment and is anticipated to continue to develop and grow throughout London’s future. However, the remaining physical evidence of the city’s historical beginnings is something that this HCD intends to preserve and compliment. The guidelines that ensue are written to help ensure that new construction respects the history that will surround it in material, massing and other aesthetic choices.</p>	<ul style="list-style-type: none"> ■ <i>The proposed development achieves compatibility by retaining all heritage buildings on the site and by using compatible and compatible contrast materials and stepping back of upper stories to ensure that the new construction respects the historical environment that surrounds it.</i>
<p>6.1.4.1 Principles</p> <p>Any new construction shall ensure the conservation of character-defining elements of the buildings it will neighbour and also the building being added to when considering additions. New work is to be made both physically and visually compatible with the historic place while not trying to replicate it in the whole. The new work should easily be decipherable from its historic precedent while still complementing adjacent heritage buildings.</p>	<ul style="list-style-type: none"> ■ <i>The proposed development retains all existing heritage buildings on site and it is intended to implement mitigation measures to ensure compatibility of the proposed development with adjacent heritage resources and landscapes.</i>
<p>Façade composition and height are two major components in maintaining the character of the current streetscapes. A single excessively tall and imposing structure can completely alter the pedestrian-focused atmosphere of the Downtown. Use roof shapes and major design elements that are complementary to surrounding buildings and heritage patterns.</p>	<ul style="list-style-type: none"> ■ <i>The proposed development is one of a number of tall buildings located in the immediate vicinity. By utilizing progressive measures in building form and materials the proposed development achieves greater compatibility than any existing high-rise buildings in the vicinity with the heritage environment.</i>
<p>Setbacks of new development should be consistent with adjacent buildings. New buildings and entrances must be oriented to the street and are encouraged to have architectural interest to contribute to the streetscape. Respond to unique conditions or location, such as corner properties, by providing architectural interest and details on both street facing facades.</p>	<ul style="list-style-type: none"> ■ <i>The proposed development achieves compatible setbacks to Ridout Street and Queen’s Avenue by retaining the existing buildings on site and rendering at street level the heritage buildings as the dominant form in the redeveloped property.</i> ■ <i>In consideration of the prevailing high-rise environment that already exists downtown and the progressive efforts made to achieve compatibility in this phase of the development it is determined that the proposed development would not cause significant negative</i>

Policy	Analysis and Commentary
	<p><i>impacts on views from adjacent and nearby public parts and community spaces.</i></p>
<p>596_ A property owner may apply to alter a property within a heritage conservation district. The City may, pursuant to the Ontario Heritage Act, issue a permit to alter the structure. In consultation with the London Advisory Committee on Heritage, the City may delegate approvals for such permits to an authority.</p>	<ul style="list-style-type: none"> ▪ <i>A heritage permit would be required for alterations to the designated properties which are located within a heritage conservation district. The owners are not proposing any alterations to the existing heritage properties, however, if alterations are subsequently proposed a heritage permit would be required to be obtained by the applicant after review by the City and its municipal heritage advisory committee as required under the Act. The connection of the proposed tower to 451 Ridout Street would require a heritage permit and would be subject to the level of review and compatibility analysis as described in this report.</i>
<p>597_ Where a property is located within a heritage conservation district designated by City Council, the alteration, erection, demolition, or removal of buildings or structures within the district shall be subject to the provisions of Part V of the Ontario Heritage Act.</p>	<ul style="list-style-type: none"> ▪ <i>The removal of the modern rear addition to 451 Ridout would require review against the provisions of Part V of the Ontario Heritage Act.</i>
<p>598_ Development and site alteration on adjacent lands to a heritage conservation district may be permitted where the proposed development and site alteration has been evaluated and it has been demonstrated that the heritage attributes of the protected heritage property will be conserved.</p>	<ul style="list-style-type: none"> ▪ <i>The subject property is located adjacent to the Blackfriars-Petersville Heritage Conservation District. Key policy goals stemming from the District Plan which are relevant to this development include:</i> <ul style="list-style-type: none"> • Views along streets terminating at the river • Views of the Blackfriars bridge • Views to the Thames River and associated dyke ▪ <i>The district plan references adjacent Areas under Section 7.16. Key policies under 7.16 which are relevant to this development include:</i> <ul style="list-style-type: none"> • A heritage impact assessment may be required for proposed development that has the potential to impact any heritage attribute of the resource (The District) ▪ <i>The City shall ensure that the identified heritage attributes of the district are protected during adjacent development and site alteration</i> ▪ <i>The City may require preparation of a heritage impact assessment</i> ▪ <i>Planning authorities shall not permit development and site alteration on adjacent lands to protected heritage property except where the proposed development has been evaluated and it has been demonstrated that the heritage attributes of the protected heritage property will be conserved.</i>

Policy	Analysis and Commentary
	<ul style="list-style-type: none"> ■ The proposed development is compatible with the intent of the policies of the District plan in that: <ul style="list-style-type: none"> • Views to the Thames River are not impeded; ■ Views of the City of London already present a high-rise landscape over downtown ■ Built and cultural attributes of the district are not negatively impacted by the proposed development ■ The purpose of this report is to demonstrate that the heritage attributes of protected heritage properties will be conserved.
<p style="text-align: center;">CULTURAL HERITAGE LANDSCAPES</p> <p>602_ Areas of the city that City Council considers to be of cultural heritage value or interest may be recognized as cultural heritage landscapes consistent with the Provincial Policy Statement and in conformity with the policies of this Plan and will be added to the Register.</p>	<ul style="list-style-type: none"> ■ <i>The City of London has not formally adopted the areas adjacent to the subject development site as an official cultural heritage landscape as defined in the Provincial Policy Statement.</i> ■ <i>Areas in the vicinity of the development which are heritage conservation districts may be considered de-facto cultural heritage landscapes, however, compatibility of the proposed development with the Downtown and BlackFriars-Petersville Heritage Conservation Districts have been demonstrated elsewhere in this report.</i>
<p>293_ High-rise buildings should be designed with slender towers that reduce shadow impact, minimize the obstruction of views, and are less massive to neighbouring properties. A typical floor plate of approximately 1,000m² is a reasonable target to achieve this goal. Commercial towers may have larger floor plates but should still have effective separations between towers to allow access to sunlight and views.</p>	<ul style="list-style-type: none"> ■ <i>The proposed development is designed in accordance with this policy with a slender tower with a floor plate of 860 m in area, less than the 1000 m area required by the Official Plan</i>

A review of the proposed development against relevant policies of the Official Plan and heritage conservation district plans demonstrate that proposal and its design support and enhances the cultural heritage value and interest of heritage resources impacted on the subject site, adjacencies and the heritage districts as follows:

- The proposed development aims to preserve all existing heritage resources on-site.
- The design for the Tower which includes an articulated podium, framed towards Ridout Street by heritage buildings draws the eye to and maintains the prominence of the heritage streetscape at pedestrian level.
- The use of compatible materials (buff brick) and compatible contrasting materials (glazing) provides the tower with compatibility with the existing heritage context and buildings and also provides a distinct visual separation between old and new.

- Downtown London is an environment of economic activity which has resulted in the construction of a number of high rise commercial and residential buildings in the vicinity which already intersect with traditional views within, to and from the site and heritage districts. The proposed tower therefore does not offend existing viewsheds, and indeed, with the use of modern techniques of compatibility (podium and point tower, materials, retention of heritage resources and other mitigation measures, achieves a greater level of compatibility than those which have preceded it.
- Mitigation measures related to shadow impacts resulting from the tower would be designed to minimise the impact of shadows on existing mature vegetation.

The positive economic impact of the proposed development on the downtown area, bringing more business and residents to the downtown core would enhance overall prosperity in the downtown and create better conditions for the rehabilitation and maintenance of heritage resources

Note the London Plan is deemed to be in compliance with the Provincial Policy Statement, therefore analysis against the London Plan satisfies this requirement of evaluating compatibility with the PPS.

4.2 Potential Impacts on Heritage Attributes

The MTCS identifies typical types of direct and indirect impacts that can be anticipated to impact recognized or potential heritage properties as a result of a proposed undertaking. Direct impacts consist of destruction or alteration of a heritage property, while the indirect impacts includes factors such as shadows, isolation, obstruction, change in land use, and land disturbance.

These impacts were assessed according to the MTCS’ *Ontario Heritage Toolkit: Heritage Resources in the Land Use Planning Process*. The sections below outline the potential impacts identified by MTCS, and their relevance to this project.

4.2.1 Ridout Street Complex

The Ridout Street Complex is to be preserved as part of the development; however, there would be other direct and indirect impacts on the complex resulting from the development which are articulated below.

Table 4: Potential Direct Impacts and Relevance to the Project

Direct Impacts	Relevance to the Project
<p>Loss/Destruction/Demolition of any or part of any significant heritage attribute or feature.</p>	<p>The three heritage structures are proposed to be retained in situ, and are not proposed for demolition or loss as a result of this project. As a result, the heritage attributes, or character-defining elements will not be impacted as a part of the proposed project.</p> <p>At this time no alterations to interior elements of 451 Ridout are anticipated; however, some transition will need to be planned in order to facilitate the connection between the old and new structures.</p> <p>Protected interior features as described in the designation by-law include the following:</p>

	<p><i>Any surviving original interior detailing, furnishing and fittings, including: shutters, door and window trim, ceiling mouldings, cornices, mantelpieces, fireplaces, columns and brackets</i></p> <p>Any alterations to protected heritage elements as described in the designation by-law would require a heritage permit.</p>
<p>Displacement/Alteration that is not sympathetic, or is incompatible, with the historic fabric or appearance.</p>	<p>Alterations to the exteriors of the three heritage structures are not anticipated as a part of this project. The property as a whole is proposed to be developed and will undergo significant alteration; however, this proposal includes the retention of the three heritage buildings as is. It is understood that the buildings will be integrated for the proposed project.</p>

Table 5: Potential Indirect Impacts and Relevance to the Project

Indirect Impacts	Relevance to the Project
<p>Shadows created that alter the appearance of a heritage attribute or change the visibility of a natural feature or plantings, such as a garden.</p>	<p>Given that the proposed project will result in the construction of a proposed tower immediately adjacent to a heritage structure on the same property, it is likely that the proposed project will result in shadows on the property. Potential shadows may alter or change the visibility of the scale of the three heritage buildings, and of their location along the Thames River described in the designating-by-laws and recognitions.</p>
<p>Isolation of a heritage attribute from its surrounding environment, context, or a significant relationship.</p>	<p>The proposed project is not anticipated to isolate a heritage attribute or individual heritage building from its surrounding environment, context, or a significant relationship. Although the construction of the new tower may visually alter the views and relationship between the subject property and the adjacent Eldon House property, it is not anticipated to isolate either structure or property.</p>
<p>Land Disturbance such as a change in grade that alters the historic patterns of topography or drainage.</p>	<p>Completion of the proposed project will result in land disturbance; however, the construction will be temporary in nature and is not anticipated to affect any historic patterns, or result in permanent impacts to heritage resources. In addition, it is understood that archaeological assessments will be completed for the project, which will also assess the archaeological potential for the subject property.</p>
<p>Changes in Land Use such as rezoning a battlefield from open spaces to residential use, allowing new development of site alteration to fill in the formerly open spaces.</p>	<p>The proposed project will result in a change in use for the property. The property is currently being used for commercial purposes, but is proposed for use as commercial, residential, and public space purposes.</p>
<p>Obstruction of significant views or vistas from, within, or to a built and natural feature.</p>	<p>Given that the proposed project will result in the construction of a proposed tower immediately adjacent to a heritage structure on the same property, it is likely that the proposed project will result in shadows on the property that may alter or change the visibility of the scale of the three heritage buildings, and of their location along the Thames River described in the designating-by-laws and recognitions.</p>

Based on the extensive cultural heritage value that the subject property exhibits and the large-scale development that is proposed, it is anticipated that the project has potential to result in both direct and indirect impacts to the identified cultural heritage value of the property. The views to and from the property will be significantly altered as a result of the proposed tower.

4.2.2 Eldon House

Eldon House and its grounds are located immediately to the north of the subject property. There would be no disturbance from this development as described previously on the Eldon House property. The primary impacts appear to be related to shadowing and land disturbance during construction. There are no direct impacts identified to the resource.

Table 6: Potential indirect impacts and their relevance to the project

Indirect Impacts	Relevance to the Project
Shadows created that alter the appearance of a heritage attribute or change the visibility of a natural feature or plantings, such as a garden.	Given that the proposed project will result in the construction of a proposed tower immediately adjacent to the Eldon House property, it is likely that the proposed project will result in shadows on the property. Potential shadows may alter or change the visibility of the scale of Eldon House and impact the mature trees and gardens. The shadow study required as part of the complete application more fully determines the impact of shadowing on the Eldon House property. See Section 4.4 of this report for further details.
Land Disturbance such as a change in grade that alters the historic patterns of topography or drainage.	Completion of the proposed project will result in land disturbance on the adjacent property which may result in noise and vibration being experienced on the Eldon House property; however, the construction will be temporary in nature and is not anticipated to affect any historic patterns, or result in permanent impacts to heritage resources.

Based on the extensive cultural heritage value that the subject property exhibits and the large-scale development that is proposed adjacent to it, it is anticipated that the project has potential to result indirect impacts to the identified cultural heritage value of the property. Specifically, the integration of the new tower with the surrounding heritage environment has the potential to increase the amount of shadowing experienced on the buildings and grounds of Eldon House.

4.2.3 Downtown Heritage Conservation District

The Ridout Complex is located within the Downtown Heritage Conservation District. Potential Impacts to the district resulting from the development are as follows:

Table 7: Potential Direct Impacts to the Downtown HCD

Direct Impacts	Relevance to the Project
Loss/Destruction/Demolition of any or part of any significant heritage attribute or feature.	No heritage resources in the district are proposed to be demolished as a result there would be no impact due to Loss/Destruction.

<p>Displacement/Alteration that is not sympathetic, or is incompatible, with the historic fabric or appearance.</p>	<p>Alterations to the exteriors of the three heritage structures located within the district are not anticipated as a part of this project. The property as a whole is proposed to be developed and will undergo significant alteration and intensification, and therefore the overall appearance of the site will be altered; however, this proposal includes the retention of the three heritage buildings. It is understood that the buildings will be integrated and retained for the proposed project.</p> <p>The design and form of the project with a podium framed by heritage buildings helps to minimise the impact of the development by maintaining the ground level focus towards the heritage buildings. Use of complimentary material at the ground level of the tower will aid in minimizing impacts as well.</p>
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Table 8: Potential Indirect Impacts to the Downtown HCD

Indirect Impacts	Relevance to the Project
<p>Shadows created that alter the appearance of a heritage attribute or change the visibility of a natural feature or plantings, such as a garden.</p>	<p>Given that the proposed project will result in the construction of a proposed tower immediately within the heritage district, it is likely that the proposed project will result in shadows on the district. Given that the bulk of the district is located south of the subject property, shadow impacts on the district itself would be minimal. The district currently includes multiple high rise buildings.</p>
<p>Isolation of a heritage attribute from its surrounding environment, context, or a significant relationship.</p>	<p>The proposed project is not anticipated to isolate a heritage attribute or individual heritage building from its surrounding environment, context, or a significant relationship. Although the construction of the new tower may visually alter the views and relationship between the subject property and the adjacent Eldon House property and other properties in the district, it is not anticipated to isolate either structure or property.</p>
<p>Land Disturbance such as a change in grade that alters the historic patterns of topography or drainage.</p>	<p>Completion of the proposed project will result in land disturbance; however, the construction will be temporary in nature and is not anticipated to affect any historic patterns or result in permanent impacts to heritage resources. In addition, it is understood that archaeological assessments will be completed for the project, which will also assess the archaeological potential for the subject property.</p>
<p>Changes in Land Use such as rezoning a battlefield from open spaces to residential use, allowing new development of site alteration to fill in the formerly open spaces.</p>	<p>The proposed project will result in a change in use for the property. The property is currently being used for commercial purposes, but is proposed for use as commercial, residential, and public space purposes.</p>
<p>Obstruction of significant views or vistas from, within, or to a built and natural feature.</p>	<p>Given that the proposed project will result in the construction of a proposed tower immediately adjacent to a heritage structure on the same property and others in the vicinity such as Eldon House, it is likely that the proposed project will result in shadows on the property that may alter or change the visibility of the scale of the three heritage buildings and others in the</p>

Indirect Impacts	Relevance to the Project
	vicinity, and of their location along the Thames River described in the designating-bylaws and recognitions.

Based on the extensive cultural heritage value that the heritage conservation district exhibits and the large-scale development that is proposed, it is anticipated that the project has potential to result in both direct and indirect impacts to the identified cultural heritage value of the district. Specifically, the integration of the new tower with the heritage structures on site and in the vicinity has the potential to result in alterations. Views to and from properties in the district would be significantly altered as a result of the proposed tower. The impact would be lessened by the fact that this part of Downtown London is an area of intensification with a number of high-rise buildings already in place.

4.3 Shadow Impacts

The juxtaposition of the proposed tall building (40 storeys) adjacent to low to mid-rise 19th and early 20th Century heritage resources and landscapes necessitates a review of the impacts of shadows on buildings and landscapes in the vicinity, as required by the City of London for a complete application.

The applicant subsequently completed a shadow study to identify impacts of the development during quarterly intervals of the year: March 31st, June 21st, September 2nd and December 21st. These figures have been compared with the impacts on specific heritage resources in the vicinity of the development which include the Eldon House building, Eldon House Gardens, Harris Park and the subject buildings from 435 to 451 Ridout Street. Images of Shadow Impacts are attached in the Appendix to this report, and results of the study are provided in Table 8 below.

Table 9: Shadow Impacts

Eldon House Building		
March 21st	9:00 a.m.	No Shadow
	12:00 noon.	Shadow
	3:00 p.m.	No Shadow,
	5:00 p.m.	No Shadow
June 21st	9:00 a.m.	No Shadow
	12:00 noon.	Shadow
	3:00 p.m.	No Shadow
	5:00 p.m.	No Shadow
September 2nd	9:00 a.m.	No Shadow
	12:00 noon	Shadow
	3:00 p.m.	No Shadow
	5:00 p.m.	Shadow
December 21st	9:00 a.m.m	
	12:00 noon	Shadow
	3:00 p.m.	Partial Shadow
	5:00 p.m.	
COMMENTARY:		
Negligible shadow impact on Eldon House Building		

Eldon House Garden		
March 21st	9:00 a.m.	Partial Shadow
	12:00 noon.	Partial Shadow
	3:00 p.m.	Partial Shadow from trees only
	5:00 p.m.	Partial Shadow from trees
June 21st	9:00 a.m.	Partial Shadow
	12:00 noon.	Partial Shadow
	3:00 p.m.	Partial Shadow from Trees
	5:00 p.m.	Partial Shadow from Trees
September 2nd	9:00 a.m.	Partial Shadow
	12:00 noon	Partial Shadow
	3:00 p.m	Shadow from Trees only
	5:00 p.m	Shadow from Trees Only
December 21st	9:00 a.m.m	
	12:00 noon	Partial Shadow
	3:00 p.m.	Partial Shadow
	5:00 p.m.	
COMMENTARY		
Negligible shadow impact on Eldon House Gardens. Shadow moves through the site.		
Some shadow from existing tree canopy.		
Consultation with an arborist may be considered to determine the need to bolster future tree canopy with shade tolerant trees.		

Harris Park		
March 21st	9:00 a.m.	Partial Shadow
	12:00 noon.	No Shadow
	3:00 p.m.	No Shadow
	5:00 p.m.	No Shadow
June 21st	9:00 a.m.	Partial Shadow
	12:00 noon.	No Shadow
	3:00 p.m.	No Shadow
	5:00 p.m.	No Shadow
September 2nd	9:00 a.m.	Partial Shadow
	12:00 noon	No Shadow
	3:00 p.m.	No Shadow
	5:00 p.m.	No Shadow

December 21st	9:00 a.m.	
	12:00 noon	No Shadow
	3:00 p.m.	No Shadow
	5:00 p.m.	
COMMENTARY		
Negligible Shadow impact on Harris Park		

435, 441-447 and 451 Ridout Street		
March 21st	9:00 a.m.	Shadow from Court House
	12:00 noon.	No Shadow
	3:00 p.m.	Partial Shadow
	5:00 p.m.	Shadow – Partially from subject building
June 21st	9:00 a.m.	Shadow from Court House
	12:00 noon.	No Shadow
	3:00 p.m.	Partial Shadow
	5:00 p.m.	Partial Shadow including from subject buildings
September 2nd	9:00 a.m.	Shadow from Court House
	12:00 noon	No Shadow
	3:00 p.m.	Partial Shadow
	5:00 p.m.	Shadow Partially from subject buildings
December 21st	9:00 a.m.	
	12:00 noon	Shadow, partially from subject buildings
	3:00 p.m.	Shadow partially from subject buildings
	5:00 p.m.	
COMMENTARY		
Negligible Shadow impact. Shadow impacts in the afternoon from the buildings themselves.		

Due to the location of the subject development relative to buildings and landscapes to the east, no significant impact was identified on the Thames River, Labatt Park or the Black Friars-Petersville Heritage Conservation District.

The greatest impact from shadows on the Eldon House and grounds will be experienced in the winter when the sun is low in the sky. Because the greatest impact will be in the winter, there will be no negative impact as a result of shadows to the trees on the grounds. Ron Koudys, landscape architect involved in the project and a member of the Eldon House Board, notes that the gardens on the grounds are currently in shade as they are located under the trees. It is not anticipated that the increased shadows will negatively impact the trees and gardens on the Eldon House grounds.

With respect to shadows resulting from the height of the development, because the building is located right next door to the Eldon House there would be no difference to the shadow impact based on the height of the proposed tower once it is over 10-storeys tall. As in, the shadows resulting from the development will be the same if the building is 20-storeys or 40-storeys.

Overall due to the location of the subject development relative to other heritage sites, the design with its slender point tower, and existing impacts from tall buildings and features such as the Court House, it was determined that the impact on shadow and sunlight on heritage resources was negligible. The greatest potential impact appears to be in the vicinity of the Eldon House gardens which retain reasonable sunlight; however, as the greatest shadow impacts will occur in the winter impacts to vegetation are anticipated to be minimal.

4.4 Views and Vistas

Views to and from the Ridout Complex are important and identified in the designation by-law as features to be conserved as “Viewscape on the complex as a whole from surrounding streets”.

All three buildings are proposed to be conserved as part of this proposed development, The retention of the streetscape, combined with adjacent buildings would combine to create a strong street level heritage presence, as a result, views to and from the complex would remain unaltered.

Generally, views of the River are not achievable between the buildings of the Ridout Complex so development of the complex behind the Ridout Streetscape would not impede vistas.

The height of the tower would change the overall appearance of the site from a distance, however, this is mitigated by the fact that the subject area is home to a number of high-rise residential and employment buildings signifying the location of a major urban centre, which as the centre of the sixth largest city in Ontario, Downtown London is.

The Tower would change views of the downtown from the River as well as communities and public spaces, such as the Forks and Labatt Park, however, these views are already interspersed with multiple high-rise buildings meaning that the proposed tower would not look out of place on the skyline from these areas.

4.5 Impacts on the Thames River Corridor and “The Forks”

The Thames River is recognized as Canadian Heritage River and its importance in the natural and cultural development of the City of London is well documented. The policy framework which regulates cultural heritage, appearance and vistas of new development within the corridor are outlined in the Thames River Corridor. An analysis of relevant policies and the guidance provided by them related to this particular development is outlined and analyzed in Table 9 below.

Table 10: Impacts to the Thames River Corridor and Forks

POLICY	ANALYSIS AND COMMENTARY
<p>THAMES RIVER CORRIDOR PLAN Areas identified in the Corridor Plan as having significant Views and Vistas should be considered visually sensitive. New development, as potentially seen within these viewsheds, (or in other highly visible areas as may be identified through the planning and development process) should be subjected to a visual impact assessment to ensure that significant views into and from the Corridor are protected.</p>	<p>The Thames River Corridor plan references Views and vistas. There is no specifically defined vista identified, however, it is generally understood to be traditional views of the city from the time of early postcards.</p> <p>The view of the downtown from the Forks is a major vista as are others in the vicinity.</p> <p>The vista to the east from across the Thames already includes high rise buildings. The vistas are therefor anticipated to be not adversely impacted. While the subject proposal is taller than other high-rise buildings in the vicinity as a general rule, once a certain height is reached with higher density developments, the number of stories above that may be negligible other than shadow impacts and capacity of local services to accommodate higher densities.</p>
<p>THAMES RIVER CORRIDOR PLAN Areas of Special Interest North Branch 8. Harris Park (Figure 4a)</p> <p>Recommendation: A Master Plan is recommended to review the park’s potential and to develop recommendations and a plan for upgrading the park. The Plan should consider river hydrological functions; address the site’s physical issues; and identify an appropriate program and design that responds to current recreation needs and trends (as identified in the Parks and Recreation Strategic Master Plan) along with better integration with the Downtown</p> <p>THAMES RIVER CORRIDOR PLAN Referencing another site – the London Hydro Lands, the plan anticipates areas of intensification, noting that should this particular site area become an area for redevelopment the design parameters for Urban Noted and Edges would apply, and better connections to the TVP are needed.</p>	<p>The Thames River Plan appears to anticipate development of a higher density along its corridor. This is not specifically applicable to this area, but illustrates considerations related to higher densities along the waterfront.</p>
<p>LONDON PLAN (OFFICIAL PLAN)</p> <p>123_ Recognizing the important role of the Thames Valley Corridor, the following actions will be taken: 1. Promote and enhance the Forks of the Thames River and the Thames Valley Corridor as an important natural, cultural, recreational, and aesthetic resource within our city.</p>	<p>The view from the Forks to the City across the Ridout site already includes higher density buildings, so the proposed new construction would not violate an established low-density trend. The positive economic impact resulting from the proposed development has the potential to enhance the overall vitality of the area and use of public park facilities.</p>

POLICY	ANALYSIS AND COMMENTARY
<p>SUPPORT CULTURAL AND INNOVATIVE PROGRAMMING TO CREATE A CITY THAT EXUDES INNOVATION, VIBRANCY, CREATIVITY AND ENTREPRENEURIALISM</p> <p>539_ Improve the vibrancy of Dundas Street, our Cultural Corridor, which runs through Old East Village and the Downtown, and enhance connections to the Thames River.</p>	<p>The positive economic impact resulting from the proposed development has the potential to enhance the overall vitality of the area and use of public park facilities.</p>
<p>10. Create strong connections to the Thames River through our streets and pathways, and by including active frontages of Downtown development onto the Thames Valley Corridor wherever possible in conformity with the Environmental Policies of this Plan</p>	<p>The proposed development presents a strong podium and landscaped grounds to the riverfront and therefore achieves the policy of achieving an active riverfront frontage.</p>
<p>802. 3) The evaluation of height and built form will take into account access to sunlight by adjacent properties, wind impacts, view corridors, visual impacts on the Thames Valley Corridor, and potential impacts on public spaces and heritage properties located in close proximity to proposed development.</p>	<p>Impacts on sunlight and daylight are analysed elsewhere in this report, however, due to the location of the riverfront to the west of the subject site, the impact of shadowing or reduction of daylight on the Thames River corridor is minimal to none.</p>

A review of the relevant policy framework confirms that the proposed development is compatible with local and conservation authority the objectives of the Thames River Corridor. The subject building fronts onto the river and addresses the river front both in built form and in landscape in its design. The proposed development would serve to enhance the economic vitality of the immediate area resulting in better use and potential physical enhancement of the riverfront resulting from parkland dedication fees. The Tower is one of a number of high rise buildings in the vicinity and adds to the already established trend of a high rise downtown skyline as viewed from the Thames Valley.

4.6 Construction Related Impacts

The subject development may result in impacts within the property and upon adjacent properties related to vibration, construction activities and upon the health of mature vegetation.

It is anticipated that analysis of detailed impacts to the heritage buildings on the subject property and adjacent properties related to vibration and other construction practices would be documented and assessed by a qualified structural professional, and mitigation recommendations identified prior to commencement of excavation on the site, as well as a strategy for dealing with unanticipated impacts as a result of vibration during construction.

It is anticipated that analysis of detailed impacts to significant mature vegetation on the subject property and adjacent properties resulting from construction practices would be documented and assessed by a qualified arbourist, and mitigation recommendations identified prior to commencement of excavation on the site.

4.7 Record of Consultation

As part of the preparation of this HIA AECOM began the process of consultation with the City of London, as well as members of the Eldon House board. The AECOM heritage and Farhi development team have attended two meetings with the City of London project team, including Laura Dent, Heritage Planner, and Michael Tomazincic project lead, in late September and early October to review comments on the draft HIA report and discuss revisions to the scope of information included. Numerous update and planning calls, emails, and meetings have occurred throughout the revision process and it is expected that consultation with the city will continue.

Ron Koudys, of Ron Koudys Landscape Architects Inc. is the landscape architect for the development and also a member of the Eldon House Board. He has noted his conflict of interest to the board and continues to operate on both teams. AECOM spoke with Ron who has indicated that he sees a real opportunity for the Eldon House in working together with the Farhi team while developing this project. While any development near a heritage property of such significance as the Eldon House generates an emotional response, there are opportunities for the Eldon House in partnering with the Farhi development team. The Eldon House is located in the downtown core and is surrounded by high rise buildings, which will only increase as urban intensification continues. During the discussion Ron identified a number of opportunities that could be explored for the Eldon House to gain positives from this project, including an increased focus on the grounds of the Eldon House, creating a Horticultural Destination that would attract repeat tourists; reconnecting the Eldon House to the river flood plain could be completed, as was the case in the past during the time the Harris family lived there; and the possibility of engagement with Fanshawe college to conduct landscape classes in the gardens. Greater traffic of people to the Eldon House grounds would help reduce the transient population's use. Ron felt that by working together there could be benefits and funding made available to further Eldon House initiatives, and allowing the Eldon House board to have influence on the final design of the neighbouring development.

As this project moves forward consultation with the full Eldon House Board of Directors must occur in order to understand and incorporate their concerns and/or ideas. The project team would like to present to the Eldon House Board the design plans and listen to questions and comments the board has with the proposed plan for consideration. During the writing of this HIA members of both AECOM and Farhi Holdings have attempted to set up a meeting with Mark Tovey, Chairman of the Eldon House Board, and set a date for the presentation to the board. Numerous attempts to set dates have occurred over the month of November; however, due to other commitments we have been unable to arrange a time with Mr. Tovey for discussions. Tara Whitman, Curator at the Eldon House has been consulted and has indicated to the team that this time of year is very busy and availability is limited. On November 20 Mr. Tovey communicated that the Eldon House Board of Directors had no available time to meet with Farhi, nor the time to prepare for a meeting. Based on the need for the HIA to be submitted to further the re-zoning application process the city has agreed to accept this report as is, with the understanding that further consultation with the Eldon House Board of Directors is essential and must be conducted prior to site plan approval.

5. Consideration of Alternatives, Mitigation and Conservation Methods

There is no one correct way to mitigate the adverse impacts of new construction on, or adjacent to historic structures and/or heritage properties. Strictly from the perspective of best practice for heritage conservation, the preferred option is one that conserves a property's cultural heritage value. The Provincial Policy Statement, 2014, identifies the requirement to conserve cultural heritage value; specifically, Section 2.6.1 states, "Significant built heritage resources

and significant cultural heritage landscapes shall be conserved.”⁴ Typically, this involves maintaining a heritage resource *in situ*. In reality, socio-economic, technical, and/or environmental site considerations may require some form of compromise and/or alternate means of conservation.

The MTCS identifies mitigation or avoidance strategies that can be used to mitigate the extent of impacts as a result of a proposed undertaking. These include:

- Alternative development approaches;
- Isolating development and site alteration from significant built and natural features and vistas;
- Design guidelines that harmonize mass, setback, setting, and materials;
- Limiting height and density;
- Allowing only compatible infill and additions;
- Reversible alterations; and,
- Buffer zones, site plan control, and other planning mechanisms.

The subsection below outlines various forms of mitigation that should be considered more specifically for this project, when refining the proposed tower for the subject property. The mitigation options described below outline strategies which will mitigate the impacts described above. From the cultural heritage perspective, preserving and enhancing the cultural heritage value of the property should be considered a key opportunity and priority for this property and project.

Table 11: Mitigation Measures

ADDRESS	IMPACT IDENTIFIED	PROPOSED MITIGATION MEASURE (S)
<p>435 to 451 Ridout Street</p>	<p>Potential impact of the Tower design, scale and massing on the heritage environments of the subject properties, adjacent heritage districts and the Thames River Corridor</p>	<p>In order to mitigate the significant visual impacts that will be experienced on the property, the project should be designed in a manner that minimizes visual impacts or impacts to views to and from the subject property and its heritage buildings. Specifically, this should result in considerations about how the proposed tower will integrate with the historic structures.</p> <p>Some refinements that may assist in the transition between the historic structures include the siting of the tower in relation to the heritage structures, the use of a podium design to minimize the massing impact on the heritage structures, and the use of exterior materials as they relate to the historic building fabric.</p> <p>The current siting of the tower on the property, identifies that the tower is located towards the north end of the property and is located to the rear of the 451 Ridout Street North building. Where feasible, the footprint of the tower should be minimized in order to not visually overwhelm the heritage dwelling. Siting options for the tower on the property should continue to separate the heritage structures from the new development as much as possible, in order to allow for the two-to-three-storey streetscape appearance of the heritage buildings to remain a defining feature of this portion of Ridout Street North.</p>

⁴ Provincial Policy Statement, 2014.

		<p>The massing between the heritage buildings and the proposed tower should also be considered. The tower should be designed in manner that does not dominate the streetscape character or overwhelm the two- and three-story heritage buildings on the property. Given the scale of the proposed tower, it is inevitable that the massing will be an obvious juxtaposition in scale/mass; however, a podium design may assist in mitigating the impact of the height differences between structures. If the tower is designed to be set back from the heritage dwelling and the bottom stories of the tower are sensitive in terms of their design materials to allow for a sympathetic transition between the old and newer structures, the massing and scale of the tower may be reduced at the streetscape level.</p> <p>Lastly, some consideration should be given to the exterior materials for the tower and their relation to the heritage structures. It is assumed that glazing will be the defining exterior material of the building. If feasible, special consideration to the materials and design of the bottom few storeys should be given to consider a design approach that connects the two eras of architecture. For instance, the use of buff brick similar in appearance to the heritage structures could be considered for any decorative elements that could conceptually tie the structures together. The <i>Standards and Guidelines for the Conservation of Historic Places in Canada</i> should be consulted while refining the design in order to consider how the new development will relate to the existing heritage structures. Particularly, Standard 11 of the document captures the ways in which new development should be considered in a historic place. The standard states “Conserve the heritage value and character-defining elements when creating any new additions to an historic place or any related new construction. Make the new work physically and visually compatible with, subordinate to and distinguishable from the historic place.”⁵</p>
<p>435 to 451 Ridout Street</p>	<p>Potential impact on the usability of the heritage structures at 435 to 451 Ridout Street resulting from the adjacent development, access to parking, etc. and the need for identification of a suitable re-use</p>	<p>If the retention of the three heritage structures requires further adaptive re-use as a part of their on-going integration with the proposed project, suitable re-use opportunities should be identified and pursued for the structures. Heritage buildings are often at most risk when a suitable use is not found. If feasible, their continued commercial use is encouraged. If further re-use is required for the structures, the interior elements described within the designating by-laws and recognitions should be retained and preserved.</p>
<p>435 Ridout</p>	<p>Potential impact on the usability of 435 Ridout and appearance of the overall site in the long term while in a damaged condition</p>	<p>The structure known as 435 Ridout Street North should be restored to its previous condition, prior to the fire experienced within the structure in September 2018. Restoration efforts should be completed in a manner that preserves and enhances the property's cultural heritage value, and all conservation work should be</p>

⁵ *Standards and Guidelines for the Conservation of Historic Places in Canada, Second Edition, 2010.*

		<p>completed by qualified experts with heritage conservation experience.</p>
<p>435 Ridout, Eldon House and Harris Park</p>	<p>Potential Impact on the viability of mature vegetation and long-established gardens.</p>	<p>It is understood that plantings and naturalization activities will be completed for the proposed project on the west side of the property, but also along the border with the Eldon House property. When designing new plantings for the property, the Downtown Heritage Conservation District should be consulted for appropriate new plantings and streetscape enhancements that should be utilized for the property and its integration within the broader context.</p> <p>Public space proposed on the property should also be considered for historic interpretation. As a result of the various levels of heritage recognition and protection for the property, opportunities to include interpretative signage, or commemorative materials should be explored, so as to enhance the public awareness and knowledge of the history of the property, and its role within London's larger municipal history. These public spaces may also provide an appropriate space for public art, or installations that help enhance the relationship between the Thames River and the city.</p> <p>While the design of the proposed development is conceived to minimize the shadowing resulting from a building of this scale, there will be some increased shadowing on existing mature vegetation in the vicinity of Harris Park, the subject lands and on the Eldon House Museum property.</p> <p>A shadow study has been prepared as part of the complete application which identifies the level of increased shadowing that certain areas will receive.</p> <p>To mitigate any negative impacts resulting from increased shadowing, a qualified arborist should be retained to undertake an assessment of the long term impacts on existing vegetation and if it is determined that the Tree canopy would be impacted a landscape architect should be retained to identify shade tolerant plantings to be introduced to the site to maintain the tree canopy in the long term.</p>
<p>435 to 451 Ridout Street</p>	<p>Potential land disturbances from vibration caused by construction activities.</p>	<p>The effect of construction vibrations on heritage and/or historic structures is not fully understood, yet negative effects have been demonstrated on buildings with a setback of less than 40 metres from roadside. Given the proximity of the proposed construction activity to the heritage buildings, there is potential for the project to result in vibration impacts. In order to mitigate vibration impacts on the heritage buildings during construction, preconstruction building surveys and vibration monitoring should be carried out during construction to identify any adverse effects to this resource resulting from project related construction activities.</p>

Eldon House	Potential Land disturbances from vibration caused by construction activities	Vibration monitoring should be carried out during construction to identify any adverse effects to this resource resulting from project related construction activities.
466 Ridout Street North	Potential Land disturbances from vibration caused by construction activities	Vibration monitoring should be carried out during construction to identify any adverse effects to this resource resulting from project related construction activities.
468 Ridout Street North	Potential Land disturbances from vibration caused by construction activities	Vibration monitoring should be carried out during construction to identify any adverse effects to this resource resulting from project related construction activities.
470 Ridout Street North	Potential Land disturbances from vibration caused by construction activities	Vibration monitoring should be carried out during construction to identify any adverse effects to this resource resulting from project related construction activities.
472 Ridout Street North	Potential Land disturbances from vibration caused by construction activities	Vibration monitoring should be carried out during construction to identify any adverse effects to this resource resulting from project related construction activities.
435 to 451 Ridout Street	Avoiding potential impacts due to staging and construction through implementation of protection measures.	A plan should be developed for screening and protecting heritage buildings and mature vegetation including root areas and tree canopies and prior to staging and excavation commencing on the site.
Eldon House	Avoiding potential impacts due to staging and construction through implementation of protection measures.	A plan should be developed for screening and protecting heritage buildings and mature vegetation including root areas and tree canopies and prior to staging and excavation commencing on the site

5.1 Additional Studies Required

It is clear that additional studies will be required to assist in the mitigation of adverse impacts to the heritage buildings of the Ridout Complex. Through consultation with the City of London’s Heritage Planner and Planning team a holding provision will be placed on the property to ensure the following studies are completed:

- Building Conditions Assessment Report, assessment is required before and after construction and should be subject to a peer review.
- Vibration Study, to assist in determining the level of vibration that would be acceptable to avoid negative impacts during construction.
- Heritage Conservation Plan, this should address the heritage attributes in the interior of the buildings as well as exterior features.
- Arborist Report, to effectively determine the impacts on vegetation and assist with tree preservation or replacement.
- Implementation and Monitoring

5.2 Mitigation Strategies

The table above identified potential mitigation measures to reduce adverse impacts of the proposed development identified on cultural heritage resources and attributes of the Downtown London HCD. Generally, five primary impacts were identified; the potential for vibration effects resulting from construction, the impact on the character of the area, impact of shadowing on mature vegetation, impact resulting from construction of a 40 storey building on site, the long-term use of the buildings at 435-451 Ridout Street, and potential impacts during the construction process.

In order to mitigate the potential direct and indirect impacts to identified cultural heritage values described within this report, the mitigation strategies described in this report should be considered in further project refinements and approaches. Details related to the exterior design, the streetscape character, and the future re-use of the heritage structures should be considered in depth as a part of the proposed project in order to mitigate impacts, and conserve the cultural heritage value of the property.

The impacts resulting from the proposed development are addressed below.

5.2.1 Vibration

Some impacts, such as the potential for vibration on properties within 40 metres of the proposed development, can be mitigated with vibration assessments to identify whether vibration from construction activities has affected historic masonry. It is recommended that an assessment occur before construction, to identify a benchmark for impacts, and post-construction, to identify whether impacts have occurred.

In order to prevent negative indirect impacts, the heritage resources should be isolated from construction activities. It is recommended that site plan controls be put in place prior to construction to prevent potential indirect impacts as a result of the Project. The site plan control methods shall be determined in advance of construction by the proponent to indicate where Project activities are restricted as described below. These controls should be indicated on all construction mapping and communicated to the construction team leads. It is recommended that a buffer zone be established where no construction activities can occur. The proponent's construction team should monitor that buffer zone delineation, outlining the limit of the construction footprint and subsequent setback from heritage features, is maintained throughout construction. A qualified building condition specialist should be retained to determine if any damage was incurred as a result of the construction activities.

5.2.2 Mature Vegetation

The existence and health of mature vegetation is important to maintaining the character of the area, the heritage district, public realm and heritage properties. Impacts from construction due to construction activities or long-term shadowing increases should be planned for and mitigated where feasible. It is understood some that trees at the limit of the Eldon House property will be removed in order to construct this development.

- The completed shadow study shows that all vegetation has access to sunlight at all times of the year. Review by a qualified arborist would be necessary to determine whether the new lighting conditions would be sufficient to maintain the health of the existing tree canopy and if there is concern a landscape architect should be retained to determine the potential for new plantings on the site and adjacent public sites to replenish the tree canopy in the long term.

A plan should be developed and implemented prior to disturbance of the site to the satisfaction of a qualified arborist to ensure protection of vegetation during the staging, excavation and construction process.

5.2.3 Existing Ridout Block Buildings

The successful rehabilitation and integration of the buildings of the Ridout Block National Historic Site within the new development is key to ensuring that the cultural heritage resources on the existing site are retained and preserved for the future. A detailed timed and budgeted plan should be developed to achieve the full restoration and integration of the Ridout Block buildings into the complex.

5.3 Heritage Impact Assessment Update

Upon further design refinement, this HIA should be updated in order to capture any design alterations or changes that have been made to the proposed site plan or tower design to reflect the heritage conservation efforts as a part of the proposed development. Continued consultation with City of London Heritage Planning staff is encouraged as a follow-up step in order to ensure that the significant heritage attributes and cultural heritage value of the property will be conserved as a part of the proposed development. Due to the extensive cultural heritage value of this property, collaboration with Heritage Planning staff will ensure that the mitigation strategies will be appropriately identified and undertaken in order to preserve and enhance the heritage value of the site.

6. Summary Statement of Conservation Recommendations

The subject property contains and is surrounded by a wealth of cultural heritage and natural heritage resources. The site itself has designation under Part IV and Part V of the Ontario Heritage Act and is also a National Historic Site, together with adjacent features such as Eldon House Museum, the valley of the River Thames, designated a Canadian Heritage River and nearby listed and designated properties, such as the London Museum and Old Middlesex County Court house comprise a landscape almost unprecedented in Ontario for its significance and level of protection.

The site is also within the growing downtown core of the sixth largest city in Ontario and has been identified as an area for intensification.

The proponent of the project has recognized the significance of the site by proposing to retain all heritage resources on site. The following recommendations serve to provide a guideline and a plan to ensure that the heritage of the site and environment is protected.

Properties within and adjacent to the proposed development site contain cultural heritage resources. Based on the impacts identified to cultural heritage resources, the following mitigation measures are recommended:

Establish a 40 metre buffer, or the maximum possible, between construction activities and structures identified as cultural heritage resources during the construction phase for the properties located at:

- 435 Ridout Street North
- 441 Ridout Street North
- 451 Ridout Street North
- 481 Ridout Street (Eldon House)

- 468 Ridout Street North
- 470 Ridout Street North
- 472 Ridout Street North
- 466 Ridout Street North

Monitor vibration on adjacent identified cultural heritage resources before and after the construction phase is completed.

Develop a plan, timeline and budget for the conservation and use of the Ridout Block Buildings at 435, 441 and 451 Ridout Street.

Continue to utilize high quality materials such as brick inspired by the Ridout Buildings and glazing as the primary materials of the proposed new building to achieve compatibility with the existing heritage context.

Continue to utilize the podium and point tower massing as proposed which provides the optimal scale and mass for integration of this scale of building into the historic context

Retain the services of a qualified arborist to undertake a review of the shadow impact plan to determine the long-term health of the mature tree canopy on the Eldon House and adjacent Harris Park. If concerns are identified with the long term health of the tree canopy, retain the services of a landscape architect to identify a planting plan for shade tolerant species of trees to maintain the tree canopy in the long term.

7. Closure

This report has been prepared for the sole benefit of Farhi Holdings Corporation and may not be used by any third party without the express written consent of AECOM Consulting Ltd. Any use which a third party makes of this report is the responsibility of such third party.

We trust this report meets your current requirements. Please do not hesitate to contact us should you require further information or have additional questions about any facet of this report.

8. Sources

Primary and Secondary Sources

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Ontario Ministry of Tourism, Culture and Sport: Eight Guiding Principles in the

Conservation of Historic Properties

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Reference Guide on Physical and Cultural Heritage Resources (1996)

Guidelines for Preparing the Cultural Heritage Resource Component of Environmental Assessments (1992)

Guidelines on the Man-Made Heritage Component of Environmental Assessments (1981)

Environmental Guide for Built Heritage and Cultural Heritage Landscapes (2007)

National and International Standards and Resources:

Canadian Register of Historic Places

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Parks Canada Standards and Guidelines for the Conservation of Historic Places in Canada

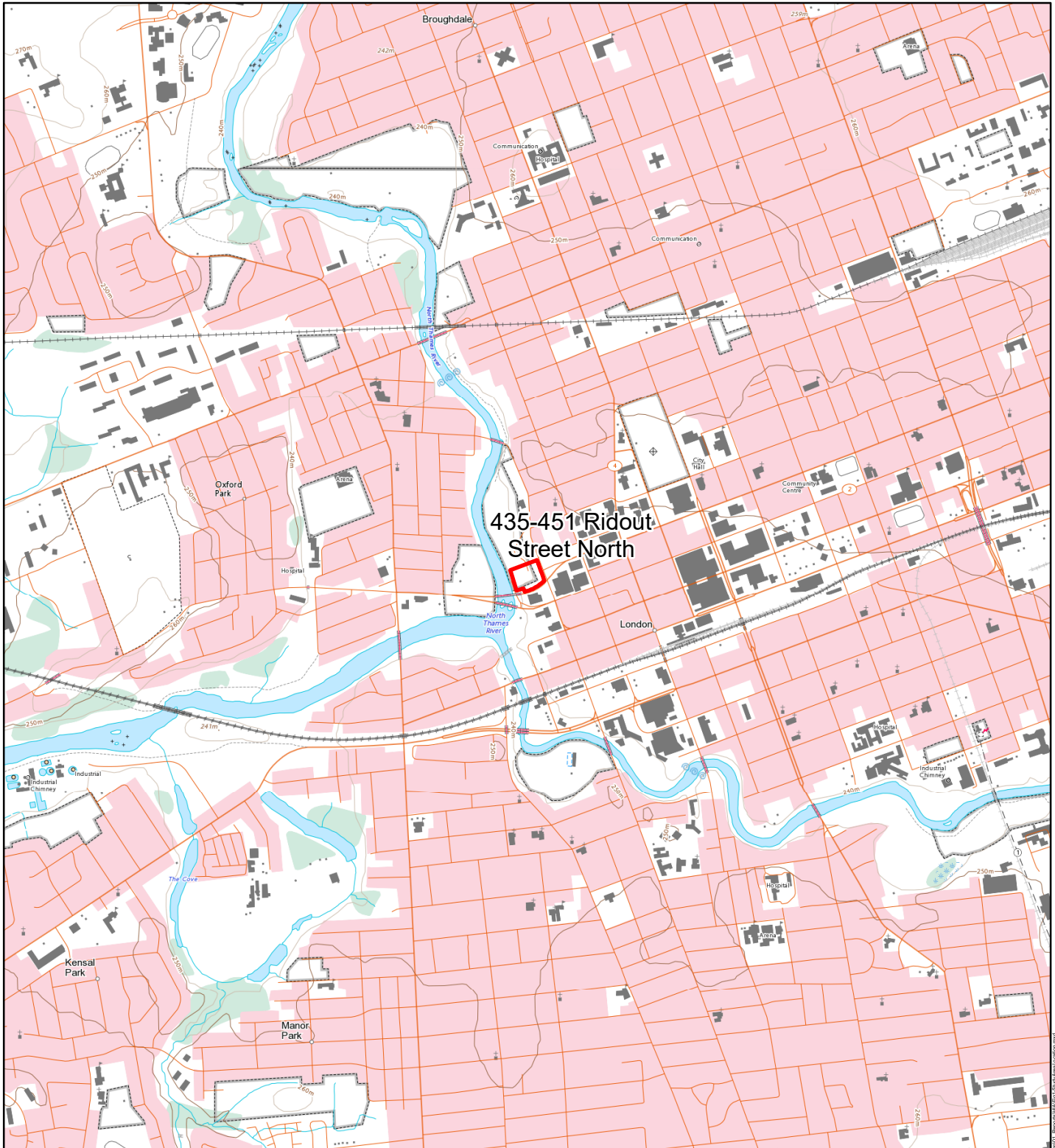
http://www.pc.gc.ca/docs/pc/guide/nldclpc-sgchpc/index_E.asp

Parks Canada National Historic Sites of Canada


http://www.pc.gc.ca/progs/lhn-nhs/index_e.asp

9. Figures

The following section provides the figures referred to in this report.



Legend

 Study Area



Heritage Impact Assessment
435-451 Ridout Street North
London, Ontario

Location of Study Area

November 2019

1:25,000

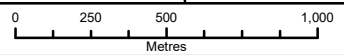
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AECOM


Figure 1

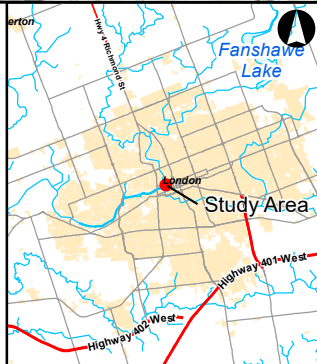


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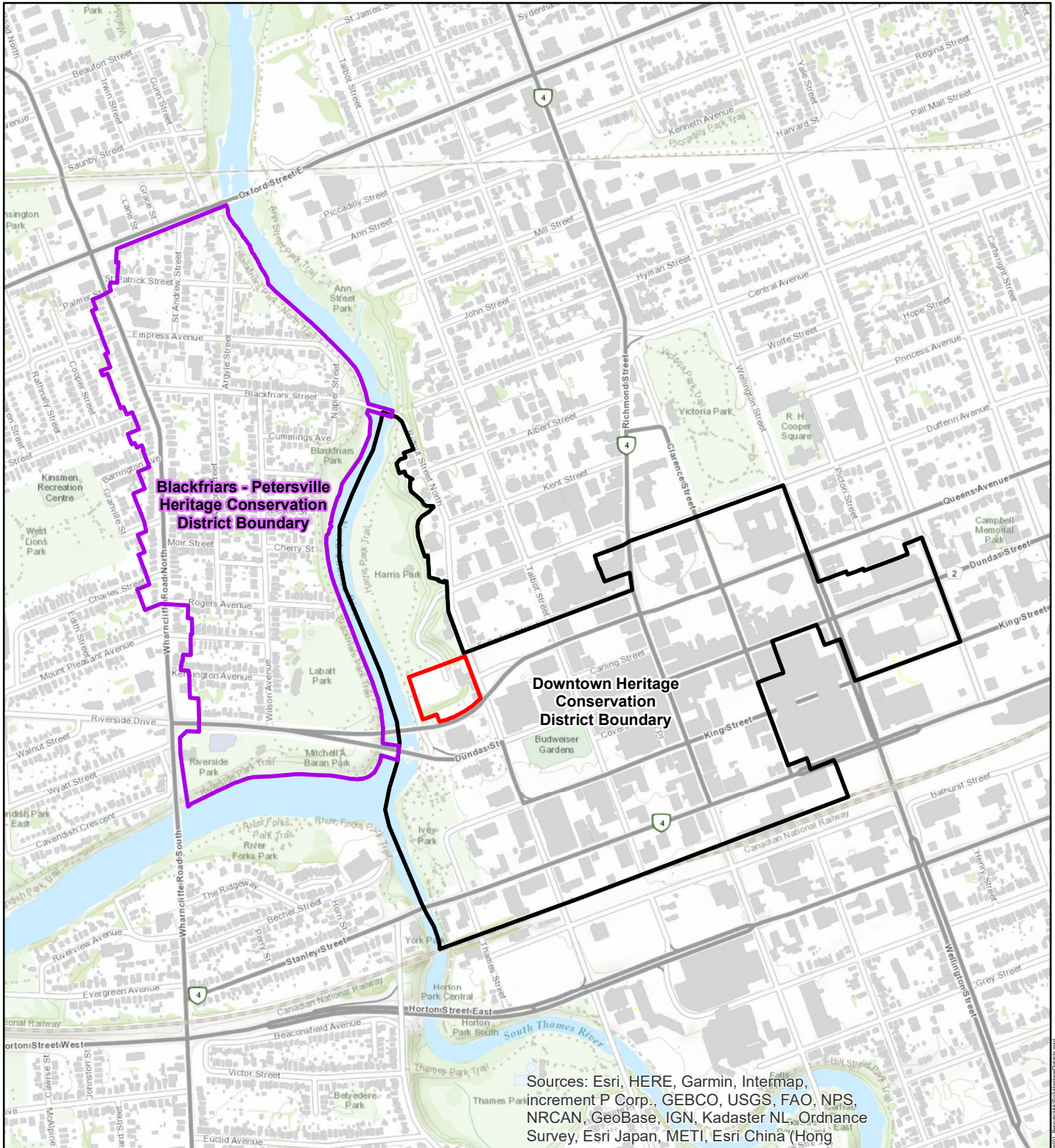


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 Study Area



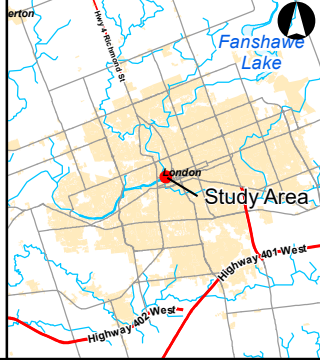
Heritage Impact Assessment 435-451 Ridout Street North London, Ontario		
Study Area in Detail		
November 2019	1:1,000	Datum: NAD 83 UTM17 Source: LIO 2016, City of London 2017
PH: 60587405	V#:	
AECOM		Figure 2
0 10 20 40 Metres		
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Sources: Esri, HERE, Garmin, Intermap, increment P Corp., GEBCO, USGS, FAO, NPS, NRCAN, GeoBase, IGN, Kadaster NL, Ordnance Survey, Esri Japan, METI, Esri China (Hong

Legend

- Study
- Blackfriars - Petersville Heritage Conservation District Boundary
- Downtown Heritage Conservation District Boundary



Heritage Impact Assessment 435-451 Ridout Street North London, Ontario		
Heritage Districts		
November 2019	1:12,000	Datum: NAD 83 UTM17 Source: City of London 2019
PH: 60587405	V#:	Figure 3
AECOM		
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Legend

- Study Area
- Heritage Property
- Blackfriars - Petersville Heritage Conservation District Boundary
- Downtown Heritage Conservation District Boundary



Heritage Impact Assessment 435-451 Ridout Street North London, Ontario		
Heritage Features		
November 2019	1:2,500	Datum: NAD 83 UTM17 Source: LIO 2016, City of London 2018
PH: 60587405	V#:	
AECOM		Figure 4
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FARHI HOLDINGS - 451 RIDOUT STREET



- NEW CONSTRUCTION
- EXISTING HERITAGE STRUCTURES TO REMAIN AS IS

SITE / FLOOR PLAN AT STREET LEVEL (49 SURFACE PARKING SPACES)



10. Resumes

The following section includes the resumes of people who contributed to the preparation of this HIA report.

Adria Grant, MA, CAHP

Ontario Department Manager – Impact, Assessment & Permitting

Education

Bachelor of Art (Hons), Anthropology & English. University of Western Ontario, 2000

Master of Art, Applied Archaeology. University of Western Ontario, 2016

Years of Experience

With AECOM: 6
With Other Firms: 13

Licenses/Registrations

Ontario Ministry of Tourism, Culture and Sport (MTCS) Professional Archaeological License (P131)

Canadian Association of Heritage Professionals (CAHP)

Ontario Association of Professional Archaeologists (APA)

RAQS Certification

Memberships

Canadian Archaeological Society

Ontario Archaeological Society

Summary

Adria Grant has been active in the field of cultural resource management since 1999, specializing in cultural heritage and archaeological assessments for provincial and federal government, municipal corporations, and private sector organizations. Adria is an experienced project manager having completed formal project management training through the Project Management Institute (PMI) as well as comprehensive and stringent company specific project management courses during her employment at Golder Associates, Stantec and AECOM. Adria consistently applies the knowledge, tools, and techniques of project management practices to the heritage field, streamlining processes and procedures to achieve client objectives. Adria has a wealth of experience working with municipal heritage planners in the context of development activities and has the ability to provide sound technical advice to proponents on the heritage process in Ontario.

