

# Agenda

## Planning and Environment Committee

16th Meeting of the Planning and Environment Committee

October 22, 2024

1:00 PM

Council Chambers - Please check the City website for additional meeting detail information. Meetings can be viewed via live-streaming on YouTube and the City Website.

The City of London is situated on the traditional lands of the Anishinaabek (AUh-nish-in-ah-bek), Haudenosaunee (Ho-den-no-show-nee), Lūnaapéewak (Len-ah-pay-wuk) and Attawandaron (Add-a-won-da-run).

We honour and respect the history, languages and culture of the diverse Indigenous people who call this territory home. The City of London is currently home to many First Nations, Métis and Inuit today.

As representatives of the people of the City of London, we are grateful to have the opportunity to work and live in this territory.

Members

Councillors S. Lehman (Chair), S. Lewis, C.Rahman, S. Franke, S. Hillier

The City of London is committed to making every effort to provide alternate formats and communication supports for meetings upon request. To make a request specific to this meeting, please contact [PEC@london.ca](mailto:PEC@london.ca) or 519-661-2489 ext. 2425.

	Pages
<b>1. Disclosures of Pecuniary Interest</b>	
<b>2. Consent</b>	
2.1 10th Report of the Community Advisory Committee on Planning	3
2.2 Quarterly Heritage Report	5
2.3 Building Division Detailed Update: 2024 Year-To-Date	7
2.4 Development Pipeline - Developer of Lands within Open Site Plans, Open Condominium Plans, and Registered and Reference Plans (Subdivisions)	21
2.5 Dundas Streetscape Master Plan for the Argyle Core Area	27
2.6 Reducing Off-the-Clock Permit Applications	41
<b>3. Scheduled Items</b>	
3.1 Public Participation Meeting - Not to be heard before 1:00 PM - 763-773 Dundas Street (Z-9777)	57
3.2 Public Participation Meeting - Not to be heard before 1:00 PM - 2118 Richmond Street (OZ-9770)	83
3.3 Public Participation Meeting - Not to be heard before 1:00 PM - 566, 568, 572 and 578 Colborne Street (Z-9775)	116
3.4 Public Participation Meeting - Not to be heard before 1:00 PM - 313-323 Horton Street East (Z-9616)	140
3.5 Public Participation Meeting - Not to be heard before 1:00 PM - 1856-	168

1910 Oxford Street West (O-9484 & Z-9458)

3.6 Public Participation Meeting - Not to be heard before 1:30 PM - 279 Sarnia Road (Z-9774) 214

3.7 Public Participation Meeting - Not to be heard before 1:30 PM -1210-1240 Wharncliffe Road South (Z-9778) 243

**4. Items for Direction**

4.1 Hyde Park BIA Boundary Expansion Request

a. Request for Delegation - D. Szpakowski and T. Delaney - Hyde Park Business Improvement Association 272

**5. Deferred Matters/Additional Business**

5.1 Deferred Matters List 276

**6. Adjournment**

# Community Advisory Committee on Planning Report

10th Meeting of the Community Advisory Committee on Planning  
October 9, 2024

Attendance PRESENT: J.M. Metrailler (Chair), M. Bloxam, J. Dent, J. Gard, S. Jory, M. Rice, S. Singh Dohil, K. Waud, M. Whalley, M. Wojtak and J. Bunn (Committee Clerk)

ABSENT: M. Ambrogio, I. Connidis, A. Johnson and M. Wallace

ALSO PRESENT: L. Dent, K. Gonyou and K. Mitchener

The meeting was called to order at 5:32 PM; it being noted that M. Bloxam, J. Dent, S. Singh Dohil were in remote attendance.

## 1. Call to Order

### 1.1 Disclosures of Pecuniary Interest

That it BE NOTED that no pecuniary interests were disclosed.

## 2. Scheduled Items

None.

## 3. Consent

### 3.1 9th Report of the Community Advisory Committee on Planning

That it BE NOTED that the 9th Report of the Community Advisory Committee on Planning, from the meeting held on September 11, 2024, was received.

### 3.2 Notice of Planning Application and Public Meeting - Zoning By-law Amendment - 1195 Gainsborough Road

That it BE NOTED that the Notice of Planning Application and Public Meeting, dated September 17, 2024, from B. House, Planner, with respect to a Zoning By-law Amendment related to the property located at 1195 Gainsborough Road, was received.

## 4. Sub-Committees and Working Groups

None.

## 5. Items for Discussion

### 5.1 Public History Student Presentation Budget Ask