Adria is professionally licensed by the Ontario MTCS, is a member of the Canadian Association of Heritage Professionals and the Ontario Association of Professional Archaeologists. In addition to professional memberships Adria actively participates in local heritage and archaeological events and is active and well known within the cultural resource community. She currently acts as the Department Manager of the Impact, Assessment & Permitting - Cultural Resources, Planning and Communications teams in Ontario, and Canadian lead for AECOM's North American Cultural Resources team.

Experience

Metrolinx, Ontario Line West Cultural Heritage Assessments, Toronto, Ontario. Technical lead for heritage assessments to support the construction of a new rail corridor to improve transit within the city of Toronto. Provided technical support to assist Metrolinx and the Ontario MTCS in creating project specific work plan procedure with a unique strategy for reporting. Will be responsible for consultation with the MTCS, senior review and technical excellence.

Metrolinx, Ontario Line North Cultural Heritage Assessments, Toronto, Ontario. Technical lead for heritage assessments to support the construction of a new rail corridor to improve transit within the city of Toronto. Provided technical support to assist Metrolinx and the Ontario MTCS in creating project specific work plan procedure with a unique strategy for reporting. Will be responsible for consultation with the MTCS, senior review and technical excellence.

Farhi Holding Corporation, 120 York Street Heritage Impact Assessment, London, Ontario. Project manager for a HIA to support the demolition of a structure located within the Downtown Heritage Conservation District. Responsible for senior review of the report.

County of Bruce, Old Reid Bridge Cultural Heritage Evaluation Report, Teeswater, Ontario. Senior review of cultural heritage evaluation report for the removal of an abandoned bridge in Bruce County.

City of London, Downtown Sewer Separation Phase 3, Heritage Impact Assessment, London, Ontario. Senior reviewer for HIA completed to evaluate impacts of infrastructure improvements within the Downtown Heritage Conservation District and adjacent to numerous listed or otherwise recognized heritage buildings.

City of London, 78-88 Oxford Street Cultural Heritage Evaluation Report, London, Ontario. Project manager for the CHER of a row of six residential structures in the City of London that will be negatively impacted by proposed road widening. The CHER determined that the houses as a group had cultural heritage value and would be eligible for designation.

Metrolinx Ontario Line South Cultural Heritage Assessments, Toronto, Ontario. Technical lead for heritage assessments to support the construction of a new rail corridor to improve transit within the city of Toronto. Gap analysis to be completed to determine

Union Gas, Stratford Reinforcement Project Cultural Heritage Evaluation Report, Perth County, Ontario. Project manager for the CHER conducted as part of an Environmental Assessment for a natural gas pipeline twinning project. The study involved a windshield study, the identification of built heritage and cultural heritage landscapes, creation of a heritage inventory, and the assessment of impacts to identified cultural heritage resources. The report included the direct application of the *Ontario Heritage Toolkit* and the *Cultural Heritage and Archaeology Policies of the Ontario Provincial Policy Statement, 2005*.

MHBC Planning, Cobourg Heritage Master Plan, Cobourg, Ontario. Technical specialist providing information on archaeological heritage resources and archaeological management of resources for the background Heritage Report and Heritage Master Plan created for the Town of Cobourg. The document is intended to provide high level legal advice to Town staff on a variety of conservation matters.

York Region, Stouffville Road Improvements Cultural Heritage Evaluation Report, Regional Municipality of York, Ontario. Technical lead for the CHER conducted as part of an Environmental Assessment for the proposed realignment of Stouffville road east of Yonge Street. Research identified a cultural heritage conservation district, listed and designated heritage structures as well as cultural heritage landscapes that should be considered during project design. A Heritage Impact Assessment was recommended once project design was better understood to mitigate any negative impacts to the identified heritage resources.

City of Toronto, Heritage Impact Assessment - Downsview Secondary Area Plan, Toronto Ontario. Primary researcher and technical lead for the HIA conducted as part of the Downsview Secondary Area Plan redevelopment. Numerous built heritage features are present within the study area, impacts to heritage features were assessed and it was determined that there were no anticipated direct or indirect impacts as a result of the undertaking.

Metrolinx, Technical Cultural Resource Services - Eglinton Crosstown Light Rail Transit (ECLRT), Toronto, Ontario. Peer reviewer of cultural heritage evaluation, assessment and impact assessment reports for the project. Provided strategic advice to the greater project team in relation to heritage requirements and conducted gap analysis.

Varna Wind Inc., Bluewater Wind Energy Centre Heritage Assessment Addendum, Huron County, Ontario. Technical specialist for an addendum to the original Heritage Assessment Report, conducted as part of an Application for a Renewable Energy Approval under Ontario Regulation 359/09 made under the *Environmental Protection Act*. The additional participating properties were screened for potential heritage resources and if potential heritage resources were identified they were evaluated according to the criteria outlined in O. Reg. 09/06 under the *Ontario Heritage Act*, as required by O. Reg. 359/09. A total of 49 structures were identified as as having cultural heritage value or interest according to O. Reg. 09/06. No further mitigation was recommended as it was determined that there were no anticipated direct or indirect impacts as a result of the undertaking.

Goshen Wind Inc., Goshen Wind Energy Centre Heritage Assessment Addendum, Huron County, Ontario. Technical specialist for an addendum to the original Heritage Assessment Report, conducted as part of an Application for a Renewable Energy Approval under Ontario Regulation 359/09 made under the *Environmental Protection Act*. The additional participating properties were screened for potential heritage resources. No features were identified as as having cultural heritage value or interest according to O. Reg. 09/06. No further mitigation was recommended as it was determined that there were no anticipated direct or indirect impacts as a result of the undertaking.

Jericho Wind Inc., Jericho Wind Energy Centre Heritage Assessment Addendum, Lambton and Middlesex Counties, Ontario. Technical specialist for an addendum to the original Heritage Assessment Report, conducted as part of an Application for a Renewable Energy Approval under Ontario Regulation 359/09 made under the *Environmental Protection Act*. The additional participating properties were screened for potential heritage resources and if potential heritage resources were identified they were evaluated according to the criteria outlined in O. Reg. 09/06 under the *Ontario Heritage Act*, as required by O. Reg. 359/09. A total of 51 structures were identified as as having cultural heritage value or interest according to O. Reg. 09/06. No further mitigation was recommended as it was determined that there were no anticipated direct or indirect impacts as a result of the undertaking.

NextEra Energy Canada ULC, Northpoint Wind Energy Centre, Eastern Ontario. Project manager for the identification and assessment of cultural heritage resources within the proposed limits of a large wind energy centre. Research included searches of the Ontario Heritage Properties Database, Parks Canada, municipal heritage planners, the Canadian Register of Historic Places, the Ontario Heritage Trust Plaques Database, and unregistered and registered cemetery databases.

NextEra Energy Canada ULC, Northpoint Wind Energy Centre Transmission Line, Eastern Ontario. Project manager for the identification and assessment of cultural heritage resources for a proposed transmission line related to a wind energy centre. Research included searches of the Ontario Heritage Properties Database, Parks Canada, municipal heritage planners, the Canadian Register of Historic Places, the Ontario Heritage Trust Plaques Database, and unregistered and registered cemetery databases.

MHBC Planning, Toronto Garden Heritage Conservation District Technical specialist providing information on archaeological heritage resources and archaeological management of resources for Toronto Garden District HCD Report.

Trout Lake I Solar, LP, LRP Cultural Heritage Resources Review, Trout Lake I Solar Energy Centre. Technical lead for information gathering as part of the LRP process related to heritage and archaeology. Research included searches of the Ontario Heritage Properties Database, the City of London Inventory of Heritage Resources, the Canadian Register of Historic Places, the Ontario Heritage Trust Plaques Database, and unregistered and registered cemetery databases.

Battersea Solar, LP, LRP Cultural Heritage Resources Review, Battersea Solar Energy Centre. Technical lead for information gathering as part of the LRP process related to heritage and archaeology. Research included searches of the Ontario Heritage Properties Database, the City of London Inventory of Heritage Resources, the Canadian Register of Historic Places, the Ontario Heritage Trust Plaques Database, and unregistered and registered cemetery databases.

Cordukes Solar, LP, LRP Cultural Heritage Resources Review, Cordukes Solar Energy Centre. Technical lead for information gathering as part of the LRP process related to heritage and archaeology. Research included searches of the Ontario Heritage Properties Database, the City of London Inventory of Heritage Resources, the Canadian Register of Historic Places, the Ontario Heritage Trust Plaques Database, and unregistered and registered cemetery databases.

Trout Lake I Solar, LP, LRP Cultural Heritage Resources Review, Trout Lake I Solar Energy Centre. Technical lead for information gathering as part of the LRP process related to heritage and archaeology. Research included searches of the Ontario Heritage Properties Database, the City of London Inventory of Heritage Resources, the Canadian Register of Historic Places, the Ontario Heritage Trust Plaques Database, and unregistered and registered cemetery databases.

Clarabelle I Solar, LP, LRP Cultural Heritage Resources Review, Clarabelle I Solar Energy Centre. Technical lead for information gathering as part of the LRP process related to heritage and archaeology. Research included searches of the Ontario Heritage Properties Database, the City of London Inventory of Heritage Resources, the Canadian Register of Historic Places, the Ontario Heritage Trust Plaques Database, and unregistered and registered cemetery databases.

Clarabelle II Solar, LP, LRP Cultural Heritage Resources Review, Clarabelle II Solar Energy Centre. Technical lead for information gathering as part of the LRP process related to heritage and archaeology. Research included searches of the Ontario Heritage Properties Database, the City of London Inventory of Heritage Resources, the Canadian Register of Historic Places, the Ontario Heritage Trust Plaques Database, and unregistered and registered cemetery databases.

Clarabelle III Solar, LP, LRP Cultural Heritage Resources Review, Clarabelle III Solar Energy Centre. Technical lead for information gathering as part of the LRP process related to heritage and archaeology. Research included searches of the Ontario Heritage Properties Database, the City of London Inventory of Heritage Resources, the Canadian Register of Historic Places, the Ontario Heritage Trust Plaques Database, and unregistered and registered cemetery databases.

Northpoint I, LP, Northpoint I Wind Energy Project. Technical lead for information gathering as part of the LRP process related to heritage and archaeology. Research included searches of the Ontario Heritage Properties Database, the City of London Inventory of Heritage Resources, the Canadian Register of Historic Places, the Ontario Heritage Trust Plaques Database, and unregistered and registered cemetery databases.

Northpoint II, LP, Northpoint II Wind Energy Project. Technical lead for information gathering as part of the LRP process related to heritage and archaeology. Research included searches of the Ontario Heritage Properties Database, the City of London Inventory of Heritage Resources, the Canadian Register of Historic Places, the Ontario Heritage Trust Plaques Database, and unregistered and registered cemetery databases.

Michael J. Seaman, BES, MEDS, MCIP, RPP, CAHP, CMA

Senior Heritage Planner

Education	Years of Experience	Memberships	Training and Certifications
Masters of Environmental Design Studies, Conservation, Faculty of Architecture, Dalhousie University, NS (Technical University of Nova Scotia), 1995	With AECOM: <1 With Other Firms: 28	American Planning Association, 2016-Present National Main Street Centre (USA), 2013-Present National Trust for Canada, 1999-Present US National Trust, 2013-Present	Next Gen Municipal Leadership Certificate Program, Brock University, 2014 Building Effective Leaders Program, Sheridan Corporate, 2008 Media Relations Training, Perfect 10 Communications, 2004 Exchange Module in Planning in the UK, Oxford Brookes University (Oxford Polytechnic), UK, 1991 Various Continuous Professional Learning Courses through Municipal Learning Programs (Managing in a Union Environment, Employee Empowerment, Essentials of Management, Delegation, Emergency Management, Accessibility)
Honours Bachelor of Environmental Studies, Honours Co-op Urban and Regional Planning, University of Waterloo, 1992	Professional Affiliations Member of the Canadian Institute of Planners Registered Professional Planner Member of the Canadian Association of Heritage Professionals Member of the Canadian Museums Association		
Completed Two Credit Course in Archaeology, Saint Mary's University, NS, 1994			

Michael Seaman has over 28 years of experience in progressive municipalities providing quality level urban planning service, including extensive municipal management experience. He is recognized nationally and provincially as a proactive, resourceful, and collaborative planner and Director with a diverse portfolio who achieves positive results. He has a proven record of accomplishment of Municipal professional leadership, strategic vision and staff management success in a public service environment as well as additional leadership experience as a member of national boards of directors, and municipal committees. Michael has demonstrated a high level of achievement of best practices and positive results in a municipal planning and development environment, including seven national and four provincial major awards. He possesses strong communication, facilitation, negotiation and relationship building skills from over two decades of diverse and complex projects involving a broad array of internal and external stakeholders, and has a deep understanding and application of legislation/regulations affecting municipalities in Ontario, contemporary issues in local government.

With a strong background in urban planning, downtown revitalization, special events, design, heritage conservation, economic development, communication, marketing and tourism, Michael is a recognized leader in developing, promoting and implementing best practices in urban design. With more than 20 years of working closely with building approvals and inspection and enforcement processes through heritage related work, coordination of restoration projects, as well as conventional municipal development planning. He has strong interpersonal, communication and report writing skills, and a superb ability to use independent judgment, with an unmatched record of success in good planning and achieving results as part of a management team. Michael has the ability to train and manage subordinate staff, and is recognized by peers as an expert in field, serving as Chair of the Board of Governors of the National Trust for Canada, past member of the Board of CAHP, heritage editor for OPPI Journal.

Experience Prior to AECOM

National Trust for Canada, Chair of the Board of Governors. Principal Officer of Canada's leading national charity related to heritage conservation. Established by the Government of Canada in 1973, The National Trust for Canada is a national charitable not-for-profit organization that leads and inspires action for historical places in Canada. Presides over a National Board of Directors and oversees its activity. Senior representative of the board in working with Chief Executive Officer on matters related to budget and financial sustainability, human resources matters, performance management, governance, advocacy, staff and board recruitment and succession, property management, and fundraising. October 2018-present.

Municipality of Clarington, Director of Planning Services, Bowmanville, Ontario. Led the Planning and Development Function and a team of 35 staff. Part of the Durham GO Train Team, land acquisition portfolio, Community Planning and Urban Design and Special Projects (Agriculture, Nuclear, Heritage, Environment), and Development Review. Department work included nine new and updated secondary plans, comprehensive zoning by-law. Feb.-June 2019.

Town of Grimsby, Director of Planning, Grimsby, Ontario. Led the Town of Grimsby Planning Department as Planning Director, leading a team of six staff and four committees, and leading the Town's Economic Development and Heritage Planning

portfolios. Also oversaw the Development Planning Function in a rapidly growing municipality, and led the Urban Design Function. Responsible for guiding Downtown Revitalization through implementation of the Community Improvement Plan and Main Street Committee. Coordinated the Comprehensive Zoning By-law Review, coordinated the Development Charge Study, and led the Transit Investigation. Coordinated the commenting, approval and appeal Process for completion of a new Official Plan, and a West End Waterfront Master Plan and Trail Plan Study and Hospital Corridor Secondary Plan. Coordinated Grant Applications achieving almost \$2-million in project specific grants to the municipality for the waterfront, public realm and community stakeholders (downtown revitalization). Involved attendance and testimony at Ontario Municipal Board Hearings, working with Niagara Area Planners and Niagara Region to advance the planning function in Niagara, participating in and implementing process improvement initiatives at the municipal and regional level. Municipal Lead in Regional Studies and Initiatives (Greenbelt Plan: Niagara Perspective, GO Train Attraction, GO Hub Study). Administering Department and committee budgets (\$922,000). Coordinated the Town's Economic Development Strategy, led the Town's response to the Greenbelt Plan Review, and led Grimsby's Tourism strategy, including coordinating the Community Video Series. Instigated and Coordinated Prototype Doors Open Grimsby and Happening on the Street Festivals, a successful tourism generator and community building event. 2010 – February 2019.

Town of Oakville, Manager of Heritage Planning, Oakville, Ontario. Managed the Heritage Planning Division, including hiring and supervision. Established the Heritage Planning Division – a new division within planning department. Developed productive working relationships with a variety of internal and external stakeholders to achieve municipal objectives related to heritage conservation and new development in older communities; reviewed and provided commentary related to development applications, building permits and heritage permits. Set and implement annual and long-term work plan with the aim of achieving national leadership in heritage conservation. Helped make Oakville the recognized leader in heritage conservation planning in Canada through winning of the Lt. Governor's Ontario Heritage Award for Community Leadership and Prince of Wales Prize. Administered a \$300,000 budget for the heritage planning division. Developed and refine planning policies. Of greatest significance was the development of policies for the new Official Plan related to heritage, older communities (Bronte, Palermo Village, Old Oakville), urban design and sustainability. Another significant policy related work was the development of policies and undertaking of studies related to the North Oakville Secondary Plan. These included a heritage resource strategy for the entire urban expansion area, and a study of the community of Palermo Village. Administered inspections, enforcement and permitting related to heritage properties, working closely with building department and by-law enforcement division staff. Provided expert advice to Council, committees and public and give testimony before provincial boards. Coordinated inspections and enforcement related to heritage buildings and areas. Served as lead municipal contributor to "Conserve Preserve", a handbook for heritage and sustainability. 2008 – 2010.

Town of Aurora, Community Planner, Aurora, Ontario. Managed the Community Planning Division including hiring and supervision, Development of work plan and budget for the division. Coordinated complex and high-profile development applications (subdivision, major commercial). Provided internal leadership in urban design – reviewing all types of projects (industrial, major commercial, subdivision, infill, streetscaping). Provided commentary related to urban and building design and heritage conservation on development applications. Reviewed and coordinated planning staff reports to the committee of adjustment. Hired and supervised contract support staff and consultants. Worked with Economic development to develop and implement a strategy for enhancing the economic viability of the downtown through focussing on existing strengths. Worked with the Public Works department to provide municipal review and commentary related to infrastructure and public works projects (Wellington Street Median, York Region Transit). Coordinated the Northeast Old Aurora Heritage Conservation District Study and implemented the pre-consultation for the southeast Old Aurora heritage conservation district study. Instigated and collaborated with the leisure services department and other local partners to establish Doors Open Aurora, a successful local tourism generator. Coordinated the heritage planning function of the Municipality. Helped make Aurora the recognized leader in heritage conservation planning in Canada through winning of the Prince of Wales Prize for municipal heritage leadership in Canada. Implemented policy and programs (Evaluation System, Heritage District Studies). Coordinated development applications: major commercial, residential plan of subdivision, site plan control, Rezoning, OPA. Reviewed building permits and heritage permits in the heritage resource area including all of Old Aurora and related to heritage resources. Developed and implemented policy and procedures. Implemented education and outreach programs including website, publications and in-house training. 2005-2008.

Town of Markham, Markham, Ontario. 1995 – 2005. Senior Planner, 2001-2005 / Planner II, 1999-2001 / Planner, Heritage and Conservation, 1995-1999. Processed development applications: Rezoning, Site Plan Approval, Plans of Sub- Division, Official Plan Amendments, Plan of Condominium, Committee of Adjustment Applications. Participated in the development of secondary plans in urban expansion areas. Implemented and educated principles of New Urbanism. Provided internal leadership in Urban Design and Building Design review for development applications in the four heritage conservation districts (Markham Village, Unionville, Thornhill, Buttonville) and all inventory properties town wide. Developed and coordinated policy and guidelines (Unionville Heritage Conservation District Plan; Design Guidelines for Townhouses and awnings; Markham Heritage Estates Building Envelope Review Study; Heritage District Identification Study; Main Street Façade Improvement Study; Main Street Thornhill Study; Thornhill Heritage District Plan Review; and the Highway 7 Streetscape Study). Participated and conducted studies related to infrastructure planning; such as Highway #407, Markham By-pass, Highway 7 Streetscape

Project, and Main Street Markham Streetscaping. Worked with issues of sewer and water, railway infrastructure, public transit, future airport lands, etc. Markham Heritage Estates Sub-Division: Responsible to prepare and coordinate detailed restoration plans and monitor restoration approach (coordinated lot allocation and sale; provided advice on relocation, costing, design of additions, and physical analysis of structures; and worked closely with Building department related to restoration plans). Staff support for Heritage Markham and Heritage District Study Committees in Unionville and Thornhill Village. Instigated and provided staff support for Doors Open Markham festival. Worked closely with the Building department related to permits, inspections and enforcement. Provided Expert testimony at the Ontario Municipal Board.

City of Brampton, Planning Assistant, Brampton, Ontario. Brampton Memories of a Small-Town television series. Brampton Heritage Inventory. Designation of Properties and Heritage Review. 1993 & 1994.

Town of Oakville, Planning Assistant (Heritage), Oakville, Ontario. Coordinated Municipal Heritage Planning function as fill in for a maternity leave. 1990-1993.

Awards

The Prince of Wales Prize – for municipal heritage leadership in Canada. Awarded to the Town of Grimsby 2015. Awarded to the Town of Oakville 2010. Awarded to The Town of Aurora 2008. Awarded to the Town of Markham 2000.

Lt. Governor’s Ontario Heritage Award for Community Leadership. Awarded to the Town of Grimsby 2015. Awarded to The Town of Oakville 2010. Awarded to The Town of Aurora (Recognized by Aurora Council for contribution to award) 2010.

Canadian Institute of Planners Award for Planning Excellence. “Northeast Old Aurora Heritage Conservation District Plan” (HM) 2007. “The Unionville Heritage Conservation District Plan” 2000.

B. Napier Simpson Award of Merit. Presented by the Ontario Historical Society to Heritage Markham to the outstanding municipal heritage committee in Ontario 2004.

Communities in Bloom – National and Provincial Champions. Awarded to The Town of Markham. Prepared submission package and gave presentations with respect to Heritage Conservation aspects of Markham’s overall program to judging panel 1997 & 1998.

Ontario Professional Planners Institute, Outstanding Planning Award. “The Unionville Heritage Conservation District Plan” 1999.

Ontario Heritage Foundation Community Heritage Recognition Award. Individual Award for commitment to heritage in local community (Brampton) 1999.

Heritage Markham Award. The first Doors Open Markham 2003. The Unionville Bandstand 2002.

Niagara Community Design Awards. Planning Department Assisted with nine Award winning Projects from Grimsby 2013, 2014 and 2015.

Journal Articles

A recognized writer, Michael has written numerous articles for journals and other publications:

OPPI Journal. Contributing Editor for Heritage Planning 2007 to present.

Municipal World. Regular invited contributor to the magazine 2007 to present.

Contributed articles, 1992 to present, for the following:

- Municipal World (regular feature in annual heritage issue since 2008)
- OPPI Journal
- Heritage Canada
- Community Heritage Ontario News
- Canadian Association of Professional Heritage Consultants Forum
- Ontario Historical Society News,
- The Auroran (Doors Open Aurora Series 2006 and 2007)
- Founded the Brampton Heritage Board Newsletter and Markham Heritage News
- Children’s Book “Bampy’s Wartime Coconut” presented to the RC Legion

Other Related Services

Heritage Canada Foundation / National Trust for Canada.

- Chair of the Board of Governors, 2018-present
- Ontario Representative on the Board of Governors, 2009-2012, 2015-present
- Ontario Governor, member of the Board of Governors of the Heritage Canada Foundation
- Member of Strategic Initiatives and Governance Committees
- Coordination of Team Canada Presentation

Ontario Professional Planners Institute. Editor for Heritage, Ontario Planning Journal, 2007-present.

Willowbank School of Restoration, Queenston. Faculty Associate. 2010-present.

University of Waterloo, Heritage Resources Centre. Faculty Associate. 2011-present.

Heritage Canada Foundation – Urban Planning Advisor. Urban Planning Advisor to Foundation as part of Engaging Stakeholders in Heritage and Development Initiative funded by Parks Canada. The initiative is intended to engage stakeholders (Development Community and Municipal Councils) to determine informational and procedural needs to conserve heritage resources. Presented to St. Catharine's Council with Executive Director of Heritage Canada about initiative in September 2007. Advisor to the Heritage Canada Foundation with planning for 2007 annual conference in Edmonton related to Heritage Planning. 2007-2008.

Town of Aurora, Committee of Adjustment. Chair for 2003 and 2004. 2001-2005.

Canadian Association of Heritage Professionals. Member of the Board of Directors. 2000-2002.

Brampton Historical Society. Co-Founder and Past President (1999). 1998-present.

City of Halifax, Heritage Advisory Committee. Member. 1995.

City of Brampton, Heritage Board (MHAC). Chair from 1996 to 1999. 1993-1999.

City of Brampton, Churchville Heritage District Committee. 1994-1996.

Rogers Community 10 Television – Brampton. Co-ordinator, host, writer and researcher for *Brampton: Memories of a Small Town*, a six-part television series about Brampton's History and Architecture. 1993.

Public Speaking

An accomplished speaker and excellent communicator, Michael has been invited to present at various conferences, workshops and community meetings:

- National Trust Conference (Fredericton), 2018
- OPPI Conference (Sudbury), 2018
- Ontario Heritage Conference (Sault Ste. Marie), 2018
- Hamilton/Burlington Real Estate Board, 2017
- National Trust Conference, 2016
- Ontario Heritage Conference – Niagara on the Lake, 2015
- Buffalo Niagara Heritage Awards (Kleinhans Music Hall), 2014
- Ontario Heritage Conference – Midland, 2013
- Town of Perth, 2012
- Municipality of Brockton, 2012
- Peel Heritage Conference, 2012 and 1997
- Town of Centre Wellington, 2013 and 2014
- City of Burlington – Heritage Register Public Meeting, 2010
- Willowbank School of Restoration – Heritage Planning Course, 2010 and annually from 2012-2017
- CIP/OPPI Conference, Niagara Falls, 2009
- Town of Richmond Hill, 2009
- Town of Milton, 2009
- Oakville Historical Society, 2009
- King Township, 2008
- Heritage Canada Foundation Annual Conference, 2000, 2007, 2008 and 2012 (Montreal *in French*, Quebec City, Edmonton and Toronto)
- Heritage Canada Foundation Engaging Stakeholders Initiative, 2007
- Community Heritage Ontario Workshops, 2003, 2004 and 2007

- Ministry of Culture Workshops, 2003 and 2004
- Brampton Historical Society, 1998, 2004 and 2009
- Aurora Heritage District Study Pre-consultation (prior to employment), 2003 and 2004
- Ontario Heritage Conference (Kitchener), 1998
- Town of Newmarket Heritage District, 1996
- Lectured to OMA course students about Researching Heritage Buildings, 1997
- Heritage Gardens – Markham Green Thumb Festival, 1996
- Various Council, General Committee and Planning Committee and Community meeting presentations for Brampton, Aurora and Markham.

Liam Smythe, B.URPI

Heritage Researcher

Education

B.URPI

Ryerson University (2014)

Professional Affiliations

Architectural Conservancy of Ontario

Years of Experience

With AECOM: 1

With Other Firms: 2

Training and Certifications

Certificate in Cultural and Heritage Site Management/Centennial College/2015

Summary

Liam Smythe is a Heritage Researcher in the Cultural Resource Management Department at AECOM. He completed his Bachelor of Urban and Regional Planning at Ryerson University, and a Postgraduate Certificate in Cultural and Heritage Site Management at Centennial College. Liam has previously worked as a policy and program assistant with the Ministry of Tourism Culture and Sport, assisting in the development and implementation of the *Standards and Guidelines for the Conservation of Provincial Heritage Properties*. With MTCS he helped develop a compliance survey to assess the level at which ministries and prescribed public bodies were complying with the regulations, as well as updating the heritage bridge list and identifying a series of best practices in heritage conservation at the provincial level. Working with other Ryerson University students, he conducted a comprehensive study of built heritage resources and cultural heritage landscapes in Toronto's Corktown neighborhood on behalf of the Corktown Business and Residents Association. The purpose of the study was to identify areas with the potential for designation as a Heritage Conservation District under Part V of the *Ontario Heritage Act*. He has formerly volunteered with Heritage Toronto, and completed a collections management internship with the Ontario Heritage Trust.

Liam's responsibilities at AECOM include: Conducting archival and other historic research, field reviews, photography, preparing documents and reports.

Liam is a member of the Cultural Resources Department at AECOM.

Project Experience

City of London, London BRT – Wellington Road Cultural Heritage Evaluation Reports, London, Ontario. (October 2018-January 2019). Large-scale project conducted as part of the EA process for the City of London's proposed Bus Rapid Transit system, required the completion of CHERs for six individual properties, and a group CHER for 35 properties, all located along Wellington Road in the City of London. Conducted historical research, site investigation, prepared and reviewed reports.

Hydro One, Toronto Power Transformer Station, Cultural Heritage Evaluation Report, Niagara Falls, Ontario. (October 2018). Conducted historical research for an electrical substation located south of the City of Niagara Falls, Ontario.

Hydro One, Leaside Transformer Station Cultural Heritage Evaluation Report, Toronto, Ontario. (October 2018). Conducted historical research for an electrical substation located in the Leaside neighbourhood of Toronto, Ontario.

Metrolinx, Union Station Bus Terminal Staging Facility Cultural Heritage Screening Report, Toronto, Ontario. (September 2018). Conducted site investigation and historical research for a proposed Go Transit staging facility in the Port Lands area of Toronto, Ontario.

City of Toronto, Basement Flooding Study Cultural Heritage Screening Assessment, Toronto, Ontario. (August-September 2018). Cultural Heritage Screening Assessment developed to identify built heritage resources and cultural heritage landscapes within the study area, in North York. Completed as part of the Basement Flooding Remediation and Water Quality Improvement Master Plan Class EA Studies. Tasks included research, and a field review of the study area with photo documentation.

Ainley Group, 12th Line Bridge Cultural Heritage Evaluation Report. New Tecumseth, Ontario. (July 2018) Conducted historical research, field review and documentation for a small municipally-owned bridge in the Town of New Tecumseth.

MTO, Heritage Screening Technical Memo - Rehabilitation and Replacement of Highway 9 Holland Drainage Canal Bridges. King, Ontario. (May 2018). Completed background research and site investigations in support of Heritage Screening Technical Memo as part of a Detail Design and Class Environmental Assessment (Class EA) study

Ainley Group, Humber River Bridge Cultural Evaluation Reports. Caledon, Ontario. (April 2018-June 2018). Completed Cultural Heritage Evaluation Reports for two municipally owned bridges in the former Village of Bolton, Town of Caledon. Tasks included historical research, field review, and final preparation of the document for submission to the client.

MTO, Cultural Heritage Evaluation Report, 355 Lesmill Road: Clark Locke House and William Goodwin House, Highway 401 Eastbound Collectors - Avenue Road to Warden Avenue. Toronto, Ontario. (May-June 2018). Completed site investigations and background research as part of the Detail Design and Class Environmental Assessment Study for the rehabilitation of the Highway 401 eastbound collector lanes between Avenue Road and Warden Avenue.

City of Toronto, Basement Flooding Study Cultural Heritage Screening Assessment, Toronto, Ontario. (May 2018). Cultural Heritage Screening Assessment developed to identify built heritage resources and cultural heritage landscapes within the study area, in Etobicoke. Completed as part of the Basement Flooding Remediation and Water Quality Improvement Master Plan Class EA Studies. Tasks included a research, and a field review of the study area with photo documentation.

MTO, Cultural Heritage Screening Report, Highway 401 Eastbound Collectors-Avenue Road to Warden Avenue, Toronto, Ontario. (April 2018) Completion of a Cultural Heritage Screening Report to identify built heritage resources and cultural heritage landscapes along Highway 401 between Avenue Road and Warden Avenue in Toronto. Completed as part of the Detail Design and Class EA study for the expansion of the eastbound collector lanes of Highway 401.

Biglieri Group, Heritage Impact Assessment, 45 Raynes Avenue, Bowmanville, Ontario. (April 2018). Conducted background research and site investigation for a Heritage Impact Assessment of the former Goodyear plant in Bowmanville, Ontario.

MTO, Highway 401 Neilson-Whites Road Technical Screening Memo, Toronto, Ontario. (November 2017-January 2018). Production of a Technical Heritage Screening Memo for a series of MTO-owned bridges along Highway 401 between Neilson Road in Toronto and Whites Road in Pickering. Used to determine cultural heritage value or interest prior to bridge rehabilitations. Tasks included research, field assessment and documentation.

MTO, Highway 12 Triple Bay Roundabout Cultural Heritage Evaluation Report, Midland, Ontario. (November 2017). Field assessment and photo documentation of two cultural heritage landscape. Completed for the proposed realignment of the Highway 12/Triple Bay Road intersection.

Metrolinx, Rouge River Bridge Documentation Report, Toronto, Ontario. (November 2017-May 2018). Production of a documentation report to meet the requirements of a Minister's Consent for the removal of an 1898 Truss bridge over the Rouge River on Metrolinx's Lakeshore East line. Completed extensive photo documentation of the bridge and associated landscape in March 2018 to record the condition of the site prior to its removal.

MTO, Highway 427 Expansion, Vaughan, Ontario. (December 2017-January 2018). Assisted with the architectural salvage of a property at 10220 Huntington Road in Vaughan. Worked with staff from Black Creek Pioneer Village to remove and document architectural elements including doors, railings and trim for transportation to the museum.

Appendix A

**Planning Justification Report
435, 441 and 451 Ridout Street
July 2019
Farhi Holdings Corp.
Tillman Ruth Robinson
Architects**

PLANNING JUSTIFICATION **REPORT**

OFFICIAL PLAN AMENDMENT AND
ZONING BY-LAW AMENDMENT APPLICATIONS

435, 441 and 451 Ridout Street North
City of London

Date:

July 2019

Prepared for:

Farhi Holdings Corporation

Prepared by:

MacNaughton Hermsen Britton Clarkson Planning Limited (MHBC)

630 Colborne Street, Suite 202

London ON N6B 2V2

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CONTENTS

1.0

Introduction 4

- 1.1 Overview 4
- 1.2 Report Framework 6
- 1.3 Planning Applications 6

2.0

Site Location and Surrounding Land Uses 8

- 2.1 Site Description 8
- 2.2 Surrounding Land Uses 9
- 2.3 Spatial Analyses 11

3.0

Description of the Proposal 12

- 3.1 Design Goals and Objectives 12
- 3.2 Concept Plan 13
- 3.3 Design Considerations 14
- 3.4 Heritage Considerations 17

4.0

Planning Analysis 18

- 4.1 Provincial Policy Statement 18
- 4.2 City of London Official Plan (1989) 25
- 4.3 City of London New Official Plan (The London Plan) 42

CONTENTS

5.0

Zoning By-law Analysis 50

5.1 Existing Zoning 50

5.2 Proposed Zoning 51

6.0

Conclusions 53

Appendix A

Architectural Renderings

Appendix B

Conceptual Building Elevations

Appendix C

Conceptual Landscape Plan

Appendix D

Sun Study

LIST OF FIGURES

Figure 1:	Context Plan	Following 8
Figure 2:	Perspective of Site	10
Figure 3:	Regional Spatial Analysis	Following 11
Figure 4:	Local Spatial Analysis	Following 11
Figure 5:	Conceptual Site Plan	Following 13
Figure 6:	Ridout Street North Streetscape	14
Figure 7:	Ridout Street Podium Entrance (Rendering)	Following 14
Figure 8:	West Elevation of Site	15
Figure 9:	West Façade Park View (Rendering)	Following 15
Figure 10:	1989 Official Plan Designation (Schedule 'A')	Following 26
Figure 11:	The London Plan Place Type (Map 1)	Following 42
Figure 12:	Existing Zoning	Following 50
Figure 13:	Proposed Zoning	Following 52

LIST OF TABLES

Table 1.0:	Required Planning Applications	6
Table 2.0:	Surrounding Land Uses	10
Table 3.0:	Consistency with Provincial Policies	18
Table 4.0:	Selected Zoning Regulations (DA2 Zone)	51

1.0

INTRODUCTION

1.1 Overview

MHBC has been retained by Farhi Holdings Corporation to assist with applications to amend the City of London Official Plan (1989), the new City of London Official Plan (The London Plan) and the City of London Zoning By-law Z.-1 in support of a redevelopment proposal for lands addressed as 435, 441 and 451 Ridout Street North, London ('the Site'). The subject property is located at the northwest corner of Queens Avenue and Ridout Street North, and forms part of the City's Central London planning district and its downtown core. The intent of these applications is to permit a new mixed-use, high-rise tower on a portion of the Site currently containing the three storey addition to 451 Ridout Street. The proposed tower would accommodate residential and office/commercial uses, and would be designed to integrate with existing heritage structures on the property.

The subject lands are a single property located at the northwest corner of Queens Avenue and Ridout Street North, immediately east of the Thames River corridor and Harris Park. In total, this project site measures approximately 1.4 ha in area and contains three existing office buildings, as well as surface parking facilities. The property is designated under Part IV of the *Ontario Heritage Act*, and is also designated under Part V of the *Ontario Heritage Act* as a part of the Downtown London Heritage Conservation District. Further, the property is identified as a National Historic Site of Canada.

Tillmann Ruth Robinson Architects has prepared a design concept for the Site in support of several core project objectives. Most notably, the planned development is intended: to create a landmark, mixed-use development for London; to redevelop and intensify the property in a manner that is sensitive to existing heritage buildings; and to align with the applicable planning policy framework and related planning initiatives (as discussed in this Report).

In summary, the redevelopment concept incorporates a 40 storey, mixed-use tower to replace the three storey office building addition located to the rear (west) of the historic structure addressed as 451 Ridout Street. The proposed tower, which would be connected to 451 Ridout Street, would contain approximately 6,308 m² of office/commercial space on lower floors and a total of 280 residential suites on the upper floors. No modifications are proposed to the heritage buildings addressed 435 and 441 Ridout Street North.

The planning merits of this proposal are evaluated in detail within this Report and are summarized below:

- The project supports the intensification of lands in the City's downtown that are designated in the City's 1989 Official Plan and The London Plan for high density residential, office, and commercial uses;
- The property is well suited for this development proposal considering its physical size/shape, its location within a mixed-use development area and its proximity to existing offices, retail/service commercial enterprises, government activities, recreational, entertainment and cultural facilities, public transit services and the proposed bus rapid transit (BRT) system. Residents and employees of this new development would therefore have access to, and would support, a range of local businesses, public institutions and transit services;
- Design elements integrated into the proposal promote an attractive and prominent building form that would contribute positively to both the local streetscape and the City's downtown skyline. Additionally, the high-rise building design would create a landmark for the Forks of the Thames activity area and, more broadly, Downtown London;
- Introduces greater housing choice within the downtown core to help broaden the range and mix of housing available to current and future residents of Central London. In effect, this project would help the City accommodate forecasted housing demands and achieve intensification targets; and
- Integrates a mix of higher density residential and complementary office/commercial uses which, collectively, would support efforts to revitalize the downtown core and encourage its continued development as a multi-functional regional centre. The proposal would also help to promote neighbourhood stability in the downtown core by diversifying the mix of housing available in this area (to better meet the changing needs of local residents over the long-term).

In light of these considerations, in our opinion this proposal is appropriate for the subject lands and the downtown development context, and should not generate significant land use conflicts with adjacent properties.

1.2 Report Framework

This Planning Justification Report has been prepared for submission to the City of London in support of the aforementioned Official Plan Amendment (OPA) and Zoning By-law Amendment (ZBA) applications. The Report includes the following primary components:

- An introduction and general description of the subject lands, surrounding uses and existing conditions to provide an understanding of the locational context;
- A summary of the concept plan prepared for the Site, including an overview of the core design elements associated with the development proposal;
- A review of the planning permissions applicable to the property; and
- An assessment of the proposal relative to: (1) the policy framework of the Provincial Policy Statement, the 1989 Official Plan and The London Plan; and (2) the development regulations of the City's Zoning By-law.

As part of this planning analysis, the following documents were reviewed:

- Provincial Policy Statement;
- City of London Official Plan (1989);
- City of London Official Plan (The London Plan);
- City of London Zoning By-law Z.-1;
- Our Move Forward (London's Downtown Plan);
- Downtown Heritage Conservation District Plan; and
- Downtown London Community Improvement Project.

1.3 Planning Applications

Our analysis has confirmed that the following applications are required to permit the proposed redevelopment and intensification project:

Table 1.0 – Required Planning Applications

Planning Application	Approval Authority
Official Plan Amendment to add a Specific Policy area to the 1989 Official Plan and new Official Plan supporting the proposal.	City of London
Zoning By-law Amendment to amend the Downtown Area Zone applying to the Site to facilitate the redevelopment proposal.	City of London
Site Plan Approval (future application)	City of London

A 'Pre-Application Consultation' meeting was held on June 13, 2017 to review the submission requirements for the development concept. As set out in the associated 'Record of Pre-Application Consultation', the following reports have been prepared in support of the proposal and are enclosed with the OPA and ZBA applications:

- Planning Justification Report, with urban design considerations;
- Heritage Impact Statement;
- Environmental Impact Statement (Preliminary);
- Geotechnical Engineering Review (Draft);
- Sanitary Servicing Feasibility Study;
- Stage 1-2 Archaeological Assessment; and
- Transportation Impact Assessment.

Following further discussion with City staff on December 19, 2018, it was agreed that urban design considerations would be included with the Planning Justification Report rather than in a separate Urban Design Brief. It was also agreed that more detailed design materials would be prepared in conjunction with the Site Plan Approval process for consideration by City staff and the City's Urban Design Peer Review Panel.

2.0

SITE LOCATION AND SURROUNDING LAND USES

2.1 Site Description

The subject lands are a single property located at the northwest corner of Queens Avenue and Ridout Street North. These lands are addressed as 435, 441 and 451 Ridout Street North; reflecting the street addresses of the three buildings located on the premises. As discussed, the Site forms part of the Central London planning district and the City's downtown core.

Figure 1 illustrates the general location of the Site.

The lands, which are generally rectangular in shape, measure approximately 1.4 ha in area and include approximately 73 m of frontage along Ridout Street North and approximately 103 m of flankage on Queens Avenue. Three buildings are currently contained on the Site: a two storey office building (435 Ridout Street North), a three storey office building (441 Ridout Street North), and a split level, three storey office building (451 Ridout Street North). These buildings are listed under Part IV of the Ontario Heritage Act, and collectively encompass a National Historic Site.

The subject lands are currently accessed via three right-in, right-out entrances; two accessed from Ridout Street North and one accessed from Queens Avenue. Off-street parking is provided in surface fields adjacent to the Ridout Street North and Queens Avenue corridors, and at an internal (rear) parking area immediately west of 451 Ridout Street North. An area of manicured lawn is also located adjacent to the rear parking lot which interfaces with the Thames Valley Parkway and Harris Park. Vehicular access to the internal parking area is provided by way of a private driveway connecting to the Harris Park entrance (located at the western terminus of Dufferin Avenue). A series of walkways and stairs provide pedestrian access to this parking area and the broader property.

Existing building entrances are positioned at generally the same elevation as the Ridout Street North and Queens Avenue corridors. Internal to the Site, the lands slope to a lower elevation

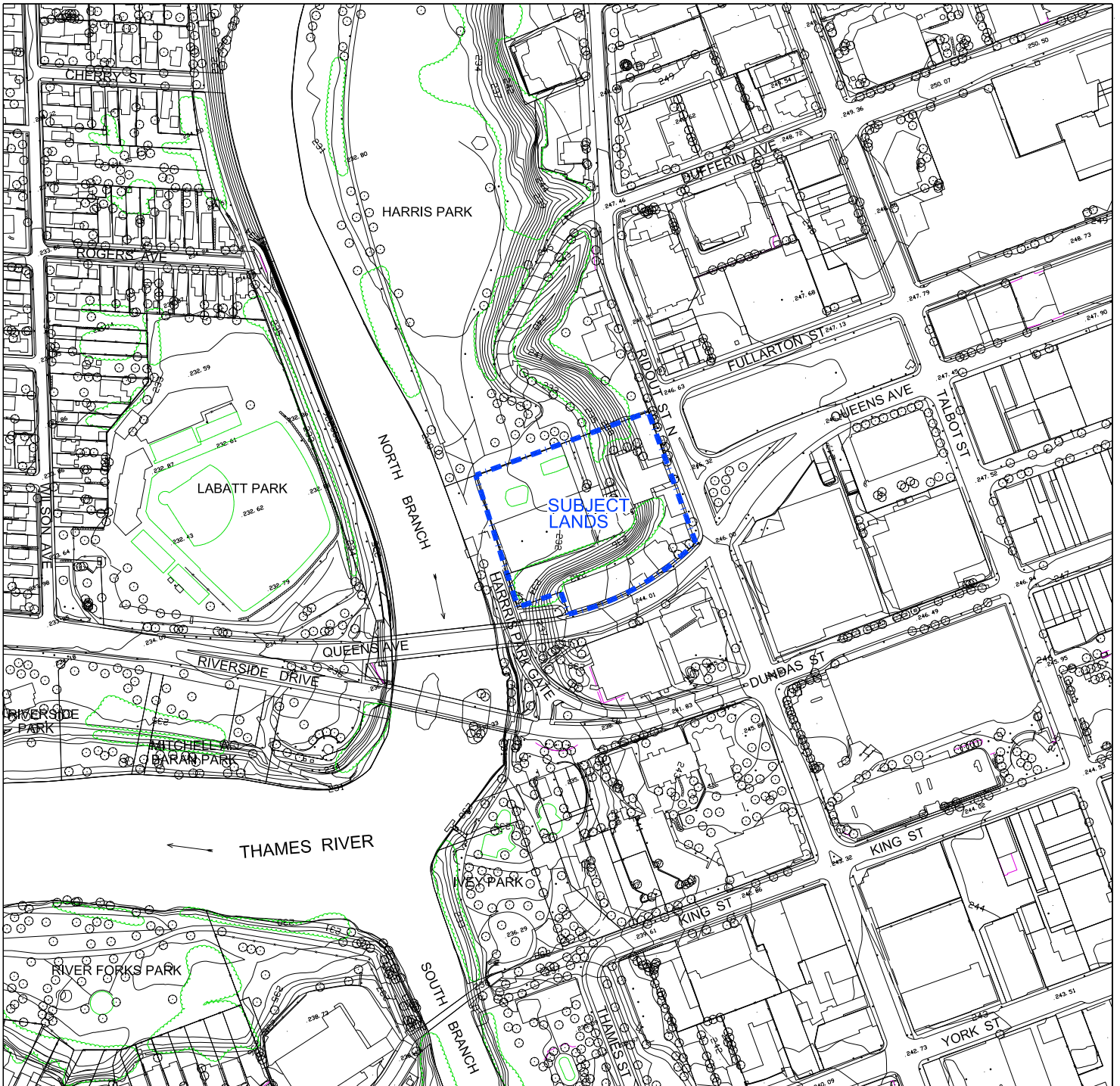
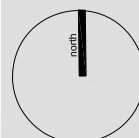


Figure 1
Context Map

 SUBJECT LANDS



and towards the Thames River. The rear elevations of the Site generally match the Harris Park and Thames Valley Parkway, which abut the western property boundary.

Generally, the northwestern portion of the Site is identified as being within the floodplain regulated by the Upper Thames River Conservation Authority (UTRCA).

2.2 Surrounding Land Uses

The subject lands form part of the 'Central London' planning area which encompasses the City's downtown core and is comprised of a diverse mix of intensive land uses (e.g., offices, retail/service commercial enterprises, government activities, recreational, entertainment and cultural facilities). The Site is also located in proximity to a variety of these downtown land uses including major civic attractions, such as Covent Garden Market, Budweiser Gardens, the Grand Theatre, Victoria Park, Museum London, the Forks of the Thames Interpretative Centre, Harris Park and the Thames Valley Parkway.

Adjacent to the eastern perimeter of the Site, Ridout Street North integrates two lanes of traffic in a southerly direction with a sidewalk on the western side of the street. This section of Ridout Street North also incorporates a 'split' which provides access to Queens Avenue in an easterly direction. Adjacent to the Site's southern perimeter, Queens Avenue accommodates two lanes of traffic in a westerly direction, with sidewalks provided on both sides of the corridor. Schedule 'C' (Transportation Corridors) of the 1989 Official Plan classifies Queens Avenue as an 'Arterial' road. According to the transportation network classification prescribed in Section 18.2.2 of this Official Plan, arterial roads are designed to serve high volumes of intra-urban traffic with controlled or limited property access. Ridout Street North is classified as a 'Primary Collector'. According to Section 18.2.2, primary collectors serve light to moderate volumes of inter-neighbourhood traffic at moderate speeds and have limited property access.

The Site is also located within convenient walking distance of London Transit Commission (LTC) bus stops in service along Queens Avenue and Ridout Street North. Additionally, in May 2016, City Council endorsed a BRT system that includes proposed routing along the Queens Avenue corridor adjacent to the property. Further, on March 26, 2019, City Council endorsed a funding application supporting the 'Downtown Loop' route of the BRT system. This route is to include a new transit stop on Queens Avenue in the vicinity of the Talbot Street intersection.

Table 2.0 identifies that there is a diverse mix of cultural, institutional and residential uses in the immediate vicinity of the subject lands.

Table 2.0 – Surrounding Land Uses

Relative Location	Existing Land Uses
TO THE NORTH:	Harris Park, including entrance driveway; Eldon House and Gardens; The Harrison high-rise apartment (500 Ridout Street North)
TO THE EAST:	Ridout Street North corridor; offices in converted single detached dwellings; Surface parking lot; Ontario Court of Justice/Richard Pierpoint Building; Budweiser Gardens
TO THE SOUTH:	Queens Avenue; Museum London; Dundas Street; First Hussars Museum; Old Courthouse Building
TO THE WEST:	Harris Park Gate; The Thames Valley Parkway; The Thames River; Labatt Park

Figure 2 illustrates the Site, surrounding lands and the local development setting.

Figure 2: Perspective of Site: View North from Forks of the Thames Activity Area



Source: Google Maps: February 4, 2019

2.3 Spatial Analyses

2.3.1 Regional Spatial Analysis

A Regional Analysis was conducted around the Site at a 400 and 800 metre radius as illustrated on **Figure 3**.

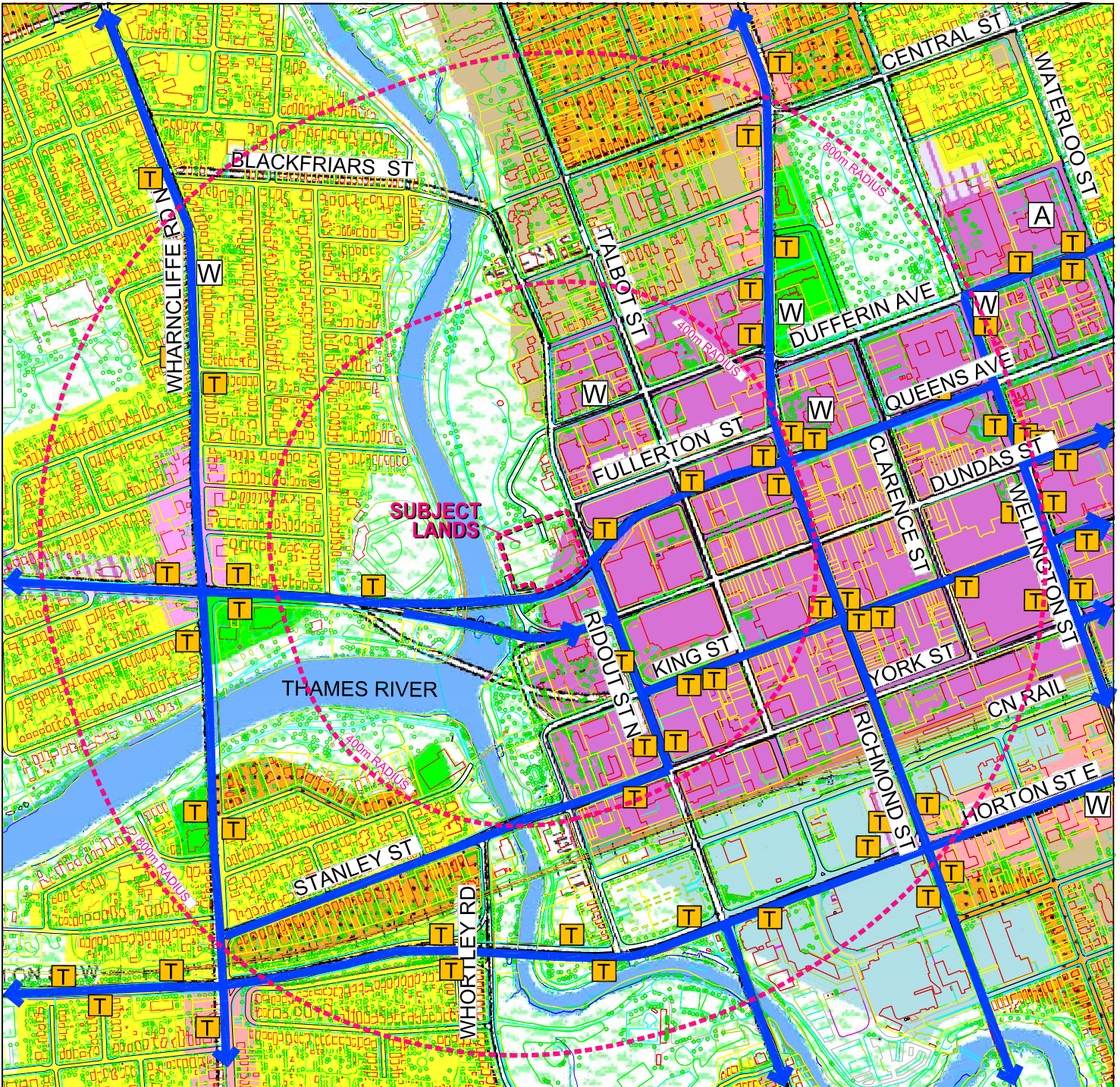
The 400 metre radius incorporates a large portion of the City's downtown core and includes a diverse range of land uses (e.g., offices, retail/service commercial enterprises, government activities, recreational, entertainment and cultural facilities, high density residential uses). Additionally, light industrial uses are situated within this radius south of the railway corridor and, as discussed, major open space (Thames Valley Parkway, Harris Park) is situated immediately east of the Site. Major transit routes along Richmond Street, Dundas Street, King Street also fall within this radius, including the Downtown Loop of the planned BRT system.

A broader range of land uses are evident within the 800 metre radius including additional downtown development, light industrial areas, schools, parkland and residential neighbourhoods containing low and medium density residential forms. The number of accessible transit routes is also increased within this larger radius.

2.3.2 Local Analysis

A Local Analysis was conducted to understand the Site's relationship with the surrounding uses in the immediate area. The key findings of the analysis are as summarized below and illustrated on **Figure 4**:

- An active frontage zone has been identified along the Queens Avenue and King Street frontages. Appropriate pedestrian walkways, extensive glazing, enhanced façade treatments and weather protection will be provided in the vicinity of these frontages to enhance and activate the streetscape;
- A zone of sensitivity has been identified where the subject lands directly abut other lands designated for residential purposes and parkland. Section 3.3 of this Report provides details respecting the design response along this interface; and
- The Site is well served by transit and is within convenient walking distance of several LTC transit routes operating along Ridout Street North, Queens Avenue, Dundas Street and Richmond Street. The site is also located within convenient walking distance of the City's planned BRT route, which is to extend along the Dundas and Richmond Street corridors in the downtown core.



LEGEND

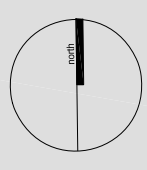
- MULTI FAMILY - HIGH DENSITY RESIDENTIAL
- MULTI FAMILY - MEDIUM DENSITY RESIDENTIAL
- LOW DENSITY RESIDENTIAL
- MAIN STREET COMMERCIAL NODE
- NEIGHBOURHOOD COMMERCIAL NODE
- DOWNTOWN AREA
- OFFICE AREA
- COMMUNITY FACILITY
- OPEN SPACE
- LIGHT INDUSTRIAL

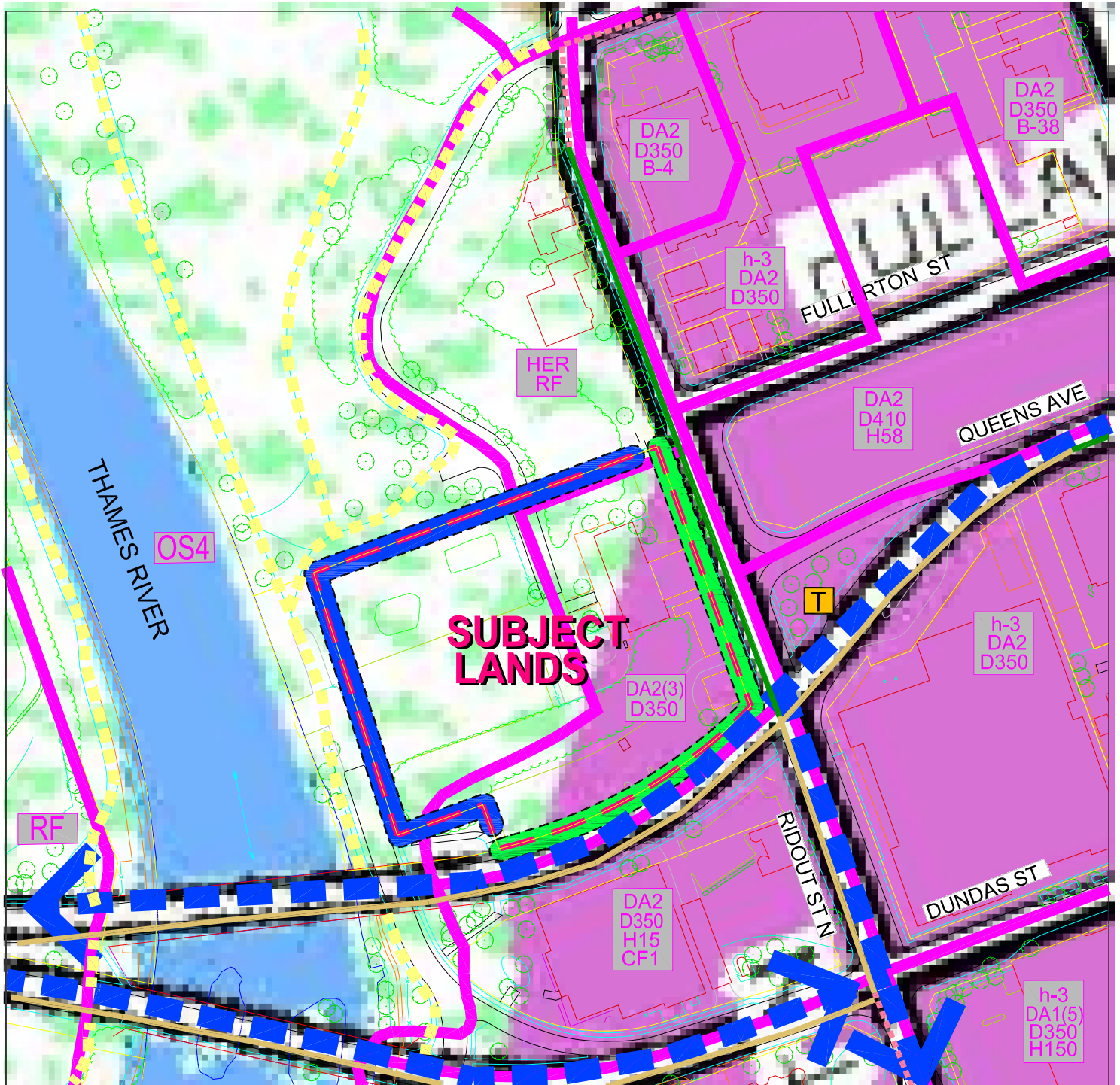
- A PUBLIC ELEMENTARY SCHOOL
- W PLACE OF WORSHIP
- TRANSIT ROUTE
- T TRANSIT STOP

Figure 3
Regional Spatial Analysis
 400-800m
 SUBJECT LANDS

PLANNING URBAN DESIGN & LANDSCAPE ARCHITECTURE
MHBC PLANNING
630 COLBORNE STREET, SUITE #202, LONDON, ON, N6B 2V2
 P: 519 858 2797 F: 519 858 2920
 WWW.MHBCPLAN.COM

DATE: July 15, 2019 **SCALE:** 1 : 10,000
 P:\18159\A\Graphics\ Figure 3 -Regional Spatial Analysis - 400-800m- July 152019





LEGEND

- DOWNTOWN AREA
- OPEN SPACE

ZONING LEGEND

- ZONE LIMIT
- h HOLDING PROVISION
- B BONUSING PROVISION
- H HEIGHT PROVISION
- D DENSITY PROVISION
- HER HERITAGE ZONE
- DA DOWNTOWN AREAS ZONE
- RF REGIONAL FACILITY ZONE
- CF COMMUNITY FACILITY ZONE
- OS OPEN SPACE ZONE

- ACTIVE FRONTAGE ZONE
- ZONE OF SENSITIVITY
- TRANSIT ROUTE
- TRANSIT STOP

- SIGNED BIKE LANE
- BIKE LANE
- SHARROW
- THAMES VALLEY PARKWAY

Figure 4
Local Spatial Analysis
Site Specific

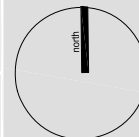
SUBJECT LANDS

**PLANNING
URBAN DESIGN
& LANDSCAPE
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DATE: July 15, 2019

SCALE 1 : 2,000



P:\18159\A\Graphics\ Figure 4 -Local Spatial Analysis - Site Specific - July 15, 2019

3.0

DESCRIPTION OF THE PROPOSAL

3.1 Design Goals and Objectives

Farhi Holdings Corporation is proposing to amend the City's new Official Plan and Zoning By-law to support the development of a prominent, mixed-use, high-rise tower on these lands. The proposed tower form would be integrated into the existing heritage structure addressed as 451 Ridout Street North. The rear portion of this building, which currently encompasses the three-storey addition to the building, would be removed and replaced with the new high-rise structure and underground parking facilities.

The following design objectives have been identified to help achieve the project goal:

1. Integrate a mix of higher density residential units and complementary office/commercial uses to collectively support downtown revitalization;
2. Design a high-rise tower design that will create a strong visual presence to further define the western gateway into the City's downtown core and to help frame the Thames Valley Parkway and Harris Park;
3. Create a positive addition to the Queens Avenue and Ridout Street North corridors, by: enhancing the visual qualities of the streetscape, providing an engaging pedestrian environment, having regard for local heritage considerations and contributing to the larger downtown context;
4. Redevelop the Site in a manner that will be supportive of transit and alternative transportation modes, and will allow residents to walk to nearby commercial, office, entertainment, and public amenities;
5. Incorporate enhanced indoor and outdoor amenity space for residents, and improve pedestrian connectivity to local streets, adjacent parkland and nearby pathway systems;

6. Develop a vehicular ingress/egress arrangement that minimizes local traffic disruptions; and
7. Apply the design direction set out in the City's Official Plans and applicable guideline documents, where practical and appropriate.

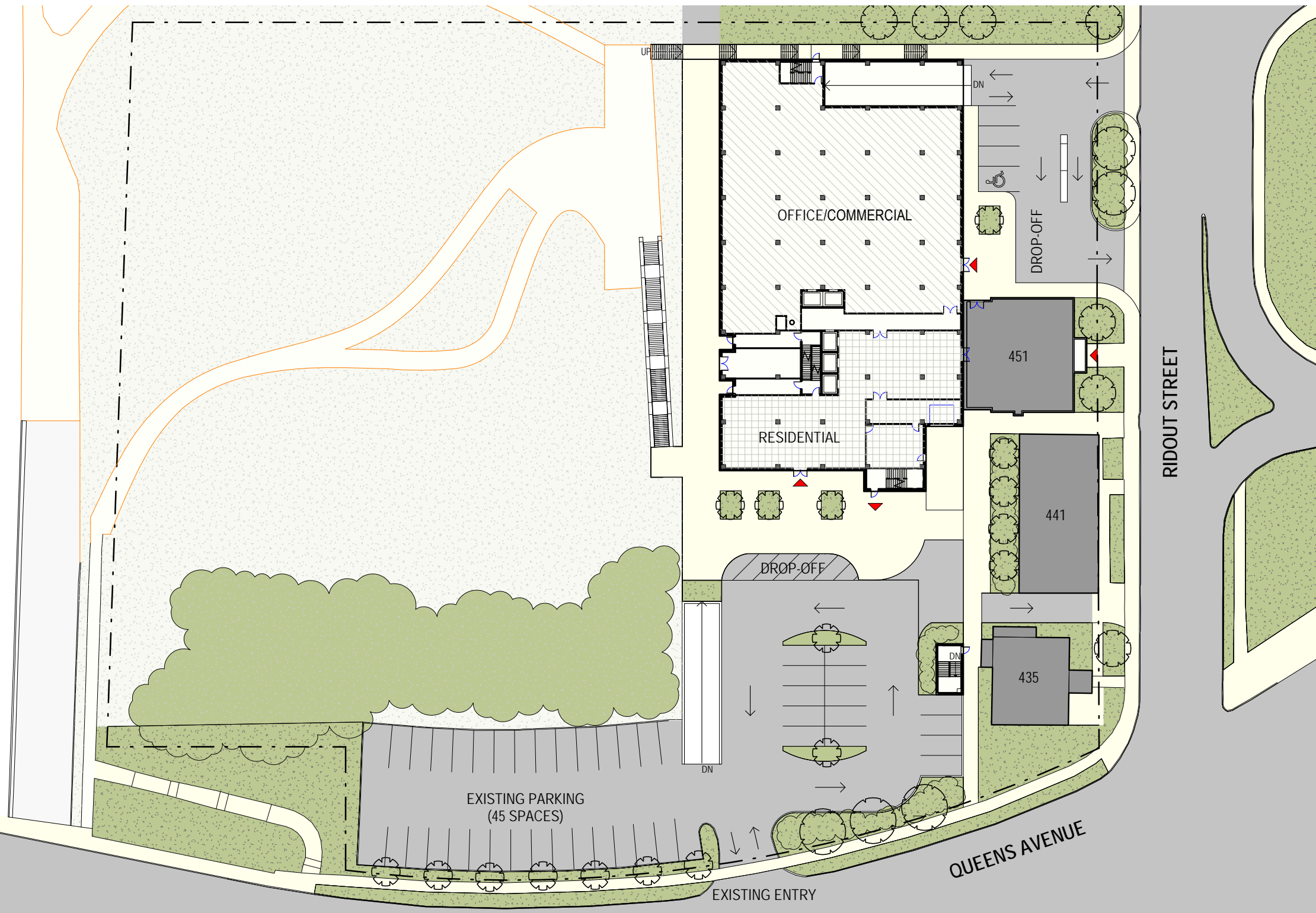
3.2 Concept Plan

Tillmann Ruth Robinson Architects have prepared a preliminary building design and site redevelopment plan that is intended to achieve the aforementioned design goals and objectives. A conceptual site plan (concept plan), architectural renderings, conceptual building elevations and contextual section drawings have been prepared to illustrate the layout, scale and character of the proposed development (refer to **Figure 5, Appendix 'A'** and **Appendix 'B'** respectively). **Appendix 'C'** includes the conceptual landscape plan designed by Ron Koudys Landscape Architects (RKLA) for this development. The RKLA plan generally illustrates the pathways, amenity areas and landscaping features intended for this development site. Additionally, **Appendix 'D'** provides a shadow assessment of this redevelopment plan.

The key components of the project are as follows:

- A 40 storey tower incorporating an enhanced level of architectural design and containing 6,308 m² of office/commercial space within the first four levels and a maximum of 280 apartment units within levels five (5) to 40. The tower base, which would include the office/commercial space, main lobby space, storage areas and administrative offices, would have a total building area of approximately 1,670 m². The main tower component would incorporate a slender floorplate (approximately 860 m² in area) and would contain a range of suite configurations;
- A variety of indoor amenities within the tower, including a lounge area, fitness facility, party room and outdoor patio space on the fifth floor;
- Outdoor amenity space interspersed throughout the property and visible from the public realm, including landscaped forecourts adjacent to the entrances and vehicle drop-off areas;
- A multi-level parking area integrating four storeys of underground parking and at-grade parking areas, as well as a loading area, bicycle storage and waste/recycling storage. A total of 372 vehicle stalls would be accommodated in this arrangement, with parking (315 stalls) predominately accommodated underground. It is envisioned that at-grade stalls would primarily function as visitor parking;
- Right-in, right-out entrances under existing street configurations providing street access to Queens Avenue (one entrance) and Ridout Street North (two entrances). A right-in entrance from Ridout Street North is also proposed at the northern limit of the Site;
- Access driveways to the four storey parking garage provided at-grade from Queens Avenue and Ridout Street North. Additionally, delivery vehicles would access the

FARHI HOLDINGS - 451 RIDOUT STREET



ZONE :		
SITE USAGE:		
SITE DATA	REQUIRED	PROVIDED
SITE AREA		14,008 m2 (150,780 SF)
BUILDING AREA (NEW)		1,670 m2 (17,975 SF)
BUILDING AREA (EXISTING)		601.67 m2 (6,476.29 SF)
TOTAL BUILDING AREA		37,587 m2 (404,583 SF)
TOTAL PARKING AREA (GARAGE)		12,500 m2 (134,548 SF)
BUILDING HEIGHT		123.9 m (406'-6")
LANDSCAPE AREA		7,193 m2 (77,425 SF)
LANDSCAPE OPEN SPACE		51.3%
HARDSURFACE AREA		
PAVED AREA		2,400 m2 (25,833 SF)
SIDEWALK AREA		1,845 m2 (19,859 SF)
TOTAL		4,245m2 (4569 SF)
LOT COVERAGE MAXIMUM		17.2%
LOT FRONTAGE		97.1 m
LOT DEPTH		132.6 m
FRONT YARD SETBACK		17.89 m
REAR YARD SETBACK		75.26 m
INTERIOR SIDE YARD SETBACK		43.43 m
INTERIOR SIDE YARD SETBACK		4.93 m
BICYCLE PARKING		TO BE DETERMINED

NUMBER OF SUITES	
TOTAL SUITES	280

PARKING STATISTICS					
LEVEL	AREA	NUMBER OF PARKING SPACES (IN GARAGE)	NUMBER OF BF PARKING SPACES (IN GARAGE)	NUMBER OF PARKING SPACES (AT GRADE)	TOTAL PARKING
G	2,265 m2	N/A	N/A	49	49
P1	3,125 m2	78	2	N/A	80
P2	3,125 m2	89	1	N/A	90
P3	3,125 m2	92	1	N/A	93
P4	3,125 m2	59	1	N/A	60
TOTALS	14,765 m2	315	5	49	372

SITE / FLOOR PLAN AT STREET LEVEL (49 SURFACE PARKING SPACES)



underground parking facility via the existing private driveway connecting to the Harris Park entrance; and

- Pedestrian connections to improve access to the street frontages, the Thames Valley Parkway and Harris Park in order to support greater active transportation linkages between the Downtown and the Thames River corridor.

3.3 Design Considerations

The following discussion outlines several design elements integrated into the proposed redevelopment plan. Additional design details are presented under Section 4.2.6 of this Report.

- **Building Form and Orientation.** The proposed mixed-use, high rise building would be positioned at the rear of the heritage structure at 451 Ridout Street North, generally at the location of the existing building addition. The tower component of the new building is positioned immediately west of this heritage structure, with the four-storey office/commercial element extending north from the tower base.

The proposed building arrangement is intended to preserve the unobstructed view of 435, 441 and 451 Ridout Street North from the street frontage and to promote a landmark vista at the western terminus of Queen Street.

Figure 6 illustrates the existing streetscape at the Site from the east elevation; **Figure 7** provides a conceptual rendering of this elevation from a similar location.

Figure 6: Ridout Street North streetscape: View west from Queens Avenue



Source: Google Maps: February 4, 2019

FARHI HOLDINGS - 451 RIDOUT STREET



RIDOUT STREET PODIUM ENTRANCE

By positioning the tower to the rear of these buildings, the design layout also enhances the prominence of the structure from the Thames River corridor (particularly from the Forks of the Thames activity area). Moreover, the tower location would help to frame Harris Park and the Thames Valley Parkway, while also providing increased surveillance into this area (contributing to a safer pedestrian environment).

Figure 8 illustrates the existing building arrangement from the entrance to Harris Park (west elevation); **Figure 9** provides a conceptual rendering of this elevation from a similar location.

Figure 8: West elevation of Site: View east from Thames Valley Parkway



Source: Google Maps: February 4, 2019

In summary, the building positioning is intended to complement, and be sympathetic to, the character of these heritage buildings and the broader Downtown London setting. In this respect, the form and orientation of the tower would establish an iconic gateway feature, while also helping to accentuate the overall character and vitality of Downtown London.

- **Building Massing.** The massing of the proposed point tower is designed to create a comfortable and engaging pedestrian environment that is compatible with, and sensitive to, the adjacent streetscapes, the local development context and the broader downtown setting. Further, the building integrates three principal elements above-grade:
 1. **'Base' Element.** The 'base' component of the proposed tower generally encompasses the main building entrance, building lobby and office/commercial space. In total, the tower base integrates the first four floors of the development and is designed to both

FARHI HOLDINGS - 451 RIDOUT STREET



WEST FACADE PARK VIEW

integrate with 451 Ridout Street North and complement nearby heritage buildings. The positioning of the base component also maintains the continuity of the existing street edge along Ridout Street North and helps to frame the development interface with Harris Park.

2. **'Middle' Element.** Residential units are massed in a defined 'middle' feature situated above the office/commercial space and deck parking and extending from the fifth to 29th floors. The middle element includes the core apartment component of the project, as well as amenity space provided on the fifth floor. Apartment suites incorporate individual balconies oriented towards Ridout Street North, Queens Avenue and Harris Park. These protruding balconies provide shadowing into and across the building surface. Further, distinctive patterned design elements extend the length of the middle feature along the front portions of the east and west façades. Collectively, the components of the middle element serve to soften the massing of the building while also accentuating its relationship with the adjacent streetscapes and the Thames Valley corridor.
3. **'Top' Element.** The top of the building incorporates the 30th to 40th floors and has been designed to be distinctive and to positively contribute to the downtown skyline. This tower component is intended to house residential suites having individual balconies consistent in purpose as those planned for the middle element. The rooftop element would integrate a penthouse suite, a large terrace and mechanical equipment.

- **Façade Treatment.** The tower design incorporates a consistent, contemporary façade style integrating extensive glazing and repetition of lines and windows through horizontal articulations (to help break up the continuous mass of the structure). Most notably, curved elements are integrated the base, middle and top façades to acknowledge and reflect the Thames River corridor interfacing with, and inspiring this design. Interruptions (cut-outs) in the balcony design are also proposed to provide unique design components within the tower to further break up the building mass. Moreover, large windows and entrance features in the lower floors would help to promote more active street frontages along Ridout Street North and Queens Avenue, and to further animate the west elevation (viewed from the Thames River corridor). Appendices 'A' and 'B' present several renderings of the proposed development which illustrate the aforementioned design treatments.
- **Pedestrian Environment/Outdoor Amenity Space.** The office/commercial and residential elements of the proposed tower are to be accessed by a common (main) pedestrian entrance from the Ridout Street North sidewalk to the building lobby. The walkway extends along the north face of 451 Ridout Street North. A secondary building access is also provided along the south building elevation to provide connectivity from the surface parking lot and the Queens Avenue sidewalk. Internal walkways would also provide linkages to surface parking areas, the Thames Valley Parkway and Harris Park.

The building lobby is also accessible from the parking garage. Additionally, as illustrated in Appendix 'C', the landscaping concept for the development provides outdoor amenity space for residents, employees, and customers, as well as the general public.

3.4 Heritage Considerations

The three existing buildings on the Site (435, 441 and 451 Ridout Street North) are listed on the City of London's Inventory of Heritage Resources (Register) as 'Priority 1' structures. Section 4.0 of the Register sets out that Priority I buildings are considered to be London's most important heritage structures and merit designation under Part IV of the *Ontario Heritage Act*. In this respect, the subject property is designated under Part IV of the Act. The property is also designated under Part V of the Act as a part of the Downtown London Heritage Conservation District, and is identified as a National Historic Site of Canada (referred to as the 'Ridout Street Complex'). Further, the Site is adjacent to other listed heritage properties, including the Eldon House (481 Ridout Street North) and the Middlesex Courthouse and Gaol (399 Ridout Street North).

AECOM Canada Inc. (AECOM) has conducted a Heritage Impact Assessment (HIA) in conjunction with this proposal to evaluate the potential heritage impacts of the proposed redevelopment plan. It is generally concluded in Section 7 of the HIA that, to mitigate the potential direct and indirect impacts to identified cultural heritage value, mitigation strategies described in the report should be considered in conjunction with project refinements.

4.0

PLANNING ANALYSIS

The Official Plan and Zoning By-Law Amendment applications defined in Table 1.0 of this Report must be assessed in terms of applicable policies set out by the Province of Ontario and the City of London. The following section outlines how the proposal addresses relevant policies from the Provincial Policy Statement, the City of London Official Plan (1989) and the new City of London Official Plan (The London Plan).

4.1 Provincial Policy Statement

The Provincial Policy Statement (PPS) sets the policy foundation for regulating the development and use of land in Ontario. The 2014 PPS was issued under the authority of Section 3 of the Planning Act and came into effect on April 30, 2014.

Section 3(5) of the Planning Act requires that all decisions affecting planning matters 'shall be consistent with' policy statements issued under the Act. Table 3.0 demonstrates how the proposed Official Plan and Zoning By-law Amendments are consistent with Policies of the 2014 PPS that, in our opinion, have particular relevance to this proposal.

Table 3.0 – Consistency with Provincial Policies

PPS Policy	Response
1.1.1 Healthy, livable and safe communities are sustained by: a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long-term; b) accommodating an appropriate range and mix of residential (including second units, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including	This development proposal promotes an efficient land use pattern that would support the long-term financial well-being of the Province of Ontario and the City of London (in the form of increased property tax assessment and the optimization of existing infrastructure). Further, the residential population, employees and patrons of this development would help to support the overall viability of the City's commercial core. Economic benefits would also be derived from construction of the high-rise tower.

PPS Policy	Response
<p>places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;</p> <p>c) avoiding development and land use patterns which may cause environmental or public health and safety concerns;</p> <p>e) promoting cost-effective development patterns and standards to minimize land consumption and servicing costs;</p> <p>g) ensuring that necessary infrastructure, electricity generation facilities and transmission and distribution systems, and public service facilities are or will be available to meet current and projected needs.</p>	<p>The Site is located within the Central London planning area, which functions as the City's primary business, office, cultural and administrative centre. Development of a mix-use, high-rise on the Site aligns with City objectives to encourage a broader range of residential and employment opportunities in the Downtown London. Further, the core area contains a diverse mix of intensive uses, including office towers and apartment buildings. This development proposal is designed to be compatible with these intensive land uses and appropriate for the existing development context.</p> <p>The proposed development should not result in environmental or public health and safety concerns.</p> <p>Preliminary servicing reviews have been completed to confirm that (1) the development can proceed in a cost-effective manner and (2) the necessary servicing infrastructure would be available to meet projected needs. It is anticipated that existing public service facilities provided in Central London would accommodate the demands of this development.</p>
<p>1.1.2 Sufficient land shall be made available to accommodate an appropriate range and mix of land uses to meet projected needs for a time horizon of up to 20 years. However, where an alternative time period has been established for specific areas of the Province as a result of a provincial planning exercise or a provincial plan, that time frame may be used for municipalities within the area.</p> <p>Within settlement areas, sufficient land shall be made available through</p>	<p>The development of the Site represents an intensification of lands within the City's downtown core that are intended for a wide variety of land uses (including high-rise building forms). This proposal would accommodate a mix of high density residential and office/commercial uses which, collectively, would support efforts to revitalize the downtown core and encourage its continued development as a multi-functional regional centre. Moreover, implementation of this proposal would help the City accommodate forecasted housing</p>

PPS Policy	Response
intensification and redevelopment and, if necessary, designated growth areas.	needs and meet residential intensification targets.
1.1.3.1 Settlement areas shall be the focus of growth and development, and their vitality and regeneration shall be promoted.	The subject lands are situated within the City's Urban Growth Boundary and are therefore considered part of a settlement area, as defined by the PPS. Additionally, this proposal supports the growth and development objectives prescribed for the downtown core as set out in the City's Official Plans (discussed in the following sections of this Report).
<p>1.1.3.2 Land use patterns within settlement areas shall be based on:</p> <p>a) densities and a mix of land uses which:</p> <ol style="list-style-type: none"> 1. efficiently use land and resources; 2. are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and / or uneconomical expansion; 3. minimize negative impacts to air quality and climate change, and promote energy efficiency; 4. support active transportation; 5. are transit-supportive, where transit is planned, exists, or may be developed; 	<p>This development proposal would help to promote efficient use of land and services in Downtown London. Preliminary servicing reviews have also been carried out to confirm that adequate servicing capacity is available to accommodate the intended development, including a preliminary sanitary servicing feasibility study completed by Strik Baldinelli Moniz (SBM).</p> <p>Where practical and appropriate, the project would incorporate sustainable development principles to help minimize environmental impacts.</p> <p>The site layout promotes active transportation through the provision of (1) pedestrian connections to Queens Avenue and Ridout Street North and (2) bicycle storage. Moreover, the project site is located adjacent to public trails associated with the Thames Valley Parkway.</p> <p>The subject lands are located within convenient walking distance of existing LTC bus stops serving the downtown core and providing direct transit connectivity to employment areas, major public institutions and shopping centres.</p>
1.4.3 Planning authorities shall provide for an appropriate range and mix of	Development of a high-rise apartment on the subject lands would help broaden the

PPS Policy	Response
<p>housing types and densities to meet projected requirements of current and future residents of the regional market area by:</p> <ul style="list-style-type: none"> c) directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs; d) promoting densities for new housing which efficiently use land, resources, infrastructure, and public services facilities, and support the use of active transportation and transit in areas where it exists or is to be developed; 	<p>range and mix of housing available to current and future residents of Central London. Additionally, this intensification project would help the City accommodate forecasted housing needs, achieve intensification targets and support downtown revitalization initiatives.</p> <p>Preliminary servicing reviews have been conducted to confirm that servicing infrastructure is available to meet projected needs. Additionally, it is anticipated that existing public service facilities available in the downtown core would accommodate the projected demands of this development.</p> <p>The planned development would (1) utilize existing land and servicing resources in an efficient manner and (2) would be supported by the road transportation system, the active transportation network and transit services (existing and planned).</p>
<p>1.5.1 Healthy, active communities should be promoted by:</p> <ul style="list-style-type: none"> a) Planning public streets, spaces and facilities to be safe, meet the needs of pedestrians, foster social interaction and facilitate active transportation and community connectivity; 	<p>The proposal incorporates two key elements to help support healthy, active communities. Firstly, active transportation and social interaction would be encouraged by (1) the provision of walkway connections to municipal sidewalks, outdoor amenity areas for passive recreation, and bicycle storage in the parking facility, as well as (2) proximity to Harris Park and the Thames Valley Parkway. Secondly, to promote community connectivity, this project integrates residential development and employment activities in proximity to existing neighbourhoods, business, office, cultural and administrative activities, parks, public transit and pathway systems.</p>
<p>1.6.1 Infrastructure, electricity generation facilities and transmission and distribution systems, and public service facilities shall be provided in a</p>	<p>Preliminary servicing reviews have been carried out to confirm that the development can proceed in a cost-effective manner, and to ensure that the necessary infrastructure is</p>

PPS Policy	Response
<p>coordinated, efficient and cost effective manner that considers impacts from climate change while accommodating projected needs.</p>	<p>available to meet projected needs. As discussed, it is anticipated that the development can be accommodated by existing public service facilities.</p>
<p>1.6.7.1 Transportation systems should be provided which are safe, energy efficient, facilitate the movement of people and goods, and are appropriate to address projected needs.</p>	<p>Paradigm Transportation Solutions (Paradigm) completed a Transportation Impact Assessment (TIA) for a preliminary site redevelopment plan, as well as subsequent review of this proposal. Through these evaluations, Paradigm concludes that the existing road network can accommodate the proposed development provided the recommended remedial measures are implemented. The implications the future BRT system on the local road network were also evaluated as part of the TIA.</p>
<p>1.6.7.2 Efficient use shall be made of existing and planned infrastructure, including through the use of transportation demand management strategies, where feasible.</p>	
<p>1.6.7.3 As part of a multimodal transportation system, connectivity within and among transportation systems and modes should be maintained and, where possible, improved including connections which cross jurisdictional boundaries.</p>	<p>The Site is located within convenient walking distance of existing LTC transit services, with several bus routes in operation along the Queens Avenue, Dundas Street and Ridout Street North corridors. The LTC transfer system provides connectivity throughout the City and to regional transportation facilities (e.g., London International Airport, VIA Rail Station, Greyhound Bus Terminal).</p>
<p>1.6.7.4 A land use pattern, density and mix of uses should be promoted that minimize the length and number of vehicle trips and support current and future use of transit and active transportation.</p>	<p>The close proximity of the Site to established service/retail commercial uses, offices, cultural, heritage and entertainment facilities, institutional uses and neighbourhood parks would help future residents, employees and patrons minimize the length and frequency of vehicle trips. Convenient access to public transit, the Greyhound Bus Terminal, and the City's sidewalk and trail networks would also support alternative transportation modes. Furthermore, introducing an intensive residential use and additional employment</p>

PPS Policy	Response
	<p>opportunities on the Site would help to support existing and future investments in public transit (including the proposed BRT system).</p>
<p>1.7.1 Long-term economic prosperity should be supported by:</p> <ul style="list-style-type: none"> b) optimizing the long-term availability and use of land, resources, infrastructure, electricity generation facilities and transmission and distribution systems, and public service facilities; c) maintaining and, where possible, enhancing the vitality and viability of downtowns and mainstreets; 	<p>This intensification proposal has been designed to optimize the use of the subject lands for residential and office/commercial uses, while retaining the existing heritage buildings. The Site also benefits from close proximity to arterial road transportation networks, transit services and public facilities. It is expected that the residents and employees of the proposed tower would support these services and facilities.</p> <p>Additionally, as discussed, the proposal would accommodate a mix of high density residential and office/commercial uses which, collectively, would support efforts to revitalize the downtown core and encourage its continued development as a multi-functional regional centre.</p>
<p>1.8.1 Planning authorities shall support energy conservation and efficiency, improved air quality, reduced greenhouse gas emissions, and climate change adaptation through land use and development patterns which:</p> <ul style="list-style-type: none"> a) promote compact form and a structure of nodes and corridors; b) promote the use of active transportation and transit in and between residential, employment (including commercial and industrial) and institutional uses and other areas; e) improve the mix of employment and housing uses to shorten commute journeys and decrease transportation congestion; 	<p>The proposal: supports intensive residential development; incorporates a compact design; and has regard for, and complements, the development context of the downtown core.</p> <p>Development of the proposed mixed-use, high-rise building would help diversify the range of residential and employment opportunities within Downtown London. Moreover, this project would help to support increased transit ridership and future investments in a BRT system.</p> <p>Introduction of additional residential units and office/commercial space on the subject lands would increase housing choice and employment in the downtown core. In light this, the project would help to shorten commuting requirements for local residents</p>

PPS Policy	Response
	and employees (thereby helping to decrease transportation congestion).
2.1.1 Natural features and areas shall be protected for the long term.	The preliminary Environmental Impact Study (EIS) prepared by Natural Resource Solutions Inc. (NRSI) in conjunction with this proposal provides mitigation measures to satisfy Policy 2.1.1.
2.4.1 Minerals and petroleum resources shall be protected for long-term use.	The subject lands do not contain any known mineral or petroleum resources.
2.5.1 Mineral aggregate resources shall be protected for long-term use and, where provincial information is available, deposits of mineral aggregate resources shall be identified.	The subject lands do not contain any known mineral aggregate resources.
2.6.1 Significant built heritage resources and significant cultural heritage landscapes shall be conserved.	The HIA prepared by AECOM sets out mitigation measures to satisfy Policy 2.6.1.
2.6.2 Development and site alteration shall not be permitted on lands containing archaeological resources or areas of archaeological potential unless significant archaeological resources have been conserved.	A Stage 1-2 Archaeological Assessment of the Site was conducted by AECOM. The assessment did not result in the identification of any archaeological resources on the property.

The 2014 Provincial Policy Statement seeks to achieve healthy, livable and safe communities by promoting efficient development and land use patterns. **Given the foregoing assessment, in our opinion the proposed redevelopment and intensification plan is consistent with the policies of the PPS.** In this regard, the intended use of the Site:

- Represents an efficient development that supports compact urban form, as it seeks to intensify lands situated within the City’s downtown core and located in the vicinity of prominent offices, retail/service commercial enterprises, government activities, recreational, entertainment and cultural facilities, and high-rise apartments;
- Proposes a development plan that: has regard for the established character of the surrounding area; is complementary with, and sympathetic to, existing heritage structures; is compatible with the existing development context; provides a pedestrian orientation; encourages active transportation; and is within close proximity of the arterial road network, transit services and municipal pathways;

- Introduces greater housing choice within the downtown core to help broaden the range and mix of housing available to current and future residents of Central London, and to help the City accommodate forecasted housing demands and achieve intensification targets;
- Integrates a mix of higher density residential and office/commercial uses, which collectively support efforts to revitalize the downtown core and encourage its continued development as a multi-functional regional centre. The proposal would also help to promote neighbourhood stability in the downtown core by diversifying the mix of housing available in this area (to better meet the changing needs of local residents over the long-term);
- Promotes a scale of residential development that would help to (1) optimize municipal investments in servicing infrastructure and public transit, (2) support the long-term viability of various non-residential activities in proximity to the Site, and (3) generate substantial tax revenues to benefit the financial well-being of the Province and the City; and
- Would not involve development on lands having known significant environmental, archaeological, mineral aggregate or petroleum resources.

4.2 City of London Official Plan (1989)

4.2.1 Overview

Policy 4.7 of the PPS states that Official Plans are the most important vehicle for implementation of the Provincial Policy Statement. It is further stated that Official Plans shall identify provincial interests and set out appropriate land use designations and policies. Accordingly, this proposal has been assessed relative to the applicable policies set out by the City of London Official Plan framework, which includes the City's 1989 Official Plan and new Official Plan (The London Plan).

The following discussion evaluates the merits of the proposal relative to the policy framework of the 1989 Official Plan. Section 1.2 of the 1989 Official Plan states that, "The Official Plan for the City of London contains City Council's objectives and policies to guide the short-term and long-term physical development of all lands within the boundary of the municipality." Policies defined within this Official Plan provide direction on a range of planning considerations, such as growth management, urban form, urban design and public consultation, in order to promote the physical development of a healthy, sustainable community.

4.2.2 Existing Designations

Figure 10 to this Report illustrates that the subject lands are designated 'Downtown' and 'Open Space' on Schedule 'A' (Land Use) of the 1989 Official Plan. Generally, the portion of the Site intended for redevelopment is designated Downtown, with lands adjacent to the Thames Valley corridor designated for open space purposes.

Section 4.1.1 of this Official Plan states that the Downtown designation supports the continued development of the core area as the primary business, administrative, institutional, entertainment and cultural centre for the City of London, as well as a regional centre for Southwestern Ontario. Pursuant to Section 4.1.6 of the Official Plan, the permitted uses in this designation include retail, service, office, institutional, entertainment, cultural, high density residential, transportation, recreational and open space uses.

In our opinion, the residential and office/commercial uses intended for the Site are consistent with the permissions of the Downtown designation. It is also our opinion that the proposal is consistent with the mixed-use development permissions of this designation as set out in Sub-Section iv):

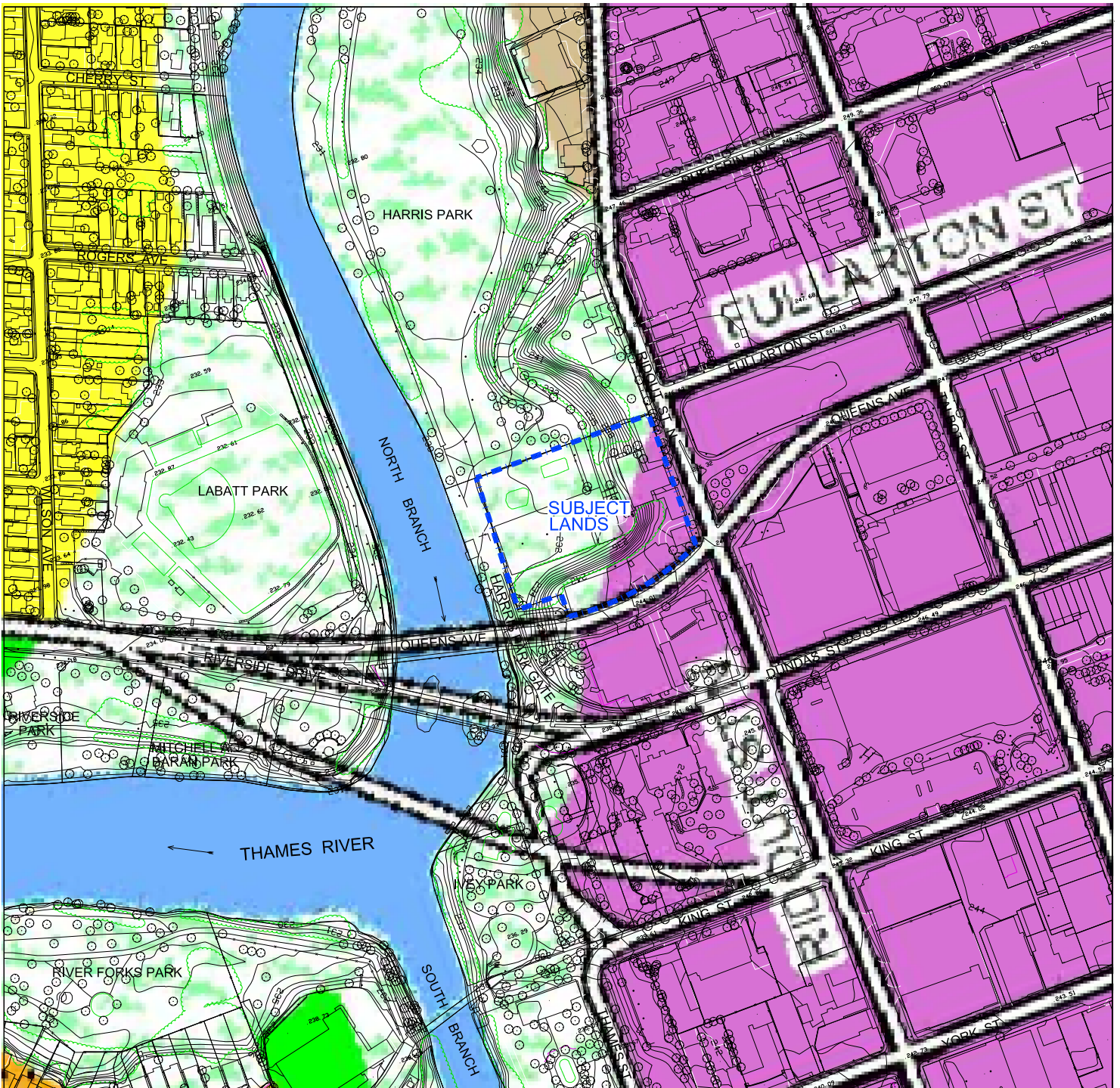
"iv) The development of a variety of high and medium density housing types in the Downtown will be supported. Residential units may be created through new development or through the conversion of vacant or under-utilized space in existing buildings. Residential development within the Downtown Shopping Area shall provide for retail or service-office uses at street level."

Pursuant to Section 8A.2.1 of the Official Plan, areas designated Open Space on Schedule 'A' shall consist of: public open space, including district, city-wide, and regional parks; private open space; flood plain lands; components of the City's Natural Heritage System; and lands that contribute to important ecological functions. In our opinion, as the proposal is consistent with these permissions, as development is not planned for the portion of the Site immediately adjacent to the Thames River corridor.

Additionally, natural heritage features contained within the Site are identified on Schedule B-1 (Natural Heritage Features) and Schedule B-2 (Natural Resources and Hazards) of the 1989 Official Plan. These features relate principally to the Open Space designation and are assessed as part of the preliminary EIS.

4.2.3 Planning Analysis: Applicable Policies

Chapter 2 (Planning Framework), Chapter 4 (Downtown and Commercial Land Use Designations), Chapter 11 (Urban Design Principles), Chapter 13 (Properties of Cultural Heritage Value or Interest) and Chapter 19 (Implementation) of the Official Plan include policies that are relevant to this application.

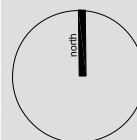


LEGEND

- MULTI FAMILY - HIGH DENSITY RESIDENTIAL
- MULTI FAMILY - MEDIUM DENSITY RESIDENTIAL
- LOW DENSITY RESIDENTIAL
- COMMUNITY FACILITY
- DOWNTOWN AREA
- OPEN SPACE

Figure 10
1989 Official Plan
Designations
 Schedule 'A'
 Plan for Land Use

SUBJECT LANDS



The following discussion: summarizes, in our opinion, the core Official Plan policies pertaining to this proposal; and evaluates the merits of this application relative to this policy framework.

4.2.4 Chapter 2 - Planning Framework

Section 2.3 of the Official Plan contains a number of guiding statements that reflect the objectives and policies contained in the Plan. Principles relevant to this proposal are discussed below.

“ii) Land use planning should promote compatibility among land uses in terms of scale, intensity of use and potentially related impacts.”

The layout and scale of the concept plan has been designed to be sensitive to the surrounding development context, which includes cultural and institutional uses, a major park and event facility, office uses, and a high-rise apartment building (The Harrison). Several design elements are incorporated into the development concept to promote compatibility with these surrounding uses, including:

- Maintaining the three heritage structures along Ridout Street North, and positioning the new tower to the rear of these structures to retain the character of the existing streetscape;
- Enhancing the interface between the downtown development context and the Thames River, particularly the Thames Valley Parkway which extends along the western property boundary; and
- Sizing the tower in a manner that complements, and is compatible with, other high-rise structures in the vicinity of the Site including the Ontario Court of Justice to the southeast and the Harrison high-rise apartment to the northeast.

“vii) Land use planning should promote attractive, functional and accessible site and building design which is sensitive to the scale and character of surrounding uses.”

The proposed building and site layout would provide a highly functional and accessible development and would integrate urban design enhancements to complement the existing downtown context.

Section 2.4.1 of the 1989 Official Plan describes a number of policies that are intended to guide the City’s structure over the life of this Plan. The following policies have particular importance to this proposal.

“vi) High and medium density residential development shall be directed to appropriate areas within and adjacent to the Downtown, near the periphery of Regional and Community

Shopping Areas, and in selected locations along major roads specifically along transit nodes and corridors as identified in Chapter 18, and near Open Space designations.”

The project would introduce a high density residential form within the City’s downtown core at a location fronting an arterial road and within convenient walking distance of public transit and the Thames Valley Parkway (a key component of the Natural Heritage System). Given these considerations and further commentary provided in this Report, in our opinion the subject lands are an appropriate location for the proposed high-rise tower.

“ix) While it is recognized that there may be redevelopment, infill, and intensification in some established residential neighbourhoods, higher intensity land uses will be directed to locations where the character of the residential area is enhanced and existing land uses are not adversely affected.”

Commentary and conceptual renderings presented in this Report demonstrate that the proposal has been designed with sensitivity to the local development context. In this respect, generally surrounding the Site are the following uses: Eldon House and Harris Park to the north, a high-rise apartment to the northeast; a parking lot and office uses to the east; the Ontario Court of Justice to the southeast; Museum London to the south, and the Thames Valley Parkway to the west. In our opinion, implementation of the proposed redevelopment plan would not adversely impact upon the mix of uses evident in this downtown neighbourhood. Regarding specific design characteristics, Section 3.1 of this Report sets out that the proposal is intended to complement the downtown area, be sympathetic to nearby heritage buildings and enhance the local streetscape.

“xviii) Within the boundaries of the City, as they exist over the planning period, the planning and servicing of land to meet residential, commercial and industrial development requirements shall incorporate a reasonable allowance for choice according to location, site area, servicing affordability and other criteria.”

The proposed redevelopment and intensification project would accommodate a maximum of 280 residential suites within the high-rise tower, as well as approximately 6,308 m² of office/commercial space. In effect, this project would support greater housing choice and employment opportunities in Downtown London. In our opinion, the project site is well suited for the intended residential and office/commercial uses, given that it is situated in a mixed-use development area and has excellent accessibility to the arterial road network, public transit facilities, entertainment and cultural facilities, shopping and employment areas, parkland and trail systems.

4.2.5 Chapter 4- Downtown Designation

(a) General Objectives

Section 4.1 of the 1989 Official Plan provides policy direction for development in the Downtown designation:

“It is intended that the Downtown will continue to be the major office employment centre and commercial district in the City, and that its major function as a location for new medium and high density residential environment will be strengthened over time. Limitations on the scale of development will be less restrictive in the Downtown and policies will allow for flexibility in the application of these limitations.”

The Site would introduce additional office/commercial space and high density residential components to help strengthen the office, commercial and residential sectors in the Downtown designation, respectively. In our opinion, the nature and scale of the proposal is in keeping with the policy direction noted above.

Section 4.1.1 of this Official Plan lists overall planning objectives for this designation. The following two objectives have particular relevance to this proposal:

- “ii) Concentrate the development of major office buildings, hotels, convention facilities, entertainment and cultural uses, major indoor sports facilities and government buildings, having City-wide or regional significance, within the Downtown;
- iii) Encourage growth in the residential population of the Downtown and adjacent gateway areas through new development and the renovation and conversion of existing buildings;”

The proposed redevelopment and intensification project would help the City achieve these Downtown objectives by adding office/commercial space and increasing the supply of residential dwelling units in a compact, intensive form.

(b) Urban Design Objectives

Section 4.1.2 of the 1989 Official Plan sets out the principal urban design policy direction for the Downtown designation:

- “i) Promote a high standard of design for buildings to be constructed in the strategic or prominent locations of the Downtown;
- ii) Discourage development and design treatments that are considered detrimental to the functional success and visual quality of Downtown;
- iii) Allow flexibility for individual design creativity and innovation;

- iv) To the extent feasible, position new development to minimize the obstruction of view corridors to natural features and landmarks;
- v) Design new development to complement the appearance and function of any significant natural features and public open spaces that are adjacent to the site;
- vi) Design new developments to provide for continuity and harmony in architectural style with adjacent uses that have a distinctive or attractive visual identity or are recognized as being of architectural or historic significance; and
- vii) Co-ordinate the planning and design of streetscape improvements in the Downtown including the upgrading of building façades, signage, sidewalks, lighting, parking areas and landscaping.”

The concept plan and renderings presented in this Report illustrate that the components of this development have regard for these matters. In summary, the design concept illustrated in these plans demonstrates that a high design standard is applied to this prominent tower. Further, the design is intended to contribute positively to both the local streetscape and the City’s downtown skyline.

Additionally, in our opinion:

- The prominent design elements of the tower would create an attractive gateway feature for the Forks of the Thames activity area; and
- The contemporary design of this high-rise building and associated landscaping complements the adjacent streetscape along Ridout Street North and Queens Avenue.

In light of these considerations, it is our opinion that the proposed development is consistent with the urban design direction set out for the Downtown designation.

(c) Permitted Uses

Section 4.1.6 of the 1989 Official Plan prescribes the permitted uses for the Downtown designation. This Section also states that Council shall support the continued development of the Downtown as a multi-functional regional centre containing a broad range of retail; service; office; institutional; entertainment; cultural; high density residential; transportation; recreational; and open space uses.

As discussed, the proposed redevelopment project would introduce a maximum of 280 residential suites within a high density housing form, as well as approximately 6,308 m² of complementary office/commercial space in the tower base. The proposed development would supplement the 1,627 m² of gross office space provided in the existing heritage buildings on the property.

(d) Scale of Development

Section 4.1.7 of the 1989 Official Plan states that the Downtown designation is to accommodate the greatest height and density of retail, service, office and residential development within London. Sub-Section i) sets out the permissions for development within this designation:

“Development in the Downtown may be permitted up to a maximum floor area ratio of 10:1 for commercial uses and will normally not exceed 350 units per hectare (140 units per acre for residential uses). Increases in density may be permitted without amendment to this Plan provided the proposal satisfies density bonusing provisions of Section 3.4.3 iv) and 19.4.4 of the Plan, conforms to the Site Plan Control By-law and addresses standards in the Downtown Design Guidelines. ...”

Pursuant to the More Homes, More Choice Act, 2019, the bonusing permissions set out in Section 37 of the Planning Act are to be removed following proclamation by the Lieutenant Governor. Given this impending amendment, we understand that the City of London will not consider bonusing zoning permissions in conjunction with new planning applications. Notwithstanding, this redevelopment plan has consideration for the bonusing criteria set out in this Official Plan (as discussed in Section 4.2.10 of this Report).

The design concept proposes a maximum of 280 dwelling units on the 0.73 ha portion of the Site zoned ‘Downtown Area (DA2(3))’; equating to a residential density of 383 units/ha. Additionally, pursuant to Section 3.4 1) of the Zoning By-Law, the non-residential component of the project is assessed at an equivalent rate of one (1) dwelling unit per 100.0 m² of gross floor area (GFA). Considering the total gross floor office area within existing heritage buildings (1,627 m²) and the area expressly dedicated for office/commercial space in the planned tower (6,308 m²), the residential density of the Site would equate to 493 units/ha. The 1989 Official Plan does not specify a maximum building height for development in the Downtown designation.

In light of these considerations and the impending removal of bonusing permissions, in our opinion, a new Specific Area policy would be required under Chapter 10 of this Official Plan in order to permit the increased residential density associated with this proposal. Specifically, a residential density of 500 units/ha is proposed to accommodate the intended development and the existing office space, and to recognize that minor refinements may be required to the DA2 Zone boundary through the course of project planning (which will impact upon the calculated density).

4.2.6 Chapter 10 – Policies for Specific Areas

In the context of the 1989 Official Plan, the development proposal requires a Specific Area policy to allow for a residential density that exceeds the standard permissions of the Downtown designation. Site-specific permissions are typically identified under Chapter 10

(Policies for Specific Areas) of this Official Plan. It is noted under Section 10.1.1 of this Plan that the adoption of policies for Specific Areas may be considered where one or more of the stated conditions apply. The following analysis evaluates this proposal relative to each condition:

"i) The change in land use is site specific, is appropriate given the mix of uses in the area, and cannot be accommodated within other land use designations without having a negative impact on the surrounding area."

The OPA and ZBA applications request site-specific permissions for a prominent, mixed-use development that provides for a more intensive design than is allowed for under the standard permissions of the Downtown designation. Given the surrounding development context and the broader downtown setting, in our opinion this redevelopment proposal is appropriate for this particular site. Further, considering the urban structure of the 1989 Official Plan, in our opinion the Downtown designation is the most appropriate designation to implement the mix of uses associated with this proposal.

"ii) The change in land use is site specific and is located in an area where Council wishes to maintain existing land use designations, while allowing for a site specific use."

The intent of the applications is to establish site-specific permissions supporting a prominent, high-rise tower located at a strategic gateway/riverfront property. In our opinion, the intended use of the subject lands is appropriate for the Downtown designation subject to approval of a Specific Area policy to recognize the proposed residential density. It is also our opinion that Council would prefer to approve this project by way of a Specific Area policy; rather than to broadly redesignate these lands and adjacent properties to allow for increased residential densities in this area.

"iii) The existing mix of uses in the area does not lend itself to a specific land use designation for directing future development and a site specific policy is required."

As outlined in Section 2.2 of this Report, the subject lands form part of the Central London planning area which includes the downtown core and is comprised of a diverse mix of intensive land uses (e.g., offices, retail/service commercial enterprises, government activities, recreational, entertainment and cultural facilities). The Site is also located in proximity to a variety of these downtown land uses including apartment buildings, office towers, commercial areas and major civic attractions. Notwithstanding this development context, it is our opinion that the existing mix of uses in this area does not lend itself to a specific designation that would broadly permit the residential density associated with this proposal. Accordingly, in our opinion, a site-specific policy should be adopted to implement this project.

"iv) The policy is required to restrict the range of permitted uses, or to restrict the scale and density of development normally allowed in a particular designation, in order to protect other uses in an area from negative impacts associated with excessive noise, traffic, loss of privacy or servicing constraints."

The development proposal is intended to permit a residential density in excess of the normal maximum density applied in the Downtown designation. In our opinion, the request to exceed the density requirement will not generate the types of land use conflicts set out in this policy. With respect to loss of privacy, in particular, it is our opinion that the proposed building has been positioned to provide adequate separation distance from surrounding development including surrounding high-rise towers.

Additionally, Section 10.1.2. of the 1989 Official Plan states that all applications contemplating policies for Specific Areas are subject to the Plan's Planning Impact Analysis. Section 4.5.2 of this Plan sets out criteria that municipal staff are to consider when evaluating the merits of planning applications pertaining to the Downtown designation. In our opinion, the following criteria are particularly relevant to this proposal:

- "i) the policies contained in the Section relating to the requested designation;
- ii) compatibility of proposed uses with surrounding land uses, and the likely impact of the proposed development on present and future land uses in the area;
- iii) the size and shape of the parcel of land on which a proposal is to be located, and the ability of the site to accommodate the intensity of the proposed uses;
- v) the potential traffic generated by the proposed change, considering the most intense land uses that could be permitted by such a change, and the likely impact of this additional traffic on City streets, pedestrian and vehicular safety, and on surrounding properties;
- vi) the height, location and spacing of any buildings in the proposed development, and any potential impacts on surrounding land uses;
- ix) the exterior design in terms of bulk, scale, and layout of buildings, and the integration of these uses with present and future land uses in the area and its conformity with the City's commercial urban design guidelines;
- x) the potential impact of the proposed development on surrounding natural features and heritage resources;
- xii) compliance of the proposed development with the provisions of the City's Official Plan, Zoning By-law, Site Plan Control By-law, and Sign Control By-law;"

Commentary provided throughout this Report and in corresponding reports demonstrates that the proposal has regard for matters relating to: the alignment of the project with the existing Downtown designation policy regime; compatibility with adjacent uses, the ability of the property and site servicing to accommodate the use; potential traffic impacts; building setbacks and the integration of the built form with surrounding development; urban design

elements; potential natural heritage impacts and compliance with City policies and regulations. In light of these considerations and other commentary provided in this Report, in our opinion implementation of this proposal will not generate significant land use impacts.

Based upon the foregoing, it is our opinion that the proposal satisfies the requirements for a Specific Area policy pursuant to Chapter 10 of the 1989 Official Plan.

4.2.7 Chapter 11 - Urban Design Principles

Section 4.1.7 ii) of the 1989 Official Plan directs that development proposals in the Downtown designation have regard for the urban design principles itemized in Chapter 11 of this Plan.

The urban design principles itemized within Chapter 11 (Section 11.1.1) supplement the applicable Urban Design Objectives identified under Section 4.1.2 of the Official Plan and discussed in Section 4.2.5 b) of this Report. These principles provide direction for matters related to the visual character, aesthetics and compatibility of land use, and to the qualitative aspects of development. As an overarching objective, it is also stated in this Sub-Section that:

“It is intended that Downtown development should enhance the street level pedestrian environment and contribute to the sensitive integration of new development with adjacent structures and land uses.”

In our opinion, the concept plan presented in Appendix ‘A’ demonstrates regard for the principles set out in Section 11.1.1. In particular, the design was developed with consideration for the following applicable design principles:

“Natural Features

- i) The form and design of new development shall complement and protect any significant natural features such as river valleys, ravines, wooded areas and parklands that form part of, or are located adjacent to, the site.”

The proposed tower design is intended to complement and frame the adjacent Thames River corridor, including the Harris Park entrance feature. As discussed, the project does not propose development on lands intended for open space.

“High Design Standards

- iv) Emphasis will be placed on the promotion of a high standard of design for buildings to be constructed in strategic or prominent locations such as within, and at the perimeter of, the Downtown, near the edge of the river valleys, or along the major entryways to the City.”

The redevelopment project proposes a high-rise tower design that would (1) create a strong visual presence along the Thames River corridor and (2) function as a focal point for the western gateway into the City's downtown core.

"Architectural Continuity

- v) The massing and conceptual design of new development should provide for continuity and harmony in architectural style with adjacent uses which have a distinctive and attractive visual identity or which are recognized as being of cultural heritage value or interest."

The proposed high-rise tower is designed to be compatible with to the surrounding context, which includes high-rise buildings to the southeast (Ontario Court of Justice), and northeast (The Harrison Apartment). The tower form is also designed to complement the character of the existing heritage buildings on the Site.

"Parking and Loading

- xiii) Parking and loading facilities and driveways should be located and designed to facilitate maneuverability on site, between adjacent sites where appropriate, and to reduce the traffic flow disruption resulting from turning movements to and from the property."

The redevelopment plan for the Site includes a reconfiguration of the vehicular access and parking arrangement. The revised parking, loading and access arrangement has been designed to promote safe and efficient ingress/egress consistent with the objectives of this policy.

"Recreational Facilities

- xvii) The developers of medium or high density residential projects shall be encouraged to provide recreational facilities appropriate to the size of the development and the needs and interests of the intended residents."

The Site benefits from a prominent location adjacent to Thames Valley Parkway and Harris Park. As illustrated in the conceptual landscape plan included in Appendix 'C', amenity areas and pedestrian walkways would be incorporated into the redevelopment plan to encourage further connectivity with these community parks.

"Gateways

- xxi) Gateways are important elements in the creation of a sense of place and arrival, and provide visual signals that both define and distinguish an area. Gateways occupy strategic and prominent locations, and are primarily associated with major entrances to the City, districts or to neighbourhoods. Gateways may be created through the placement of buildings, landscape features, or the design and architecture of the buildings or structures themselves that frame or create the gateway or entrance."

The Site is located in close proximity to the Forks of the Thames activity area and forms part of the western gateway to the City's downtown core. Given the prominence of this location, the proposed high-rise tower incorporates a high design standard to further define and distinguish this gateway, and to establish the Site as a focal point along the Thames River corridor.

4.2.8 Chapter 13 – Heritage Resources Policies

Policies set out in Chapter 13 of the 1989 Official Plan provide a framework: to encourage the protection and enhancement of heritage resources; to promote sensitive design; and to guide the designation and maintenance of Heritage Conservation Districts (HCD). Section 13.1 indicates the key objectives of the Official Plan in relation to cultural heritage including (1) the protection and enhancement of cultural resources (e.g., buildings, sites) and (2) the encouragement of new development and redevelopment to be sensitive to heritage resources.

Section 13.3.6 of this Official Plan sets out guiding policies for designated HCDs, including the following provisions applicable to the proposal:

- "i) the character of the District shall be maintained by encouraging the retention of existing structures and landscape features;
- ii) the design of new development, either as infilling or as additions to existing buildings, should complement the prevailing character of the area;
- iii) regard shall be had at all times to the guidelines and intent of the Heritage Conservation District Plan;"

The Site is located within the Downtown HCD which is generally described in Section 13.3.8.5. It is stated in this Section that this district (1) encompasses a portion of the Downtown and (2) contains the greatest collection and variety of heritage buildings in the City.

Section 3.2 of the Downtown London HCD Plan (March 2012) sets out the physical and social goals/objectives for this Heritage District, including the following which have importance to this proposal:

- Establish the framework for the retention, conservation, and adaptation of the existing stock of significant heritage buildings and spaces within the Downtown District;
- Influence the renovation or construction of modern era buildings so that it is done with regard to the District and complementary to the character and streetscape;

- Emphasize the significance of the cultural heritage and its role in defining the sense of place for the Downtown;
- Increase tourism across the District; and
- Encourage the redevelopment, intensification, and acceptance of the Downtown as the cultural and social focus of the community.

The AECOM HIA addresses the 1989 Official Plan policy framework respecting heritage resources, as well as the principles and guidelines of the HCD Plan. The study generally concludes that to mitigate the potential direct and indirect impacts to identified cultural heritage value, mitigation strategies described in the report should be considered in conjunction with project refinements and approaches. Further, it is recommended that details related to the exterior design, the streetscape character, and the future re-use of the heritage structures should be considered in depth as a part of the proposed project (in order to mitigate impacts, and to conserve the cultural heritage value of the property).

4.2.9 Chapter 19 – Implementation

(a) Guideline Documents

Section 19.2 of the Official Plan prescribes that guideline documents may be used to assist in the implementation and refinement of the Official Plan. Given the development context of the Site and the nature of this proposal, the project design has had regard for several approved guidelines; most notably ‘Our Move Forward (London’s Downtown Plan)’, dated April 2015, and the ‘City of London Placemaking Guidelines’ (November 2007).

(b) The Downtown Plan

Framework and Objectives

The planning framework for the Downtown Plan states that the plan was developed with consideration for the existing development context and establishes a planning structure for future public and private investment in the City’s core. Further, this guideline establishes several overarching values, directions and themes to achieve the defined Downtown vision statement: “London’s face to the world. A vibrant destination. A unique neighbourhood”.

Nine specific ‘Downtown’ values are established in the Downtown Plan: Leadership, Prosperity, Sustainability, Livability, Innovation, Partnership, Inclusivity, Experience and Heritage. Moreover, six ‘Strategic Directions’ are identified to support the identified values for this area:

1. Make Dundas Street the most exciting place in London;
2. Reconnect with the Thames River;
3. Forge connections with the downtown neighbourhoods;
4. Green our downtown;
5. Build a great neighbourhood; and
6. Create the buzz.

In our opinion, the characteristics of this development proposal illustrate that the project is in keeping with the values, strategic directions and associated urban design objectives of the Downtown Plan. In particular, it is our opinion that the project supports those initiatives related to reconnecting with the Thames River, building a great neighbourhood and 'creating the buzz'. The merits of this project relative to each of these Strategic Directions is summarized below.

- **Reconnect with the Thames River.** Strategic Direction 2 of the Downtown Plan addresses a City objective to reconnect London residents with the Forks of the Thames activity area. In this regard, it is noted in the Plan that, "The river is also a draw for events and represents an opportunity for new active uses. In re-establishing the connection to the Thames River, we can help to create a London that is a vibrant and dynamic city that embraces both its cultural heritage and natural environment". As discussed in this Report, it is our opinion that the form and design of the planned tower would create an attractive gateway feature to help frame this activity area. Additionally, the project would integrate improvements to pedestrian connections in the vicinity of the Thames River corridor. In our opinion, both of these measures support the core objectives of this Strategic Direction.
- **Build a Great Neighbourhood.** In relation to Strategic Direction 5, it is stated in the Downtown Plan that, "The downtown is the "hub" for the city, but it is also a neighbourhood where people can live and be social. It is imperative for the downtown's success to ensure that it is designed to be a neighbourhood first, by providing for the local needs of the residents in the area". As identified in this Report, the proposed high-rise tower is designed to function as a community focal point that complements surrounding land uses, the local development context and the larger downtown core. In light of the building positioning, site layout, architectural design, and intended uses, it is our opinion that the proposal supports the broad objective of this Strategic Objective to enhance the function of the local neighbourhood.
- **Create the buzz.** The Downtown Plan states the following in relation to Strategic Direction 6, "The diversity, intensity and density that uniquely characterize the core makes it ideally suited to consider unconventional ideas and fertile ground for testing new ways of doing things". In our opinion, the nature of this proposal, to integrate a high-rise tower with existing heritage buildings to further define the Downtown's western gateway and the Thames River corridor, closely aligns with the type of unique, intensive project envisioned to support this Strategic Direction.

Transformational Projects

The Downtown Plan also sets out a variety of initiatives that support ten 'Transformational Projects' designed to help implement the vision, values and strategic directions of this guideline. Transformational Projects are described in the Downtown Plan as initiatives, "... designed to act as anchors for future development and to encourage more private sector investment to provide maximum benefit for the downtown and assist with continued revitalization of this important area of the city".

Four Transformational Projects have been identified in proximity to this Site. The following discussion provides a general overview of these projects as set out in the Downtown Plan:

- **Cross-River Connection.** This project consists of a significantly enhanced connection between the east and west sides of the Thames River, by improving the pedestrian crossing experience and creating a distinctive sense of place when entering the downtown. In particular, this project would convert Kensington Bridge into a non-vehicular connection across the Thames River to connect the downtown with the westerly neighbourhoods and Labatt Park. In effect, repurposing the bridge would establish a plaza between Museum London and the former Middlesex Court House, and connect Dundas Place with the Forks of the Thames.
- **Forks of the Thames.** The Forks of the Thames project envisions an urban promenade along the Thames River corridor and an urban park space at the western terminus of Dundas Street. Generally, the intent of this initiative is to better connect activity on Dundas Place with riverfront activities.
- **Laneway Connections.** The Downtown Plan supports additions and enhancements to the laneway network, as well as the integration of these laneways into future developments (to create small-scale restaurant and retail 'spill out' spaces). In the vicinity of the project site, this Plan identifies a laneway enhancement opportunity for the mid-block connection between Queens Avenue and Dundas Streets.
- **Queens Station.** This project intends to establish a new transit hub focused at the intersection of Queens Avenue and Richmond Street. This transit hub would be developed to be easily distinguishable as a primary city transit transfer point.

In our opinion, this proposal supports the Transformational Projects identified above, as it would intensify nearby lands with new residents and employees who would utilize, and derive benefit from, these projects. In this respect, it is fully anticipated that development of the proposed tower would contribute to the overall vibrancy of the activities, services and public spaces identified above.

(c) City of London Placemaking Guidelines

The City of London Placemaking Guidelines are intended to promote liveable communities, an identifiable character and a sense of place. Notwithstanding that this Guideline is generally intended for large scale 'greenfield' development, a number of goals and themes set out in this document are relevant to redevelopment and intensification plans.

The following summarizes the principal (core) Placemaking Guidelines that provided design direction for this project:

Site Context and Community Elements

- Establish key social, pedestrian and public transit connections between new development and the existing fabric of the city.
- Develop an overall concept plan which clearly identifies the relationship between land uses, built form and natural features of the community.
- Develop a circulation plan identifying where linkages would be established to support pedestrian, automobile, cycling movements throughout the community.
- Buildings should be located close to the street and should be architecturally articulated to provide an appropriate level of detail that would visually animate the streetscape.

Focal Points, Public Realm and Residential Design

- Utilize paving and landscaping treatments in the design of visual focal points.
- Design convenient pedestrian linkages to focal points and public transit.
- Employ exterior building materials and colours that are stylistically appropriate.
- Avoid architectural designs that are ubiquitous and non-descript because they do not contribute to a sense of place.
- Where appropriate design for the mutually compatible integration of low, medium and high density residential uses through the effective use of: site planning, building massing and orientation, architectural and landscape design, visual impact analysis.
- A mix of housing opportunities can accommodate the changing lifestyles of an aging population while allowing neighbours to continue to live in the community.

Pedestrian Environments

- Design pedestrian environments that provide a sense of safety and separation from automobile traffic.
- Orient buildings, their massing, architectural elements and habitable areas so that they promote an eyes-on-the-street approach to streetscapes and public spaces.
- Use architectural and landscape design to enhance visually prominent locations.
- Design buildings and spaces to encourage social interaction.

4.2.10 Height and Density Bonusing

Section 19.4.4 of the 1989 Official Plan defines the principles and objectives for implementing height and density bonusing opportunities within the Zoning By-law (pursuant to the permissions prescribed in Section 37 of the Planning Act). Sub-Policy ii) states that bonusing will be used to support the City's urban design principles and other policies of the Plan, and to support one or more of the defined bonusing objectives. Notwithstanding the impending removal of bonusing permissions from the Planning Act, the merits of this application have been reviewed relative to the bonusing objectives of this Section.

For this proposal, a maximum residential density of 500 units/ha is requested to permit the proposed mixed-use development in the context of the Downtown designation. In light of the enhanced urban design elements associated with this proposal as described and illustrated in this Report, it is our opinion that increased density is warranted for this project in light of the City's bonusing objectives set out in Sub-Policy ii):

- “(b) to support the provision of common open space that is functional for active or passive recreational use;
- (c) to support the provision of underground parking;
- (d) to encourage aesthetically attractive residential developments through the enhanced provision of landscaped open space;
- (h) to support innovative and environmentally sensitive development which incorporates notable design features, promotes energy conservation, waste and water recycling and use of public transit;
- (j) to support the provision of design features that provide for universal accessibility in new construction and/or redevelopment.”

Section 5.2.2 of this Report discusses the site specific zone structure proposed to accommodate the increased residential density associated with the planned development.

4.2.11 Summation

Given these considerations and our broader review of the 1989 Official Plan, with a Specific Area policy permitting a residential density of 500 units/ha, in our opinion the proposal is consistent with the principles, policy direction and development permissions of the Downtown designation. In this regard, implementation of this project would complement the surrounding development context and support the redevelopment and intensification of lands designated for a broad mix of uses including high density residential, office and commercial uses. Moreover, the concept plan incorporates elements that collectively support the urban design principles and bonusing objectives of this Official Plan, the revitalization

objectives of the City's Downtown Plan and the design direction of the City's Placemaking Guidelines.

4.3 City of London Official Plan (The London Plan)

4.3.1 Status

The City of London Council adopted a new Official Plan on June 23, 2016 and the Ministry of Municipal Affairs (MMA) issued its approval of The London Plan, with modifications, on December 30, 2016.

Several policies and schedules of the new Official Plan applicable to the Site were appealed to the Ontario Municipal Board and are not currently in effect. Notwithstanding, the following discussion describes how the proposal aligns with the policy direction and development provisions of The London Plan as approved by the MMA.

4.3.2 Place Type and Permitted Uses

Under The London Plan, the portion of the Site to be developed is generally located within the 'Downtown' place type on the land use schedule ('Map 1 - Place Types') with the balance of the property designated 'Green Space' place type (refer to **Figure 11**).

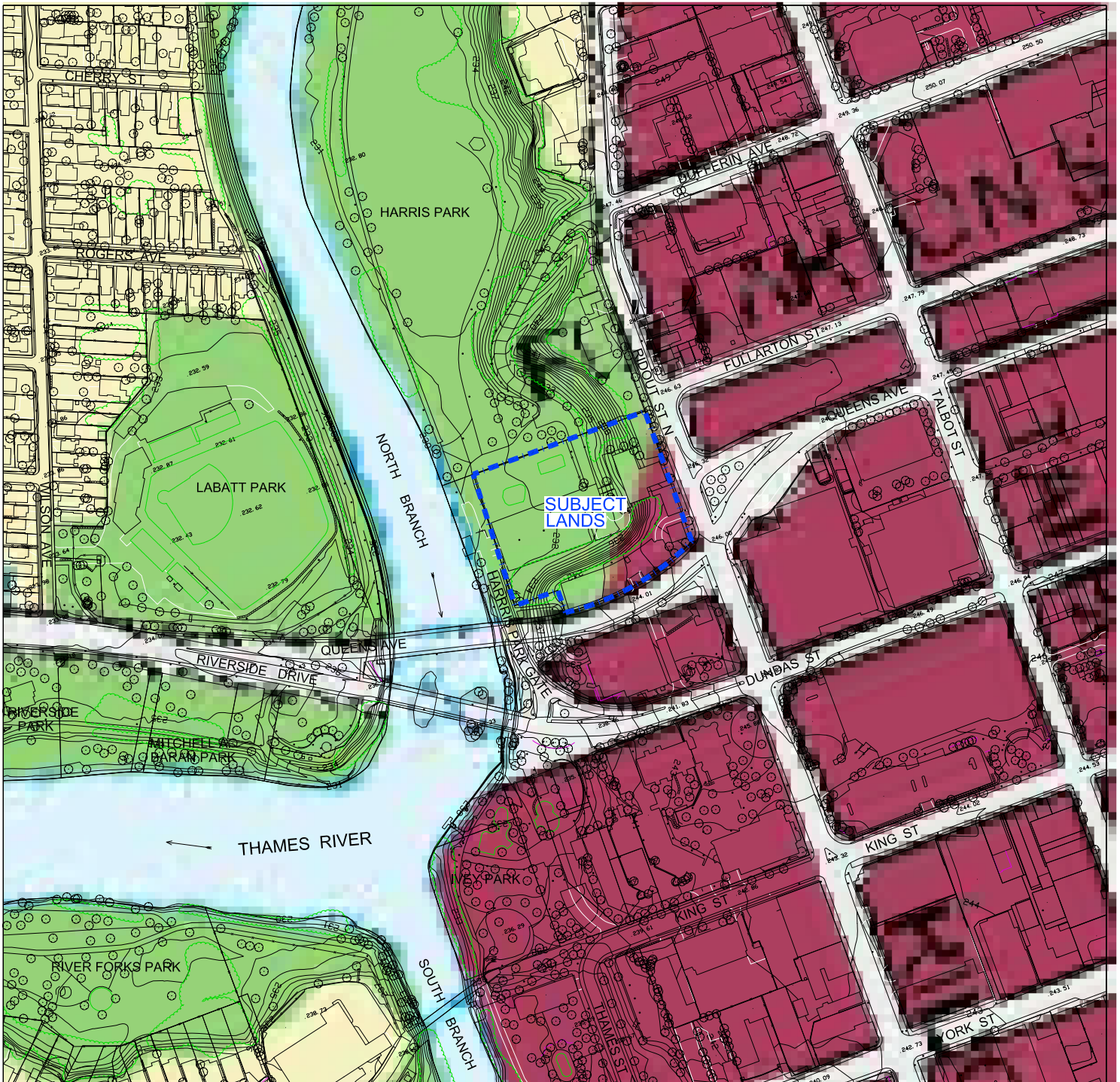
Policies 793 and 798 address the vision and role of the Downtown in the City of London structure, and emphasize the importance of a diverse, vibrant and connected core community:

"793. Our Downtown will exude excitement, vibrancy, and a high quality of urban living.

It will be the preeminent destination place for Londoners, residents from our region, and tourists to experience diverse culture, arts, recreation, entertainment, shopping and food. Our Downtown will showcase our history and offer vibrant and comfortable public places filled with people, ranging from large city-wide gathering places, to heavily treed urban plazas and intimate parkettes.

798. As shown in our City Structure Plan, **the Downtown will serve as the highest-order mixed-use centre, and will be unique in the city.** It will be connected to the transit villages through rapid transit corridors, and will also be connected to our recreational network, at the confluence of the two branches of the Thames River. Major rail lines, serving commuter traffic, strongly connect our Downtown to the surrounding region, nationally and internationally." (emphasis added)

Policy 800 of this Official Plan states that a broad range of residential, retail, service, office, cultural, institutional, hospitality, entertainment and recreational uses are permitted in the Downtown place type. Pursuant to Policy 802(1), the minimum permitted building height



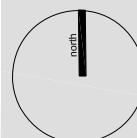
LEGEND

- NEIGHBOURHOODS
- GREEN SPACE
- DOWNTOWN

Figure 11
The London Plan

Map 1
Place Types

 SUBJECT LANDS



within this designation is three (3) storeys or 12 metres, with the maximum building height being 20 storeys with 'Type 1' bonus zoning or 35 storeys with 'Type 2' bonus zoning. The London Plan does not specify minimum or maximum residential densities for this place type.

Policy 762 of the new Official Plan generally prescribes that lands in the Green Space place type can be utilized for passive/active recreational uses, community facilities, private green space, agricultural, woodlot management, conservation and stormwater management (depending upon the natural heritage features contained on the lands and potential hazards). In the context of this proposal, lands in the Green Space place type are to be retained for open space consistent with the above-noted permissions. Additionally, natural heritage features contained within the Site are identified on Map 5 (Natural Heritage) and Map 6 (Hazards and Natural Resources) of The London Plan. These features relate principally to the Green Space place type and are discussed in the preliminary EIS.

Given this framework, the height of the proposed high-rise tower exceeds the permissions set out in the new Official Plan under both standard and bonused conditions. Accordingly, to implement the project under the policies of The London Plan, a Specific Area policy for the Downtown place type will be required to permit a building form on the Site having a maximum building height of 40 storeys.

City staff have advised that notwithstanding active appeals to applicable policies of The London Plan, an Official Plan Amendment is necessary (1) to add a Specific Area policy for the Downtown place type and (2) to delineate the subject lands on Map 7 (Specific Policy Areas). The Downtown and Green Space place type boundaries of Map 1 may also be refined in conjunction with this application.

4.3.3 Built Form Policies (Downtown Place Type)

Policy 803 of The London Plan identifies a series of built form policies for the Downtown place type. In our opinion, the following policies are most relevant to this application:

- "1. Downtown will be subject to the following policies: All planning and development applications will conform with the City Design policies of this Plan, Our Move Forward: London's Downtown Plan and the Downtown Design Manual.
2. All planning and development applications will conform with the Downtown Heritage Conservation District Plan and related guidelines.
3. All the planning and design that is undertaken Downtown will place a priority on the pedestrian experience through site layout, building location, and a design that reinforces pedestrian comfort and safety.
4. The design and positioning of new buildings in the Downtown will not negatively impact pedestrian comfort by introducing inappropriate wind turbulence and velocity within the public realm. A wind assessment will be required for all buildings of 6 storeys or more,

with the intent of mitigating wind impacts on the pedestrian and other ground level environments.

5. Buildings should be designed to include architectural features that protect pedestrians from rain and sun exposure. Such features include, but are not limited to, awnings, arcades, and overhangs and should be designed in an unobtrusive manner.
6. The design of new development will provide for continuity and harmony in architectural style with adjacent uses that are of architectural or historical significance.
7. Building design that represents individual creativity and innovation will be encouraged within the Downtown to create landmarks, develop a distinctive character for the Downtown, and contribute to the city's image."

The design direction of The London Plan identified above is largely consistent with the 1989 Official Plan and has been considered in the development of this proposal as detailed in this Report. Taking these matters into account, in our opinion the design concept is in keeping with the built form policy permissions of this Official Plan.

4.3.4 City Design

With respect to the City Design component of The London Plan referenced above, Policies 189 to 306 of The London Plan define the urban design principles and policies that are intended to guide the character and form of development.

The overarching objectives of these policies are outlined in Policy 193:

"In all of the planning and development we do and the initiatives we take as a municipality, we will design for and foster:

1. A well-designed built form throughout the city.
2. Development that is designed to be a good fit and compatible within its context.
3. A high-quality, distinctive and memorable city image.
4. Development that supports a positive pedestrian environment.
5. A built form that is supportive of all types of active mobility and universal accessibility.
6. High-quality public spaces that are safe, accessible, attractive and vibrant.
7. A mix of housing types to support ageing in place and affordability.

8. Sustainably designed development that is resilient to long-term change.
9. Healthy, diverse and vibrant neighbourhoods that promote a sense of place and character.”

In our opinion, the project description, concept plan and renderings included in this Report demonstrate that the proposed redevelopment and intensification project has been designed to achieve these broad objectives, where applicable. Further, it is our opinion that the project design aligns with the intent of the character, street network, streetscape, public space, site layout and building form policies set out in the City Design section of the new Official Plan.

Most notably, it is our opinion that the design of the proposed tower described and illustrated in this Report supports The London Plan policy direction for high-rise buildings. Policies 289, 292 and 293 are particularly relevant to high-rise design and, in our opinion, are satisfied by the proposed tower arrangement:

”289_ High and mid-rise buildings should be designed to express three defined components: a base, middle, and top. Alternative design solutions that address the following intentions may be permitted.

1. The base should establish a human-scale façade with active frontages including, where appropriate, windows with transparent glass, forecourts, patios, awnings, lighting, and the use of materials that reinforce a human scale.
2. The middle should be visually cohesive with, but distinct from, the base and top.
3. The top should provide a finishing treatment, such as a roof or a cornice treatment, and will serve to hide and integrate mechanical penthouses.

292_ High-rise buildings should incorporate a podium at the building base, to reduce the apparent height and mass of tall buildings on the pedestrian environment, allow sunlight to penetrate into the right-of-way, and reduce the wind tunnel effect.

293_ High-rise buildings should be designed with slender towers that reduce shadow impact, minimize the obstruction of views, and are less massive to neighbouring properties. A typical floor plate of approximately 1,000m² is a reasonable target to achieve this goal. Commercial towers may have larger floor plates, but should still have effective separations between towers to allow access to sunlight and views.” (emphasis added)

4.3.5 Type 2 Bonus Zoning

Policy 1652 of The London Plan states that for Type 2 bonus zoning, additional building height may be permitted in exchange for certain facilities, services, or matters. Notwithstanding that the proposal would be implemented by way of a Specific Area policy rather than a Type 2 bonusing permission, in our opinion, design elements encompassed with the concept plan align with bonusable items set out in Policy 1652 as discussed below.

- Exceptional site and building design. As discussed in Sections 3.2 and 4.2.6 of this Report, collectively the building design and site layout incorporate contemporary architectural themes and design elements to establish a prominent, intensive high-rise design that is compatible with adjacent heritage buildings, surrounding land uses, the local development context and the larger downtown core. Additionally, vehicular parking is to be predominately accommodated in structured parking facilities and screened from view from the Ann and St. George Street frontages.
- Cultural Heritage Resources and Conservation. Three buildings are currently contained on the Site: a two storey office building (435 Ridout Street North), a three storey office building (441 Ridout Street North), and a split level, three storey office building (451 Ridout Street North). These buildings are listed under Part IV of the Ontario Heritage Act, and collectively encompass a National Historic Site. The proposed tower has been designed in a manner that is sensitive to these existing heritage buildings, and to effectively integrate with 451 Ridout Street. Moreover, the proposed building arrangement is intended to preserve the unobstructed view of 435, 441 and 451 Ridout Street North from the street frontage and to promote a landmark vista at the western terminus of Queen Street. No modifications are proposed to the heritage buildings addressed 435 and 441 Ridout Street North.
- Sustainable Forms of Development. The proposed development will be designed and built with consideration for suitable sustainability techniques, materials and systems. Additionally, landscape plans for common outdoor amenity areas will consider several sustainable design elements, including hard landscape elements and drought resistant landscaping to reduce water consumption.
- Measures to Enhance Natural Heritage System. Bank stabilization measures, including a landscape program, will be implemented along the western limit of the development area. Grading and plantings associated with the stabilization project will help to enhance the Thames River corridor natural heritage system.
- Large quantities of secure bicycle parking and cycling infrastructure. Secure bicycle parking will be provided in the underground parking structure for building residents. Public bicycle parking would also be provided for the new high-rise tower at convenient outdoor locations, where appropriate. Moreover, it is anticipated that internal pathway connections from the tower site and surface parking areas will provide additional connectivity to the street frontages, transit stops and the Thames River corridor.
- Other facilities, services, or matters that provide substantive public benefit. Further discussions may be required with City staff to explore opportunities for the inclusion of additional items in a community benefits program for this project (e.g., public plazas, on-site local art displays).

Additionally, the Policies 1653 and 1654 provide direction to City Council for the application of Type 2 bonusing:

“1653_Type 2 Bonus Zoning will only be permitted where it is demonstrated that the resulting intensity and form of the proposed development represents good planning within its context.

1654_ Greater height or density offered through Type 2 Bonus Zoning will be commensurate with the public value of the facility, service or matter that is provided.”

The planning merits and enhanced design elements of this proposal are described and illustrated in this Report. In light of these considerations, in our opinion bonusing for additional building height on the Site would be (1) appropriate for the local development context and (2) commensurate with the public benefit derived from project enhancements (notwithstanding the impending removal of bonusing permissions from the Planning Act). It is therefore our opinion that the proposal fulfills the requirements of Policies 1653 and 1654, as well as the broader bonus zone policy direction set out in The London Plan.

4.3.6 Specific Area Policies

Policy 1730 of the Plan states that, in limited circumstances, adoption of policies for Specific Areas may be considered. This Policy further prescribes that Specific Area policies may be considered where five specific conditions are satisfied. The following evaluates the proposal relative to each condition:

“1. The proposal meets all other policies of the Plan beyond those that the specific policy identifies.”

Based upon our review of The London Plan policies and permissions, in our opinion, the redevelopment proposal satisfies all policies of the Plan other than the maximum building height policy.

“2. The proposed policy does not have an adverse impact on the integrity of the place type policies or other relevant parts of this Plan.”

The OPA application seeks to exceed the 35 storey maximum building height permission of the Downtown place type (where Type 2 bonusing is applied). In our opinion, the merits of this proposal set out in this Report warrant the requested 40 storey building height (e.g., unique building arrangement, prominent tower design, gateway location, high design standard, opportunity for land dedication).

Given these considerations and related commentary in this Report, in our opinion this proposal would not adversely impact upon the integrity of the building form and intensity policies established for the Downtown place type or applicable City Design policies.

“3. The proposed use is sufficiently unique and distinctive such that it does not establish an argument for a similar exception on other properties in the area.”

As discussed in this Report, the proposed redevelopment plan would integrate a prominent high-rise tower with existing heritage buildings at a strategic gateway/riverfront property. In our opinion, this proposal represents a distinct, site-specific development opportunity that would not establish a precedent for other properties in the downtown core.

“4. The proposed use cannot be reasonably altered to conform to the policies of the place type.”

The proposed tower has been designed and positioned to integrate effectively with existing heritage buildings and to avoid encroachment into the adjacent floodplain. The requested building height permits the proposed unit yield to be contained within a slender point tower, while also accommodating four storeys of office/commercial space in a tower base. In our opinion, this base and tower configuration contributes positively to the existing, pedestrian oriented streetscape.

In order to bring the proposal into conformity with The London Plan Type 2 bonusing permissions, at a minimum, the tower height would need to be reduced by five storeys. To accommodate the intended apartment yield, the floorplate would need to be widened in a manner that could potentially (1) compromise the tower’s relationship with the adjacent heritage buildings and streetscape and/or (2) require encroachment into the adjacent floodplain. In our opinion, these modifications would detract from the overall merits of the proposal and may not be feasible given the development constraints of this Site. Accordingly, it is our opinion that the tower design cannot be reasonably altered to align with the maximum building height permission of the Downtown place type.

“5. The proposed policy is in the public interest, and represents good planning.”

Given planning rationale set out in this Report, with the benefit of the proposed Specific Area policy, in our opinion the applications are consistent with the policy direction and objectives of the Provincial Policy Statement, the City’s 1989 Official Plan and The London Plan. Accordingly, it is our opinion that for this distinctive project, a Specific Area policy represents good planning and would appropriately implement the project.

Additionally, Policy 1732 of The London Plan states that all applications contemplating policies for Specific Areas are subject to the Planning and Development Applications component of the Plan. Specifically, Policy 1578 of the Plan sets out a variety of criteria to be considered when evaluating the merits of planning applications.

In our opinion, relevant matters set out in Policy 1578 have been contemplated in the building design and site layout, and related policies have been addressed in this Report. Most notably, Sub-Policy 6 identifies a number of potential impacts to nearby properties to be evaluated in the context of a development application. In our opinion, collectively, the design elements of this proposal effectively address several key considerations identified in this Sub-Policy, including traffic and access management, parking, shadowing, visual impacts, impacts on cultural heritage resources and impact on natural heritage areas. Additionally, with respect to Sub-Policy 7 which addresses 'fit', in our opinion the proposed building form and site layout demonstrate that the project is designed to be compatible with, and sympathetic to, the surrounding development context.

Given the foregoing, in our opinion, the proposal satisfies the framework of planning criteria prescribed in The London Plan for a Specific Area policy.

4.3.7 Conclusion

In light of these considerations, it is our opinion that the proposed development supports the broad vision and role established for the Downtown place type in The London Plan. Further, with a Specific Area policy allowing for a maximum building height of 40 storeys, the proposal would align with the development permissions prescribed for this place type (recognizing that applicable development intensity and building form policies are under appeal and are not currently in effect).

5.0

ZONING BY-LAW ANALYSIS

5.1 Existing Zoning

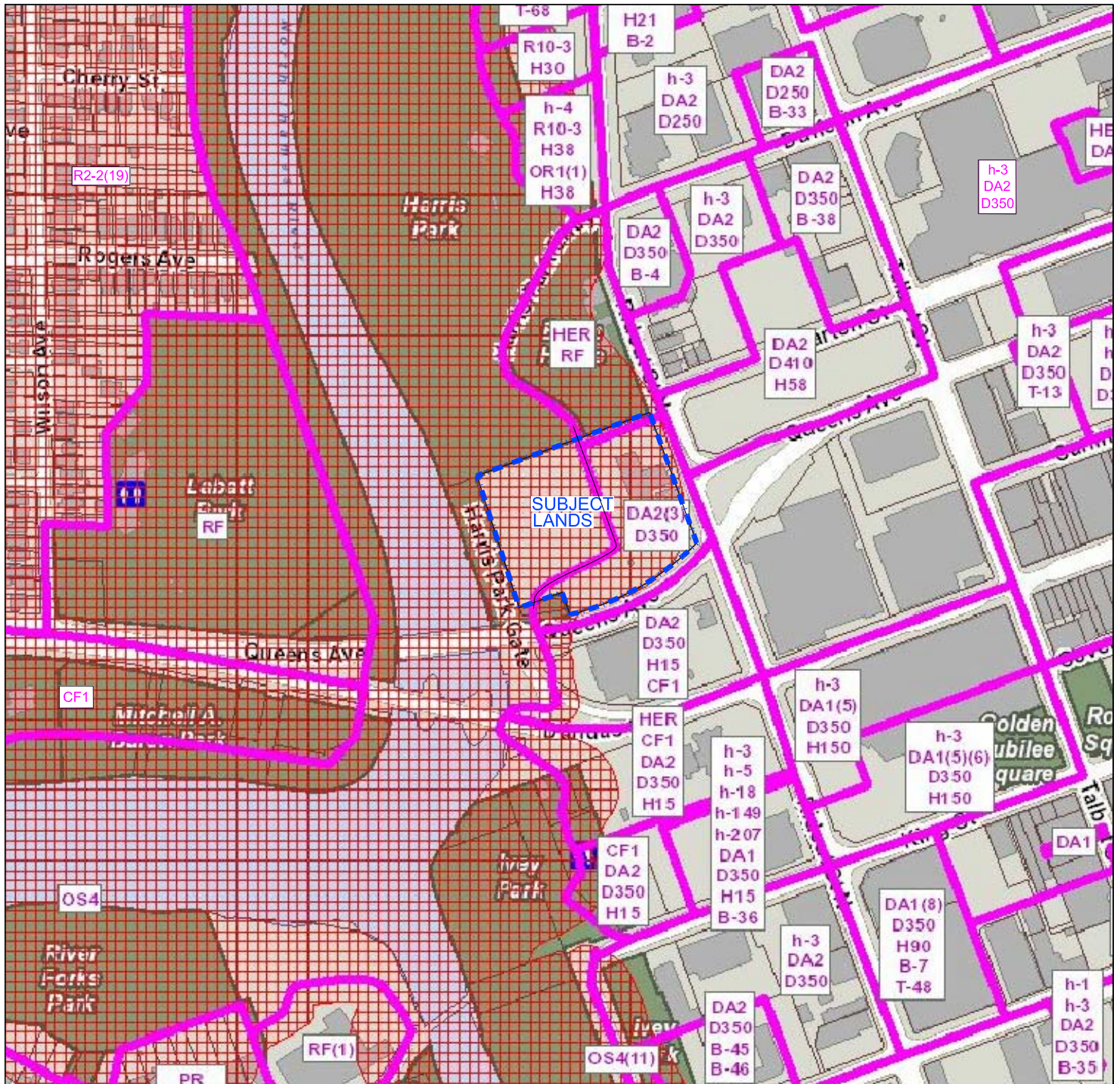
Figure 12 illustrates that two separate zones are applied to the Site pursuant to Zoning By-law No. Z-1. The developed portion of the property fronting the Queens Avenue and Ridout Street North corridors is zoned 'Downtown Area (DA2(3); D350)'. The balance of the property is zoned 'Open Space (OS4)', which is consistent with the zoning applied to the Thames River corridor and Harris Park.

Section 20.1 of the Zoning By-law states that the Downtown Area (DA) Zone, "... provides for and regulates the City's most dominant and intensive commercial business area which serve the City and region". It is also stated in this Section that the DA2 Zone variation is applied to peripheral areas of the Downtown located outside of the main retail shopping area centred along Dundas and Richmond Streets.

Generally, the DA2 Zone permits a wide variety of land uses including retail stores, entertainment facilities, institutional uses, public facilities, offices and apartment buildings. In accordance with Section 20.4 b) of the Zoning By-law, the DA2(3) Zone Variation applied to the Site limits development to existing uses. Specifically, permitted uses are only those in existing buildings and the permitted height is as existing on the date of the passing of the Zoning By-law. The 'D350' provision applied to the zoning establishes a maximum residential density of 350 units/ha.

Section 36.1 of the Zoning By-law states that the OS4 Zone is applied to lands having physical and/or environmental constraints to development. Additionally, it is stated in this Section that:

"The OS4 Zone variation is intended to be applied to hazard lands; specifically the floodway, steep slopes and lands that may be subject to erosion as well as landfills and contaminated sites. Development within the OS4 Zone is regulated pursuant to the Conservation Authorities Act. The variation is intended to provide for development of low impact recreational facilities that do not normally include structures or buildings, and require locations within or adjacent to the floodplain."



- ZONING LEGEND**
- ZONE LIMIT
 - R2 RESIDENTIAL R2 ZONE
 - DA DOWNTOWN AREA ZONE
 - OR OFFICE RESIDENTIAL ZONE
 - RF REGIONAL FACILITY ZONE
 - HER HERITAGE ZONE
 - CF COMMUNITY FACILITY ZONE
 - T TEMPORARY ZONE
 - OS OPEN SPACE ZONE
 - h HOLDING PROVISION
 - H HEIGHT PROVISION

- PR PRIVATE ROAD PROVISION
- D DENSITY PROVISION
- B BONUSING PROVISION
- CONSERVATION AUTHORITY REGULATED AREAS

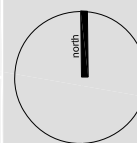
**Figure 12
Existing Zoning**

SUBJECT LANDS

MHBC PLANNING URBAN DESIGN & LANDSCAPE ARCHITECTURE
 630 COLBORNE STREET SUITE #202 LONDON, ON, N6B 2V2
 P: 519 858 2797 F: 519 858 2920 | WWW.MHBCPLAN.COM

DATE: July 15, 2019

SCALE 1 : 4,000



P:\18159\A\Graphics\ Figure 12 -Existing Zoning - July 15 2019

In light of these considerations, the existing DA2(3) and OS4 Zone structure does not permit the intended building form or site layout. Accordingly, a Zoning By-law Amendment is required to facilitate the proposed development.

5.2 Proposed Zoning

5.2.1 Zoning Review

Following an analysis of the Zoning By-law, it is our opinion that the form and scale of the proposed development most closely aligns with the planned function of the Downtown Zone regime and the standard permissions of the DA2 Zone variation.

Table 20.3 of the Zoning By-law prescribes the site and building regulations for the DA2 Zone. The following table compares the concept plan layout with several of applicable development regulations. Additionally, a Zoning Data Sheet providing a more detailed zoning analysis of the proposal been prepared by MHBC in support of the OPA/ZBA application.

Table 4.0 - Selected Zoning Regulations (DA2 Zone)

Regulation	Required	Provided
Lot Area (min.)	N/A	14,008 m ²
Lot Frontage (min.)	3.0 m	97.1 m
Building Setbacks (min.)		
Front Yard	0.0 m	3.1 m
Interior Side Yard	0.0 m	4.93 m
Exterior Side Yard	0.0 m	4.52 m
Rear Yard	0.0 m	3.0 m (DA2 Zone limit)
Building Height (max.)	90 m*	123.9 m
Landscaped Open Space (min.)	5%	34.7% (DA2 Zone limit)
Lot Coverage (max.)	97%	35.0% (DA2 Zone limit)
Residential Density (max.)	350 units/ha	500 units/ha
Off-Street Parking (min.)	71 spaces	371 spaces
Bicycle Parking (min.)	7	> 7 spaces

* Pursuant to the DA2(3) special provision, maximum building height for Site is the existing condition.

As illustrated, the proposed building scale and site layout meets most standard DA2 Zone regulations; however a site-specific DA2 Zone variation is proposed to address the following matters:

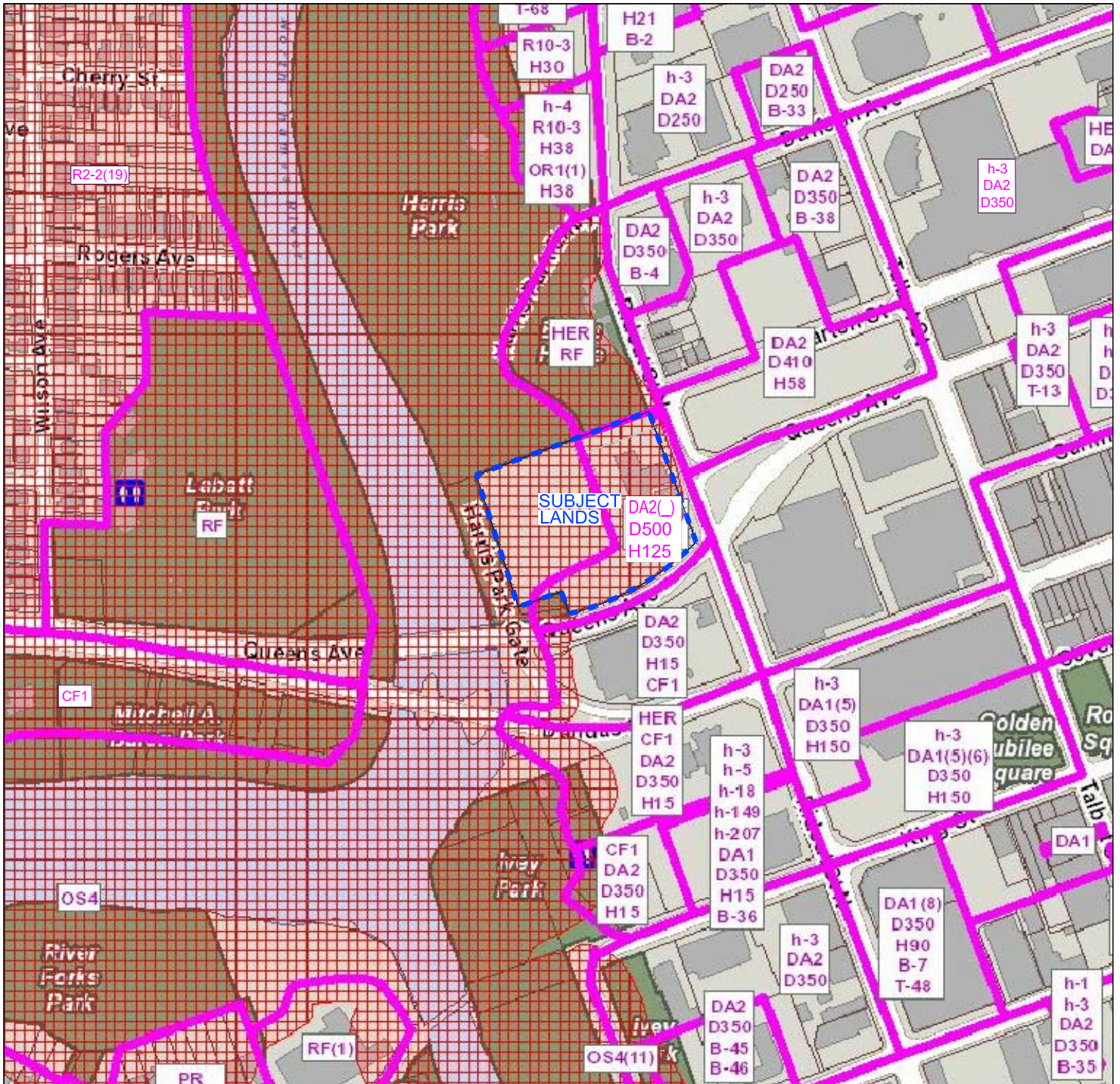
1. Apply a site-specific maximum height ('H') provision of 125 m to reflect the planned 40 storey high-rise apartment; and
2. Allow for a maximum residential density of 500 units/ha to permit the Site to be redeveloped for an intensive mixed-use form that will support the efficient use of existing infrastructure and public services.

Considering the building density permissions prescribed for the Downtown designation and the impending removal of bonusing permissions, in our opinion a site-specific DA2 Zone variation is the most appropriate zoning mechanism to implement the requested Specific Area policies and to accommodate the proposed development.

5.2.2 Site-Specific Zoning Provision

Figure 13 illustrates the **Downtown Area Special Provision (DA2(□)*D500*H125)) Zone** proposed to facilitate the various components of the proposed mixed-use, high-rise tower. Collectively, in our opinion, Sections 3.3, 4.2.10 and 4.3.5 of this Report identify urban design enhancements that support permissions for increased height and density for this project.

Alternatively, a 'Bonus (B) Zone' may be the most appropriate mechanism to implement the Specific Area policies proposed for the 1989 Official Plan and The London Plan. Further discussion with City staff may be required to confirm if a Bonus Zone should be applied to facilitate this project.

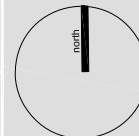


- ZONING LEGEND**
- ZONE LIMIT
 - R2 RESIDENTIAL R2 ZONE
 - DA DOWNTOWN AREA ZONE
 - OR OFFICE RESIDENTIAL ZONE
 - RF REGIONAL FACILITY ZONE
 - HER HERITAGE ZONE
 - CF COMMUNITY FACILITY ZONE
 - T TEMPORARY ZONE
 - OS OPEN SPACE ZONE
 - h HOLDING PROVISION
 - H HEIGHT PROVISION

- PR PRIVATE ROAD PROVISION
- D DENSITY PROVISION
- B BONUSING PROVISION
- CONSERVATION AUTHORITY REGULATED AREAS

Figure 13
Proposed Zoning

SUBJECT LANDS



6.0

CONCLUSIONS

In conclusion, the requested amendments to the City's 1989 Official Plan, new Official Plan and Zoning By-law are appropriate for the Site, compatible with the existing development context and in keeping with the framework of planning policy. This assessment is based, in part, upon consideration of the following merits:

1. The subject lands are situated within the Central London planning area, form part of the City's downtown core, and are currently designated for both high density residential development and office/commercial uses;
2. The property is well suited for residential intensification given its physical size, its location within Downtown London and its proximity to prominent offices, retail/service commercial enterprises, government activities, recreational, entertainment and cultural facilities;
3. The Site is located within convenient walking distance of bus routes operating along the Talbot Street, Dundas Street, Richmond Street and Ridout Street North corridors (providing connectivity throughout the City and to regional transportation facilities). The City's proposed BRT system also includes routing along Queens Avenue in the vicinity of the subject lands;
4. The massing, orientation and articulation of the proposed mixed-use, high-rise tower are designed: to be compatible with surrounding land uses; sympathetic to existing heritage buildings, complementary to the local development context and supportive of the pedestrian environment along Ridout Street and Queens Avenue. Accordingly, in our opinion, this redevelopment project should not generate significant land use conflicts with adjacent properties;
5. The residential component of this project introduces greater housing choice within the downtown core to help broaden the range and mix of housing available to current and future residents of Central London, and to help the City accommodate forecasted housing demands and achieve residential intensification targets. In this regard, the proposal would also help to promote neighbourhood stability by diversifying the mix of housing available

in the downtown core to better meet the changing needs of local residents over the long-term;

6. Collectively, the mix of higher density residential and office/commercial uses associated with this project would support efforts to revitalize the downtown core and encourage its continued development as a multi-functional regional centre;
7. This residential development proposal is consistent with the Provincial Policy Statement;
8. This intensification project: is in keeping with the applicable goals, objectives, policies and guidelines of the 1989 Official Plan, The London Plan, Our Move Forward and the Downtown Heritage Conservation District Plan;
9. The proposed site-specific zoning structure would appropriately implement the intended building design and site layout; and
10. The development represents good planning.

Given the noted considerations, it is recommended that the City of London approve the Official Plan Amendment and Zoning By-law Amendment applications, as proposed.

Respectfully submitted,

MHBC



Scott Allen, MA, RPP
Partner



Eric Miles, MPL
Planner

APPENDIX A

ARCHITECTURAL RENDERINGS

FARHI HOLDINGS - 451 RIDOUT STREET



EAST FACADE ARIAL VIEW

FARHI HOLDINGS - 451 RIDOUT STREET



NORTHEAST CORNER ARIAL VIEW

FARHI HOLDINGS - 451 RIDOUT STREET



WEST FACADE ARIAL VIEW (PARK)

FARHI HOLDINGS - 451 RIDOUT STREET



QUEENS AVENUE VIEW (EAST FACADE)

FARHI HOLDINGS - 451 RIDOUT STREET

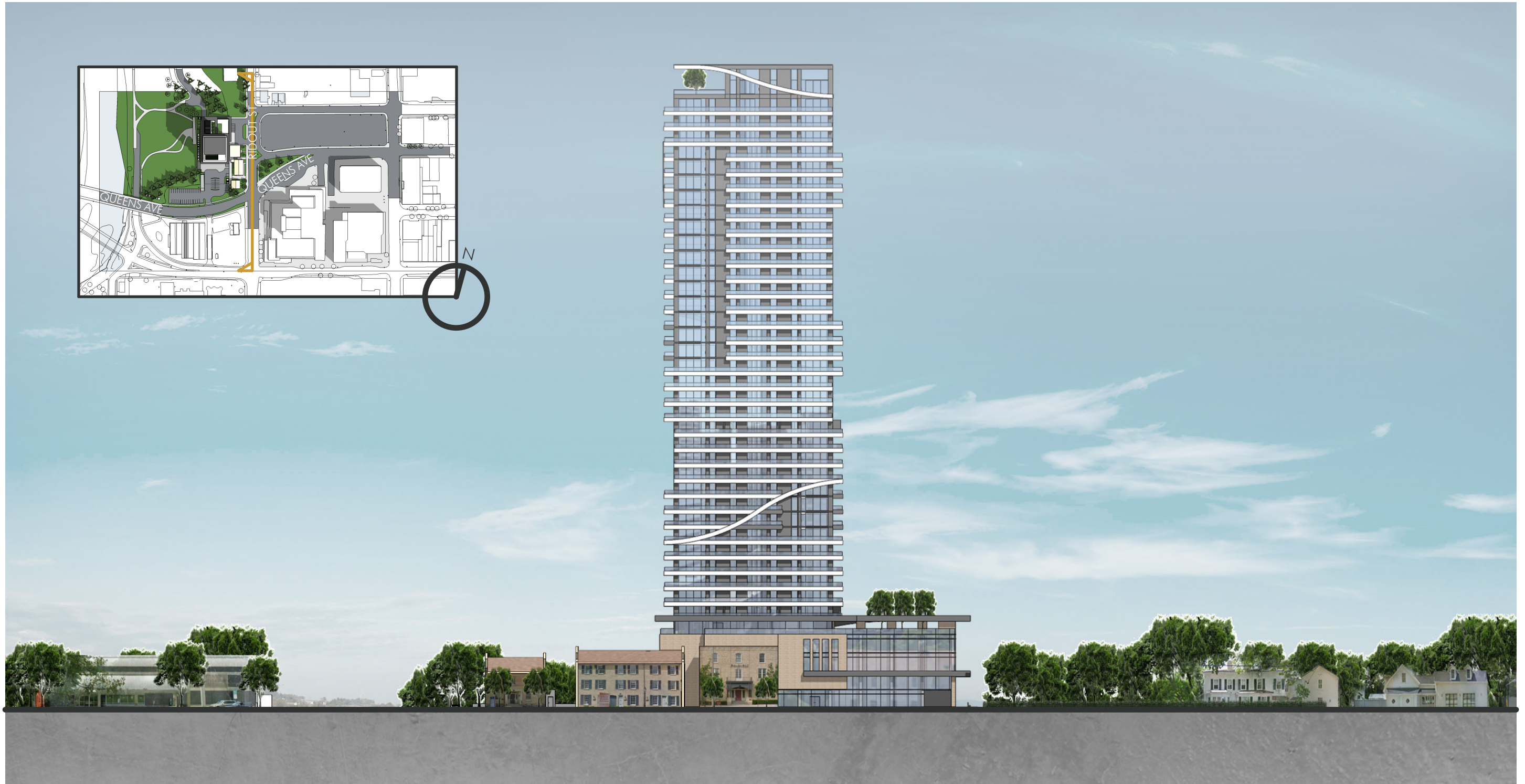


QUEENS AVE VIEW (BRIDGE CROSSING)

APPENDIX B

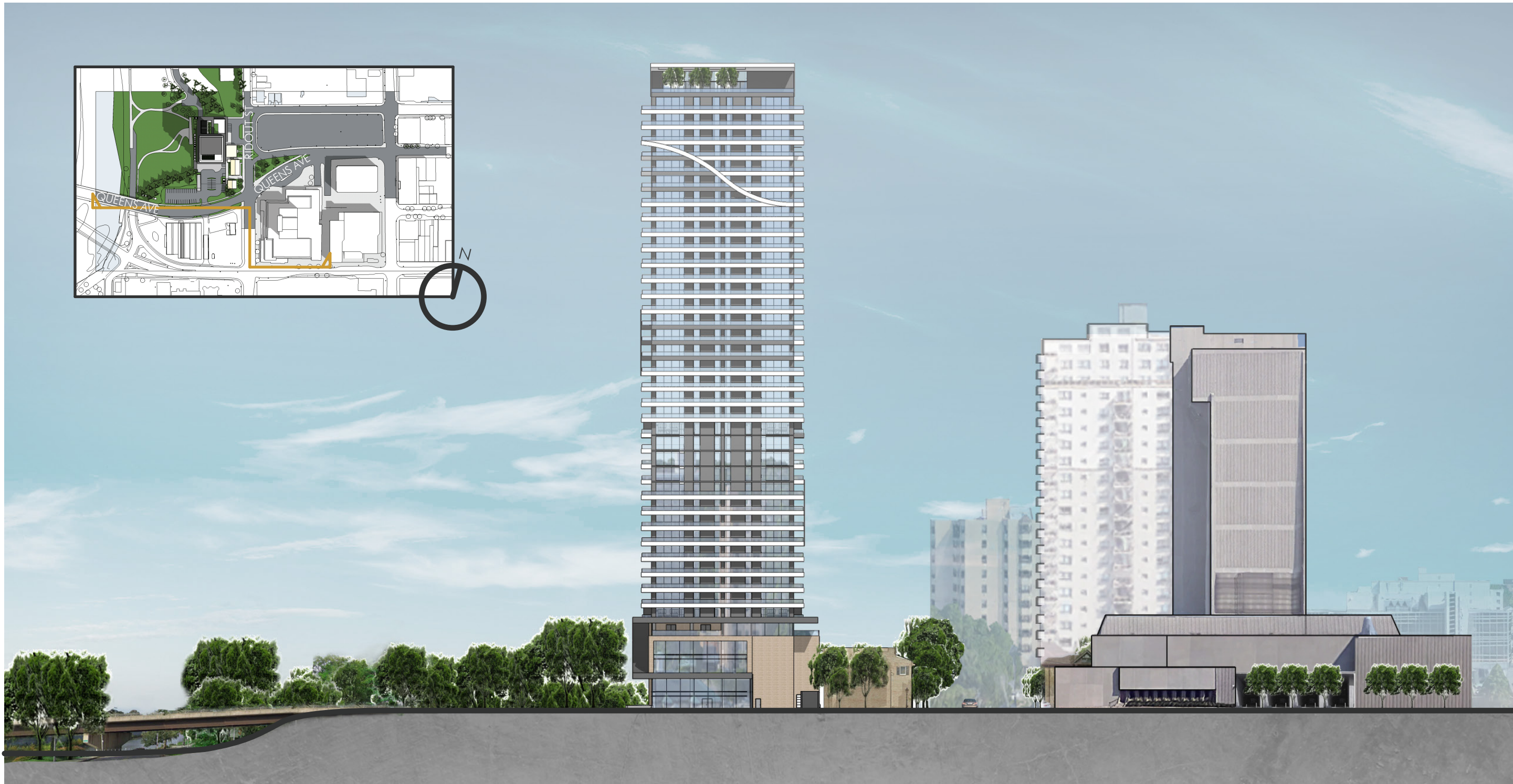
CONCEPTUAL BUILDING ELEVATIONS/SECTION DRAWINGS

FARHI HOLDINGS - 451 RIDOUT STREET



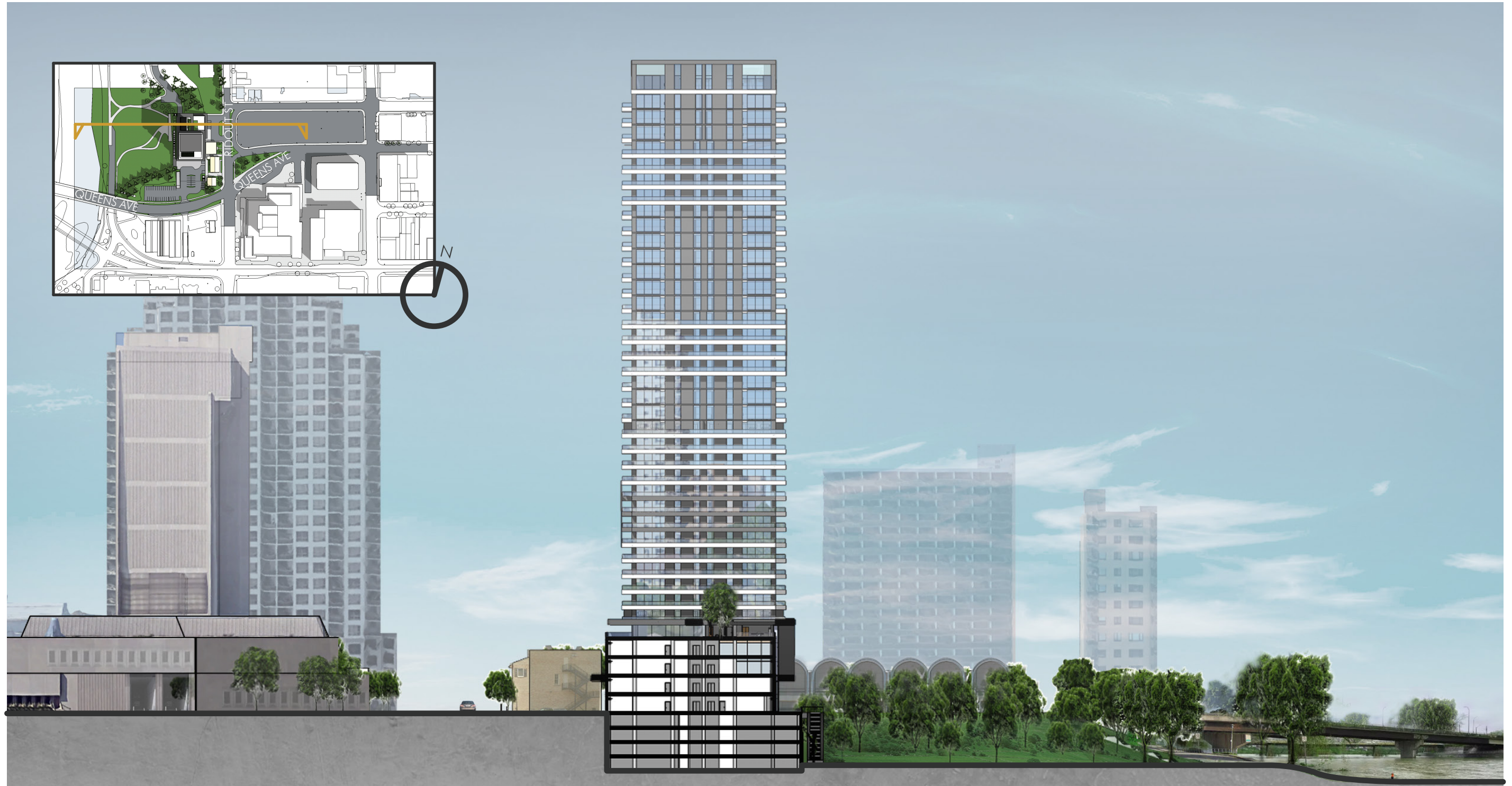
RIDOUT STREETSCAPE (EAST ELEVATION)

FARHI HOLDINGS - 451 RIDOUT STREET



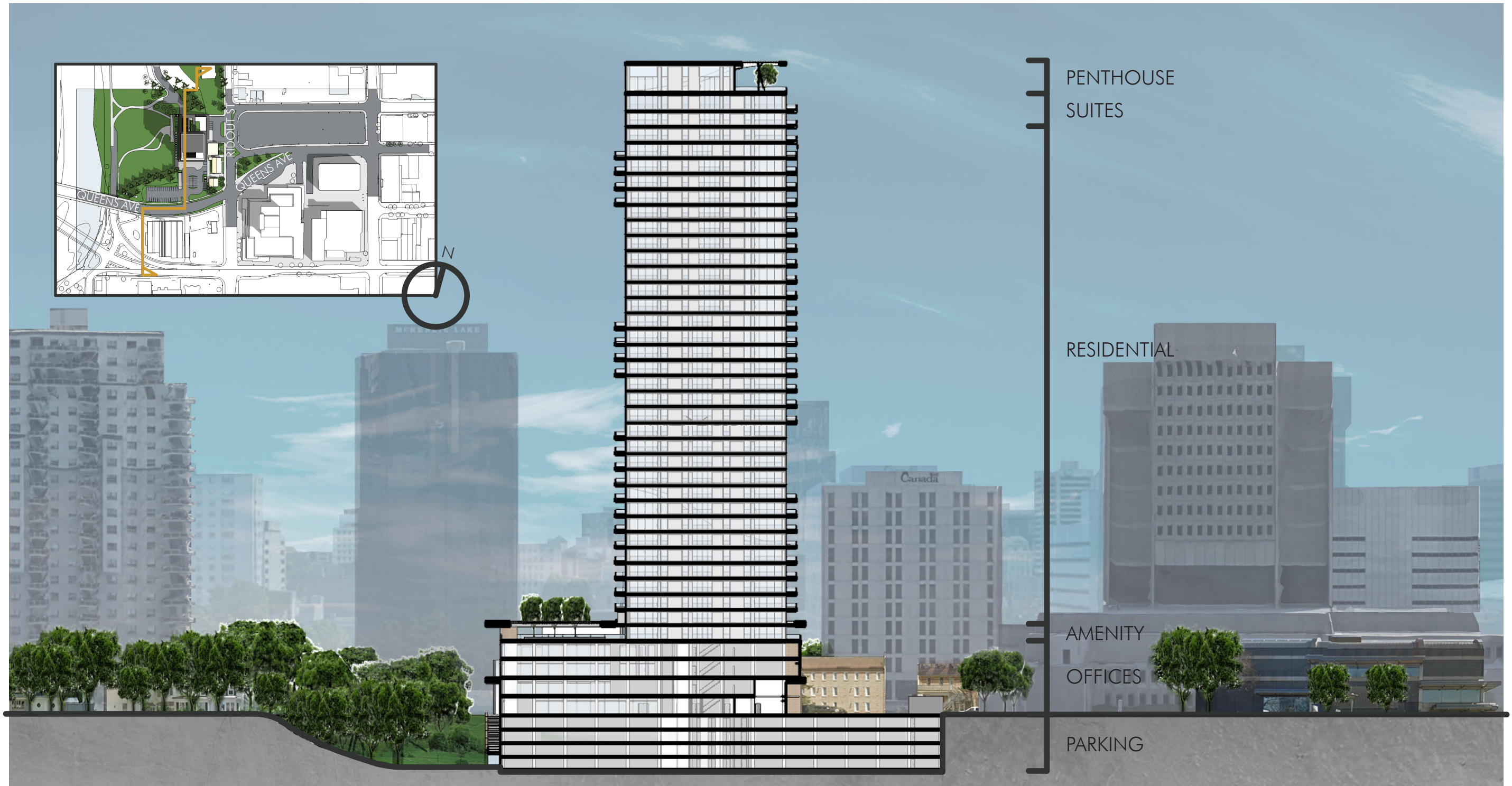
QUEENS AVE. STREETScape (SOUTH ELEVATION)

FARHI HOLDINGS - 451 RIDOUT STREET



EAST TO WEST CONTEXTUAL SECTION

FARHI HOLDINGS - 451 RIDOUT STREET



NORTH TO SOUTH CONTEXTUAL SECTION

FARHI HOLDINGS - 451 RIDOUT STREET



SOUTH ELEVATION (PODIUM)

FARHI HOLDINGS - 451 RIDOUT STREET



WEST ELEVATION (PODIUM)

FARHI HOLDINGS - 451 RIDOUT STREET



NORTH ELEVATION (PODIUM)

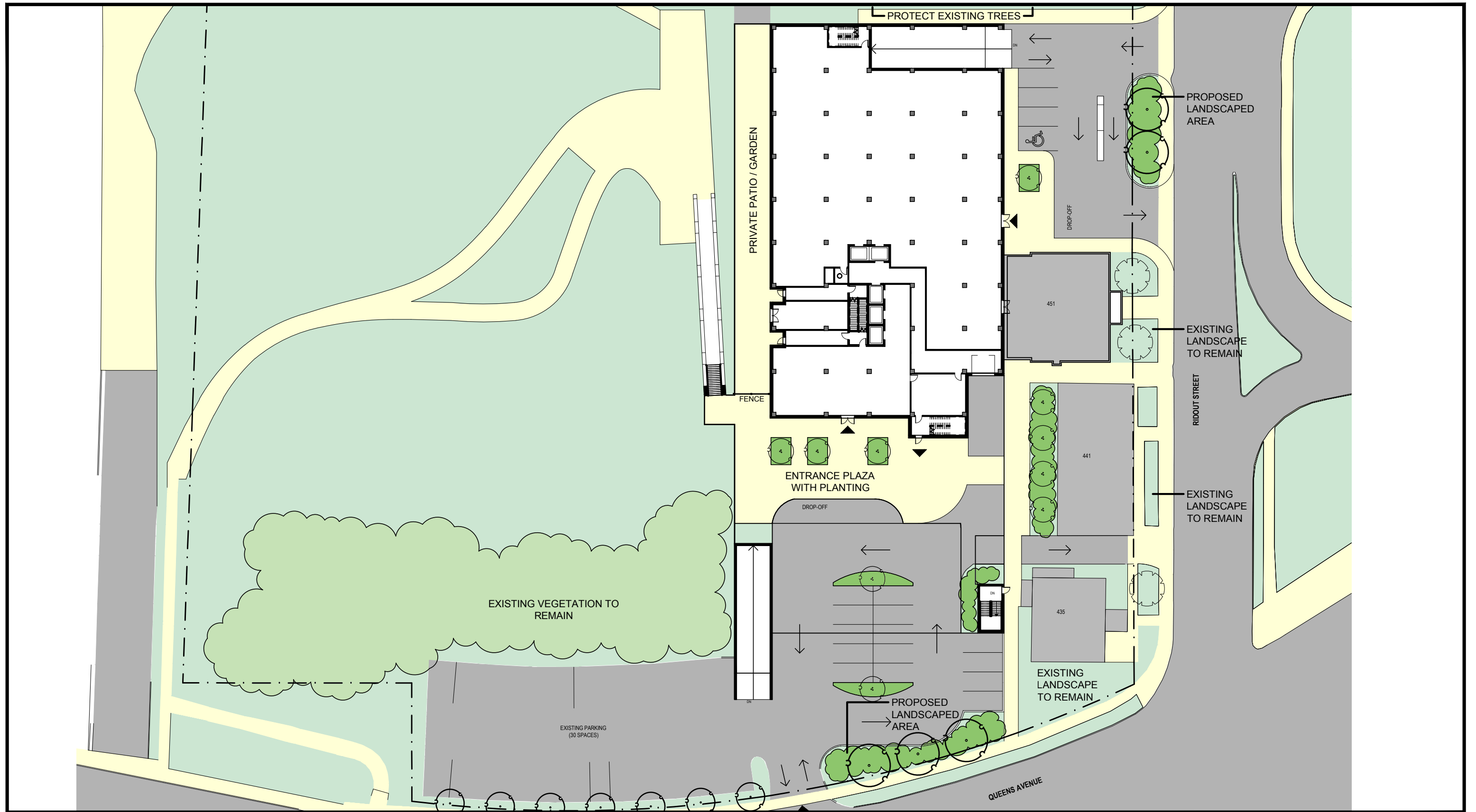
FARHI HOLDINGS - 451 RIDOUT STREET



EAST ELEVATION (PODIUM)

APPENDIX C

CONCEPTUAL LANDSCAPE PLAN

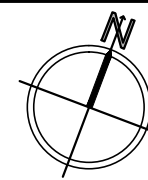


TITLE:

FARHI HOLDINGS

435 RIDOUT STREET, LONDON, ON

214



PROJECT NUMBER:
19-203 (Lb)

SCALE:
1:500

DRAWN BY: RKLA Inc.

DATE: JULY 22, 2019

DRAWING NUMBER:

CONCEPT 1

APPENDIX D

SUN STUDY

FARHI HOLDINGS - 451 RIDOUT STREET



SUN STUDY (MARCH 21ST)

FARHI HOLDINGS - 451 RIDOUT STREET



SUN STUDY (JUNE 21ST)

FARHI HOLDINGS - 451 RIDOUT STREET



SUN STUDY (SEPTEMBER 2ND)

FARHI HOLDINGS - 451 RIDOUT STREET



12PM



5PM

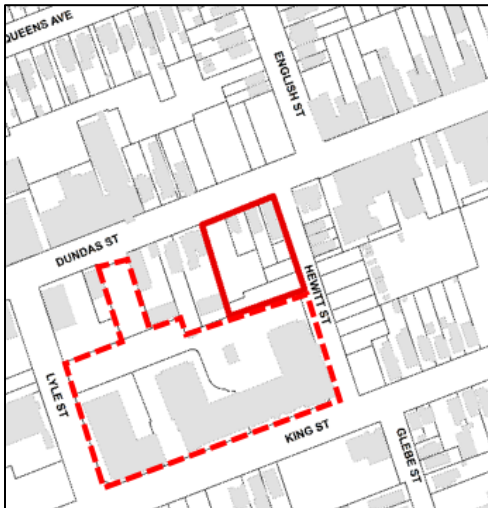
SUN STUDY (DECEMBER 21ST)

Contact
Adria Grant
Manager, Cultural Resources
T +519-963-5861
E adria.grant@aecom.com

NOTICE OF PLANNING APPLICATION

Zoning By-Law Amendment

725-735 Dundas St, 389-393 Hewitt St, a portion of 700 King St, and other properties



File: Z-9155

Applicant: East Village Holdings Ltd.

What is Proposed?

Zoning amendment to allow:

- A 24-storey (78.2m), mixed use commercial/residential apartment building containing 278 residential units;
- 5 commercial units with a total of 870m² of commercial floor area;
- 259 parking spaces;
- A maximum density of 750 units per hectare for all properties; and
- A comprehensive zone for the subject site and broader land holding.

LEARN MORE & PROVIDE INPUT

Please provide any comments by **January 16, 2020**

Sonia Wise

swise@london.ca

519-661-CITY (2489) ext. 5887

Development Services, City of London, 300 Dufferin Avenue, 6th Floor,
London ON PO BOX 5035 N6A 4L9

File: Z-9155

london.ca/planapps

You may also discuss any concerns you have with your Ward Councillor:

Jesse Helmer

jhelmer@london.ca

519-661-CITY (2489) ext. 4004

**If you are a landlord, please post a copy of this notice where your tenants can see it.
We want to make sure they have a chance to take part.**

Application Details

Commonly Used Planning Terms are available at london.ca/planapps.

Requested Zoning By-law Amendment

To change the zoning from a Business District Commercial Special Provision Bonus (BDC(24)*D160*H36*B-32) Zone and a Business District Commercial Special Provision (BDC(19)*D250*H46) Zone to a Business District Commercial Special Provision (BDC(24)*D160*H36*B-()) Zone. Changes to the currently permitted land uses and development regulations are summarized below. The complete Zoning By-law is available at london.ca/planapps.

Current Zoning

Zone: Business District Commercial Special Provision Bonus (BDC(24)*D160*H36*B-32) Zone and a Business District Commercial Special Provision (BDC(19)*D250*H46) Zone

Permitted Uses: animal hospitals, apartment buildings with any or all other permitted uses on the first floor, bake shops, clinics, commercial recreation establishments, commercial parking structures and/or lots, converted dwellings, day care centres, dry cleaning and laundry depots, duplicating shops, emergency care establishments, existing dwellings, financial institutions, grocery stores, laboratories, laundromats, libraries, medical/dental offices, offices, personal service establishments, private clubs, restaurants, retail stores, service and repair establishments, studios, video rental establishments, lodging house class 2, cinemas, brewing on premises establishment, food store, animal clinic, convenience store, post office, convenience service establishment, dwelling units restricted to the rear portion of the ground floor or on the second floor or above with any or all of the other permitted uses in the front portion of the ground floor, bed and breakfast establishments, antique store, police stations, artisan workshop, craft brewery, hotels, taverns, assembly halls, places of worship, community centres, funeral homes, institutions, schools and fire halls.

Special Provisions: BDC(19): Permitted Uses: Any use permitted in the BDC1 and BDC2 Zone variations; dwelling units, emergency care establishments, lodging house class 2 units and accessory dwelling units may only be permitted in the rear portion of the ground floor or on the second floor or above, with any or all of the other permitted uses in the front portion of the ground floor; theatres. Prohibited Uses: commercial parking lots and structures and accessory parking lots fronting on Dundas Street, and within 15m of Dundas Street; uses with drive-through facilities. Regulations: front and exterior side yard setback – 0m; front yard depth 1.2m per 3m of main building height or fraction thereof above 12m, parking – Notwithstanding the requirements in Section 4.19, within Parking Standard Area 1 no parking shall be required for residential development.

BDC(24): Permitted Uses: Any use permitted in the BDC1 and BDC2 Zone variations; dwelling units, emergency care establishments, lodging house class 2 units and accessory dwelling units may only be permitted in the rear portion of the ground floor or on the second floor or above, with any or all of the other permitted uses in the front portion of the ground floor; theatres. Prohibited Uses: commercial parking lots and structures and accessory parking lots fronting on Dundas Street, and within 15m of Dundas Street; uses with drive-through facilities. Regulations: front and exterior side yard setback – 0m; front yard depth 1.2m per 3m of main building height or fraction thereof above 12m.

Residential Density: 160 - 250 units per hectare

Height: 36m - 46m

Bonus Zone: The B-32 bonus zone allows for a maximum height of 80m and a maximum density of 600 units per hectare, in return for enhanced urban design underground parking, the promotion of transit usage and the revitalization of the Old East District.

Requested Zoning

Zone: Business District Commercial Special Provision (BDC(24)*D160*H36*B-()) Zone

Permitted Uses: the full range of existing permitted uses are requested

Special Provisions: the BDC(24) described above is requested for the entirety of the property

Residential Density: A base density of 160 units per hectare

Height: A base height of 36m

Bonus Zone: A bonus zone is requested to permit the 24 storey height at 78.2m (80m) and a total density of 750 units per hectare for all properties within the proposed zone, in return for eligible facilities, service and matters outlines in Section 19.4.4 of the 1989 Official Plan, policies 1638-1655 of The London Plan and section 3.4 of the Old East Village Dundas Street Corridor Secondary Plan.

Planning Policies

Any change to the Zoning By-law must conform to the policies of the Official Plan, London's long-range planning document. These lands are currently designated as Main Street Commercial Corridor in the 1989 Official Plan, which are long-established, mixed-use, pedestrian-oriented business districts. The main permitted uses include: small-scale retail uses, service and repair establishments, food stores, convenience commercial uses, personal and business services, pharmacies, restaurants, financial institutions, small-scale offices, small-scale entertainment uses, galleries, studios, community facilities, and residential uses.

The subject lands are in the Rapid Transit Corridor Place Type and in the Old East Village Main Street segment in *The London Plan*, permitting a wide range of uses at a walkable and neighbourhood scale. The lands are also within the Old East Village Dundas Street Secondary Plan within the Old East Village Core and King Street Character Areas which encourages mixed use buildings as the preferred form of development and contemplates high rise residential uses through the use of bonusing up to 24 storeys.

How Can You Participate in the Planning Process?

You have received this Notice because someone has applied to change the zoning of land located within 120 metres of a property you own, or your landlord has posted the notice of application in your building. The City reviews and makes decisions on such planning applications in accordance with the requirements of the *Planning Act*. The ways you can participate in the City's planning review and decision making process are summarized below. For more detailed information about the public process, go to the [Participating in the Planning Process](#) page at [london.ca](#).

See More Information

You can review additional information and material about this application by:

- visiting Development Services at 300 Dufferin Ave, 6th floor, Monday to Friday between 8:30am and 4:30pm;
- contacting the City's Planner listed on the first page of this Notice; or
- viewing the application-specific page at [london.ca/planapps](#).

Reply to this Notice of Application

We are inviting your comments on the requested changes at this time so that we can consider them as we review the application and prepare a report that will include Development Services staff's recommendation to the City's Planning and Environment Committee. Planning considerations usually include such matters as land use, development intensity, and form of development.

Attend a Community Information Meeting

A community information meeting will be held in your neighbourhood to present this proposal and obtain input from interested members of the public. The meeting has not yet been scheduled, but will be in advance of the Future Public Meeting described below. You will receive a separate notice inviting you to this meeting. The Community Information Meeting is not the public meeting required by the Planning Act and attendance at this meeting does not create a right to appeal the decision of Council to the Local Planning Appeal Tribunal.

Attend a Future Public Participation Meeting

The Planning and Environment Committee will consider the requested zoning changes on a date that has not yet been scheduled. The City will send you another notice inviting you to attend this meeting, which is required by the *Planning Act*. You will also be invited to provide your comments at this public participation meeting. The Planning and Environment Committee will make a recommendation to Council, which will make its decision at a future Council meeting.

What Are Your Legal Rights?

Notification of Council Decision

If you wish to be notified of the decision of the City of London on the proposed zoning by-law amendment, you must make a written request to the City Clerk, 300 Dufferin Ave., P.O. Box 5035, London, ON, N6A 4L9, or at docservices@london.ca. You will also be notified if you speak to the Planning and Environment Committee at the public meeting about this application and leave your name and address with the Secretary of the Committee.

Right to Appeal to the Local Planning Appeal Tribunal

If a person or public body would otherwise have an ability to appeal the decision of the Council of the Corporation of the City of London to the Local Planning Appeal Tribunal but the person or public body does not make oral submissions at a public meeting or make written submissions to the City of London before the by-law is passed, the person or public body is not entitled to appeal the decision.

If a person or public body does not make oral submissions at a public meeting or make written submissions to the City of London before the by-law is passed, the person or public body may not be added as a party to the hearing of an appeal before the Local Planning Appeal Tribunal unless, in the opinion of the Tribunal, there are reasonable grounds to do so.

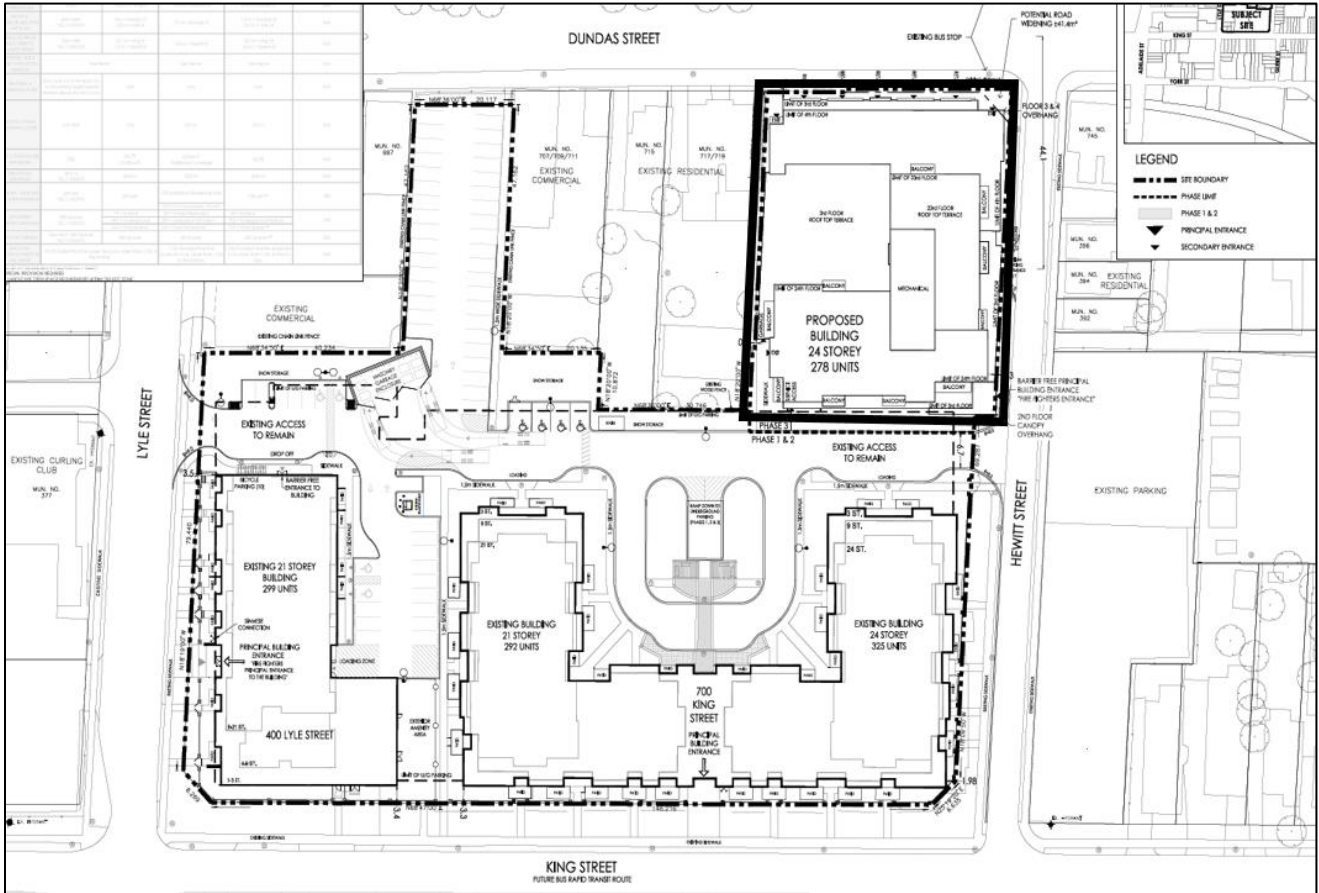
For more information go to <http://elto.gov.on.ca/tribunals/lpat/about-lpat/>.

Notice of Collection of Personal Information

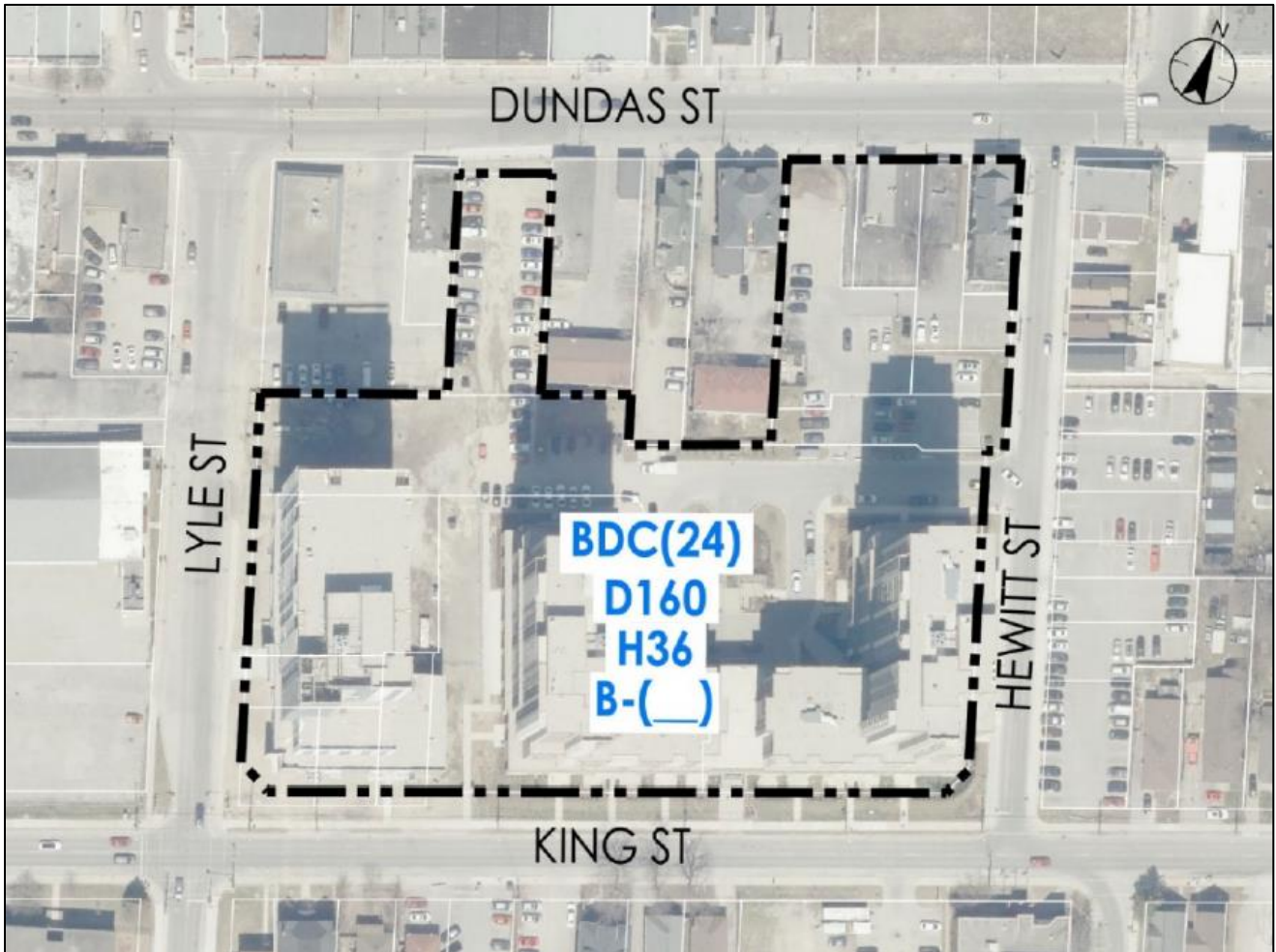
Personal information collected and recorded at the Public Participation Meeting, or through written submissions on this subject, is collected under the authority of the *Municipal Act, 2001*, as amended, and the *Planning Act, 1990 R.S.O. 1990, c.P.13* and will be used by Members of Council and City of London staff in their consideration of this matter. The written submissions, including names and contact information and the associated reports arising from the public participation process, will be made available to the public, including publishing on the City's website. Video recordings of the Public Participation Meeting may also be posted to the City of London's website. Questions about this collection should be referred to Cathy Saunders, City Clerk, 519-661-CITY(2489) ext. 4937.

Accessibility – Alternative accessible formats or communication supports are available upon request. Please contact accessibility@london.ca or 519-661-CITY(2489) extension 2425 for more information.

Site Concept



Site Concept Plan



Proposed Zoning Amendment

The above image represents the applicant's proposal as submitted and may change.

Building Renderings



Conceptual Rendering – Northwest View



Conceptual Rendering – Northeast View

The above images represent the applicant’s proposal as submitted and may change.



**HERITAGE IMPACT ASSESSMENT—
719-737 DUNDAS STREET,
LONDON, ONTARIO**

FINAL REPORT

September 20, 2019

File: 161413817

Prepared for:

Medallion Corporation
970 Lawrence Avenue West
Toronto, Ontario M6A 3B6

Prepared by:

Stantec Consulting Ltd
600-171 Queens Avenue
London, Ontario N6A 5J7

Table of Contents

EXECUTIVE SUMMARY	I
PROJECT PERSONNEL.....	III
1.0 INTRODUCTION.....	1.1
1.1 STUDY PURPOSE.....	1.1
2.0 METHODOLOGY.....	2.1
2.1 POLICY FRAMEWORK.....	2.1
2.1.1 Planning Act.....	2.1
2.1.2 The 2014 Provincial Policy Statement.....	2.1
2.1.3 City of London Official Plan.....	2.2
2.2 BACKGROUND HISTORY.....	2.2
2.3 FIELD PROGRAM.....	2.3
2.4 EVALUATION OF CULTURAL HERITAGE VALUE OR INTEREST.....	2.3
2.4.1 Ontario Regulation 9/06.....	2.3
2.4.2 Assessment of Impacts.....	2.3
3.0 SITE HISTORY	3.1
3.1 INTRODUCTION.....	3.1
3.2 PHYSIOGRAPHY.....	3.1
3.3 HISTORICAL DEVELOPMENT.....	3.1
3.3.1 Survey and Settlement.....	3.1
3.3.2 19 th Century Development.....	3.3
3.3.3 20 th Century Development.....	3.4
3.4 PROPERTY HISTORY.....	3.5
3.4.1 737 Dundas Street.....	3.5
3.4.2 735 Dundas Street.....	3.6
3.4.3 729 Dundas Street.....	3.6
3.4.4 719-721 Dundas Street.....	3.6
4.0 SITE DESCRIPTION.....	4.1
4.1 INTRODUCTION.....	4.1
4.2 LANDSCAPE SETTING.....	4.1
4.3 737 DUNDAS STREET.....	4.3
4.4 735 DUNDAS STREET.....	4.8
4.5 729 DUNDAS STREET.....	4.10
4.6 719-721 DUNDAS STREET.....	4.11
5.0 EVALUATION OF CULTURAL HERITAGE VALUE OR INTEREST.....	5.1
5.1 INTRODUCTION.....	5.1
5.2 HERITAGE EVALUATION.....	5.1
5.2.1 737 Dundas Street.....	5.1
5.2.2 735 Dundas Street.....	5.4



HERITAGE IMPACT ASSESSMENT—719-737 DUNDAS STREET, LONDON, ONTARIO

5.2.3	729 Dundas Street	5.6
5.2.4	719-721 Dundas Street	5.7
6.0	IMPACT ASSESSMENT	6.1
6.1	DESCRIPTION OF PROPOSED UNDERTAKING	6.1
6.2	ASSESSMENT OF IMPACTS	6.1
6.3	DISCUSSION OF IMPACTS	6.2
7.0	MITIGATION, IMPLEMENTATION AND MONITORING	7.1
7.1	POTENTIAL MITIGATION MEASURES	7.1
7.2	MITIGATION DISCUSSION	7.1
7.2.1	737 Dundas Street	7.1
7.2.2	719-721 Dundas Street	7.3
8.0	RECOMMENDATIONS	8.1
8.1	DEPOSIT COPIES	8.1
9.0	CLOSING.....	9.1
10.0	REFERENCES.....	10.1

LIST OF TABLES

Table 1: Evaluation of 737 Dundas Street According to O. Reg. 9/06	5.2
Table 2: Evaluation of 735 Dundas Street According to O. Reg. 9/06	5.5
Table 3: Evaluation of 729 Dundas Street According to O. Reg. 9/06	5.7
Table 4: Potential Impacts to Identified Cultural Heritage Resources	6.2
Table 5: Proposed Mitigation Measures	7.1

LIST OF FIGURES

Figure 1: Project Area	1.2
Figure 2: Study Area	1.3
Figure 3: 1922 Fire Insurance Plan	3.8
Figure 4: 1949 Fire Insurance Plan	3.9
Figure 5: 1958 Fire Insurance Plan	3.10
Figure 6: 1967 Aerial Photo.....	3.11

LIST OF APPENDICES

APPENDIX A PROPOSED DRAWINGS

APPENDIX B DESIGNATION BYLAW FOR 719-721 DUNDAS STREET

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Executive Summary

The Medallion Corporation (the Proponent) retained Stantec Consulting Ltd. (Stantec) to conduct a Heritage Impact Assessment (HIA) for the properties located at 719-737 Dundas Street, in the City of London, Ontario. The Proponent is proposing to redevelop the properties at 729-737 Dundas Street and build a 24-storey high-rise mixed-use tower.

The Study Area includes three properties at 729, 735, and 737 Dundas Street where the development is proposed and the adjacent property at 719-721 Dundas Street that is designated under Part IV of the *Ontario Heritage Act* (OHA). The three properties where the development is proposed include: 737 Dundas Street a multi-unit residential building built in 1903-1904; 735 Dundas Street a commercial building built in 1964-1966; and 729 Dundas Street a commercial building built in 1946-1948. These three properties do not have any existing heritage recognition. The designated property at 719-721 Dundas Street was residence built in 1877 as a duplex and is now home to the Unity Project. .

The purpose of this HIA is to respond to policy requirements regarding the conservation of cultural heritage resources in the land use planning process. Where a change is proposed within or adjacent to a listed or protected heritage property consideration must be given to the conservation of cultural heritage resources. The objectives of this report are as follows:

- Identify and evaluate the cultural heritage value or interest (CHVI) of properties within the Study Area
- Identify potential direct and indirect impacts to identified heritage attributes
- Identify mitigation measures where impacts to identified heritage attributes are anticipated to address conservation of cultural heritage resources, where applicable

Determination of CHVI for 737 Dundas Street, 735 Dundas Street, and 728 Dundas Street was undertaken according to the criteria outlined in *Ontario Regulation 9/06* made under the OHA. The designating bylaw for 719-721 Dundas Street was used to identify the heritage attributes of the designated property.

The HIA determined that there are direct impacts anticipated to 737 Dundas Street through its removal and potential indirect impacts to 719-721 Dundas Street through its adjacency to the development which could result in vibration effects related to construction activities. Based on the presence of cultural heritage resources which have the potential to be affected by the proposed undertaking, the following mitigation measures are recommended:

737 Dundas Street:

- Should retention of the outbuilding not be considered a feasible alternative due to site constraints associated with the emergency access road, documentation and salvage should be undertaken prior to any change in site conditions
- Documentation in the form of detailed photography should be completed under the direction of a heritage professional in good standing with the Canadian Association of Heritage Professionals



HERITAGE IMPACT ASSESSMENT—719-737 DUNDAS STREET, LONDON, ONTARIO

- Salvage activities should consist of the identification and recovery of re-useable materials by a reputable salvage company or charity
- The results of the documentation and salvage activities should be made available at local libraries for public consumption

719-721 Dundas Street:

- A pre-demolition vibration assessment should be completed to establish a baseline for vibration levels in advance of demolition activities
- Should the building be determined to be within the zone of influence, additional steps should be taken to secure the buildings from experiencing negative vibration effects (i.e. adjustment of machinery or establishment of buffer zones)

The executive summary highlights key points from the report only; for complete information and findings the reader should examine the complete report.



Project Personnel

Project Manager:	Brian Blackwell
Heritage Consultant:	Meaghan Rivard, MA, CAHP
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1.0 INTRODUCTION

1.1 STUDY PURPOSE

The Medallion Corporation (the Proponent) retained Stantec Consulting Ltd. (Stantec) to prepare a Heritage Impact Assessment (HIA) for a proposed development located at 729-737 Dundas Street in the City of London, Ontario. The Study Area is situated at the northeast intersection of Dundas Street and Hewitt Street (Figure 1 and Figure 2). The proposed development of the site includes the construction of a 23-storey residential high-rise on land presently used for commercial and residential purposes.

The Study Area includes three properties at 729, 735, and 737 Dundas Street where the development is proposed and the adjacent property at 719-721 Dundas that is designated under Part IV of the *Ontario Heritage Act* (OHA). The three properties where the development is proposed include: 737 Dundas Street a multi-unit residential building built in 1903-1904; 735 Dundas Street a commercial building built in 1964-1966; and 729 Dundas Street a commercial building built in 1946-1948. These three properties do not have any existing heritage recognition. The designated property at 719-721 Dundas Street was built as a duplex in 1877.

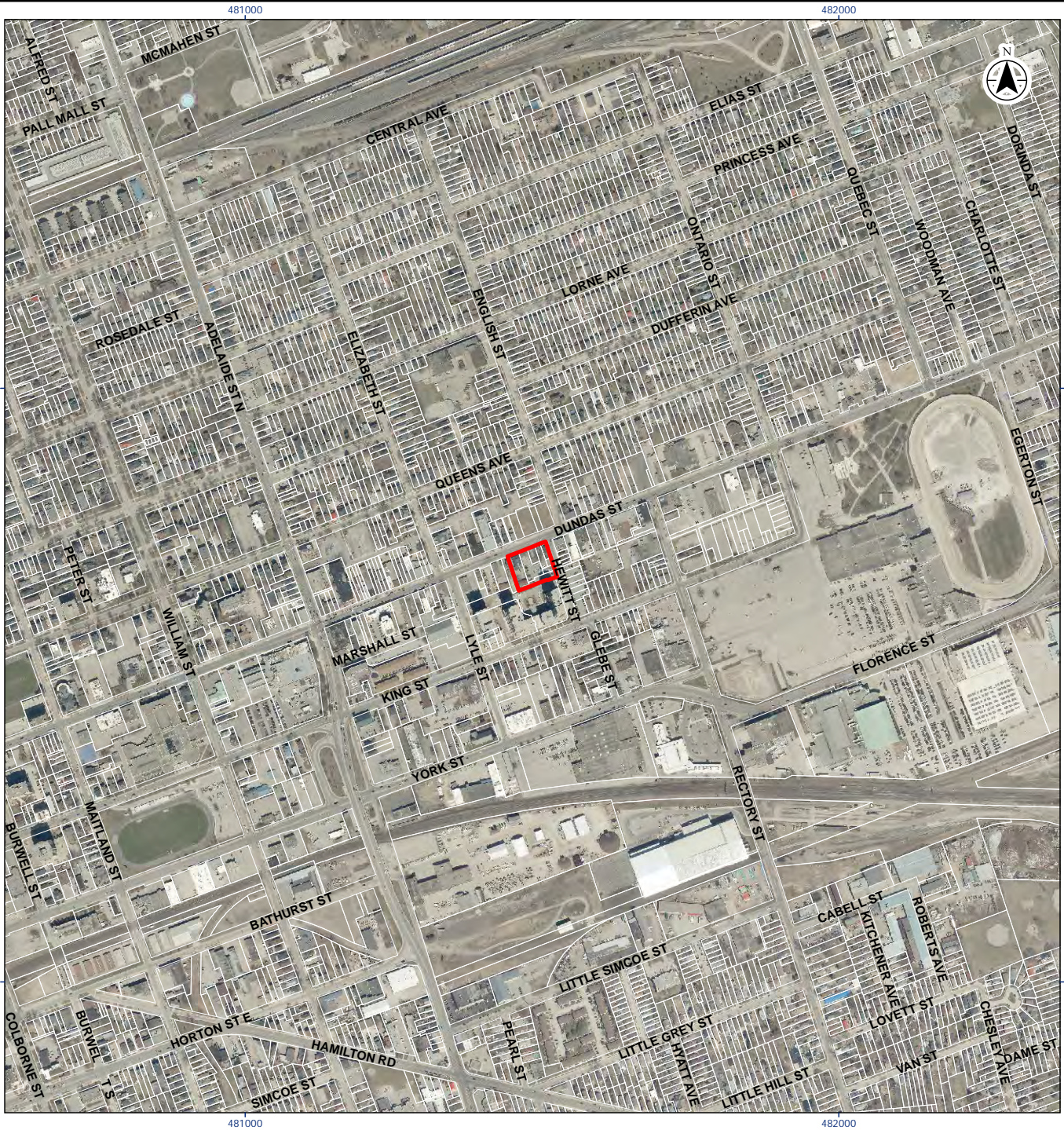
The purpose of this HIA is to respond to policy requirements regarding the conservation of cultural heritage resources in the land use planning process. Where a change is proposed within or adjacent to a protected heritage property, consideration must be given to the conservation of cultural heritage resources. The objectives of this report are as follows:

- Identify and evaluate the cultural heritage value or interest (CHVI) of properties within the Study Area
- Identify potential direct and indirect impacts to identified heritage attributes
- Identify mitigation measures where impacts to identified heritage attributes are anticipated to address conservation of cultural heritage resources, where applicable

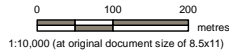
To meet these objectives, this HIA contains the following content:

- Summary of project methodology
- Review of background history of the Study Area and historical context
- Evaluation of CHVI of resources within, and adjacent to, the Study Area
- Description of the proposed site alteration
- Assessment of impacts of the proposed site alterations on cultural heritage resources
- Review of development alternatives or mitigation measures where impacts are anticipated
- Recommendations for the preferred mitigation measures





Legend
 Study Area



Project Location
 City of London

161413817 REVA
 Prepared by KDB on 2019-05-08

Client/Project
MEDALLION CORP.
 HERITAGE IMPACT ASSESSMENT
 719-737 DUNDAS STREET, LONDON, ONTARIO

Figure No.
1

Title
Project Area

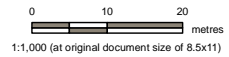
- Notes
1. Coordinate System: NAD 1983 UTM Zone 17N
 2. Contains information licensed under City of London Open Data Terms of Use.
 3. Parcels used under license with the City of London, 2016.

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Legend

Study Area



Project Location
City of London

161413817 REVA
Prepared by KDB on 2019-05-08

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719-737 DUNDAS STREET, LONDON, ONTARIO

Figure No.

2

Title

Study Area

- Notes**
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Methodology
September 20, 2019

2.0 METHODOLOGY

2.1 POLICY FRAMEWORK

2.1.1 Planning Act

The *Planning Act* provides a framework for land use planning in Ontario, integrating matters of provincial interest in municipal and planning decisions. Part I of the *Planning Act* identifies that the Minister, municipal councils, local boards, planning boards, and the Municipal Board shall have regard for provincial interests, including:

(d) The conservation of features of significant architectural, cultural, historical or scientific interest

(Government of Ontario 1990)

2.1.2 The 2014 Provincial Policy Statement

The Provincial Policy Statement (PPS) was updated in 2014 and is intended to provide policy direction for land use planning and development with regard to matters of provincial interest. Cultural heritage is one of many interests contained within the PPS. Section 2.6.1 of the PPS states that, “significant built heritage resources and cultural heritage landscapes shall be conserved”.

Under the PPS definition, conserved means:

The identification, protection, management and use of built heritage resources, cultural heritage landscapes and archaeological resources in a manner that ensures their cultural heritage value or interest is retained under the Ontario Heritage Act. This may be achieved by the implementation of recommendations set out in a conservation plan, archaeological assessment, and/or heritage impact assessment. Mitigative measures and/or alternative development approaches can be included in these plans and assessments.

Under the PPS definition, significant means:

In regard to cultural heritage and archaeology, resources that have been determined to have cultural heritage value or interest for the important contribution they make to our understanding of the history of a place, an event, or a people.

The PPS also stipulates that development adjacent to protected heritage properties must be considered, in policy 2.6.3:

Planning authorities shall not permit development and site alteration on adjacent lands to protected heritage property except where the proposed development and site alteration has been evaluated and it has been demonstrated that the heritage attributes of the protected heritage property will be conserved.



Methodology
September 20, 2019

Under the PPS, “protected heritage property” is defined as follows:

property designated under Parts IV, V or VI of the Ontario Heritage Act; property subject to a heritage conservation easement under Parts II or IV of the Ontario Heritage Act; property identified by the Province and prescribed public bodies as provincial heritage property under the Standards and Guidelines for Conservation of Provincial Heritage Properties; property protected under federal legislation, and UNESCO World Heritage Sites.

(Government of Ontario 2014)

2.1.3 City of London Official Plan

The property at 719-721 Dundas Street is designated under Part IV of the OHA.

The City’s Official Plan, *The London Plan*, contains the following policy with regard to development within or adjacent to designated and listed heritage properties:

586_ The City shall not permit development and site alteration on adjacent lands to heritage designated properties or properties listed on the Register except where the proposed development and site alteration has been evaluated and it has been demonstrated that the heritage attributes of the heritage designated properties or properties listed on the Register will be conserved.

The London Plan also contains the following general objectives with regard to cultural heritage resources:

1. Promote, celebrate, and raise awareness and appreciation of London’s cultural heritage resources.
2. Conserve London’s cultural heritage resources so they can be passed on to our future generations.
3. Ensure that new development and public works are undertaken to enhance and be sensitive to our cultural heritage resources.

(City of London 2016)

2.2 BACKGROUND HISTORY

Background history for this project was obtained through review of aerial photography, fire insurance plans, city directories, and secondary sources. Research was conducted at Western University and the London Public Library.

To familiarize the study team with the Study Area, historical mapping, fire insurance plans and aerial photographs were consulted to identify the presence of structures, and other potential cultural heritage resources in the vicinity. Specifically, material reviewed of the Study Area included Fire Insurance Plans of 1888, 1907, 1915, 1922, 1940, 1949, and 1958 and aerial photography of the Study Area from 1955 and 1967.



Methodology
September 20, 2019

2.3 FIELD PROGRAM

A site assessment was undertaken on March 19, 2019 by Frank Smith, Cultural Heritage Specialist, and Kurt Kostuk, Material Culture Analyst, both with Stantec. The weather conditions were sunny and calm. The site visit consisted of visually assessing and photographing the Study Area from the publicly accessible municipal right-of way to identify heritage attributes.

2.4 EVALUATION OF CULTURAL HERITAGE VALUE OR INTEREST

2.4.1 Ontario Regulation 9/06

The criteria for determining CHVI is defined by *Ontario Regulation (O. Reg.) 9/06*. Each potential cultural heritage resource was considered both as an individual structure and as cultural landscape. Where CHVI was identified the property was determined to contain a cultural heritage resource.

In order to identify CHVI at least one of the following criteria must be met:

1. *The property has design value or physical value because it:*
 - a. *is a rare, unique, representative or early example of a style, type, expression, material or construction method*
 - b. *displays a high degree of craftsmanship or artistic merit*
 - c. *demonstrates a high degree of technical or scientific achievement*
2. *The property has historical value or associative value because it:*
 - a. *has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community*
 - b. *yields, or has the potential to yield, information that contributes to an understanding of a community or culture*
 - c. *demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community*
3. *The property has contextual value because it:*
 - a. *is important in defining, maintaining or supporting the character of an area*
 - b. *is physically, functionally, visually or historically linked to its surroundings*
 - c. *is a landmark*

(Government of Ontario 2006a)

2.4.2 Assessment of Impacts

The assessment of impacts on cultural heritage resources is based on the impacts defined in the Ministry of Tourism, Culture and Sport (MTCS) *Infosheet #5 Heritage Impact Assessments and Conservation Plans (Infosheet #5)* (Government of Ontario 2006b). Impacts to cultural heritage resources may be direct or indirect. Direct impacts include:



HERITAGE IMPACT ASSESSMENT—719-737 DUNDAS STREET, LONDON, ONTARIO

Methodology
September 20, 2019

- Destruction of any, or part of any, significant heritage attributes or features
- Alteration that is not sympathetic, or is incompatible, with the historic fabric and appearance

Indirect impacts do not result in the direct destruction or alteration of the feature or its heritage attributes, but may indirectly affect the CHVI of a property by causing:

- Shadows created that alter the appearance of a heritage attribute or change the viability of a natural feature or plantings, such as a garden
- Isolation of a heritage attribute from its surrounding environment, context or a significant relationship
- Direct or indirect obstruction of significant views or vistas within, from, or of built and natural features
- A change in land use such as rezoning a battlefield from open space to residential use, allowing new development or site alteration to fill in the formerly open spaces
- Land disturbances such as a change in grade that alters soil and drainage patterns that adversely affect an archaeological resource

In addition to direct impacts related to destruction, this HIA also evaluated the potential for indirect impacts resulting from the vibrations of construction and the transportation of project components and personnel. This was categorized together with land disturbance. Although the effect of traffic and construction vibrations on historic period structures is not fully understood, vibrations may be perceptible in buildings with a setback of less than 40 metres from the curbside (Crispino and D'Apuzzo 2001; Ellis 1987; Rainer 1982; Wiss 1981). The proximity of the proposed development to cultural heritage resources was considered in this assessment.



Site History
September 20, 2019

3.0 SITE HISTORY

3.1 INTRODUCTION

The Study Area is located on part of Lot 12, Concession C, in the former Township of London, now City of London. The Study Area is located at the northeast corner of Hewitt Street (formerly Glebe Street) and Dundas Street, on Plan 229 and includes properties with the municipal addresses 719-721 Dundas Street, 737 Dundas Street, 735 Dundas Street, and 729 Dundas Street (Figure 2). The following sections outline the historical development of the Study Area from the time of Euro-Canadian settlement to the present day.

3.2 PHYSIOGRAPHY

The Study Area is located in the Caradoc Sand Plain and London Annex physiographic regions. Both regions are flat sand plains extending from east London to the Strathroy area in the southwest. In its entirety, the region comprises approximately 482 square kilometres in southwestern Ontario. The land is generally flat with a few rolling hills. The soil in the area consists of three types: Fox fine sandy loam, which appears on the finer soils which are deep and well drained; Berrien sandy loam, a shallow layer of sand over clay, with wet subsoil; and Oshtemo sand, which appears on sand hills and dunes (Chapman and Putnam 1984: 146).

The City of London is located along the Thames River. The well-defined river channel runs through a shallow valley. This is demonstrated through a history of critical flooding in the City as it was developed on land that, in physiographical terms, belongs to the river. This watershed area has proven from its land use history to be rich soil for agriculture development (Chapman and Putnam 1984: 139). London itself developed into the commercial centre for Southwestern Ontario because of its position along the river as an early travel route and the high alluvial terrace which offered good building sites (Chapman and Putnam 1984: 146).

3.3 HISTORICAL DEVELOPMENT

3.3.1 Survey and Settlement

During the 17th century and until 1763, southwestern Ontario was part of France's vast colonial holdings in North America called New France. In 1763, the Seven Years war concluded with the signing of the Treaty of Paris, and France relinquished nearly all of its colonial holdings in North America to Great Britain and Spain. The Thirteen British colonies along the Atlantic seaboard eagerly participated in the Seven Years War and believed that dislodging France from the continent's interior would open land west of the Appalachian Mountains to settlement by the burgeoning colonies. Instead the British *Proclamation of 1763* closed most of former New France to settlement to appease Indigenous allies and protect the fur trade. In 1774, the Quebec Act transferred the Ohio Valley and southwestern Ontario to the Province of Quebec. The Quebec Act enflamed tensions with the increasingly restless Thirteen Colonies and was a



HERITAGE IMPACT ASSESSMENT—719-737 DUNDAS STREET, LONDON, ONTARIO

Site History
September 20, 2019

contributing factor to the American Revolution, which culminated with the recognition of the independence of the Thirteen Colonies as the United States in 1783 (Craig 1963: 2 and Phelps 1989: 1).

Approximately one quarter of the population of the former Thirteen Colonies were Loyalists to the British Crown and about 50,000 people left the United States for Great Britain or other colonies, including Canada (Craig 1963: 3). Between 1778 and 1786, the Province of Quebec was governed by Frederick Haldimand. Initially, Haldimand wished to settle present-day Ontario with mostly First Nations allies of the Crown, but upon hearing of the favourable agricultural conditions throughout much of the region, he soon changed his mind. Haldimand also realized that settling the area with Loyalists would provide a bulwark against further aggression by the United States. Writing to Lord North, Prime Minister of Great Britain, Haldimand argued that the settlers would be “attached to the interests of Great Britain and capable of being useful upon many occasions” (Craig 1963: 4-5). To facilitate settlement, southern Ontario was divided into four districts, with present-day London being located in the Hesse District (Archives of Ontario 2015).

The Loyalist population wished to live under the customs and common law they were familiar with in Great Britain and the former Thirteen Colonies, instead of the French civil law practiced in Quebec as part of the *Quebec Act of 1774*. To accommodate the Loyalists, the British parliament passed the *Constitutional Act of 1791*, which divided Quebec into Upper and Lower Canada. The division was both geographic and cultural; French laws would be preserved in Lower Canada, while the British constitution and laws would rule in Upper Canada (Craig 1963: 17). John Graves Simcoe was selected as Lieutenant Governor of the newly created province. Simcoe was a veteran of the American Revolution, having served in the Queens Rangers, and eagerly planned to build a model British society in Upper Canada. He wrote of his desire to “inculcate British customs, manners, and principles in the most trivial as well as serious matters” in the new colony (Craig 1963: 20-21). In 1792, Simcoe renamed the Hesse District the Western District (Archives of Ontario 2015).

While studying maps of Upper Canada, Simcoe decided the provincial capital should be named London and located in the southwest at the confluence of the north and south branches of the river called La Tranche by the French (Finkelstein 2006). Simcoe renamed the river the Thames to match his plan for a capital city called London. He believed this strategic location would be too far inland for American forces to easily attack in the event of renewed war. Despite Simcoe’s wishes, London was still considered too remote and inaccessible a location to be a capital city. Instead, the capital was moved to York (present-day Toronto) (Armstrong 1986: 21) However, in 1793 Simcoe ordered a military road constructed by the Queen’s Rangers from York to the Thames River and named the road Dundas Street after his friend and British Secretary of State, Henry Dundas (Craig 1963: 35). The Study Area is bounded on the north by Dundas Street.

The first surveyor in the region, Abraham Iredell, reported the agricultural conditions in Southwestern Ontario to be among the finest in North America. In 1800, the Western District was divided roughly in half and the London District and Middlesex County were created (Archives of Ontario 2015). Middlesex County was further divided into townships, London Township being the largest at 12 square miles (approximately 31 square kilometres) and encompassing 96,000 acres.



Site History
September 20, 2019

The first settler in London Township was Joshua Applegarth, who arrived in 1807, and attempted to cultivate hemp before switching to other crops (Page 1878: 5). However, London Township remained almost entirely unsettled until 1810 when Thomas Talbot returned, along with surveyor Mahlon Burwell, to develop the township. Talbot would eventually be instrumental in the settlement of 29 townships in southwestern Ontario (London Township History Book Committee 2001: 12). Burwell's survey was interrupted by the War of 1812 and he completed the work in 1818. (Page 1878: 5). The first Township meeting was held in January 1819 at Joshua Applegarth's home (Armstrong 1986: 29).

3.3.2 19th Century Development

In November 1825, the London District courthouse and jail at Vittoria in Norfolk County was damaged by fire. District authorities, including Thomas Talbot, decided to move the district capital to a more central location, instead of rebuilding at Vittoria (Miller 1992: 7). In January 1826, the District Town for the London District was transferred from Vittoria to the Crown Reserve Land in London Township set aside for Simcoe's envisioned capital. The townsite for London was surveyed in May and June of 1826 by Burwell (Armstrong 1986: 33 and Miller 1992: 7).

The village continued to grow and in 1840, the Town of London was incorporated (Brock 2011: 23). When the Town of London was incorporated the boundaries of the town were extended north to present-day Huron Street and east to present-day Adelaide Street (Armstrong 1986: 67). The new town had a population of 1,716 (Armstrong 1986: 63).

As the Town of London began to develop, residents began to clamor for access to a railway. As early as 1831, merchants and farmers of London and London Township had proposed constructing a railway through the community. In the 1840s, planning began on a line that would run from Niagara to Detroit. The planned route would run through London and many prominent Londoners helped finance the project. The Great Western Railway was chartered in 1845 and construction on the London portion of the line began in October 1847. The ground-breaking ceremony in London was led by Thomas Talbot, who was then 77 years old and still deeply involved in the development of London. In December 1853, the first train pulled into London. The train had travelled from Hamilton and arrived in six hours at an average speed of 25 mph (40 km/h) (Armstrong 1986: 82-83). In 1882, the Great Western Railway became part of the Grand Trunk Railway.

In 1857, oil was discovered in Lambton County and London's location on the railway and already established population made the City an attractive choice for refining operations. However, the noxious fumes associated with refining led the City Council to ban refining operations within City limits. Many of the refineries moved just outside of city limits to the area east of Adelaide Street (Armstrong 1986: 120-121). Soon other businesses and industries moved to the area including railway car shops and stables for the City's horse drawn streetcars. Industries were attracted to London Township's low assessment rate, plethora of available land, and proximity to the city's workforce (Stantec 2004: 7). As industries developed in the area, developers began to offer one quarter to half acre lots for sale for residential construction. These subdivisions attracted the industrial workers of the area, many of whom were employed as skilled laborers, carpenters, builders, plasters, and painters (Stantec 2004: 9).



Site History
September 20, 2019

By the 1870s, the area east of Adelaide Street was commonly referred to as “London East” and had grown into London’s first suburb. In 1874, the area was separated from London Township and incorporated as a separate municipality. However, the budding municipality struggled to provide streetcar service, fire protection, and water supply to the residents and businesses of London East. The need for better fire protection became tragically evident when the Imperial Oil Refinery and Great Western Railway car shops burned down in 1883. In 1885, London East was annexed into the City of London. The annexation was mutually beneficial, the City of London would gain an expanded tax base and London East would attain the services it needed to continue to grow (Armstrong 1986: 128). The City of London grew from a population of 19,941 in 1881 to 30,705 in 1890, an increase partly attributed to the City’s annexation of London East (Miller 1992: 146).

3.3.3 20th Century Development

In 1912, the City of London had a population of 49,102, which would increase to 69,742 in 1929 (Armstrong 1986: 163). During this period, many modern improvements arrived in the City. Main roads in the central part of the City were paved in asphalt, replacing cedar blocks (Armstrong 1986: 133). The Hydro Electric Power Commission (HEPC), under the leadership of Adam Beck, commenced to service London with hydroelectricity from Niagara in 1910 (Armstrong 1986: 136). The Public Utilities Commission was established in 1914 to manage the distribution of electricity, water, and city parks (Armstrong 1986: 168).

Compared to other municipalities in Ontario, London fared relatively well during the Great Depression. Several major building projects were completed in London during the 1930s, including the underpass of Richmond Street under the CNR tracks and construction of the Dominion Public Building on Richmond Street. In 1932, only 8% of the population was unemployed, a much lower number than other cities in southern Ontario like Toronto, Hamilton, and Windsor (Armstrong 1986: 185). Nonetheless, the effects of the Great Depression and Second World War curtailed growth in the City (Curtis 1992: 15).

Infrastructure improvements during the 1960s included new overpasses over the railway at Adelaide Street, Highbury Avenue, and Quebec Street. In the 1970s, Queens Avenue was extended over the Thames River as was Dundas Street and Wonderland Road and Hutton Roads were connected via the new Guy Lombardo Bridge (Armstrong 1986: 213-214). As the population of London shifted to the suburbs during the mid-20th century it was becoming increasingly unnecessary to visit downtown London (Armstrong 1986: 234). By the 1970s, a revitalization plan was needed for the City’s downtown. A cohesive vision for the city core did not develop and a mix of infill and new construction occurred during the 1970s, including the City Centre Complex, the London Centre Arcade, the new City Hall, and new federal building and courthouse (Armstrong 1986: 234, 238).

During the 1980s, the pace of growth in the City steadied. The population of the City in 1980 was 261,841 (Armstrong 1986: 327) and most new growth in London occurred at the south and north ends of the city as subdivision development accelerated (Miller 1992: 229). The City of London is continuing to grow and develop in the 21st century. In 2016, the City of London had a population of 383,822 an increase of 4.8% since 2011 (Statistics Canada 2017).



Site History
September 20, 2019

3.4 PROPERTY HISTORY

The Study Area is located on part of Lot 12, Concession C, in the former Township of London, now part of the City of London. Lot 12, Concession C was originally set aside as a Clergy Reserve. In each newly surveyed township, one seventh of the land was set aside as Clergy Reserve as per the terms of the *Constitutional Act* which created Upper Canada. The Clergy Reserves were designed to support the Protestant Church in Upper Canada, particularly the Church of England (Craig 1963: 16). The Clergy Reserves quickly became a point of contention between many settlers and the colonial government. Opponents of the reserves believed the lands impeded settlement because they were often left undeveloped and many balked at the idea of funding the Church of England but not other Protestant denominations (Craig 1963: 273). Sales of the Clergy Reserves began in 1827 when colonial administrators allowed one quarter of the total Clergy Reserves to be sold. In 1841, legislation was passed in Britain that authorized the sale of the remaining Clergy Reserves (Fahey 2015).

Based on mapping, in 1856 the Study Area was part of a Clergy Reserve that was leased for a period of 25 years. The land had been subdivided and historical mapping shows much of the modern-day street grid was laid out (Peters 1856; Map of the City of London 1856). It is unclear when the lands within the Study Area were no longer part of a Clergy Reserve. However, the church's role within and adjacent to the Study Area is remembered by the street name, Glebe Street, which currently runs south of the Study Area between King and York Streets. Hewitt Street, the eastern border of the Study Area was formerly part of Glebe Street until the mid-20th century when the name was changed to Hewitt Street. The term "glebe" denotes a piece of land designated to support a parish.

3.4.1 737 Dundas Street

Based on Fire Insurance Plans and City Directories, the residence at 737 Dundas Street was constructed in 1903-04. The first occupant of 737 Dundas Street was Harry Rapsey (Foster 1904: 68). The Census of 1911 lists Harry Rapsey as a 65-year-old railway engineer. He lived with his wife Elizabeth, age 47 (Library and Archives Canada 1911). The Fire Insurance Plan of 1922 shows the residence as a frame structure with a brick exterior (Figure 3). The final year Harry Rapsey was listed as the occupant of 737 Dundas Street was 1944 (Vernon 1944: 708). Rapsey did not spend the final two years of his life at 737 Dundas Street and died in 1946 at the age of 97. Harry is buried at Mount Pleasant cemetery in London with his wife and two children (GenWeb Cemetery Project 2019).

The Directory of 1945 lists the occupants as R.M. Dundas and N.S. Lee (Vernon 1945: 718). In 1946, the occupants were Mrs. Edythe George, the George Radio Service, and John Millar (Vernon 1946: 766). The Fire Insurance Plan of 1949 shows that the present-day addition facing Dundas Street already constructed (Figure 4). Based on the Fire Insurance Plan and City Directories, the addition to the front façade of 737 Dundas Street was likely built between 1945-46. By 1950, commercial operations at 767 Dundas Street expanded to include the Georgina Beauty Shoppe as well as the Radio Service (Vernon 1950: 681). Based on City Directories and Fire Insurance Plans, the rear addition to 737 Dundas Street was constructed between 1951 and 1958. The Fire Insurance Plan of 1958 shows the rear addition present (Figure 5). City Directories between 1950 and 1958 show a fluctuating number of occupants, both people and businesses, at 737 Dundas Street. It is likely the rear addition was already present by 1955



HERITAGE IMPACT ASSESSMENT—719-737 DUNDAS STREET, LONDON, ONTARIO

Site History
September 20, 2019

based on the increase of occupants listed at the address. The 1955 Directory lists the occupants as E. May, Georgina Beauty Shoppe, AlSCO SIs Ltd., Air Way Distributor of Ontario, Mrs. C. Struckett, W. Morley, and J. Wood (Vernon 1955: 600).

The Georgina Beauty Shoppe continued to operate at 737 Dundas Street through the first half of the 1960s. The occupants in 1964 were the Georgina Beauty Shoppe, Universal of Canada, and F. Moore, M. Dixon, E. George, and Adalai Long (Vernon 1964: 605). By 1970, the Georgina Beauty Shoppe and Mrs. E. George remained at the building. The only other non-residential occupant of 737 Dundas Street by this time was the United Lodge of Theosophists. The other occupants were Harry Fullard, and E. Ferguson. One unit was listed as vacant (Vernon 1970: 753). Between 1971 and 1974, the Georgina Beauty Shoppe and Ms. George left 737 Dundas Street. The occupants in 1975 were listed as Gloria's Exclusive Dress Shop, James Mason, Real Estate Broker, and three apartments were occupied, and one was vacant (Vernon 1975: 935). In 1981, occupants included the Accounting Business and Consultants, Inc., Lambert's Camera Repairs, and four apartments (Vernon 1981: 122). By 1990, the commercial occupant of the building was The Buccaneer (Vernon 1990: 135). By 2000, the building was used exclusively for residential space, which it remains today (Vernon 2000: 139).

3.4.2 735 Dundas Street

Based on Fire Insurance Plans, aerial photos, and City of London Directories, the structure at 735 Dundas Street was built between 1964 and 1966. Aerial photography from 1967 shows the present-day structure on 735 Dundas Street constructed (Figure 6). The present-day structure at 735 Dundas Street replaced a frame structure that was one of the last remaining frame structures on Dundas Street in the vicinity of the Study Area. The occupant of the building in 1970 was listed as the Seaboard Fin Company (Vernon 1970: 753). By 1975, the occupant of the building was Canadian Acceptance Corporation Limited (Vernon 1975: 935). The occupant in 1990 was Cortlaw Service Limited (Vernon 1990: 135). Since at least 2000 the building has been occupied by Ever Fresh 2-for-1 Pizza (Vernon 2000: 139).

3.4.3 729 Dundas Street

Based on Fire Insurance Plans and City Directories, the structure at present-day 729 Dundas Street was built between 1946 and 1948. The City Directory of 1946 and Fire Insurance Plan of 1940 have the lot as empty while the 1949 Fire Insurance Plan shows the present-day concrete block building in place. The building housed various commercial operations in the 1950s and 1960s. Since at least 1970, the building has been occupied by U-Need-A-Cab, which remains the present-day occupant (Vernon 1970: 753). U-Need-A-Cab is a taxicab service that began operation in the City of London in 1956 (U-Need-A-Cab 2015).

3.4.4 719-721 Dundas Street

The building at 719-721 Dundas Street was built in 1877 and expanded in 1899 and 1918. The first occupant of the residence was Francis S. Bickley, a machine hand and plumber. The residence was expanded in 1899 and the present-day front façade was built (City of London 2012). In 1904, Dr. Edwin Seaborn moved in 719 Dundas Street. Seaborn was born in 1872 in Quebec and moved to London with



HERITAGE IMPACT ASSESSMENT—719-737 DUNDAS STREET, LONDON, ONTARIO

Site History

September 20, 2019

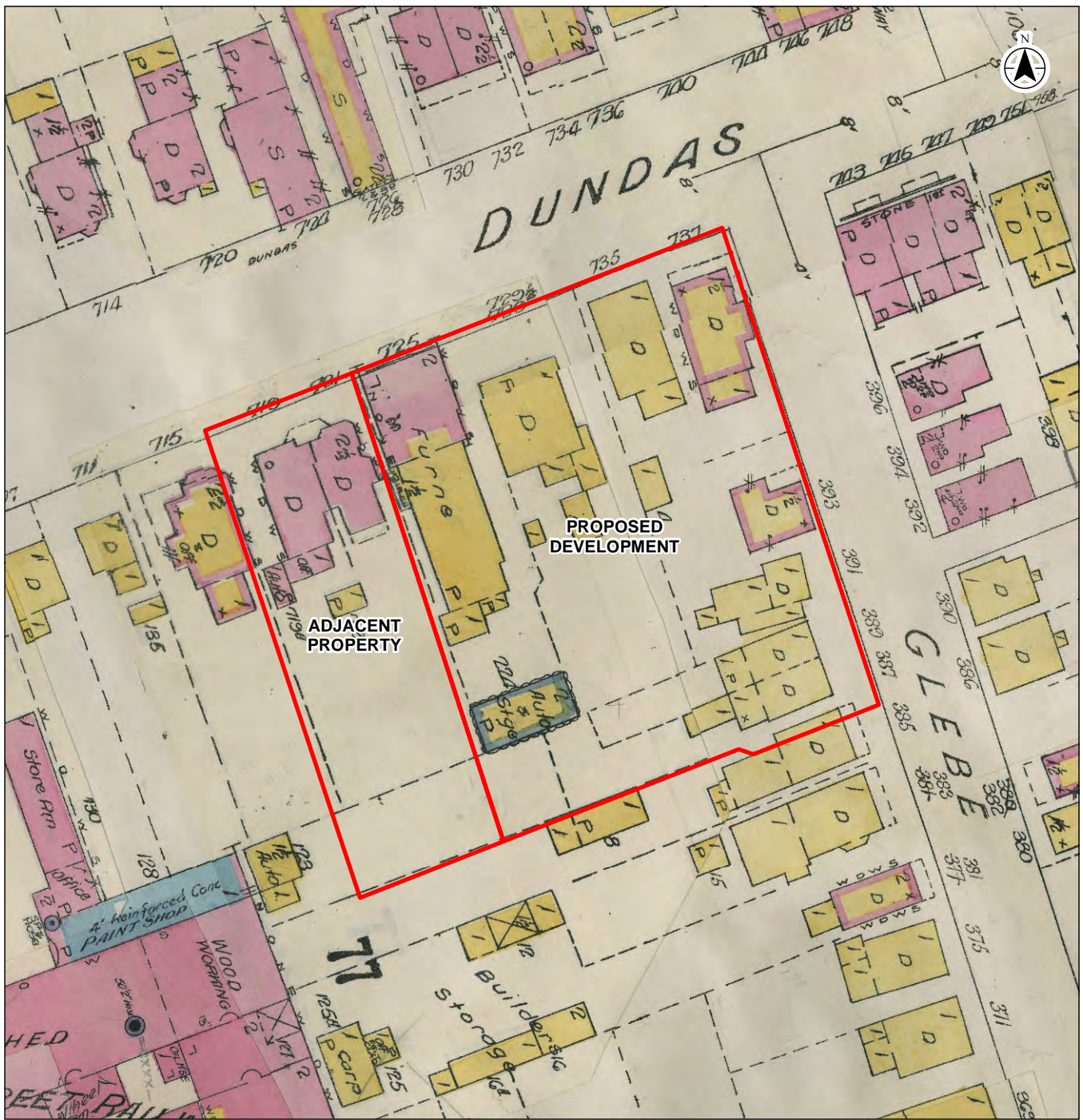
his family in 1879 where he graduated from Western's medical school in 1895. After graduation, he became a professor at Western and was appointed Chair of Anatomy. During the First World War, Seaborn served as an Army Doctor from 1916 until his demobilization in 1919. Seaborn had an interest in local history and served as president of the London Middlesex Historical Society in 1936. He passed away in 1951 (Western Archives n.d.).

Seaborn moved out of 719 Dundas Street by 1910. The residents that year included J.C. Lindsay, a physician at 719 Dundas Street and J.H. Cunningham at 721 Dundas Street (Vernon 1910: 49). The Census of 1911 lists John Lindsay as a 38-year-old doctor. He lived with his wife Isabella, age 35; son Kenneth, age 10; daughter Helen, age 8; son James, age 1, and servant, Grace, age 17. The Census also lists James Cunningham as a 70-year-old not in the work force. He lived with his wife Elizabeth, age 57, and son Oscar, age 30, who was a dentist (Library and Archives Canada 1911). Lindsay remained at 719 Dundas Street as late as 1955, while occupants of 721 Dundas Street changed with more frequency.


By the 1960s, the occupants of 719 Dundas Street had changed to commercial businesses. The City Directory of 1964 listed the occupants of 719 Dundas Street as Fred Fones Construction and Mountsfield Developments. The occupants of 721 Dundas Street were L.H. Barnes and H.L. Scarlett (Vernon 1964: 605). By 1975, both 719 and 721 Dundas Street were commercial in nature with the occupants being Crusade Evangelism International at 719 Dundas Street and Wilfred Webb Real Estate, Riviera Rentals, and Mountsfield Development being the occupants at 721 Dundas Street (Vernon 1975: 935).

The building continued to house commercial businesses and the offices of professionals such as lawyers, accountants, and real estate brokers until it was purchased by the Unity Project. The Unity Project moved into 719-721 Dundas Street in 2005 and named the building the "Unity House." The Unity House is a transitional housing program (Unity Project 2006: 5).





Legend

 Study Area

Notes
 1. Historic information not to scale.
 2. Source: Underwriters Survey Bureau. 1922. Key Plan of the City of London, Ontario. Toronto: Underwriters Survey Bureau Limited.



Project Location
 City of London
 161413817 REVA
 Prepared by KDB on 2019-05-08

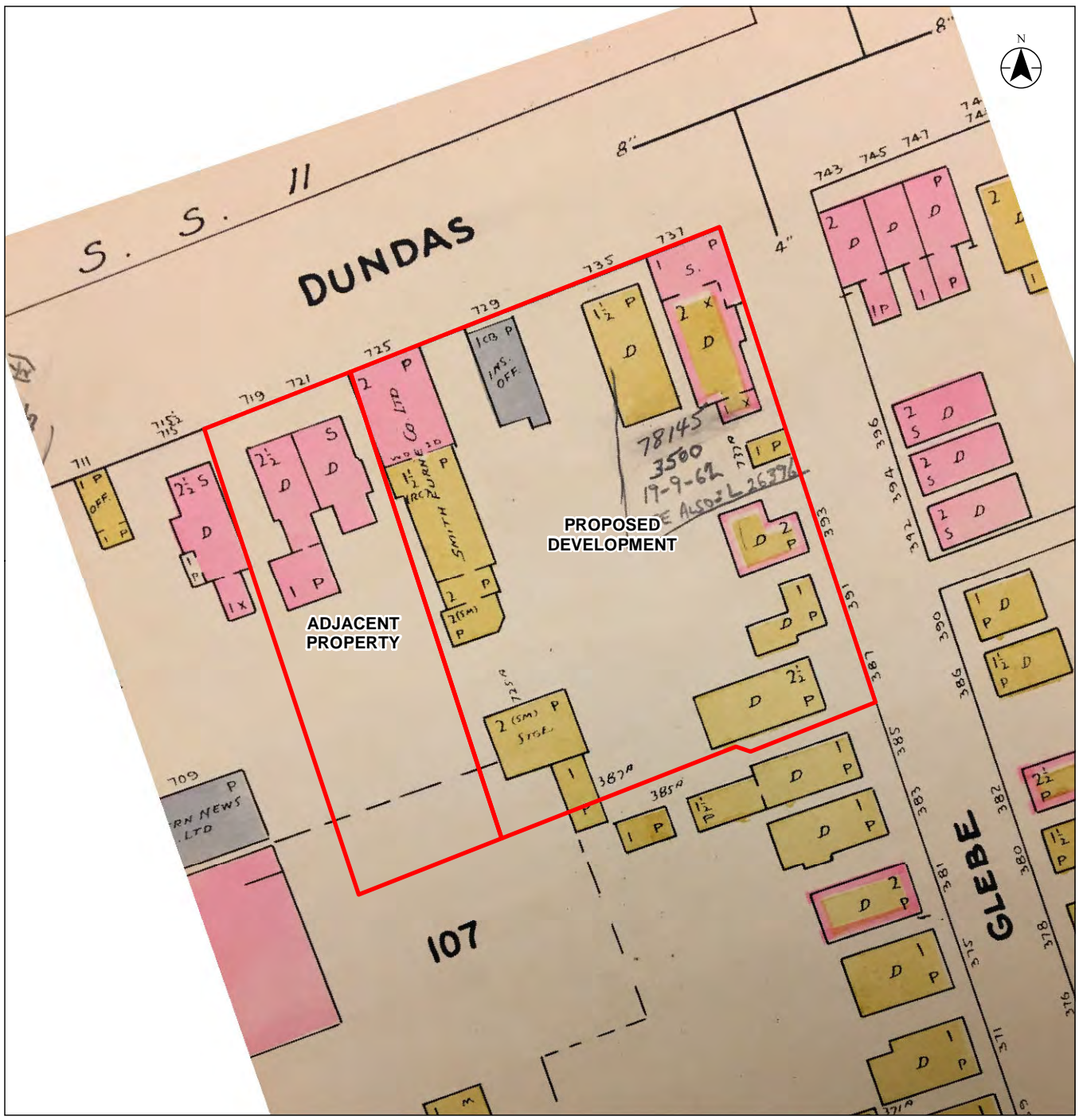
Client/Project
 MEDALLION CORP.
 HERITAGE IMPACT ASSESSMENT
 719-737 DUNDAS STREET, LONDON, ONTARIO

Figure No.

3

Title

1922 Fire Insurance Plan



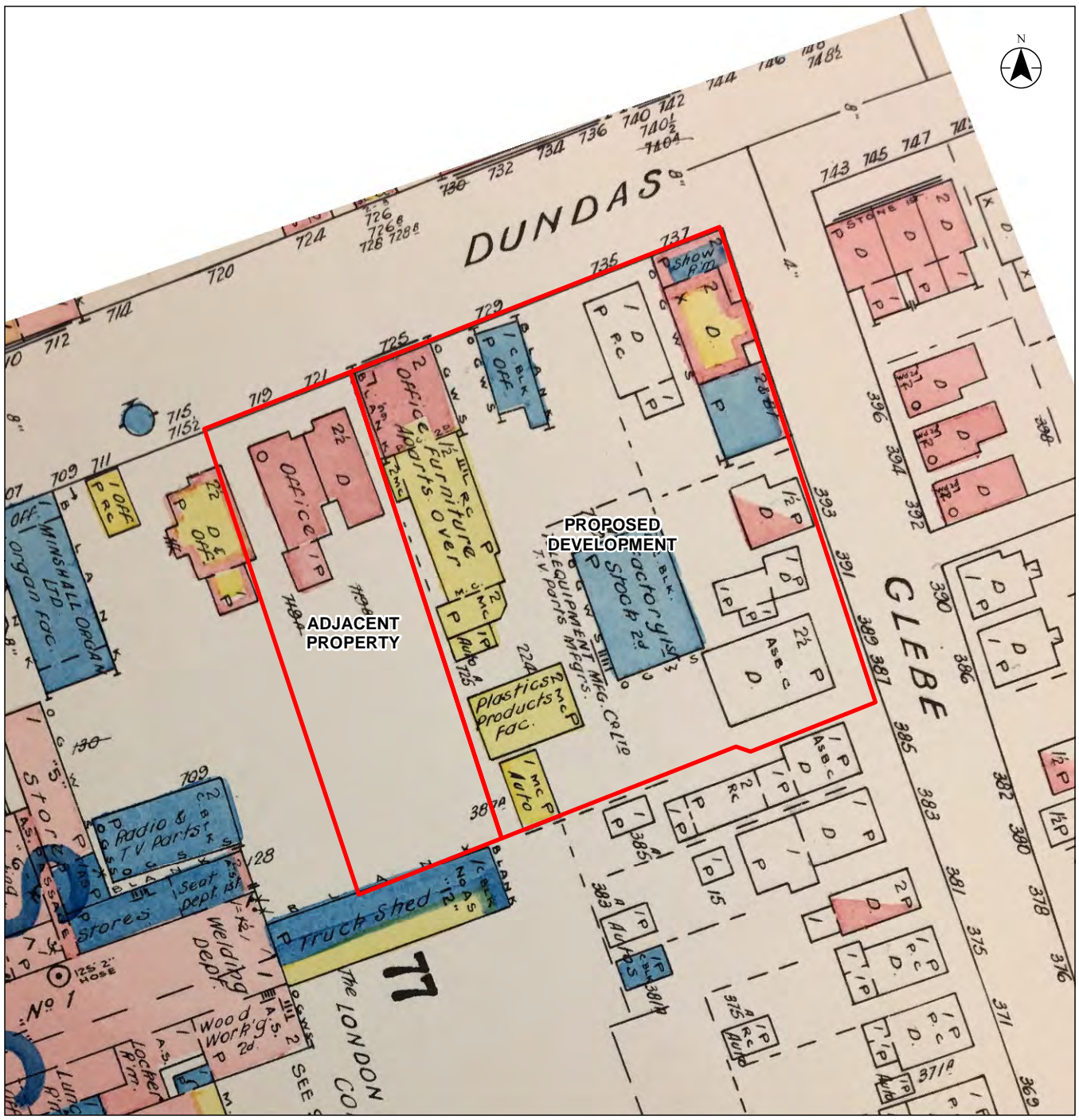
Legend
 Study Area

Stantec
 Project Location: City of London
 161413817 REVA
 Prepared by KDB on 2019-05-08

Notes
 1. Historic information not to scale.
 2. Source: Lloyd, C.N. 1949. London Ontario, Toronto: C.N. Lloyd.

Client/Project: MEDALLION CORP.
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 719-737 DUNDAS STREET, LONDON, ONTARIO
 Figure No. 4
 Title: 1949 Fire Insurance Plan

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Legend

Study Area

Notes
 1. Historic information not to scale.
 2. Source: Underwriters Survey Bureau. 1958. Insurance Plan of the City of London. Toronto: Underwriters Survey Bureau.



Project Location: City of London
 161413817 REVA
 Prepared by KDB on 2019-05-08

Client/Project: MEDALLION CORP.
 HERITAGE IMPACT ASSESSMENT
 719-737 DUNDAS STREET, LONDON, ONTARIO

Figure No.


5

Title

1958 Fire Insurance Plan



Legend

 Study Area

Notes
1. Historic information not to scale.
2. Source: Lockwood Survey Corp. 1967. London 1967. Photo 102, Line 3.



Project Location
City of London

161413817 REVA
Prepared by KDB on 2019-06-14

Client/Project
MEDALLION CORP.
HERITAGE IMPACT ASSESSMENT
719-737 DUNDAS STREET, LONDON, ONTARIO

Figure No.

6

Title

1967 Aerial Photo

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Site Description
September 20, 2019

4.0 SITE DESCRIPTION

4.1 INTRODUCTION

As outlined in Section 2.3, a site visit was conducted on March 19, 2019 by Frank Smith, Cultural Heritage Specialist, and Kurt Kostuk, Material Culture Analyst, with Stantec. The weather conditions were sunny, calm, and seasonable. The site visit included a pedestrian survey of the buildings.

4.2 LANDSCAPE SETTING

The Study Area consists of the property at 737 Dundas Street, 735 Dundas Street, 729 Dundas Street, and 719-721 Dundas Street. The property at 737 Dundas Street is a late 19th century residence that has been heavily modified with mid-20th century additions on the south and north façades. The properties at 735 Dundas Street and 729 Dundas Street contain mid-20th century commercial buildings. The property at 719-721 Dundas Street contains a late 19th century Queen Anne style residence that has been converted into an emergency/temporary shelter. Adjacent properties include lower density residential and commercial buildings and the high rise apartments that are part of the Revo complex. Adjacent to the Study Area are parking surfaces and empty lots.

Hewitt Street adjacent to the Study Area is a two-lane road paved with asphalt that is approximately 145 metres in length running in a north-south direction (Plate 1 and Plate 2). Hewitt Street has concrete sidewalks and wooden utility poles with municipal LED streetlighting. The structures on Hewitt Street are residential, including three one and one half storey late 19th to early 20th century residences (392-396 Hewitt Street), the Revo residential highrises (fronting King Street), an asphalt parking surface, and 737 Dundas Street, part of the Study Area, which is a late 19th century residence converted into apartments (Plate 3 and Plate 4).

Dundas Street adjacent to the Study Area is a two-lane road paved with asphalt running in a east-west direction (Plate 5 and Plate 6). Street parking is prohibited within the Study Area. Dundas Street has wide concrete sidewalks and mid-20th century streetlighting fixtures that have a fluted base, brackets for banners, and a bracket supported curved mast with a LED luminaire (Plate 7). The streetlights also carry medium voltage wiring. Within and adjacent to the Study Area most buildings are one to three storey residential and commercial structures dating to the late 19th century to mid-20th century (Plate 8). Within the Study Area there is one empty lot (725 Dundas Street) and adjacent to the Study Area is another empty lot (726-748 Dundas Street) (Plate 9 and Plate 10).



HERITAGE IMPACT ASSESSMENT—719-737 DUNDAS STREET, LONDON, ONTARIO

Site Description
September 20, 2019



Plate 1: Looking north on Hewitt Street



Plate 2: Looking south on Hewitt Street from Dundas Street



Plate 3: Residences on east side of Hewitt Street, looking south



Plate 4: Revo high rise apartment building, looking south

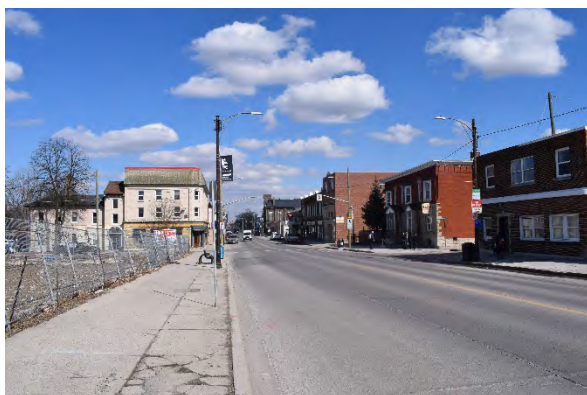


Plate 5: Looking east on Dundas Street



Plate 6: Looking west on Dundas Street



Site Description
September 20, 2019



Plate 7: Streetlight, looking east

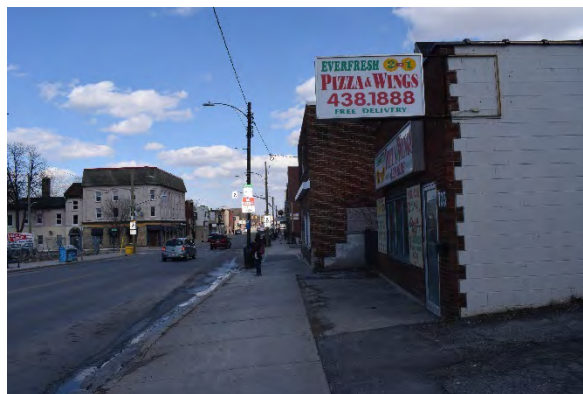


Plate 8: Varied streetscape of Dundas Street within and adjacent to Study Area, looking east

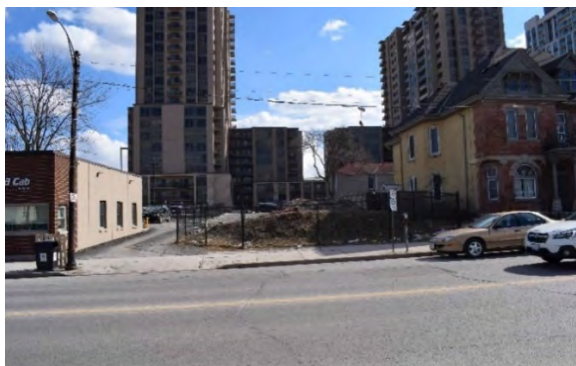


Plate 9: Empty lot, 725 Dundas Street, looking south

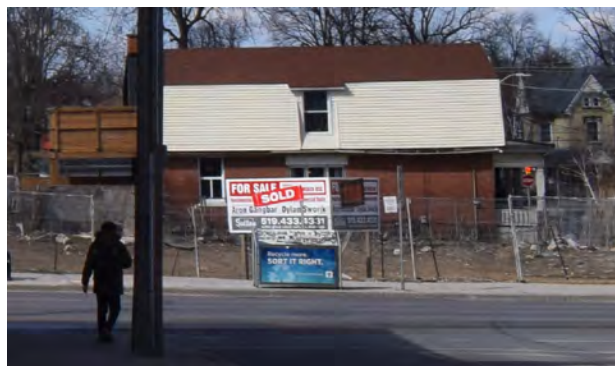


Plate 10: Empty lot, 726-748 Dundas Street, looking north

4.3 737 DUNDAS STREET

The structure at 737 Dundas Street is an early 20th century structure that has been heavily modified into a multi-unit residential building through construction of additions on the north and south façades (Plate 11).

The original portion of the structure is a one-and one-half storey Ontario vernacular residence. The residence contains elements of the Queen Anne style, popular in Ontario from about 1880 to 1910 (Blumenson 1990: 102). The Queen Anne elements of the residence are expressed by the intricate bargeboard present in the gable peaks on the north and east façades. The original residence has a medium-pitched side gable roof, brick chimney, concrete block chimney, shed roof dormers on the west and east façades, and a roof clad in asphalt shingles (Plate 12). The east façade of the original portion contains a projecting gable bay with bargeboard. The exterior of the east façade of the original portion is red brick. The original portion of the residence has a rusticated concrete block foundation (Plate 13).



HERITAGE IMPACT ASSESSMENT—719-737 DUNDAS STREET, LONDON, ONTARIO

Site Description
September 20, 2019

The exterior of the front (north) façade that is visible from the street contains bargeboard painted white within a gable peak. The east façade of the original portion contains bargeboard within the gable peak and modern 1/1 windows with concrete sills. The windows and entrance on the first storey are not original and the bricked over portions of the former openings can be seen as well as brick voussoirs (Plate 14). The first storey is accessed via concrete steps and the basement is accessed via concrete steps. No original portion of the residence is visible from the south façade. The original portion of the west façade has a buff brick exterior painted gray. The façade has modern 1/1 windows that are not in their original openings, evidenced by the brick lintels located about half a metre above the windows. The windows have concrete sills. The residence formerly had basement windows with brick lintels, but these have been blocked (Plate 15).

The front façade of the north addition is a two storey structure with a flat roof and red brick exterior with a stretcher bond (Plate 16). The second storey has modern 1/1 windows with concrete sills. Between the first and second storey is a decorative concrete band. The first storey of the front façade addition has an offset entrance with a composite door and stone door surround with keystone (Plate 17). The windows are modern 1/1 and modern 6/6 windows with concrete sills. Along the top of the window line is a decorative brick band. Located at the lower corners is decorative stonework. The poured concrete foundation of the addition is visible.

The east façade of the north addition has a red brick exterior with a decorative brick band on the first storey and decorative stonework at the northeast corner of the first storey (Plate 18). The addition has 1/1 windows with concrete sills on the second storey and a horizontal sliding window with concrete sill on the first storey. The south addition east façade has a red brick exterior. The second and first storeys have modern fixed windows flanked by modern 1/1 windows and concrete sills. The first storey had a centre entrance with a wood door. The basement level contains horizontal sliding windows with brick lintels. The poured concrete foundation is visible from this façade (Plate 19).

The south façade has a concrete block exterior and shed roof addition with a parged concrete exterior. The south façade contains an offset entrance and a single modern 1/1 window with concrete sill on the second storey (Plate 20).

The west façade of the north addition has a concrete block exterior (Plate 21). The second storey has 1/1 windows with concrete sills and a small balcony. The first storey has 1/1 windows and an entrance door. The west façade of the south addition had a red brick exterior. Part of this façade is clad in concrete block and has a decorative brick band just above the first storey (Plate 22). This section of the façade has a single modern 1/1 window with a concrete sill (Plate 23).



HERITAGE IMPACT ASSESSMENT—719-737 DUNDAS STREET, LONDON, ONTARIO

Site Description
September 20, 2019



Plate 11: Front (north) façade of 737 Dundas Street, looking south



HERITAGE IMPACT ASSESSMENT—719-737 DUNDAS STREET, LONDON, ONTARIO

Site Description
September 20, 2019



Plate 12: Original portion of 737 Dundas Street, looking west



Plate 13: Rusticated concrete block foundation, looking east



Plate 14: Replacement window and door openings, looking west



Plate 15: Original portion of west façade, looking north



Plate 16: Front façade of northern addition, looking south



Plate 17: Stone door surround, looking south



HERITAGE IMPACT ASSESSMENT—719-737 DUNDAS STREET, LONDON, ONTARIO

Site Description
September 20, 2019



Plate 18: East façade of north addition, looking west



Plate 19: East façade of rear addition, looking west



Plate 20: South façade, looking north



Plate 21: West façade of rear addition, looking east



Plate 22: West façade of north addition, looking east

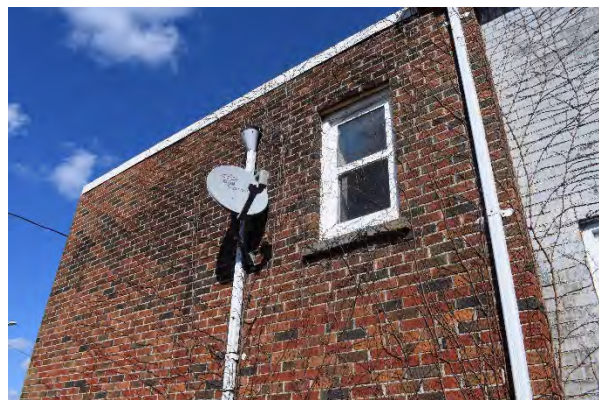


Plate 23: West façade of north addition showing window, looking east



Site Description
September 20, 2019

4.4 735 DUNDAS STREET

The structure at the address 735 Dundas Street is a one storey commercial building with a flat roof and three metal chimneys. The structure has a poured concrete foundation. The current occupant is “Everfresh 2 for 1 Pizza and Wings.” The façade gently slopes downwards towards Dundas Street, resulting in the rear (south) façade entrance being accessed via stairs but the front (north) façade entrance being at street level. The front façade is clad in red brick with a stretcher bond and decorative brick band visible just above the address number but obscured on the rest of the front façade by signage. The front façade contains an offset entrance with a metal and glass door, flat-headed fixed windows, and signage (Plate 24).



Plate 24: Front façade of 735 Dundas Street, looking south

The west façade is concrete block painted white (Plate 25). The rear façade is unpainted concrete block and contains a boarded-up window, an entrance accessed via concrete steps, and an awning (Plate 26). The east façade is unpainted concrete block and contains one boarded-up window, a casement window, utility connection, and a metal pipe. The property is landscaped with a lawn shared with 737 Dundas Street that gently slopes downward towards Dundas Street and a metal fence shared with 737 Dundas Street (Plate 27). The property has an asphalt parking lot, shed, wood fence, and a mature maple tree (Plate 28).



HERITAGE IMPACT ASSESSMENT—719-737 DUNDAS STREET, LONDON, ONTARIO

Site Description
September 20, 2019



Plate 25: West façade of 735 Dundas Street, looking east



Plate 26: South façade of 735 Dundas Street, looking north



Plate 27: East façade of 735 Dundas Street, looking south



Plate 28: Shed and maple tree, looking west



Site Description
September 20, 2019

4.5 729 DUNDAS STREET

The structure at the address 729 Dundas Street is a one storey commercial building with a flat roof and brick chimney. The structure has a poured concrete foundation. The current occupant is “U-Need-A Cab.” The façade of the property gently slopes downwards towards Dundas Street. The front (north) façade is clad in red brick with a stretcher bond. The front façade contains an offset recessed entrance with a metal and glass door and two fixed flat-headed windows with concrete sills (Plate 29). The west façade is clad in red brick and stucco and contains four 1/1 windows with concrete sills, and two light fixtures (Plate 30). The rear (south) façade is L-shaped and is clad in stucco. This façade contains a gutter with downspouts, two 1/1 windows, utility connections, and a composite door (Plate 31). The east façade is clad in stucco and contains two light fixtures (Plate 32). The property contains asphalt driveways and an asphalt parking lot (Plate 33).



Plate 29: Front (north) façade of 729 Dundas Street, looking south



Site Description
September 20, 2019



Plate 30: West façade of 729 Dundas Street, looking east



Plate 31: East façade of 729 Dundas Street, looking west



Plate 32: Rear (south) façade of 729 Dundas Street, looking north



Plate 33: Asphalt parking lot, looking north

4.6 719-721 DUNDAS STREET

The structure at 719-721 Dundas Street is a two and one half storey structure with a steeply-pitched hip roof with gable dormers, brick chimneys, and asphalt shingles (Plate 34). The building is in the Queen-Anne style, which was popular throughout North America in the late 19th century. The front (north) façade contains two front-facing projecting gables that each have decorative woodwork and bargeboard with modern windows. The exterior of the front façade is clad in red brick with a stretcher bond. The first storey has a central entrance with two entrance doors with rectangular transoms. The entrance is covered by a partial porch with classically styled columns resting on rusticated concrete blocks and top with cast-iron cresting. Flanking the central entrance are two bay windows with modern single-pane fixed and 1/1 windows with modern transoms. The front facing portion of each bay window has a semi-circular arched transom window with stone voussoirs and stone keystones. All of the windows on the first storey have stone sills. The second storey contains six modern 1/1 windows with stone lintels, drip moulds, and stone



HERITAGE IMPACT ASSESSMENT—719-737 DUNDAS STREET, LONDON, ONTARIO

Site Description
September 20, 2019

sills. The centre two windows are smaller and have transoms. Between the first and second storey is a decorative stone band. The foundation is rusticated concrete block.



Plate 34: 719-721 Dundas Street, front façade and west façade, looking south

The west façade is clad in buff brick and contains a brick quoin about midway between the second storey windows. The second storey has two modern 1/1 windows with stone sills and the first storey has two modern windows with stone lintels and stone sills. The first storey also contains an entrance with a modern composite door (Plate 35). Attached to the west façade is a small red brick addition with an entrance or window closed with buff brick and a decorative wood band with dentils. The east façade is clad in buff brick and has a central projecting bay. The façade has modern 1/1 windows with brick voussoirs and stone sills. The rear (south) façade is clad in buff brick and contains three projecting bays, modern 1/1 windows with brick voussoirs, blind windows with brick voussoirs, and a central flat roof addition clad in buff brick (Plate 36). Located south of the main building is a two storey modern structure with a hip roof. The structure is clad in modern siding (Plate 37). The property is landscaped with a small garden on the northeast corner and a sign for the “Unity Project” (Plate 38).



HERITAGE IMPACT ASSESSMENT—719-737 DUNDAS STREET, LONDON, ONTARIO

Site Description
September 20, 2019



Plate 35: East façade, looking west



Plate 36: Rear (south) façade, looking north

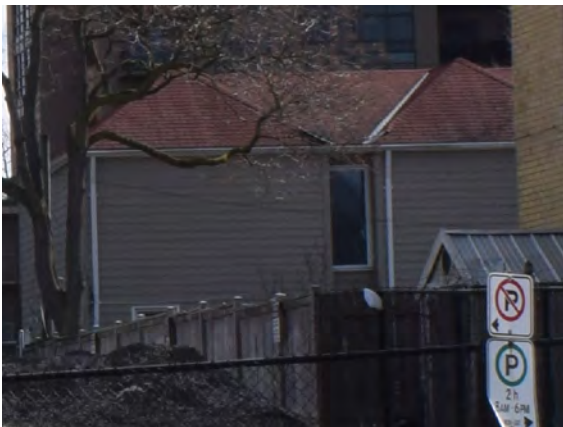


Plate 37: Partial view of modern building on property, looking south



Plate 38: Garden area, looking south



5.0 EVALUATION OF CULTURAL HERITAGE VALUE OR INTEREST

5.1 INTRODUCTION

The criteria for determining CHVI is defined by O. Reg. 9/06 (see Section 2.4.1). If a property meets one or more of the criteria it is determined to contain, or represent, a cultural heritage resource. A summary statement of cultural heritage value has been prepared, and a list of heritage attributes which define the CHVI identified. Where a cultural heritage resource is identified, consideration should be given to the effects of a proposed change on the heritage attributes of that property. The evaluation of each property according to O. Reg. 9/06 is provided in subsequent sections below.

The property at 719-721 Dundas Street is designated under Part IV of the OHA and was not evaluated as part of the HIA. The heritage attributes of 719-721 Dundas Street identified in the municipal designating bylaw are listed.

5.2 HERITAGE EVALUATION

5.2.1 737 Dundas Street

5.2.1.1 Evaluation

Design/Physical Value

The structure at 737 Dundas Street is a one and one half storey residence that has been heavily modified into a multi-unit residential building through mid-20th century two storey additions on the north and south façades. The original exterior portions of the residence retain design value as a representative Ontario vernacular residence with elements of the Queen Anne design style. The Queen Anne elements of the residence are expressed by the intricately decorated bargeboard contained within the gable peaks on the north and east façades of the residence.

The original portion of the residence and the two storey additions are not rare or unique. Although the residence contains intricate wood bargeboard, it does not display a high degree of craftsmanship or artistic merit and bargeboard of a similar quality of craftsmanship is common in southern Ontario. The original residence and additions do not display a high level of technical or scientific achievement and were constructed of common materials and built using standard early and mid-20th century practices.

Based on the above discussion, the residence at 737 Dundas Street meets the criteria of Section 1i of the O. Reg. 9/06 as a representative Ontario vernacular residence with Queen Anne influence.

Historic/Associative Value

The original portion of the residence was built between 1903 and 1904. The first occupant was Harry Rapsey, a railway engineer. Many of the early 20th century residents of East London and Old East Village worked for the railway companies. Harry Rapsey and 737 Dundas Street is not directly associated with a



HERITAGE IMPACT ASSESSMENT—719-737 DUNDAS STREET, LONDON, ONTARIO

Evaluation of Cultural Heritage Value or Interest
September 20, 2019

theme, event, belief, person, activity, or organization significant to the City of London. The residence does not have the potential to yield information that contributes to an understanding of a community or culture and does not demonstrate the ideas of an architect or builder significant to the community.

Based on the above discussion, the residence at 737 Dundas Street does not meet the criteria of Section 2 of O. Reg. 9/06.

Contextual Value

The structure at 737 Dundas Street is set in a mixed streetscape that includes late 19th and early 20th century structures, vacant lots, mid-20th century structures, and structures of varying height and massing. The structures on this portion of Dundas Street do not form a street wall and aside from being of between one to three storeys in height do not contribute to forming a cohesive character of the area. The portion of the residence along Hewitt Street is also in a mixed streetscape. To the east of the residence are other early 20th century structures and views to the south prominently feature the high rise Revo Apartment Complex. Therefore, the structure at 737 Dundas Street is not important to defining, maintaining, or supporting the character of the area. The structure at 737 Dundas Street is a heavily modified early 20th century residence set in an area of mixed residential and commercial character of varying dates of construction, style, and massing. Therefore, the structure is not physically, functionally, visually, or historically linked to its surroundings. The structure is not particularly memorable or notable along Dundas Street and is not a landmark.

Based on the above discussion, the residence at 737 Dundas Street does not meet the criteria of Section 3 of O. Reg. 9/06.

Summary

Table 1 provided below summarizes the findings of the evaluation of CHVI.

Table 1: Evaluation of 737 Dundas Street According to O. Reg. 9/06

Criteria of O. Reg. 9.06	Y/N	Comments
Design or Physical Value		
Is a rare, unique, representative or early example of a style, type, expression, material or construction method	Yes	The residence at 737 Dundas Street has been heavily modified but maintains representative features of an early 20 th century Ontario vernacular residence with elements of the Queen Anne style expressed through the intricate bargeboard contains in the gable peaks on the north and east façades.
Displays a high degree of craftsmanship or artistic merit	No	The structure is a typical mid-20 th century commercial building that utilizes common building materials and does not display a high degree of craftsmanship or artistic merit.
Demonstrates a high degree of technical or scientific achievement	No	The structure does not display a high degree of technical or scientific achievement.
Historical or Associative Value		
Has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community	No	The building has been occupied by various local businesses over the years. These businesses do not have direct associations with a theme, event, belief, person, activity, or organization that is significant to the community.



HERITAGE IMPACT ASSESSMENT—719-737 DUNDAS STREET, LONDON, ONTARIO

Evaluation of Cultural Heritage Value or Interest
September 20, 2019

Criteria of O. Reg. 9.06	Y/N	Comments
Yields, or has the potential to yield, information that contributes to an understanding of a community or culture	No	The structure does not provide evidence of notable or influential aspects of the community or contribute in a meaningful way to comparative analysis of similar properties. The structure does not yield information that contributes to an understanding of a community or culture.
Demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community	No	The architect of the structure is not known.
Contextual Value		
Is important in defining, maintaining or supporting the character of an area	No	The property is set on Dundas Street in an area of mixed commercial and residential character. The surrounding structures are of varying dates of construction, style, and massing. Therefore, the structure does not define, maintain, or support the character of an area.
Is physically, functionally, visually or historically linked to its surroundings	No	The structure is a typical mid-20 th century commercial structure set in an area of mixed residential and commercial character of varying dates of construction, style, and massing. Therefore, the structure is not physically, functionally, visually, or historically linked to its surroundings.
Is a landmark	No	The structure and property are not landmarks.

5.2.1.2 Draft Statement of Significance

The property at 737 Dundas Street contains a heavily modified early 20th century residence. The residence was built between 1903 and 1904. The residence is a representative example of an early 20th century Ontario vernacular structure with elements of the Queen Anne style expressed through the intricate bargeboard contained on the north and east façades of the residence. The first occupant of the residence was Harry Rapsey, a railway engineer. The residence remained in the Rapsey family until 1944 when it was sold. In 1946 an addition was added to the front façade containing commercial businesses. A further addition was added in the 1950s which was home to businesses and rental units. The residence is currently a multi-unit residential building.

5.2.1.3 Heritage Attributes

- One and one half storey residence
- Medium-pitched side gable roof with steeply-pitched gable peaks on north and east façades
- Red brick exterior with stretcher bond
- Bargeboard
- Brick voussoirs
- Rusticated concrete block foundation



5.2.2 735 Dundas Street

5.2.2.1 Evaluation

Design/Physical Value

The structure at 735 Dundas Street is a one storey commercial building with a flat roof and has an exterior of red brick and concrete block. It was constructed in the mid-20th century. The structure is not rare, unique, an early example, or representative of a particular building style, type, expression, material or construction method. The structure utilized readily available and common building materials in the mid-20th century and does not display a high degree of craftsmanship or artistic merit. The building does not demonstrate a high degree of technical or scientific achievement.

Based on the above discussion, the commercial building at 735 Dundas Street does not meet the criteria of Section 1 of O. Reg. 9/06.

Historic/Associative Value

The structure at 735 Dundas Street is not directly associated with a theme, event, belief, person, activity, organization or institution that is significant to the community. The occupants of the building over the years have been several local businesses, none of which played a significant role in the community. The structure does not have the potential to yield information that contributes to an understanding of a community or culture and does not demonstrate the work of a particular architect.

Based on the above discussion, the commercial building at 735 Dundas Street does not meet the criteria of Section 2 of O. Reg. 9/06.

Contextual Value

The structure at 735 Dundas Street is set in a mixed streetscape that includes late 19th and early 20th century structures, vacant lots, mid-20th century structures, and structures of varying height and massing. The structures on this portion of Dundas Street do not form a street wall and besides being of between one to three storeys in height do not contribute to forming a cohesive character of the area. Therefore, the structure at 735 Dundas Street is not important to defining, maintaining, or supporting the character of the area. The structure at 735 Dundas Street is a typical mid-20th century commercial structure set in an area of mixed residential and commercial character of varying dates of construction, style, and massing. Therefore, the structure is not physically, functionally, visually, or historically linked to its surroundings. The structure is not particularly memorable or notable along Dundas Street and is not a landmark.

Based on the above discussion, the commercial building at 735 Dundas Street does not meet the criteria of Section 3 of O. Reg. 9/06.

Summary

Table 2 provided below summarizes the findings of the evaluation of CHVI.



HERITAGE IMPACT ASSESSMENT—719-737 DUNDAS STREET, LONDON, ONTARIO

Evaluation of Cultural Heritage Value or Interest
September 20, 2019

Table 2: Evaluation of 735 Dundas Street According to O. Reg. 9/06

Criteria of O. Reg. 9.06	Y/N	Comments
Design or Physical Value		
Is a rare, unique, representative or early example of a style, type, expression, material or construction method	No	The structure is a typical mid-20 th century commercial building and is not representative of a style, type, expression material or construction method.
Displays a high degree of craftsmanship or artistic merit	No	The structure is a typical mid-20 th century commercial building that utilizes common building materials and does not display a high degree of craftsmanship or artistic merit.
Demonstrates a high degree of technical or scientific achievement	No	The structure does not display a high degree of technical or scientific achievement.
Historical or Associative Value		
Has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community	No	The building has been occupied by various local businesses over the years. These businesses do not have direct associations with a theme, event, belief, person, activity, or organization that is significant to the community.
Yields, or has the potential to yield, information that contributes to an understanding of a community or culture	No	The structure does not provide evidence of notable or influential aspects of the community or contribute in a meaningful way to comparative analysis of similar properties. The structure does not yield information that contributes to an understanding of a community or culture.
Demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community	No	The architect of the structure is not known.
Contextual Value		
Is important in defining, maintaining or supporting the character of an area	No	The property is set on Dundas Street in an area of mixed commercial and residential character. The surrounding structures are of varying dates of construction, style, and massing. Therefore, the structure does not define, maintain, or support the character of an area.
Is physically, functionally, visually or historically linked to its surroundings	No	The structure is a typical mid-20 th century commercial structure set in an area of mixed residential and commercial character of varying dates of construction, style, and massing. Therefore, the structure is not physically, functionally, visually, or historically linked to its surroundings.
Is a landmark	No	The structure and property are not landmarks.

Based on the above discussion, the property at 735 Dundas Street does not meet any of the criteria of O. Reg. 9/06 and no heritage attributes were identified. Therefore, no statement of significance or identification of heritage attributes are included.



Evaluation of Cultural Heritage Value or Interest
September 20, 2019

5.2.3 729 Dundas Street

5.2.3.1 Evaluation

Design/Physical Value

The structure at 729 Dundas Street is a one storey commercial building with a flat roof and an exterior of red brick and stucco. It was constructed in the mid-20th century and the stucco cladding was likely added in a more recent renovation. The structure is not rare, unique, an early example, or representative of a particular building style, type, expression, material or construction method. The structure utilizes readily available and common building materials and does not display a high degree of craftsmanship or artistic merit. The building does not demonstrate a high degree of technical or scientific achievement.

Based on the above discussion, the commercial building at 729 Dundas Street does not meet the criteria of Section 1 of O. Reg. 9/06.

Historic/Associative Value

The structure at 729 Dundas Street is associated with U-Need-A Cab, which has operated from 729 Dundas Street since the 1970s. The company is one of several taxi businesses within the City of London and the company has not played a significant role in the community. The structure does not have the potential to yield information that contributes to an understanding of a community or culture and does not demonstrate the work of a particular architect.

Based on the above discussion, the commercial building at 729 Dundas Street does not meet the criteria of Section 2 of O. Reg. 9/06.

Contextual Value

The structure at 729 Dundas Street is set in a mixed streetscape that includes late 19th and early 20th century structures, vacant lots, mid-20th century structures, and structures of varying height and massing. The structures on this portion of Dundas Street do not form a street wall and besides being of between one to three storeys in height do not contribute to forming a cohesive character of the area. Therefore, the structure at 729 Dundas Street is not important to defining, maintaining, or supporting the character of the area. The structure at 729 Dundas Street is a typical mid-20th century commercial structure set in an area of mixed residential and commercial character of varying dates of construction, style, and massing. Therefore, the structure is not physically, functionally, visually, or historically linked to its surroundings. The structure is not particularly memorable or notable along Dundas Street and is not a landmark.

Based on the above discussion, the commercial building at 729 Dundas Street does not meet the criteria of Section 2 of O. Reg. 9/06.

Summary

Table 3 provided below summarizes the findings of the evaluation of CHVI.



HERITAGE IMPACT ASSESSMENT—719-737 DUNDAS STREET, LONDON, ONTARIO

Evaluation of Cultural Heritage Value or Interest
September 20, 2019

Table 3: Evaluation of 729 Dundas Street According to O. Reg. 9/06

Criteria of O. Reg. 9.06	Y/N	Comments
Design or Physical Value		
Is a rare, unique, representative or early example of a style, type, expression, material or construction method	No	The structure is a typical mid-20 th century commercial building and is not representative of a style, type, expression material or construction method.
Displays a high degree of craftsmanship or artistic merit	No	The structure is a typical mid-20 th century commercial building that utilizes common building materials and does not display a high degree of craftsmanship or artistic merit.
Demonstrates a high degree of technical or scientific achievement	No	The structure does not display a high degree of technical or scientific achievement.
Historical or Associative Value		
Has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community	No	The building has been occupied by various local businesses over the years. These businesses do not have direct associations with a theme, event, belief, person, activity, or organization that is significant to the community.
Yields, or has the potential to yield, information that contributes to an understanding of a community or culture	No	The structure does not provide evidence of notable or influential aspects of the community or contribute in a meaningful way to comparative analysis of similar properties. The structure does not yield information that contributes to an understanding of a community or culture.
Demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community	No	The architect of the structure is not known.
Contextual Value		
Is important in defining, maintaining or supporting the character of an area	No	The property is set on Dundas Street in an area of mixed commercial and residential character. The surrounding structures are of varying dates of construction, style, and massing. Therefore, the structure does not define, maintain, or support the character of an area.
Is physically, functionally, visually or historically linked to its surroundings	No	The structure is a typical mid-20 th century commercial structure set in an area of mixed residential and commercial character of varying dates of construction, style, and massing. Therefore, the structure is not physically, functionally, visually, or historically linked to its surroundings.
Is a landmark	No	The structure and property are not landmarks.

Based on the above discussion, the property at 729 Dundas Street does not meet any of the criteria of O. Reg. 9/06 and no heritage attributes were identified. Therefore, no statement of significance or identification of heritage attributes are included.

5.2.4 719-721 Dundas Street

The property at 719-721 Dundas Street is designated under Part IV of the OHA under By-law No. L.S.P. 3416-121 (City of London 2012). Therefore, the property was not evaluated under O. Reg. 9/06 and the heritage attributes of the property as defined within the designating bylaw are listed below:



HERITAGE IMPACT ASSESSMENT—719-737 DUNDAS STREET, LONDON, ONTARIO

Evaluation of Cultural Heritage Value or Interest
September 20, 2019

- A high-pitched hip roof matched on either side by two elaborately decorated gables each crowning a two storey projection
- A large, elaborately detailed roof dormer centred over the recess between the two projections
- An elaborately decorated shallow porch crowned by cast-iron cresting and supported by classical columns resting on stone block piers which serves to connect the two front projections and to shelter the front doors in the recess between the two projections thereby unifying the balanced elements of the design.
- Two ground story round headed windows in the front projections either side of the porch which are outlined by large rusticated stone block voussoirs, which each radiate from a central keystone.
- Rusticated stone block coursing which extends from the voussoirs and raps around to overlap a small length of the side façades.
- Rusticated stone labels over the windows of the second story; over the single windows either side of the first story doors for 719 and 721I and over the single windows set in the angled corners
- Rounded corners on the second story meet the earlier buff brick quoins of the side façade of 719 and the side façade wall of 721.
- Rounded corners are also present on the inside corners of the second story projections
- The corner quoins on the side façade of 719 where they meet the 1899 front façade; they provide evidence of the earlier structure
- Several stained glass windows; in the transoms over the two centre windows in the recess of the second story; in the semi-circular transom over the large first story window of 721; over the windows in the diagonally cut sections of the wall at the corners that angle away on both side of the first stories of the two front projections; over the door of 721 which incorporates the 721 address (the stained glass transoms over the large first story window and the door of 719 have been removed)



6.0 IMPACT ASSESSMENT

6.1 DESCRIPTION OF PROPOSED UNDERTAKING

The Proponent is proposing a new mixed-use 24 storey building at 737 Dundas Street, 735 Dundas Street, and 729 Dundas Street. The development will be located north of Phases 1 and 2 of the existing rental development “Revo” in Old East Village. This infill project will provide retail space along Dundas Street and include wide pedestrian sidewalks. As part of this development, the existing structures at 737 Dundas Street, 735 Dundas Street, and 729 Dundas Street will be removed.

Zedd Architecture has created a proposed layout for the development (Appendix A). The plan includes a residential tower with, composed of 24 bachelor units, 154 one-bedroom units (including one bedroom plus den units), and 100 two-bedroom units (including two bedrooms plus den units). The parking areas include 207 standard spaces, 36 tandem-compact spaces, and 16 compact spaces.

The new development includes design features in its concept plans that speak to design sensitivities related to the construction of a large new structure on the streetscape. Design features include:

- The inclusion of a podium with generous setbacks from the Dundas Street and Lyle Street frontages
- Design of the building that continues the established street line along both street frontages
- The inclusion of active ground floor uses (commercial along Dundas Street and lobby and amenity space along Lyle Street)
- The inclusion of units in the above the ground floor commercial spaces that screen the parking podium
- Location of parking within the structure (underground and within the podium)

6.2 ASSESSMENT OF IMPACTS

Table 4 provides an outline the potential impacts on 737 Dundas Street and 719-721 Dundas Street. Where impacts are anticipated, ‘A’ is listed in the column. Where there may be potential for indirect impacts, ‘P’ is listed in the column. Where no impacts to heritage attributes are anticipated, ‘N’ is listed in the column. Many of the impact categories are not applicable given the scope of the proposed undertaking and the position of the identified heritage attributes. Where this is the case, ‘N/A’ is entered in the table. Further discussion is found in subsequent sections.



Table 4: Potential Impacts to Identified Cultural Heritage Resources

Property	Potential for Direct Impact		Potential for Indirect Impact					Discussion
	Destruction	Alteration	Shadows	Isolation	Obstruction	Change in Land Use	Land Disturbances	
737 Dundas Street	A	N/A	N/A	N/A	N/A	N/A	N/A	<p>Direct Impacts are anticipated to 737 Dundas Street. These impacts are related to the proposed demolition of the structure as part of the proposed development. The heritage attributes identified are limited entirely to the structure and will be directly impacted by the destruction of the structure. As the single criterion of CHVI identified for the property is related to the structure itself, no impacts related to alteration are anticipated.</p> <p>Indirect impacts are not anticipated to 737 Dundas Street related to the proposed demolition of the structure. As the heritage attributes of the property are limited to the structure itself, the demolition will remove all heritage attributes. Therefore, the property will not be indirectly impacted by shadows, isolation, or obstruction.</p> <p>Therefore, mitigation measures must be prepared to mitigate direct impacts.</p>
719-721 Dundas Street	N	N	N	N	N	N	P	<p>The residence is situated directly adjacent to the proposed development. The position of identified heritage attributes within 50 metres of the project activities has the potential for indirect impacts resulting from land disturbance during construction activities.</p> <p>Therefore, measures must be prepared to mitigate potential indirect impacts.</p>

6.3 DISCUSSION OF IMPACTS

Both direct and indirect impacts are anticipated as a result of the proposed development. Direct impacts are anticipated to 737 Dundas Street as the structure will be removed as part of the new development on the property. Indirect impacts are anticipated to 719-721 Dundas Street, which is situated directly adjacent to the proposed development. As outlined in Section 2.4.2, while impacts of vibration on heritage buildings are not well understood, studies have shown that impacts may be perceptible in buildings 50 metres from project activities including demolition of the existing structures, road traffic, and construction of the new



HERITAGE IMPACT ASSESSMENT—719-737 DUNDAS STREET, LONDON, ONTARIO

Impact Assessment
September 20, 2019

development. If left unaddressed, these could result in longer-term issues for the maintenance, continued use, and conservation of the buildings.

While the proposed development is likely to cause shadows where they may not currently exist, shadow impacts are considered according to the MTCS criteria where they will alter a heritage attribute. In the case of adjacent properties, heritage attributes relate to building fabric, form, materials and architectural details. As shadowing on these attributes is not anticipated to be permanent, alteration or destruction of the attributes is not anticipated.

Views at the Study Area or the surrounding streetscape were not identified as heritage attributes through the evaluation component of this HIA. As such, significant views will not be obstructed by the proposed development.



7.0 MITIGATION, IMPLEMENTATION AND MONITORING

7.1 POTENTIAL MITIGATION MEASURES

The proposed development has the potential to result in indirect impacts to adjacent cultural heritage resources and, as such, mitigation measures are required. Table 5 lists proposed mitigation measures for potentially impacted cultural heritage resources identified in Section 6.3.

Table 5: Proposed Mitigation Measures

Address	Impact Identified	Proposed Mitigation Measure(s)
737 Dundas Street	Direct destruction impacts with the removal of the structure as part of the new development.	Documentation of existing conditions and salvage of historic materials.
719-721 Dundas Street	Potential land disturbances from vibration caused by construction activities.	Vibration monitoring during construction to prohibit adverse effects to this resource resulting from project related construction activities.

7.2 MITIGATION DISCUSSION

7.2.1 737 Dundas Street

The structure at 737 Dundas Street was determined to have CHVI as it meets one criterion set out in O. Reg. 9/06 of the OHA. Specifically, the CHVI of the property relates to its design value as an example as an Ontario vernacular residence with elements of the Queen Anne style representative of late 19th to early 20th century construction. As identified in Tables 4 and 5, the proposed development will have an adverse impact on the CHVI of this property as it will remove the heritage attributes which define its significance. Accordingly, three mitigation options are presented, including:

- Retention of the structure on site
- Relocation of the structure
- Removal

Consideration for each option is given both for the appropriateness of the mitigation in the context of the CHVI identified and the feasibility of the mitigation option. Also considered is an understanding of the surrounding context within which the property is located.

7.2.1.1 Retention

Generally, retention *in situ* is the preferred option when addressing any structure where CHVI has been identified, even if limited. However, the benefits of retaining a structure, or structures, must be balanced with site-specific considerations. The heritage integrity and level of CHVI must be considered, as well as the structural condition of the cultural heritage resource, the site development plan and the context within which the structure, or structures, would be retained.



Mitigation, Implementation and Monitoring
September 20, 2019

In the case of 737 Dundas Street, retention of the structure does not appear feasible given the proposed undertaking. While retention is always the preferred mitigation option, the structure would remain isolated in a different environment, surrounded by high rises to the south and west. Retention must also be considered from not just a viability perspective, but also from a merit perspective. The structure at 737 Dundas Street has been extensively modified. Although the structure retains design value, according to the *City of London Inventory of Heritage Resources*, the Queen Anne style is common in the City of London. Within the City of London, there are examples of vernacular residences with Queen Anne influence that retain more design value and have not been heavily modified. Accordingly, other mitigation options are explored in the following sections.

7.2.1.2 Relocation

Where retention *in situ* is not feasible or preferred, relocation is often the next option considered to mitigate the loss of cultural heritage resources. As with retention, relocation of a structure or structures must be balanced with the CHVI identified. Relocation removes the resource from its contextual setting but allows for the preservation of noteworthy heritage attributes, particularly those identified to be of design or physical value, to be retained in a different context.

In the case of 737 Dundas Street it may be possible to move the structure, but it would remove it from its historic context and disrupt the understanding of the building within the larger community. The residence has also been extensively modified and the additions have not been identified to contain design value. Relocation of the structure would include removing the additions. It is unclear how much of the original façades remain and if removal of the additions would affect the integrity of the original structure. Therefore, relocation is not a preferred mitigative alternative for the site.

7.2.1.3 Removal

Removal of a cultural heritage resource is typically considered when the alternative development approaches outlined previously, are determined not to be feasible. Additional mitigation measures must accompany removal of a cultural heritage resource.

Detailed documentation of existing conditions and salvage of historic materials is often the preferred mitigation strategy where retention or relocation is not feasible or warranted. Documentation creates a public record of the property and structure which provides researchers and the general public with a land use history, construction details, and photographic record of the resource. Through the selective salvage of identified heritage attributes and other materials, the CHVI of the property can be retained, if in a different context. Documentation and salvage acknowledges the heritage attributes in their current context and, where feasible, allows for reuse or commemoration.

While removal of the site heritage attributes is not a preferred alternative, mitigation with documentation and salvage is an appropriate strategy if retention *in situ*, and relocation are not feasible options. In the case of 737 Dundas Street, retention *in situ* and relocation were considered not to be feasible or warranted given the CHVI identified in the context of the proposed development. Documentation would allow for the exterior and interior of the structure to be fully recorded and supplement the information



Mitigation, Implementation and Monitoring
September 20, 2019

contained within the HIA, resulting in a full record of the structure, its history, evolution, and local context. Salvage would allow for retention of some heritage attributes of the Queen Anne design of the structure to be possibly integrated into the proposed development in a commemorative way or be made available to the City.

7.2.2 719-721 Dundas Street

As 719-721 Dundas Street is situated directly adjacent to the proposed development with demolition and construction activities occurring within five metres of the residence and identified heritage attributes indirect vibration impacts are anticipated. Where construction activities are anticipated within close proximity to cultural heritage resources, monitoring activities can gauge whether construction activities exceed maximum acceptable vibration levels, or peak particle velocity levels, as determined by a qualified engineer. A typical approach to mitigating the potential for vibration effects is twofold. First, a pre-demolition vibration assessment can be completed to determine acceptable levels of vibration given the site-specific conditions (including soil conditions, equipment proposed to be used, and building characteristics). Second, depending on the outcome of the assessment, further action may be required in the form of site plan controls, site activity monitoring, or avoidance. For the purposes of this HIA, completing a pre-demolition vibration assessment will determine the need for additional assessment which should be considered prior to any site activity.



Recommendations
September 20, 2019

8.0 RECOMMENDATIONS

An assessment of impacts resulting from the redevelopment of 729-737 Dundas Street has determined that the proposed development would result in direct impacts to 737 Dundas Street through demolition and potential indirect impacts to 719-721 Dundas Street through its adjacency to the development.

Based on the impacts identified to these cultural heritage resources and the proposed undertaking, the following mitigation measures are recommended:

737 Dundas Street:

- Should retention of the outbuilding not be considered a feasible alternative due to site constraints associated with the emergency access road, documentation and salvage should be undertaken prior to any change in site conditions
 - Documentation in the form of detailed photography should be completed under the direction of a heritage professional in good standing with the Canadian Association of Heritage Professionals
 - Salvage activities should consist of the identification and recovery of re-useable materials by a reputable salvage company or charity
 - The results of the documentation and salvage activities should be made available at local libraries for public consumption

719-722 Dundas Street:

- A pre-demolition vibration assessment should be completed to establish a baseline for vibration levels in advance of demolition activities
- Should any properties within the study area be determined to be within the zone of influence, additional steps should be taken to secure the buildings from experiencing negative vibration effects (i.e. adjustment of machinery or establishment of buffer zones)

8.1 DEPOSIT COPIES

In order to further retention of historic information, copies of this report should be deposited with a local repository of historic material. Therefore, it is recommended that this report be deposited at the following location.

London Public Library Ivey Family London Room
251 Dundas Street
London, Ontario N6A 6H9



Closing
September 20, 2019

9.0 CLOSING

This report has been prepared for the sole benefit of Medallion Corporation and may not be used by any third party without the express written consent of Stantec Consulting Ltd. Any use which a third party makes of this report is the responsibility of such third party.

We trust this report meets your current requirements. Please do not hesitate to contact us should you require further information or have additional questions about any facet of this report.

Yours truly,

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September 20, 2019

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HERITAGE IMPACT ASSESSMENT—719-737 DUNDAS STREET, LONDON, ONTARIO

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September 20, 2019

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HERITAGE IMPACT ASSESSMENT—719-737 DUNDAS STREET, LONDON, ONTARIO

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September 20, 2019

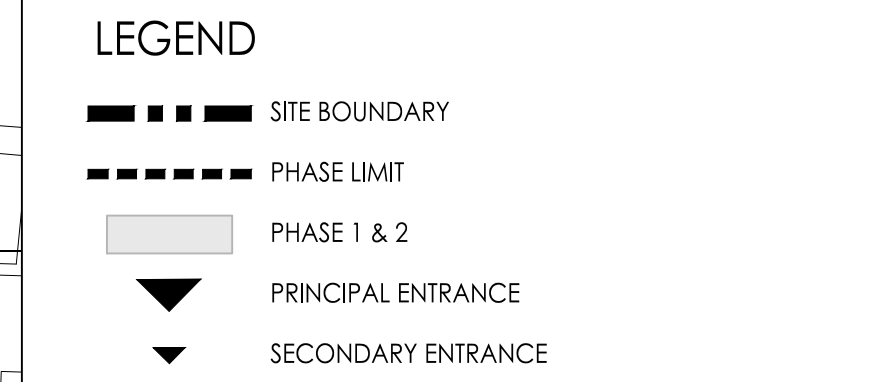
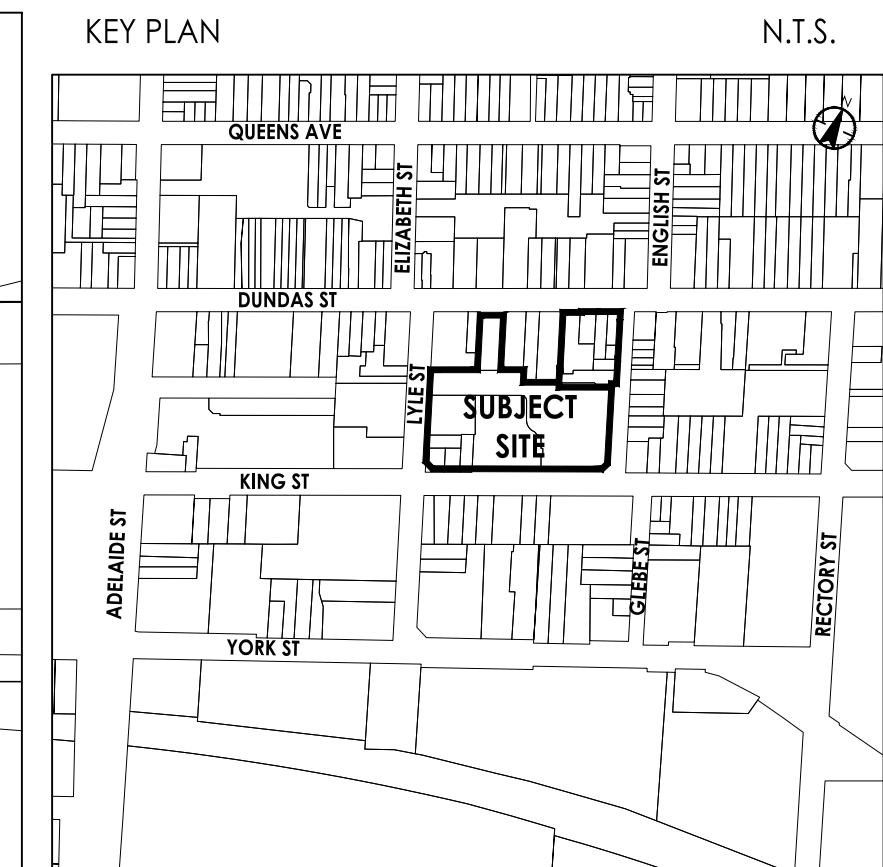
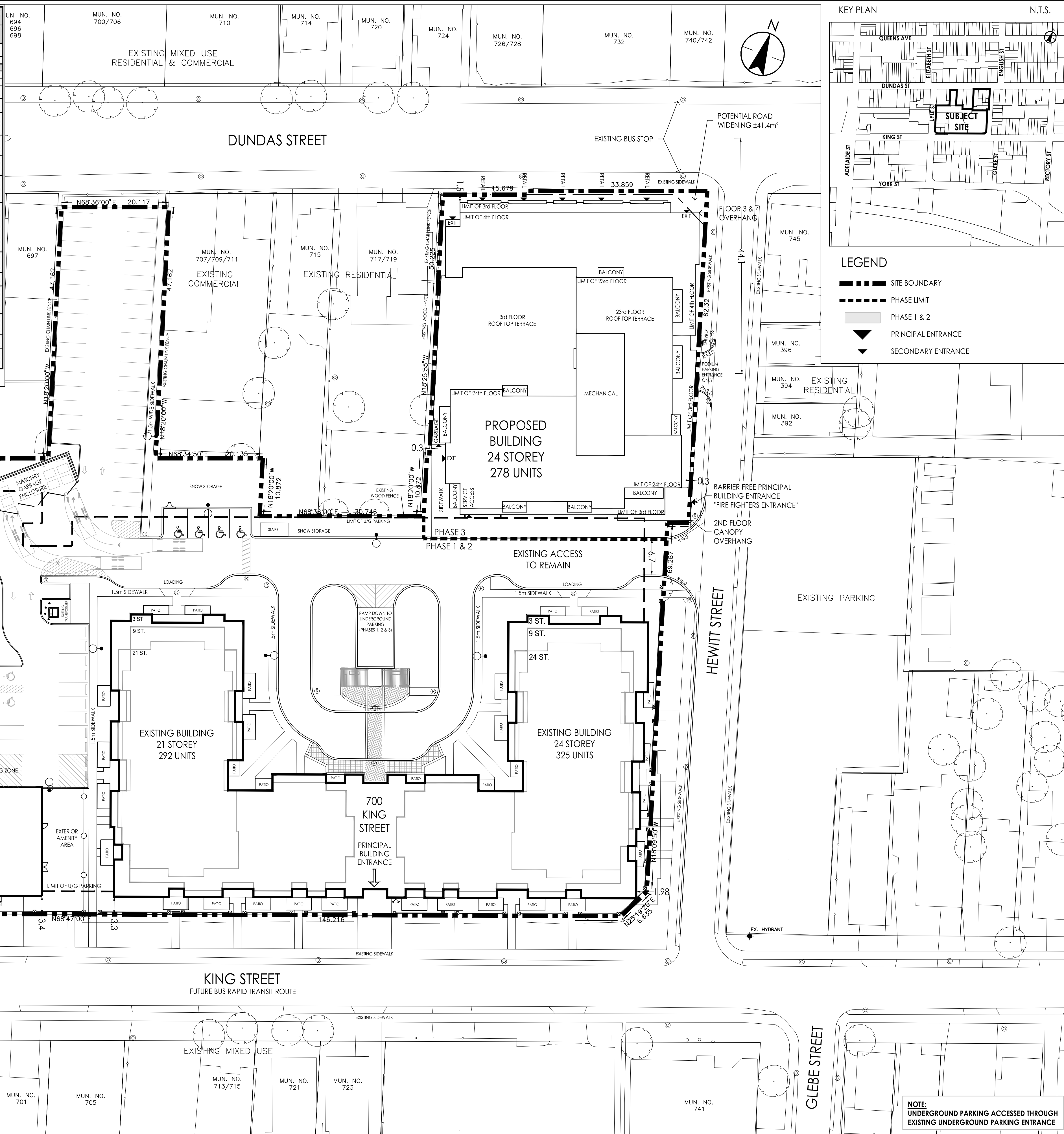
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Appendix A

Proposed Drawings

EXISTING ZONE	Existing Phase I & II Zoning By-law Z-1		Proposed Phase III Zoning By-law Z-1		Total Proposed (Combined)		Special Provisions Requested (Combined)	
	700 King St. & 400 Lyle St. (Approved Site Plan)	729, 735, 737 Dundas St. & 393 Hewitt St. (Fourth Floor)	700 King St. & 400 Lyle St. & Phase III	700 King St. & 400 Lyle St. & Phase III	700 King St. & 400 Lyle St. & Phase III	700 King St. & 400 Lyle St. & Phase III	700 King St. & 400 Lyle St. & Phase III	700 King St. & 400 Lyle St. & Phase III
8DC(24)/D160/H36/R-32	8DC(24)/D160/H36/R-32	8DC(24)/D160/H36/R-32	8DC(24)/D160/H36/R-32	8DC(24)/D160/H36/R-32	8DC(24)/D160/H36/R-32	8DC(24)/D160/H36/R-32	8DC(24)/D160/H36/R-32	8DC(24)/D160/H36/R-32
1,551 ha	1,551 ha	1,551 ha	1,551 ha	1,551 ha	1,551 ha	1,551 ha	1,551 ha	1,551 ha
796 Units	796 Units	796 Units	796 Units	796 Units	796 Units	796 Units	796 Units	796 Units
Apartment Building	Apartment Building	Apartment Building/Commercial	Apartment Building/Commercial	Apartment Building/Commercial	Apartment Building/Commercial	Apartment Building/Commercial	Apartment Building/Commercial	Apartment Building/Commercial
8.0m	146.2 m = King St	49.4 m = Dundas St	49.4 m = Dundas St	49.4 m = Dundas St	49.4 m = Dundas St	49.4 m = Dundas St	49.4 m = Dundas St	49.4 m = Dundas St
4.5m MAX (2-1-152397)	n/a = Dundas St	1.5 m = Dundas St	1.5 m = Dundas St	1.5 m = Dundas St	1.5 m = Dundas St	1.5 m = Dundas St	1.5 m = Dundas St	1.5 m = Dundas St
0.3m MIN (2-1-152397)	3.3 m = King St	0.3 m = Hewitt St	0.3 m = Hewitt St	0.3 m = Hewitt St	0.3 m = Hewitt St	0.3 m = Hewitt St	0.3 m = Hewitt St	0.3 m = Hewitt St
See Below	See Below	See Below	See Below	See Below	See Below	See Below	See Below	See Below
3.0 m plus 1.2 m for each 3.0 m of building height or part thereof above the first 3.0 m.	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
0 m MIN	n/a	0.3 m	0.3 m	0.3 m	0.3 m	0.3 m	0.3 m	0.3 m
70%	36.1% (5,600 m ²)	2,784 m ² Additional Coverage	50.7%	50.7%	50.7%	50.7%	50.7%	50.7%
80.0 m (2-1-152397)	80.0 m	78.2 m	80.0 m	80.0 m	80.0 m	80.0 m	80.0 m	80.0 m
400 uph (2-1-152397)	590 uph	278 Additional Residential Units	728 uph**	728 uph**	728 uph**	728 uph**	728 uph**	728 uph**
555 Spaces (2-1-152397)	73 = Surface 485 = Underground	25 = Surface 259 = Underground/Podium	45 = Surface 750 = Underground/Podium	45 = Surface 750 = Underground/Podium	45 = Surface 750 = Underground/Podium	45 = Surface 750 = Underground/Podium	45 = Surface 750 = Underground/Podium	45 = Surface 750 = Underground/Podium
280 Spaces (2-1-152397)	280 Spaces	85 Spaces	365 Spaces**	365 Spaces**	365 Spaces**	365 Spaces**	365 Spaces**	365 Spaces**
1.5m Provided that the projection is no closer than 1.5m to the lot line.	1.5m Provided that the projection is no closer than 1.5m to the lot line.	1.5m Provided that the projection is no closer than 1.5m to the lot line.	1.5m Provided that the projection is no closer than 1.5m to the lot line.	1.5m Provided that the projection is no closer than 1.5m to the lot line.	1.5m Provided that the projection is no closer than 1.5m to the lot line.	1.5m Provided that the projection is no closer than 1.5m to the lot line.	1.5m Provided that the projection is no closer than 1.5m to the lot line.	1.5m Provided that the projection is no closer than 1.5m to the lot line.



Revision	By	Appd.	YY.MM.DD
1. ZOR ZBA APPLICATION Issued	AB	BB	19.06.19
File Name: 161413817.dwg	AB	BB	19.04.18
Permit-Seal	Dwn.	Chkd.	AB YY.MM.DD

Client/Project
East Village Holdings Limited

Phase 3 - 400 Lyle St & 700 King St
729, 735, 737 Dundas St & 393 Hewitt St
London, ON Canada

Title
Conceptual Site Plan
October 18, 2019

Project No. 161413817
Drawing No. 1

Scale
HORZ - 1 : 400
4 0 8m

Sheet 1 of 1
Revision 0

Medallion Residential Tower

Dundas - Hewitt

18-023

2019.08.16



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Residential Units (Total)							
L2 - L24 Floor Levels							
Level	No Of Floors	Unit Type					Total No Of Units
		STUD	1BR	1BR+D	2BR	2BR+D	
L2	1	1	3	0	0	1	5
L3	1	1	3	0	0	1	5
L4	1	2	9	1	3	1	16
L5-22	18	1x18	6x18	1x18	4x18	1x18	13x18 = 234
L23-24	2	1x2	5x2	1x2	1x2	1x2	9x2 = 18
Grand Total	23	24	133	21	77	23	278

Total Floor Area				
P2 - L24 Floor Levels				
Level	No Of Floors	Total Floor Area-SQ.FT.	Type	Comment
P2	1	27,390.00	Parking	
P1	1	27,390.00	Parking	
L1	1	26,298.00	Retail/ Services	Including 9,366 SF Retail
L2	1	29,051.00	Prking/ Residential	Including 6,348SF Residential
L3	1	29,051.00	Prking/ Residential	Including 6,348SF Residential
L4	1	19,432.00	Residential/ Rooftop	Residential: 5,832.00 Sq.Ft. Residential(Tower): 11,265.00 Sq.Ft. Amenity Area: 2,335.00 Sq.Ft.
L5-22	18	239,706.00	Residential	
L23-24	2	17,206.00	Residential	
Grand Total	26	379,273.00		

Parking Spaces In P1 & P2:

- a)Standard: 102 Spaces(51/Floor)
- b)Tandem: 24 Spaces(12/Floor)
- c)Compact: 8 Spaces (4/Floor)

Parking Spaces In L1:

- a)Standard: 9 Spaces

Parking Spaces In L2 & L3:

- a)Standard: 96 Spaces(L2:47 & L3:49)
- b)Tandem-Compact: 12 Spaces(5/Floor)
- c)Compact: 8 Spaces (4/Floor)

Total Number Of Parking Spaces In All Floors:

- a)Standards: 207 Spaces
- b)Tandem: 36 Spaces
- c)Compact: 16Spaces

Scale : As indicated

18-023

Medallion Residential Tower

Dundas - Hewitt

Project Statistics

2019.06.06

SK-001

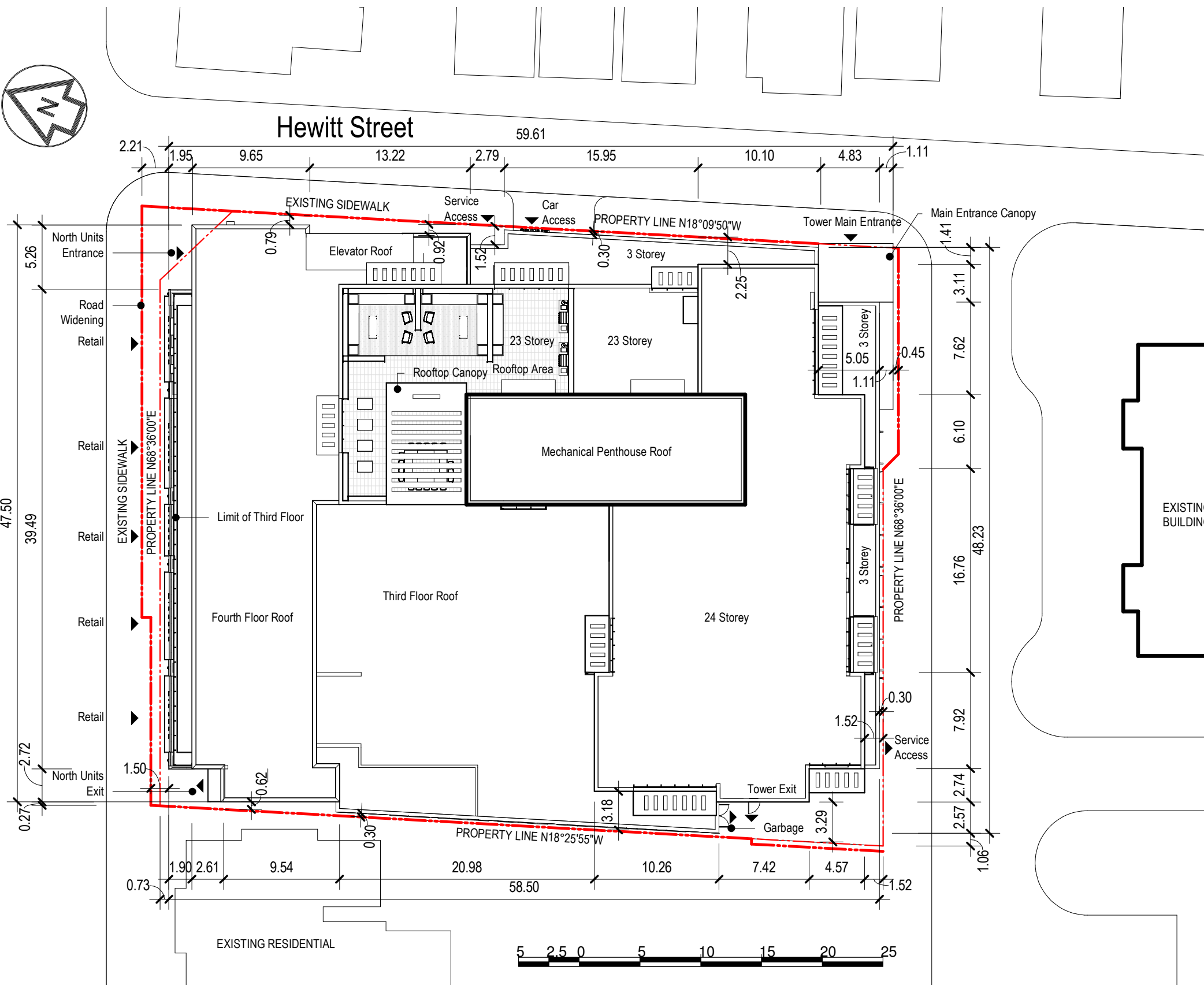
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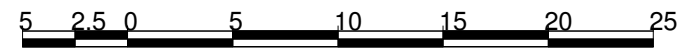


Key Map

Dundas Street
47.50
39.49



EXISTING BUILDING



PROPOSED PHASE III ZONING BY-LAW Z-1 729,735,737 Dundas Street. & 393 Hewitt Street - Fourth Tower Existing Zone: BDC(19) D250 H46 & BDC(24) D160 H36 B-32		
Proposed Zone: BDC(24) D160 H36 B-{ }		
ZONING	REQ'D	PROPOSED
SITE (LOT) AREA:(min.)	- m ²	0.120ha=Additional Area
No Of UNITS		278 UNITS & 875m ² COMMERCIAL
PERMITTED USES	APARTMENT	APARTMENT
LOT FRONTAGE (min.)	8.0 m	49.4m=DUNDAS ST
FRONT YARD (min.)	4.5m MAX *(Z-1-152397)	1.5m = DUNDAS ST
SETBACK FROM HEWITT ST	0 m MIN *(Z-1-152397)	0.30 m=HEWITT ST
EXTERIOR YARD E (min.)	0 m	0.30 m
INTERIOR SIDE & REAR YARD DEPTH MIN.	SEE BELOW	SEE BELOW
ABUTTING A NON RESIDENTIAL ZONE	0 m MIN	0.30 m
EXTERIOR YARD S (min.)	0 m	0.30 m
FRONT YARD DEPTH (min.)	39.4m	62.27m
LOT COVERAGE (max.)	70%	2.784m ² ADDITIONAL COVERAGE
BUILDING HEIGHT (max.)	80.0m *(Z-1-152397)	78.2m
OFF-STREET PARKING:		28=SURFACE REMOVED 259=UNDERGROUND/PODIUM 231=TOTAL NET SPACES
BICYCLE PARKING	SECURED =280 *(Z-1-152397)	85 SPACES
BALCONY ENCROACHMENT IN ALL YARDS	1.5m PROVIDED THAT THE PROJECTION IS NO CLOSER THAN 1.5m TO THE LOT LINE	1.5m PROVIDED THAT THE PROJECTION IS NO CLOSER THAN 1.5m TO THE LOT LINE

Scale : As indicated

18-023 Medallion Residential Tower

Dundas - Hewitt

Site Plan

2019.06.06 SK-002

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2019-10-16 10:19:07 AM

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Total P1 Level Floor Area: 27,390.00 Sq.Ft. (Each Floor)
Total P1&P2 Area: 54,780.00 Sq.Ft.
Parking Spaces In P1 & P2:
 a)Standard: 102 Spaces(51/Floor)
 b)Tandem: 24 Spaces(12/Floor)
 c)Compact: 8 Spaces (4/Floor)

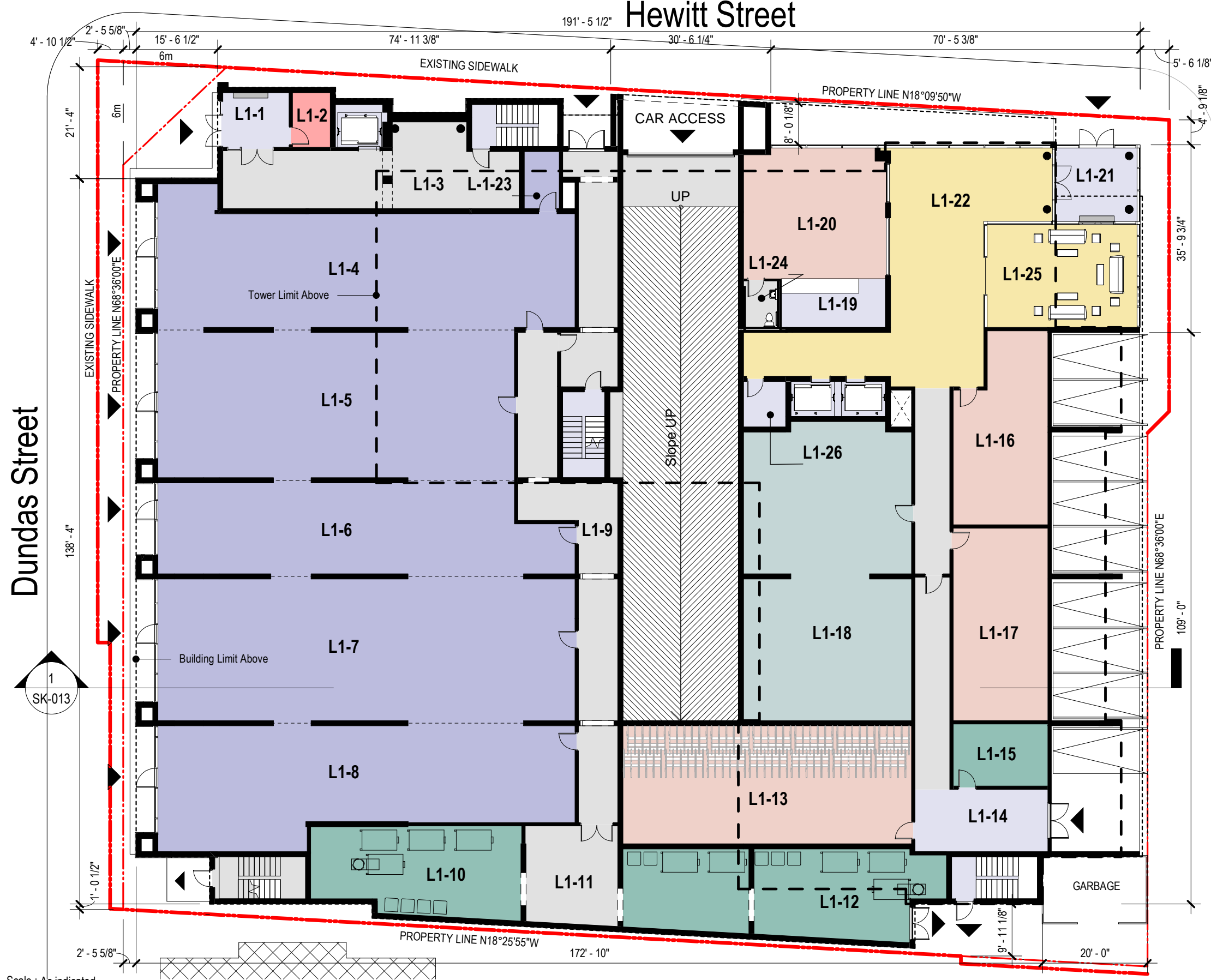
P1 & P2-Space Legend

Sym	Room	Description
[Green Box]	P1-1	M&E 145.00 SQ.FT
[Purple Box]	P1-2	Stair Vestibule 66.00 SQ.FT
[Light Purple Box]	P1-3	Lobby 398.00 SQ.FT
[Green Box]	P1-4	M&E 465.00 SQ.FT
[Light Purple Box]	P1-5	Stair Vestibule 100.00 SQ.FT
[Green Box]	P1-6	M&E 323.00 SQ.FT
[Light Purple Box]	P1-7	Electrical RM 90.00 SQ.FT
[Purple Box]	P1-8	Elevator Lobby 156.00 SQ.FT
[Light Purple Box]	P1-9	Elevator Vestibule 40.00 SQ.FT
[Light Purple Box]	P1-10	Elevator Machine Room 40.00 SQ.FT
[Light Purple Box]	P1-11	Stair Vestibule 30.00 SQ.FT

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Hewitt Street



Total First Floor Area : 26,298.00 Sq.Ft.
(Including 9,366.00 sq.ft Retail Spaces)
Parking Spaces:
a)Standard: 9 Spaces

Level 1-Space Legend					
Sym.	Room	Description	Sym.	Room	Description
[Light Blue]	L1-1	Vestibule 134.00 SQ.FT	[Light Blue]	L1-14	Moving Lobby 300.00SQ.FT
[Red]	L1-2	Mail 75.00SQ.FT	[Green]	L1-15	Temp. Moving Waste 215.00 SQ.FT
[Light Grey]	L1-3	Lobby/ Lounge 695.00 SQ.FT	[Light Orange]	L1-16	Laundry 594.00 SQ.FT
[Purple]	L1-4	Retail 1,895.00 SQ.FT	[Light Orange]	L1-17	GYM 662.00 SQ.FT
[Light Blue]	L1-5	Retail 2,011.00 SQ.FT	[Light Green]	L1-18	M&E 1,692.00 SQ.FT
[Light Blue]	L1-6	Retail 1,382.00 SQ.FT	[Light Blue]	L1-19	Kitchenette 183.00 SQ.FT
[Light Blue]	L1-7	Retail 2,286.00 SQ.FT	[Red]	L1-20	Multipurpose 668.00 SQ.FT
[Light Blue]	L1-8	Retail 1,720.00SQ.FT	[Light Blue]	L1-21	Vestibule 232.00 SQ.FT
[Light Grey]	L1-9	Service Corridor	[Yellow]	L1-22	Lobby 1,245.00 SQ.FT
[Green]	L1-10	Garbage 650.00 SQ.FT	[Purple]	L1-23	Storage 72.00 SQ.FT
[Light Grey]	L1-11	Garbage Lobby 327.00 SQ.FT	[Light Green]	L1-24	Washroom 53.00 SQ.FT
[Green]	L1-12	Garbage 872.00 SQ.FT	[Yellow]	L1-25	Lounge 580.00 SQ.FT
[Light Orange]	L1-13	Bike Storage 1,238.00 SQ.FT	[Light Blue]	L1-26	Storage 70.00 SQ.FT

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Second & Third Floor Levels:
Residential: 6,348.00 Sq.Ft. (Each Floor)
Parking: 22,703.00 Sq.Ft. (Each Floor)
Total Floor Area : 29,051.00 Sq.Ft. (Each Floor)
Total Area: 58,102.00 Sq.Ft. (Total 2 Floors)
Parking Spaces In L2 & L3:
 a)Standard: 96 Spaces(L2:47 & L3:49)
 b)Tandem-Compact: 12 Spaces(6/Floor)
 c)Compact: 8 Spaces (4/Floor)

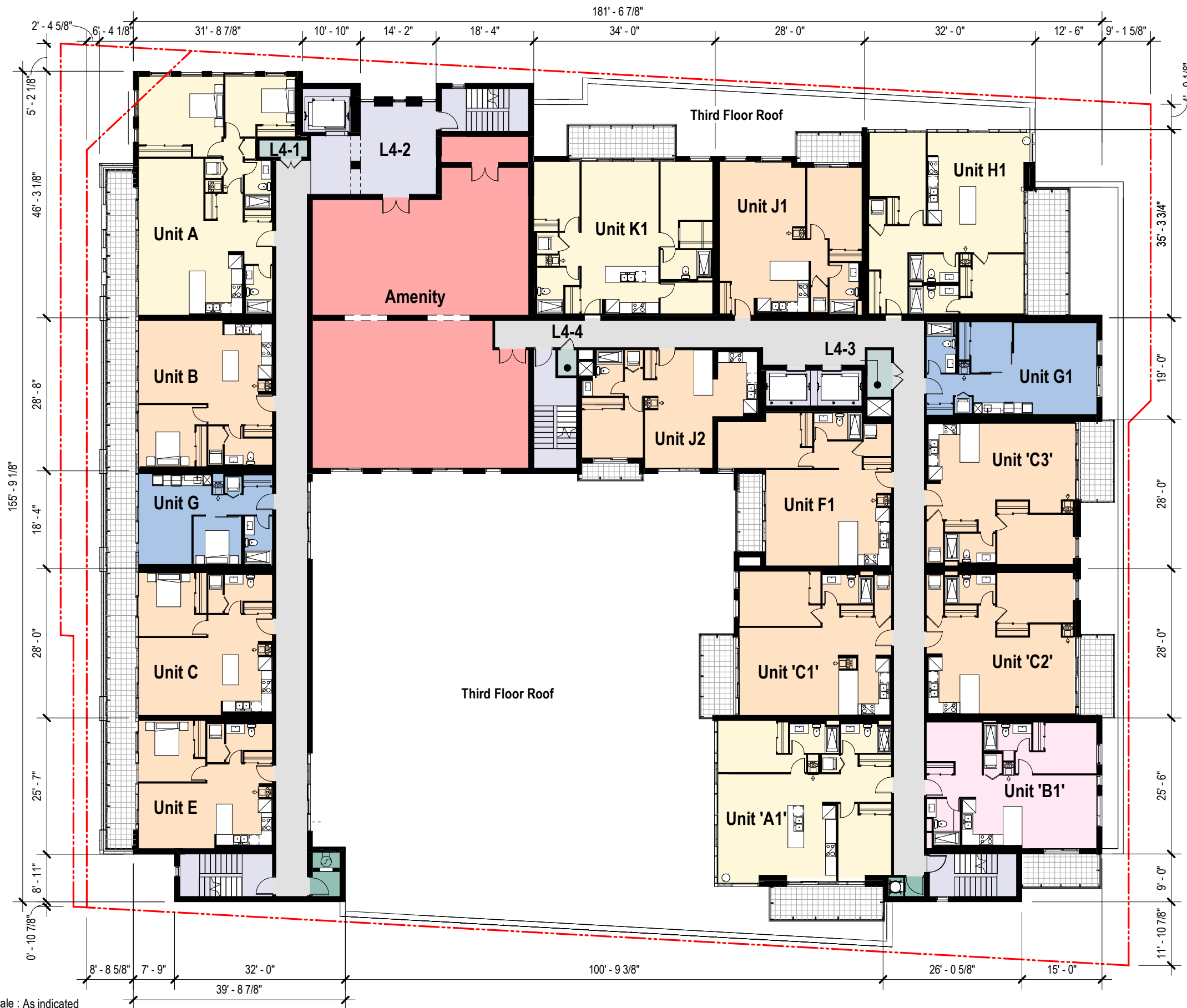
Total Number of Units (2nd &3rd): 10 Units (5/ Floor)
(1, 2BR+Den, 3, 1 BR & 1, Studio Each Floor)

Levels 2&3-Space Legend					
Sym.	Room	Description	Sym.	Room	Description
[Yellow]	Unit A	2-BR +Den 977.00SQ.FT	[Light Blue]	L2-3	Vestibule 30.00SQ.FT
[Orange]	Unit B	1-BR 680.00SQ.FT	[Light Blue]	L2-4	Vestibule 66.00SQ.FT
	Unit C	1-BR 671.00 SQ.FT	[Light Green]	L2-5	Services 113.00 SQ.FT
	Unit E	1-BR 596.00 SQ.FT		L2-6	Laundry 487.00 SQ.FT
[Blue]	Unit G	Studio 428.00 SQ.FT	[Light Blue]	L2-7	Vestibule 50.00 SQ.FT
[Light Green]	L2-1	Electrical 28.00 SQ.FT	[Light Blue]	L2-8	Elevator Lobby 155.00 SQ.FT
[Light Blue]	L2-2	Elev. Lobby 335.00 SQ.FT	[Light Blue]	L2-9	Electrical 41.00 SQ.FT
			[Light Green]	L2-10	Services 70.00 SQ.FT



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Scale : As indicated



4th Floor :
Residential: 5,832.00 Sq.Ft.
Residential(Tower): 11,265.00 Sq.Ft.
Amenity Area: 2,335.00 Sq.Ft.
Total Area: 19,432.00 Sq.Ft.
Total Number of Units (4th Floor): 16 Units
(1, 2BR+Den, 3, 2BR, 1, 1BR+Den, 9, 1BR & 2 Studio)

Level 4-Space Legend					
Sym	Room	Description	Sym	Room	Description
[Yellow]	Unit A	2-BR +Den 1,156.00SQ.FT	[Light Orange]	Unit C3	1-BR 759.00 SQ.FT
[Light Orange]	Unit B	1-BR 700.00SQ.FT	[Light Orange]	Unit F1	1-BR 748.00 SQ.FT
[Light Orange]	Unit C	1-BR 684.00 SQ.FT	[Blue]	Unit G1	Studio 577.00 SQ.FT
[Light Orange]	Unit E	1-BR 607.00 SQ.FT	[Yellow]	Unit H1	2-BR 975.00SQ.FT
[Blue]	Unit G	Studio 436.00 SQ.FT	[Light Orange]	Unit J1	1-BR 750.00 SQ.FT
[Pink]	Amen.	Amenity 2,335.00SQ.FT	[Light Orange]	Unit J2	1-BR 620.00 SQ.FT
[Yellow]	Unit A1	2-BR 960.00 SQ.FT	[Yellow]	Unit K1	2-BR 947.00 SQ.FT
[Light Orange]	Unit B1	1-BR +Den 735.00 SQ.FT	[Light Blue]	L4-1	Electrical 28.00 SQ.FT
[Light Orange]	Unit C1	1-BR 759.00 SQ.FT	[Light Purple]	L4-2	Lobby 334.00 SQ.FT
[Light Orange]	Unit C2	1-BR 759.00 SQ.FT	[Light Blue]	L4-3	Electrical 40.00 SQ.FT
			[Light Blue]	L4-4	Electrical 20.00 SQ.FT



Scale : As indicated

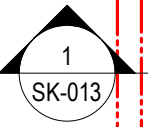
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5th Floor :
Residential(Tower): 13,317.00 Sq.Ft.
Total Number of Units (5th Floor): 13 Units
(1, 2BR+Den, 4, 2BR, 1, 1BR+Den, 6, 1BR & 1 Studio)

L5-Space Legend					
Sym	Room	Description	Sym	Room	Description
[Yellow]	Unit A1	2-BR 960.00 SQ.FT	[Yellow]	Unit H1	2-BR 975.00SQ.FT
[Pink]	Unit B1	1-BR +Den 735.00 SQ.FT	[Orange]	Unit J1	1-BR 750.00 SQ.FT
[Orange]	Unit C1	1-BR 759.00 SQ.FT	[Orange]	Unit J2	1-BR 620.00 SQ.FT
[Orange]	Unit C2	1-BR 759.00 SQ.FT	[Yellow]	Unit K	2-BR 915.00 SQ.FT
[Orange]	Unit C3	1-BR 759.00 SQ.FT	[Yellow]	Unit K1	2-BR 947.00 SQ.FT
[Orange]	Unit F1	1-BR 748.00 SQ.FT	[Yellow]	Unit L1	2-BR 860.00 SQ.FT
[Blue]	Unit G1	Studio 577.00 SQ.FT	[Grey]	L5-1	Electrical 40.00 SQ.FT
			[Grey]	L5-2	Electrical 20.00 SQ.FT

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SK-013



Typical Floor (6th-22nd) :
Residential(Tower): 13,317.00 Sq.Ft. (Each Floor)
Total Area: 226,389.00 Sq.Ft. (Total: 17 Floors)

Total Number of Units (6th to 22nd Floor):
13 Units x 17 = 221 Units(Total: 17 Floors)
(1, 2BR+Den, 4, 2BR, 1, 1BR+Den, 6, 1BR & 1 Studio
Each Floor)
Total :(17, 2BR+Den, 68, 2BR, 17, 1BR+Den, 102, 1BR
& 17 Studio in 17 Floors)

Typical (6 to 22)-Space Legend

Room	Description	Sym.	Room	Description
Unit A1	2-BR 960.00 SQ.FT	[Yellow Box]	Unit H1	2-BR 975.00SQ.FT
Unit B1	1-BR +Den 735.00 SQ.FT	[Light Pink Box]	Unit J1	1-BR 750.00 SQ.FT
Unit C1	1-BR 759.00 SQ.FT	[Light Orange Box]	Unit J2	1-BR 664.00 SQ.FT
Unit C2	1-BR 759.00 SQ.FT	[Light Orange Box]	Unit K	2-BR 915.00 SQ.FT
Unit C3	1-BR 759.00 SQ.FT	[Light Orange Box]	Unit K1	2-BR 947.00 SQ.FT
Unit F1	1-BR 748.00 SQ.FT	[Light Orange Box]	Unit L1	2-BR 860.00 SQ.FT
Unit G1	Studio 577.00 SQ.FT	[Blue Box]	L6-1	Electrical 40.00 SQ.FT
			L6-2	Electrical 20.00 SQ.FT

Scale : As indicated

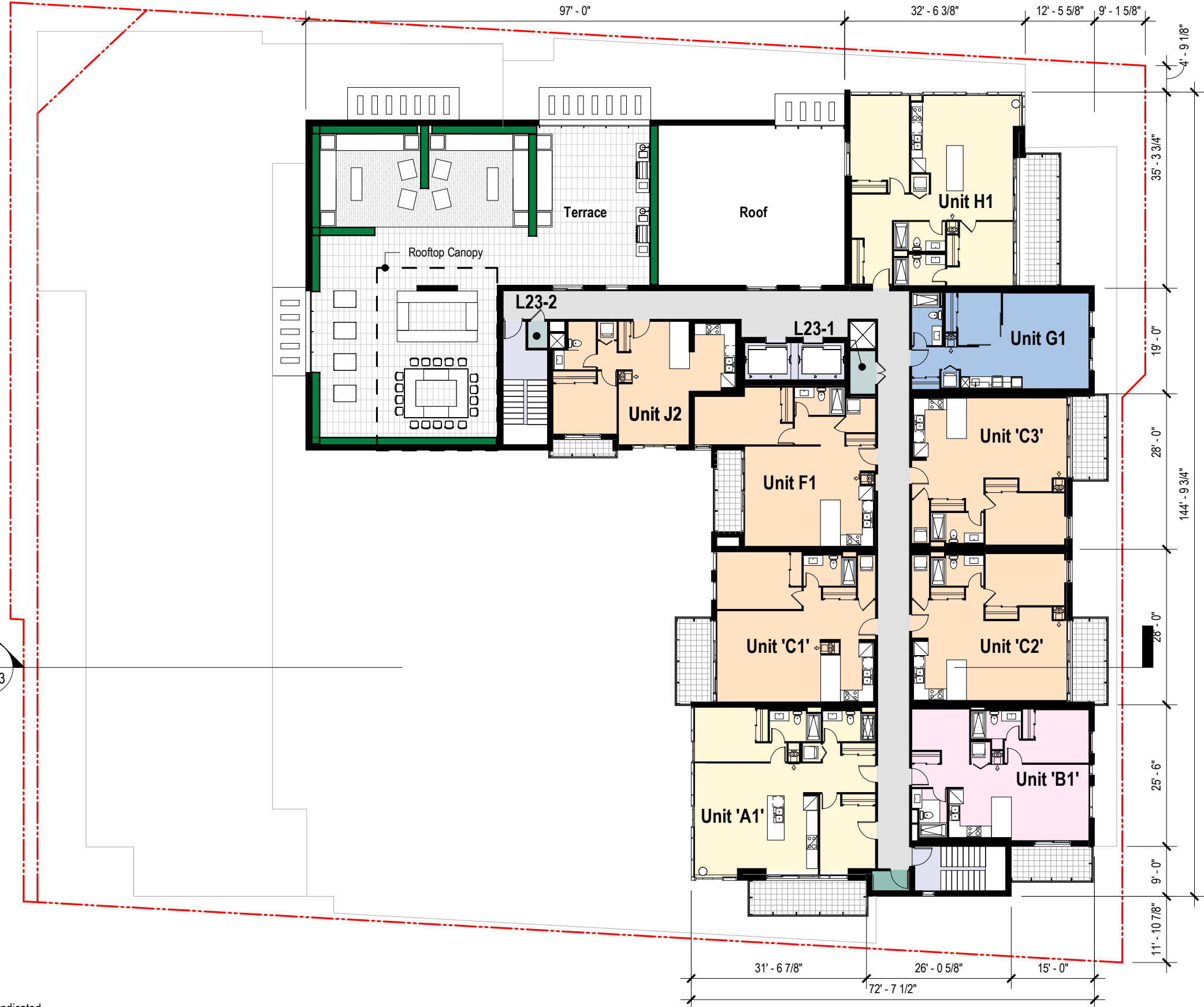
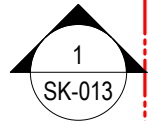


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Typical Floor (23rd-24th) :
Residential(Tower): 9,502.00 Sq.Ft. (Each Floor)
Total Area: 19,004.00 Sq.Ft. (Total: 2 Floors)
Total Number of Units (23rd to 24th Floor):
9 Units x 2 = 18 Units
(1, 2BR+Den, 1, 2BR, 1, 1BR+Den, 5, 1BR, & 1 Studio
Each Floor)
Total: (2, 2BR+Den, 2, 2BR, 2, 1BR+Den, 10, 1BR & 2
Studio)

Typical (23 to 24)-Space Legend

	Room	Description	Sym.	Room	Description
	Unit A1	2-BR 960.00 SQ.FT		Unit G1	Studio 577.00 SQ.FT
	Unit B1	1-BR +Den 735.00 SQ.FT		Unit H1	2-BR 975.00SQ.FT
	Unit C1	1-BR 759.00 SQ.FT		Unit J2	1-BR 664.00 SQ.FT
	Unit C2	1-BR 759.00 SQ.FT		L6-1	Electrical 40.00 SQ.FT
	Unit C3	1-BR 759.00 SQ.FT		L6-2	Electrical 20.00 SQ.FT
	Unit F1	1-BR 748.00 SQ.FT			



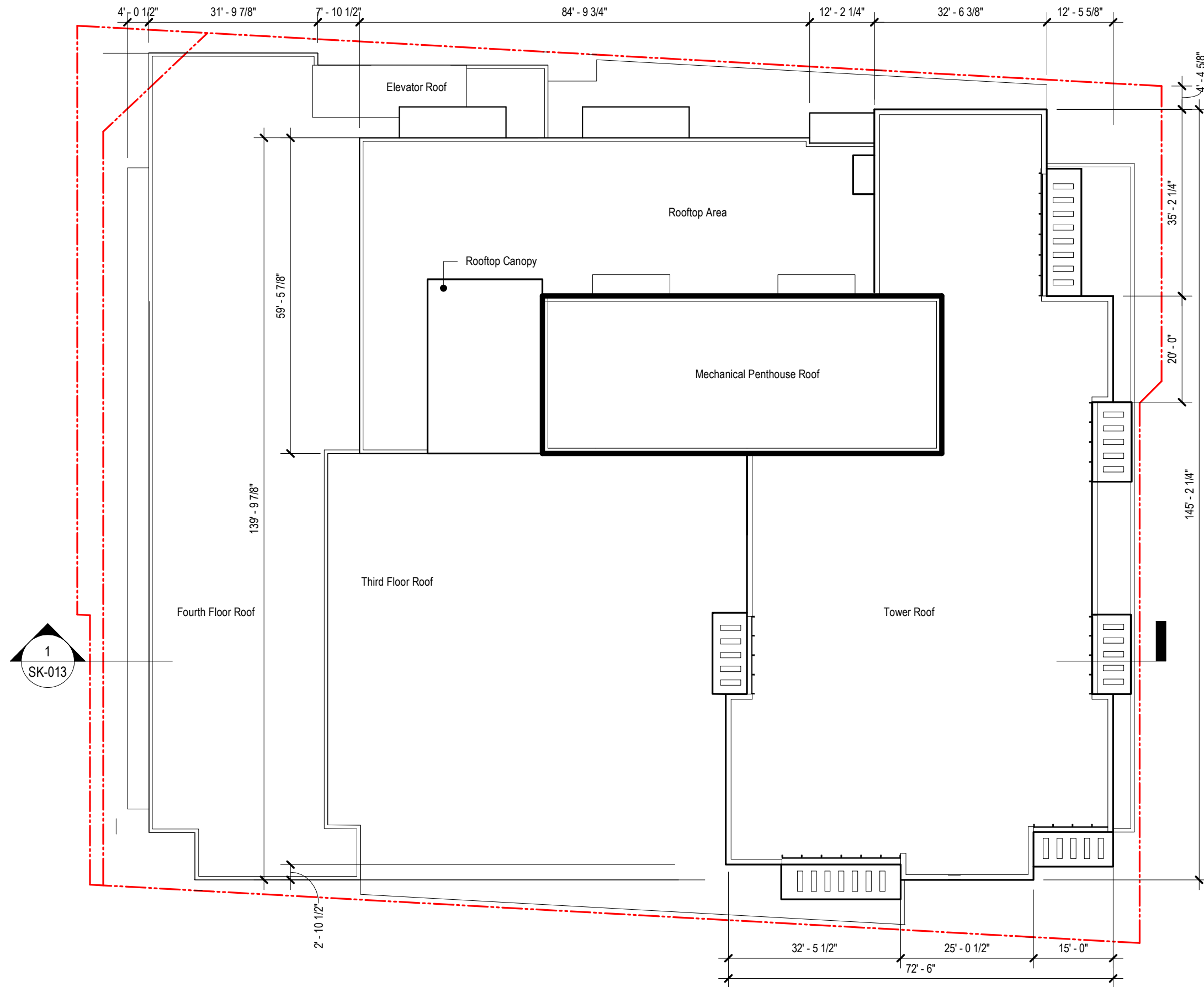
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Scale : 1" = 20'-0"

18-023

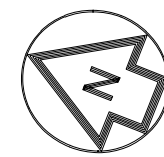
Medallion Residential Tower

Dundas - Hewitt

Roof Plan

2019.06.06












SK-010

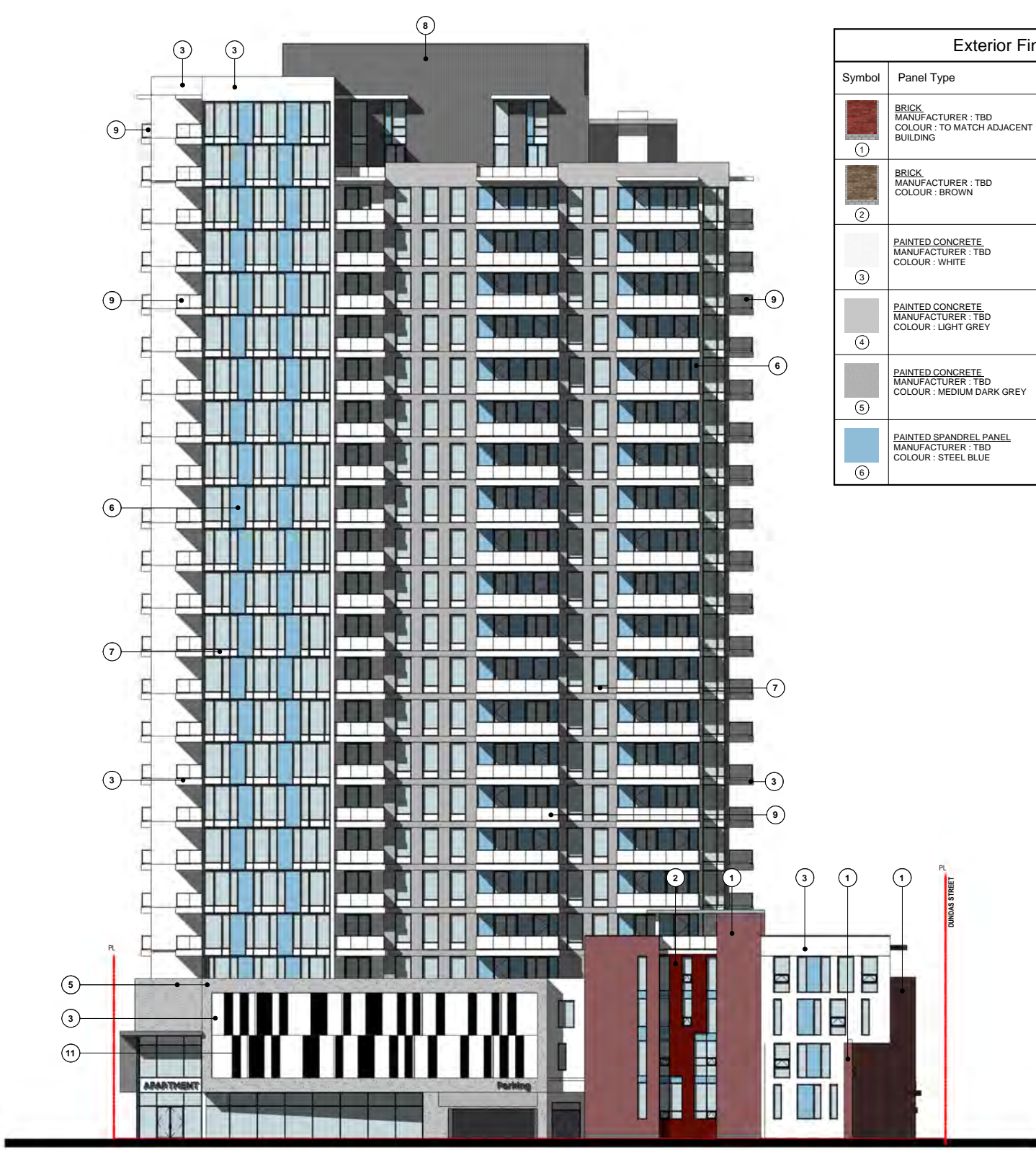


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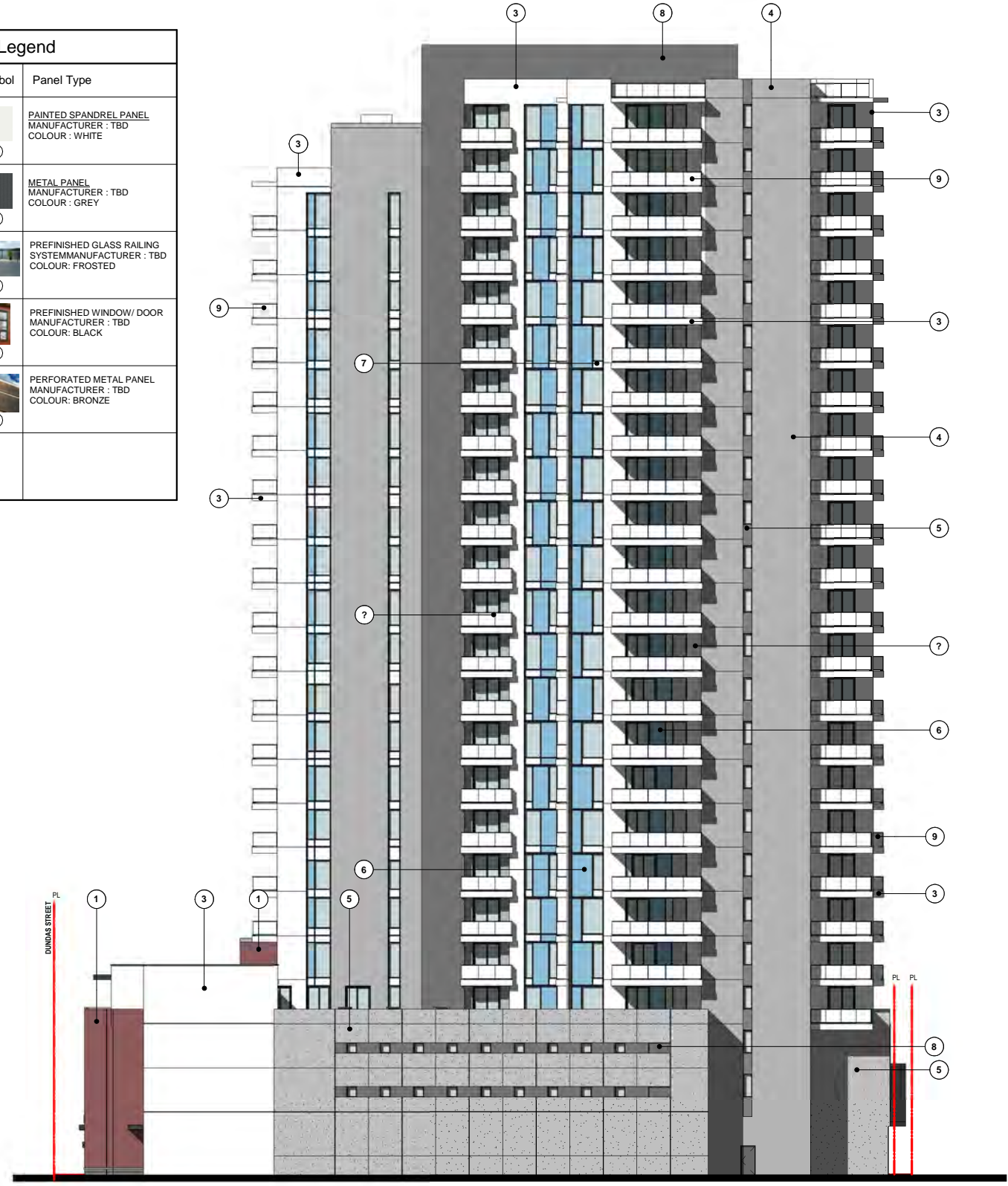
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Exterior Finish Legend			
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 ①	BRICK MANUFACTURER : TBD COLOUR : TO MATCH ADJACENT BUILDING	 ⑦	PAINTED SPANDREL PANEL MANUFACTURER : TBD COLOUR : WHITE
 ②	BRICK MANUFACTURER : TBD COLOUR : BROWN	 ⑧	METAL PANEL MANUFACTURER : TBD COLOUR : GREY
 ③	PAINTED CONCRETE MANUFACTURER : TBD COLOUR : WHITE	 ⑨	PREFINISHED GLASS RAILING SYSTEM MANUFACTURER : TBD COLOUR : FROSTED
 ④	PAINTED CONCRETE MANUFACTURER : TBD COLOUR : LIGHT GREY	 ⑩	PREFINISHED WINDOW/ DOOR MANUFACTURER : TBD COLOUR : BLACK
 ⑤	PAINTED CONCRETE MANUFACTURER : TBD COLOUR : MEDIUM DARK GREY	 ⑪	PERFORATED METAL PANEL MANUFACTURER : TBD COLOUR : BRONZE
 ⑥	PAINTED SPANDREL PANEL MANUFACTURER : TBD COLOUR : STEEL BLUE		



East Elevation














West Elevation

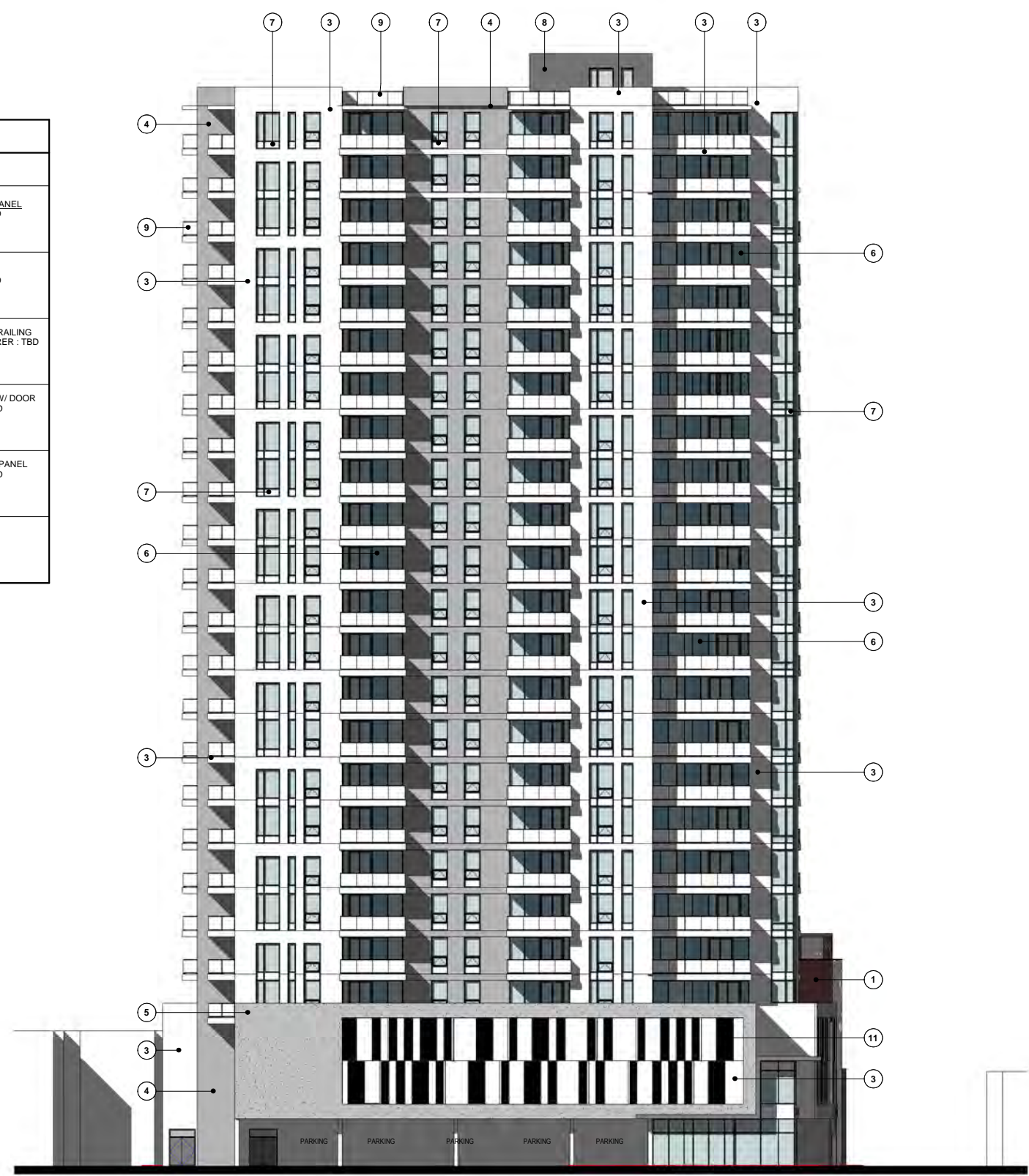
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North Elevation

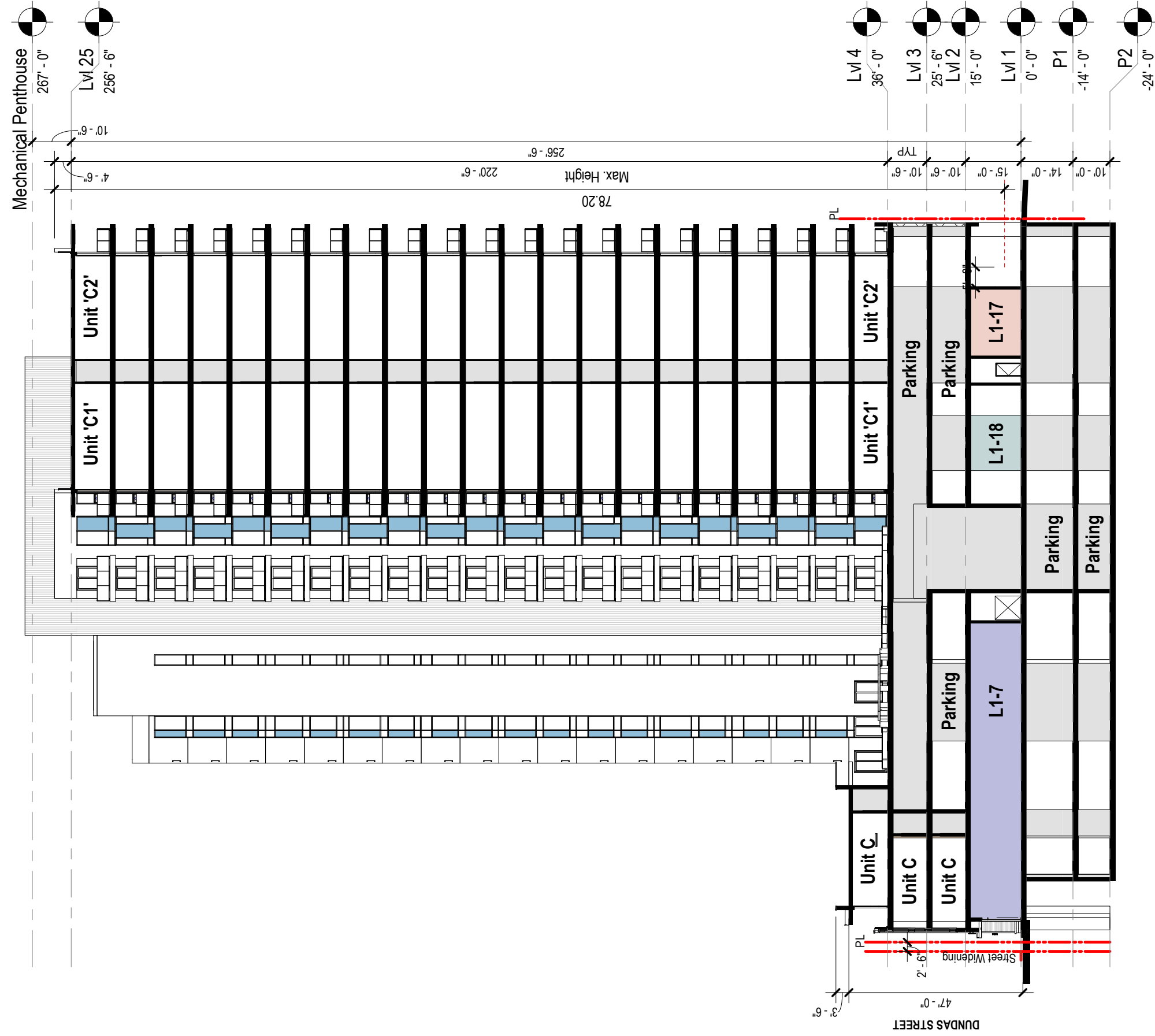
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	BRICK MANUFACTURER : TBD COLOUR : BROWN		METAL PANEL MANUFACTURER : TBD COLOUR : GREY
	PAINTED CONCRETE MANUFACTURER : TBD COLOUR : WHITE		PREFINISHED GLASS RAILING SYSTEM MANUFACTURER : TBD COLOUR : FROSTED
	PAINTED CONCRETE MANUFACTURER : TBD COLOUR : LIGHT GREY		PREFINISHED WINDOW/ DOOR MANUFACTURER : TBD COLOUR: BLACK
	PAINTED CONCRETE MANUFACTURER : TBD COLOUR : MEDIUM DARK GREY		PERFORATED METAL PANEL MANUFACTURER : TBD COLOUR: BRONZE
	PAINTED SPANDREL PANEL MANUFACTURER : TBD COLOUR : STEEL BLUE		



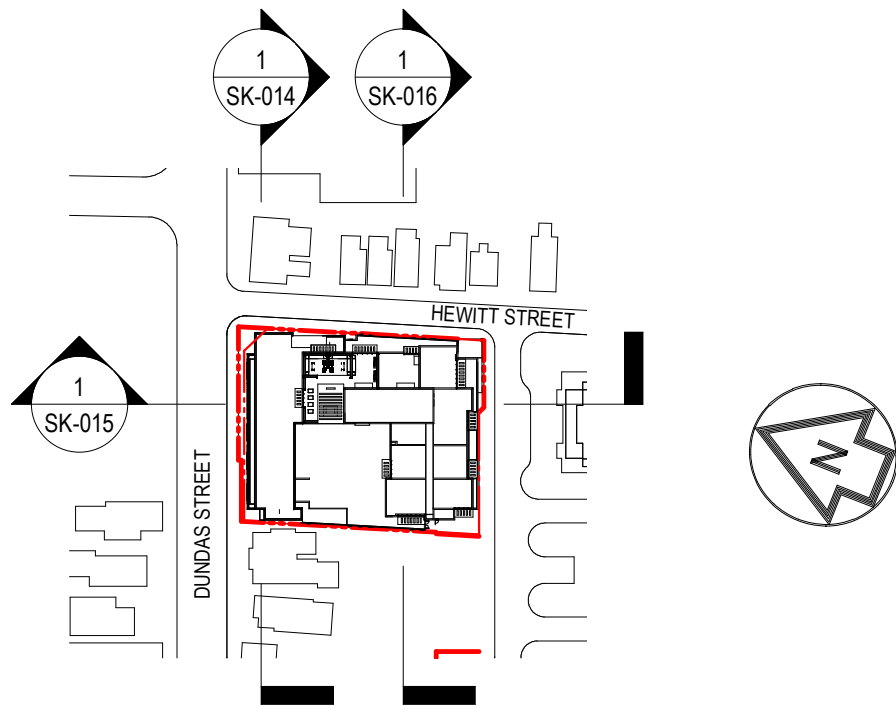
South Elevation

Typical (23 to 24)-Space Legend

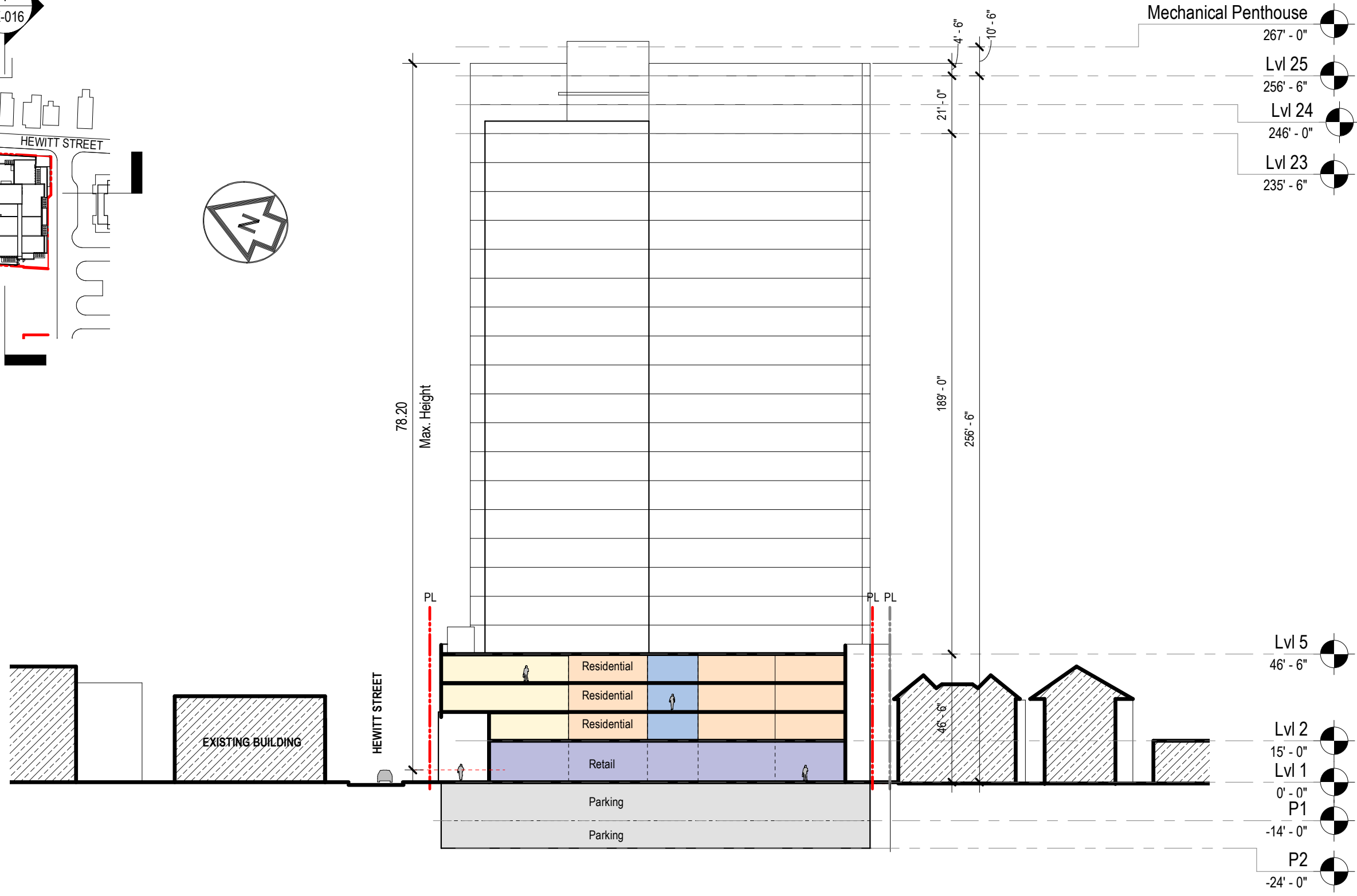
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	1-BR 759.00 SQ.FT		M&E 1,692.00 SQ.FT
	Studio 759.00 SQ.FT		PARKING
	Retail 2,286.00 SQ.FT		



Scale : As indicated



KEY MAP



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Scale : As indicated

18-023 Medallion Residential Tower

Dundas - Hewitt

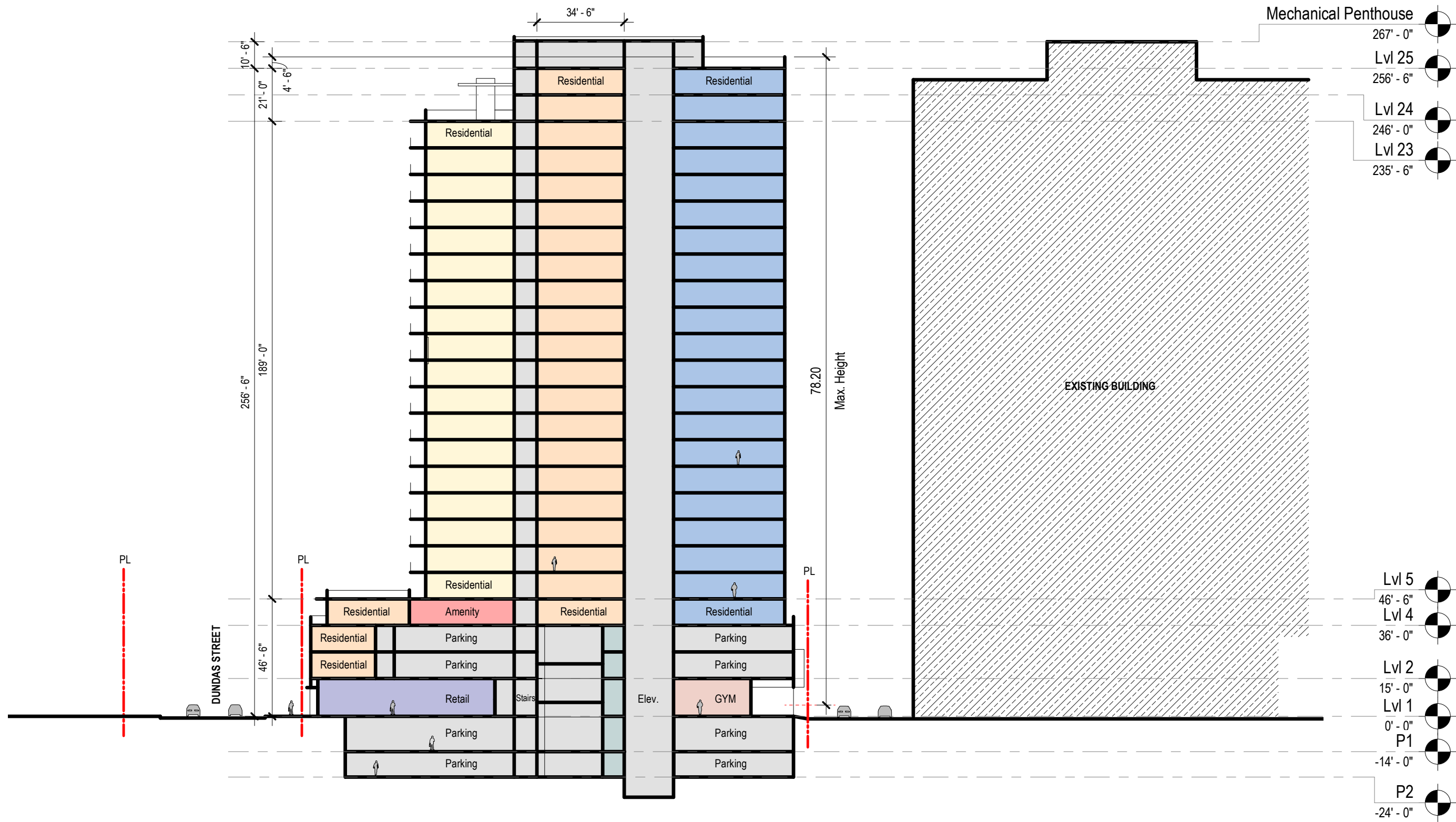
Section B-B

06/11/19 SK-014

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Scale : 1" = 40'-0"

18-023 Medallion Residential Tower

Dundas - Hewitt

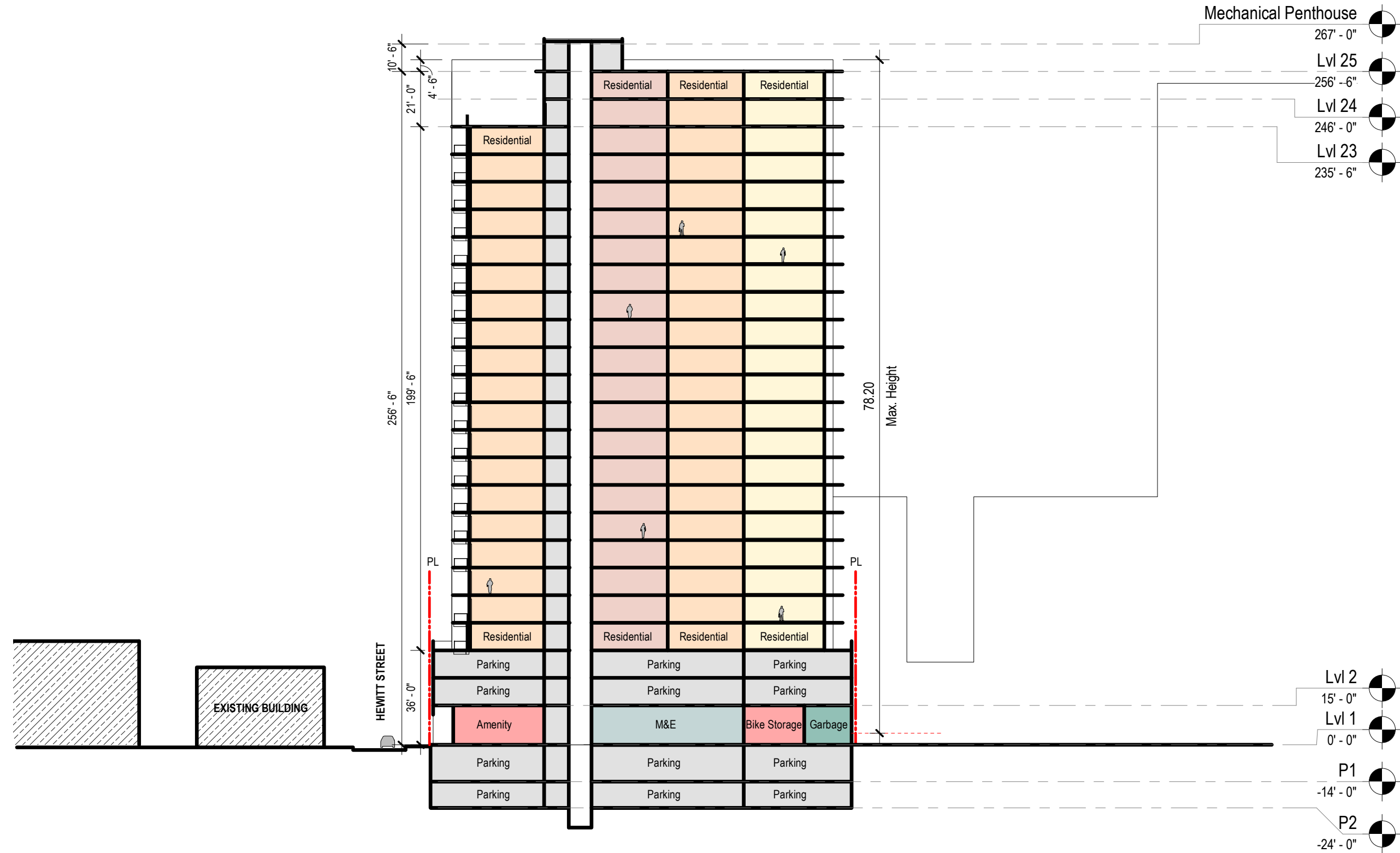
Section C-C

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Scale : 1" = 40'-0"

18-023 Medallion Residential Tower

Dundas - Hewitt

Section D-D

06/11/19 SK-016

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Scale :

18-023

Medallion Residential Tower

Dundas - Hewitt

NorthEast Corner Rendering

2019.06.06

SK-017

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Scale :

18-023

Medallion Residential Tower

Dundas - Hewitt

NorthWest Corner Rendering

2019.06.06

SK-018

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Scale :

18-023

Medallion Residential Tower

Dundas - Hewitt

North View Rendering

2019.06.06

SK-019

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Scale :

18-023

Medallion Residential Tower

Dundas - Hewitt

NorthEast Corner Rendering

2019.06.06

SK-020

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Scale :

18-023

Medallion Residential Tower

Dundas - Hewitt

EastView Rendering

2019.06.06

SK-021

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Scale :

18-023 Medallion Residential Tower

Dundas - Hewitt

SouthEast Corner Rendering

2019.06.06 SK-022

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Scale :

18-023

Medallion Residential Tower

Dundas - Hewitt

Aerial View Rendering

2019.06.06

SK-023

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Scale :

18-023 Medallion Residential Tower

Dundas - Hewitt

NorthEast Corner Aerial View Rendering 2019.06.06 SK-024

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Scale :

18-023

Medallion Residential Tower

Dundas - Hewitt

NorthEast Corner Street View Rendering

2019.06.06

SK-025

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Scale :

18-023 Medallion Residential Tower

Dundas - Hewitt

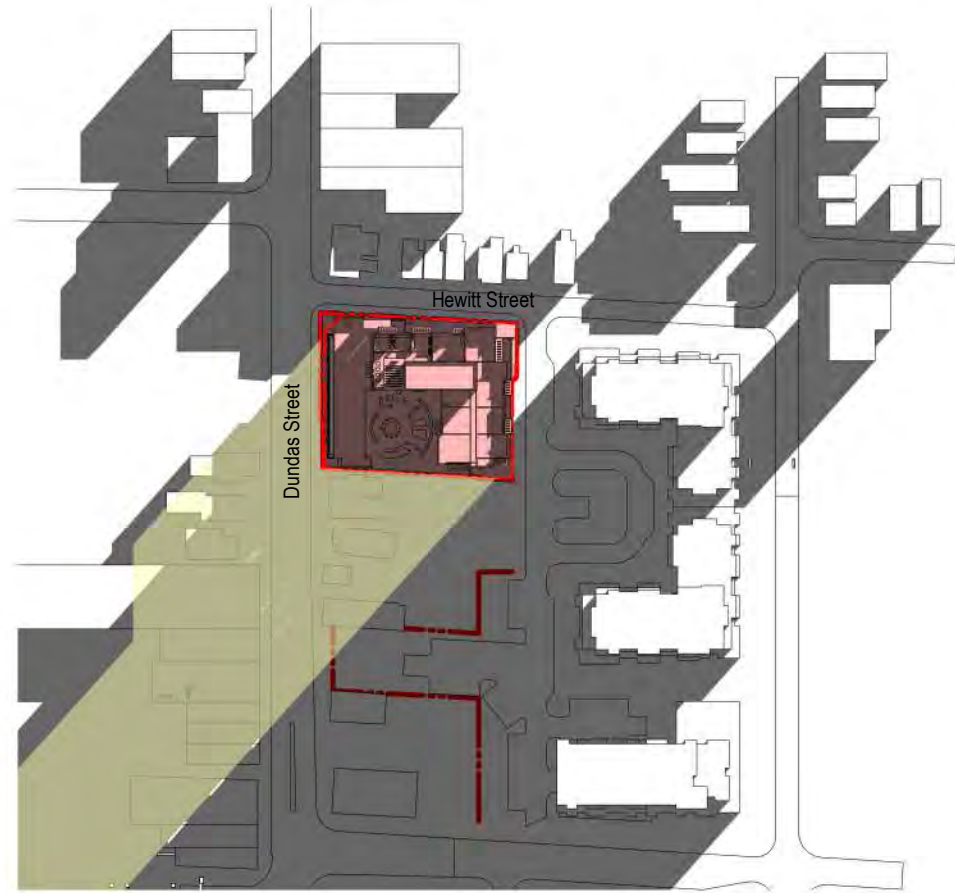
NorthWest Corner Rendering

2019.06.06 SK-026

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MARCH



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Scale : 1" = 200'-0"

18-023 Medallion Residential Tower

Dundas - Hewitt

Shadow Study - March

06/06/19 SK-027



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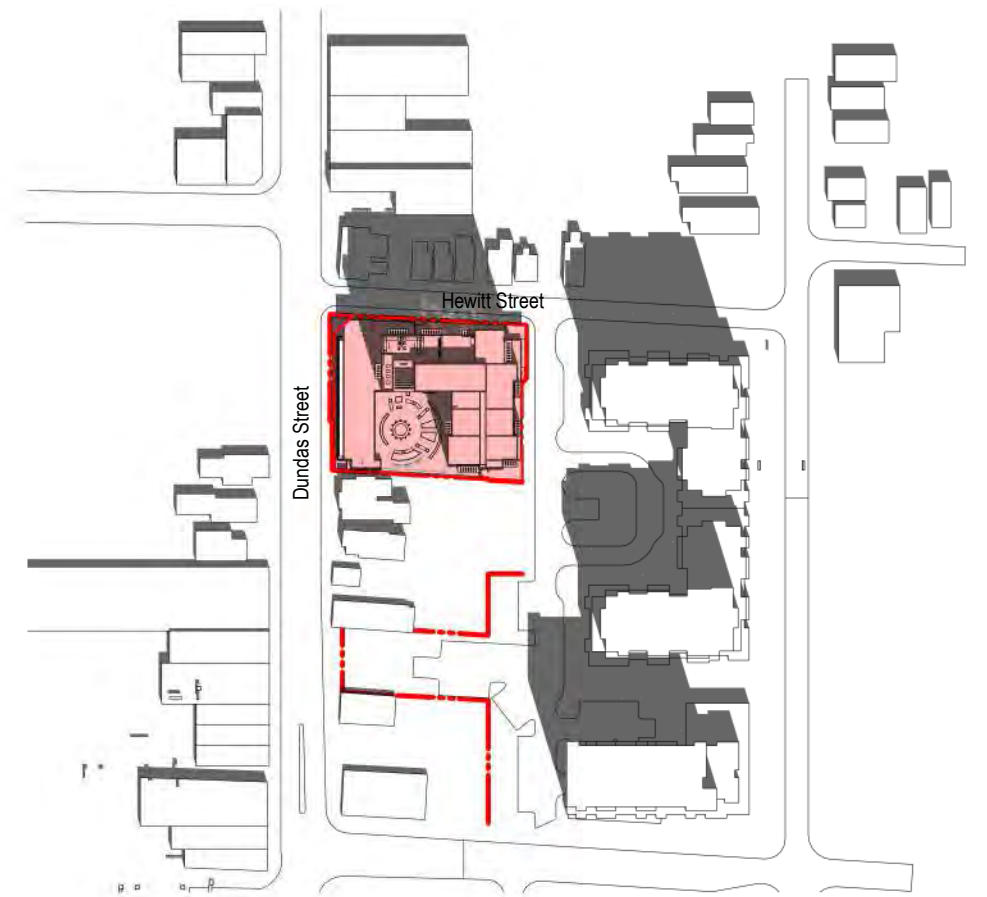
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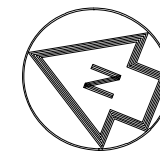
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18-023 Medallion Residential Tower

Dundas - Hewitt

Shadow Study - June

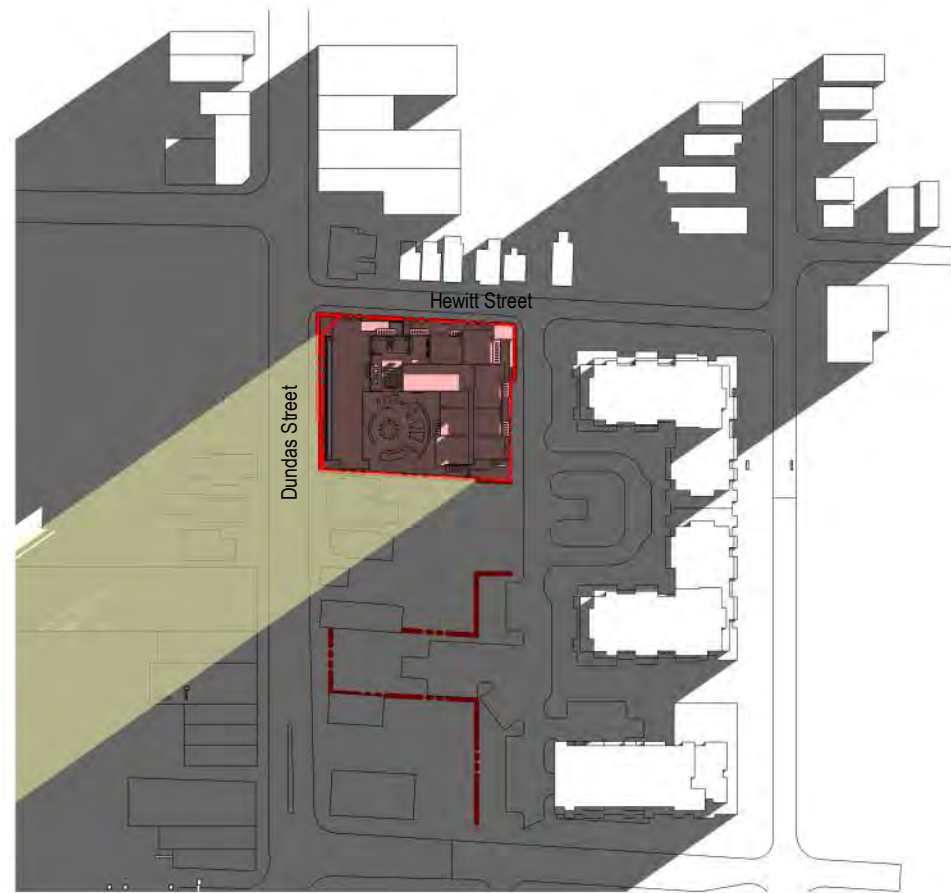
06/06/19 SK-028



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DECEMBER



9:00 AM



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Scale : 1" = 200'-0"

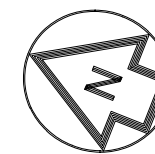
18-023 Medallion Residential Tower

Dundas - Hewitt

Shadow Study - December

06/06/19

SK-029



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Appendix B
Designation Bylaw for 719-721
Dundas Street



An agency of the Government of Ontario



Un organisme du gouvernement de l'Ontario

This document was retrieved from the Ontario Heritage Act Register, which is accessible through the website of the Ontario Heritage Trust at **www.heritagetrust.on.ca**.

Ce document est tiré du registre aux fins de la *Loi sur le patrimoine de l'Ontario*, accessible à partir du site Web de la Fiducie du patrimoine ontarien sur **www.heritagetrust.on.ca**.



300 Dufferin Avenue
P.O. Box 5035
London, ON
N6A 4L9

London
CANADA

REGISTERED

July 25, 2012

Ontario Heritage Foundation
10 Adelaide Street East
Toronto ON M5C 1J3

Unity Project for Relief of
Homelessness in London
719-721 Dundas Street
London ON

ONTARIO HERITAGE TRUST

JUL 27 2012

RECEIVED

**Re: Designation of 719-721 Dundas Street
The Ontario Heritage Act, R.S.O. 1990, c. 0.18**

Please find enclosed, for your information, a certified copy of By-law No. L.S.P.-3416-121, entitled "A by-law to designate 719-721 Dundas Street to be of Historical and Cultural value", passed by the Municipal Council of the Corporation of The City of London on April 11, 2012 and registered as Instrument No. ER822836 on May 10, 2012.

The London Advisory Committee on Heritage will be contacting you at a later date to determine whether or not you wish to have a plaque mounted on this building to designate it as a site of Historical and Cultural value.

Catharine Saunders
City Clerk

/rs

Encl.

cc : D. Menard, Planning Division

The Corporation of the City of London
Office: 519-661-2500 ext. 0916
Fax: 519-661-4892
rszwec@london.ca
www.london.ca

Bill No. 160
2012

By-law No. L.S.P.-3416-121

A by-law to designate 719-721 Dundas St to be of historical and contextual value or interest.

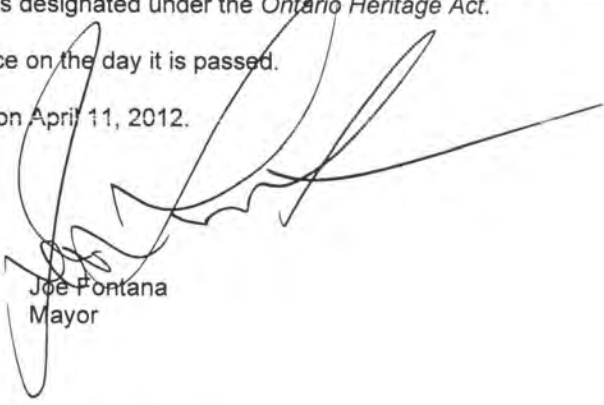
WHEREAS pursuant to the *Ontario Heritage Act, R.S.O. 1990, c. 0.18*, the Council of a municipality may by by-law designate a property including buildings and structures thereon to be of historic and contextual value or interest;


AND WHEREAS notice of intention to so designate the property known as 719-721 Dundas St has been duly published and served and no notice of objection to such designation has been received;

THEREFORE the Municipal Council of The Corporation of the City of London enacts as follows:

1. There is designated as being of historical and contextual value or interest, the real property at 719-721 Dundas St, more particularly described in Schedule "A" hereto, for the reasons set out in Schedule "B" hereto.
2. The City Clerk is authorized to cause a copy of this by-law to be registered upon the title to the property described in Schedule "A" hereto in the proper Land Registry Office.
3. The City Clerk is authorized to cause a copy of this by-law to be served upon the owner of the aforesaid property and upon the Ontario Heritage Foundation and to cause notice of this by-law to be published in the London Free Press, and to enter the description of the aforesaid property, the name and address of its registered owner, and short reasons for its designation in the Register of all properties designated under the *Ontario Heritage Act*.
4. This by-law comes into force on the day it is passed.

PASSED in Open Council on April 11, 2012.


Joe Fontana
Mayor

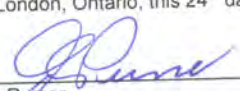

Catharine Saunders
City Clerk



CITY OF LONDON

I, James C. Purser, Manager of Records Information of The Corporation of the City of London, hereby certify that the document hereunder is a true copy of By-law No. L.S.P.-3416-121 passed by Municipal Council on April 11, 2012.

Dated at London, Ontario, this 24th day of July, 2012.


James C. Purser
Manager of Records Information

First Reading – April 11, 2012
Second Reading – April 11, 2012
Third Reading – April 11, 2012

SCHEDULE "A"

To By-law No. L.S.P.- 3416-121

"Part of Lots 5 and 6, South of Dundas Street and Part of Lot A, West of Glebe Street on Registered Plan 229(3) in the City of London and County of Middlesex."

SCHEDULE "B"

To By-law No. L.S.P.- 3416-121

Statement of Significance of 719-721 Dundas Street

Legal Description of Property

719-721 Dundas Street is a two and one half story red and buff brick duplex residence on Part of Lots 5 & 6, S/S Dundas Street and Part Lot A W/S Glebe Street. (The structure at the municipal address of 717 Dundas is not included in this statement of significance.)

Statement of Cultural Heritage Interest

The semi-detached, two and one half story residence located at 719-721 Dundas Street is recommended for designation under Part IV of the Ontario Heritage Act as a building of cultural heritage value.

The building has architectural value as an interesting example of the Queen Anne style applied to an earlier building. It is significant as a physical reminder of the former residential nature of this section of Dundas Street now greatly encroached by commercial uses.

The building has an extremely unusual architectural history. Superficially, from a cursory observation of the Queen Anne architecture of the front elevation, the assumed date of construction would be the mid to late 1890s. A close examination of the entire fabric of the building, however, most especially the side elevations together with the archival resources, reveals that the 719 half was built much earlier. City Directory research indicates that 719 Dundas Street was erected in 1877 and that its first occupant was Francis S. Bickley, listed as a "machine hand" later a "plumber" among other occupations. The 1881 updated to 1888 London fire insurance plan clearly depicts a single family house. From the evidence provided by the two sets of corner quoins of the west elevation, the original two story building was probably designed in the prevailing Italianate style. In 1899, the house was doubled in size with the side and rear elevations of the new 721 duplicating the white brick, and the number of stories increased to two-and-one-half from two. Interestingly, the original façade of 719 Dundas Street was demolished and unified with 721 by means of new façade built of red brick (now painted yellow) with gray stone block trim and a new roof. A building permit was issued by the City of London in 1918 allowing a one story, white brick addition to the rear wing of 719, which functioned as a doctor's office until 1954.

From 1899, the building served as residences, 719 until 1945, 721 until 1937. Thereafter, both sides accommodated various commercial and professional functions including real estate and chartered accountant offices, a hair stylist and a law office among many other uses. Both addresses were the location of the residence and office of several doctors most particularly for Dr. Edwin Seaborn at 721 for the years 1904-07. Seaborn authored *The March of Medicine in Western Ontario* and was a long-time member of The University of Western Ontario's Faculty of Medicine.

Description of Heritage Attributes

The residence was designed in the Queen Anne style with Romanesque Revival influences that are worthy of preservation. The important architectural features comprise (limited to the front elevation):

- a high pitched hip roof matched on either side by two elaborately decorated gables each crowning a two story projection
- a large elaborately detailed roof dormer centred over the recess between the two projections
- an elaborately decorated shallow porch crowned by cast-iron cresting and supported by classical columns resting on stone block piers which serves to connect the two front projections and to shelter the front doors in the recess between the two projections thereby unifying the balanced elements of the design
- two ground story round headed windows in the front projections either side of the porch which are outlined by large rusticated stone block voussoirs, which each radiate from a central keystone
- rusticated stone block coursing which extends from the voussoirs and raps around to overlap a small length of the side elevations
- rusticated stone labels over the windows of the second story; over the single windows either side of the first story doors for 719 and 721; and over the single windows set in the angled corners
- rounded corners on the second story meet the earlier buff brick quoins of the side elevation of 719 and the side elevation wall of 721
- rounded corners are also present on the inside corners of the second story projections
- the corner quoins of the side elevation of 719 where they meet the 1899 front elevation; they provide evidence of the earlier structure

- several stained glass windows: in the transoms over the two centre windows in the recess of the second story; in the semi-circular transom over the large first story window of 721; over the windows in the diagonally cut sections of the wall at the corners that angle away on both sides of the first stories of the two front projections; over the door of 721 which incorporates the 721 address (the stained glass transoms over the large first story window and door of 719 have been removed)
-

**LONDON ADVISORY COMMITTEE ON HERITAGE
2018 WORK PLAN
(March 14, 2018)**

	Project/Initiative	Background	Lead/ Responsible	Proposed Timeline	Proposed Budget (in excess of staff time)	Link to Strategic Plan	Status
1.	<p>-Recurring items as required by the Ontario Heritage Act (consider and advise the PEC (Planning and Environment Committee) and Municipal Council on matters related to HAPs (Heritage Alteration Permits), HIS (Heritage Impact Statement) reviews, HCD (Heritage Conservation District) designations, individual heritage designations, (etc.);</p> <p>-Research and advise the PEC and Municipal Council regarding recommendations for additions to the Register (Inventory of Heritage Resources);</p> <p>-Prioritize and advise the PEC and Municipal Council on top recommendations for heritage designation (final number to be determined by available time – taken from the Register and elsewhere as appropriate);</p> <p>-Consider and advise the PEC on ad hoc recommendations from citizens in regard to individual and Heritage Conservation District designations and listings to the Register (refer to Stewardship for advice);</p> <p>-Perform all other functions as indicated in the LACH Terms of Reference.</p>	<ul style="list-style-type: none"> Section 28 of the Ontario Heritage Act mandates that the City shall establish a municipal heritage committee. Further, Council shall consult with that committee in accordance with the Ontario Heritage Act; Please see the <u>London Advisory Committee on Heritage: Terms of Reference</u> for further details; The LACH supports the research and evaluation activities of the LACH Stewardship Subcommittee, Policy and Planning Subcommittee, Education Subcommittee, Archaeological Subcommittee, and all other LACH Subcommittees which may serve from time to time. 	LACH (main) and subcommittees	As required	None	Strengthening our Community 4d; Building a Sustainable City 1c, 6b; Growing our Economy 1f, 2d	Ongoing
2.	<p>Introduce all represented organisations and individuals on LACH at the first meeting of the new year, discuss member background and areas of knowledge/ expertise, and consider possible changes or additions.</p>	<ul style="list-style-type: none"> The LACH is made of a diverse and knowledgeable group of engaged individuals, professionals and representatives of various organizations. Once per year (or when a new member joins the committee) each member will introduce themselves to the committee and provide his/her relevant background. 	LACH (main)	January meeting	None	Building a Sustainable City 6b	Completed

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3.	Ontario Heritage Act enforcement.	<ul style="list-style-type: none"> The LACH will assist in identifying properties that have not obtained necessary approvals, and refer these matters to civic administration. The LACH will assist in monitoring alterations to HCD and heritage designated properties and report deficiencies to civic administration. 	LACH (main)	Ongoing	None	Building a Sustainable City 6b	Ongoing
4.	Great Talbot Heritage Conservation District	<ul style="list-style-type: none"> The St George Grosvenor HCD Study is complete resulting in the Great Talbot HCD and Gibbons Park HCD. The LACH will monitor, assist and advise in the preparation of the both plans, following the timeline as approved by Council. 	LACH (main)	2018 Plan Completion	None	Building a Sustainable City 6b	Ongoing
5.	Heritage Places Review	<ul style="list-style-type: none"> The LACH will participate and support the review of <i>Heritage Places</i> (1994), the guidelines document which identifies potential Heritage Conservation Districts 		2018	None	Building a Sustainable City 6b	
6.	Property insurance updates.	<ul style="list-style-type: none"> The LACH will monitor, assist and advise on matters pertaining to the securing of property insurance for heritage designated properties in the City of London. 	Policy and Planning Sub-Committee	Ongoing.	None	Building a Sustainable City 6b	With Policy and Planning Sub-Committee
7.	City Map updates.	<ul style="list-style-type: none"> The LACH will work with City staff to ensure that 'City Map' and searchable City databases are up to date in regard to the heritage register/ designations/ districts/ etc. 	Policy and Planning Sub-Committee	Ongoing	None	Building a Sustainable City 6b	With Policy and Planning Sub-Committee
8.	Heritage Impact Assessment Terms of Reference	<ul style="list-style-type: none"> The LACH will support staff in their efforts to formalize an approach to reviewing and advising on HIS reports (including what triggers the reports, expectations, and who completes them. 	Policy and Planning subcommittee	2018	None	Building a Sustainable City 6b	Partially Complete
9.	Review of Delegated Authority	<ul style="list-style-type: none"> The LACH will participate and support the review of the Delegated Authority for Heritage Alteration Permits 	LACH (main)	2018	None	Building a Sustainable City 6b	

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10.	New and ongoing heritage matters.	<ul style="list-style-type: none"> Through its connections to various heritage groups, and the community at large, the LACH is aware of emerging and ongoing heritage matters in the City of London. The LACH will monitor and report to City staff and PEC on new and ongoing cultural heritage matters where appropriate. (ex. Ontario Cultural Strategy, Community Economic Roadmap, etc.). 	LACH (main)	As required	None	Building a Sustainable City 6b	As required
11.	Archaeological Master Plan completion.	<ul style="list-style-type: none"> The LACH will work with City staff to complete the Archaeological Master Plan currently underway. 	Archaeological subcommittee	Q2 2018	None	Building a Sustainable City 6b	Partially complete
12.	The Mayor's New Year Honour List recommendation.	<ul style="list-style-type: none"> For a number of years, members of the LACH have been asked to provide advice to Council on the heritage addition to the "Mayor's New Year Honour List". The LACH will continue to serve this function as requested to do so by Council. 	Ad hoc committee of the LACH	Generally in the fall of each year	None	Building a Sustainable City 6b	Annually
13.	Provide advice to the London Community Foundation on heritage grant distribution.	<ul style="list-style-type: none"> For a number of years, members of the LACH have been asked to provide advice to the London Community Foundation on heritage grant distribution: "The London Endowment for Heritage". The LACH will continue to serve this function as requested to do so by the Foundation. 	Ad hoc committee of the LACH	Generally in April of each year	None	Building a Sustainable City 6b	Annually
14.	Conference attendance.	<ul style="list-style-type: none"> For a number of years, members of the LACH have attended the Ontario Heritage Conference when available. This conference provides an opportunity for LACH members to meet with other heritage committee members and heritage planning professionals, and to learn about current and ongoing heritage matters in the Province of Ontario (and beyond). Up to four (4) members of the LACH will attend the Ontario Heritage Conference. 	LACH (main)	May 2016	None	Building a Sustainable City 6b	Annually

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15.	Public awareness and education (& possible heritage fair/ day/ symposium).	<ul style="list-style-type: none"> The LACH initiates, assists and/or advises on education and outreach programs to inform the citizens of London on heritage matters. This year, the LACH will also consider contributing to the organization of a city wide heritage fair/ day/ symposium (to provide information and outreach including – HAP process, professional advice on repairs and maintenance, current research on heritage matters, insurance advice, real estate matters, and a general exchange of ideas (etc.)). The LACH will coordinate with the efforts of the Historic Sites Committee of the London Public Library. 	Education subcommittee	Ongoing	None	Building a Sustainable City 6b	Ongoing – in progress
16.	Public awareness and education collaboration with the London Heritage Council.	<ul style="list-style-type: none"> The LACH will be supported by the London Heritage Council in its role to promote public awareness of and education on the community's cultural heritage resources. Collaborative initiatives may include LACH-related news updates in the LHC newsletter, LACH involvement in LHC programming and events (i.e. Heritage Fair), outreach support, and/or school-related programming as part of Citizen Culture: Culture-Infused LEARNING (LHC and London Arts Council). 	LACH (main) and Education subcommittee in collaboration with the London Heritage Council	Ongoing	None	Building a Sustainable City 6b	Annually
17.	LACH member education/ development.	<ul style="list-style-type: none"> Where possible, the LACH will arrange an information session for LACH members to learn more about the Ontario Heritage Act, and the mandate and function of Heritage Advisory Committees. The LACH will also explore ongoing educational opportunities for LACH members (such as walking tours, meetings with heritage experts/ professionals, meetings with community leaders, etc.). 	LACH (main)	Ongoing	None	Building a Sustainable City 6b	Ongoing
18.	City of London Archives.	<ul style="list-style-type: none"> The LACH will continue to discuss and advise on possible locations (and contents) for a City of London Archives. 	LACH (main)	Ongoing	None	Building a Sustainable City 6b	Ongoing

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19.	LACH subcommittee member outreach.	<ul style="list-style-type: none"> The LACH will continue to reach out to heritage and planning professionals/ experts to serve on LACH subcommittees (and advise the LACH on certain matters). 	LACH (main)	Ongoing	None	Building a Sustainable City 6b	Ongoing
20.	Heritage signage and plaque placement/funding.	<ul style="list-style-type: none"> Through its connections to various heritage groups, and the community at large, the LACH is generally aware of potential locations for heritage signage and plaques. The LACH will consult with City Staff and heritage groups in regard to the occasional placement of heritage signage and/or plaques (and assist with funding where deemed appropriate by the committee). These efforts will be considered in the context of the City of London Heritage Interpretative Signage Policy. 	Education subcommittee	Ongoing	\$8000	Building a Sustainable City 6b	Ongoing
21.	Council outreach.	<ul style="list-style-type: none"> If requested, the LACH will arrange an information session for Council members to learn more about the mandate and function of the LACH, the Ontario Heritage Act, and other City heritage matters. 	LACH (main) and Education subcommittee	TBD	None	Building a Sustainable City 6b	Ongoing
22.	Work Plan review.	<ul style="list-style-type: none"> The LACH will review items on this Work Plan on a quarterly basis, and will thoroughly review this Work Plan at least once annually. 	LACH (main)	Ongoing	None	Building a Sustainable City 6b	Ongoing (March, June, Sept, Dec 2018)
23.	Rapid Transit EA	<ul style="list-style-type: none"> The LACH will participate in heritage related matters associated with the Rapid Transit (Shift) EA including review of properties identified the Cultural Heritage Screening Report; identifying where further work is or is not required for potential cultural heritage resources; and identifying properties along rapid transit corridors that have not yet been identified and merit further consideration for cultural heritage evaluation 	LACH (main) and Stewardship subcommittee	Ongoing	None	Building a Sustainable City 6b	Ongoing

\$8000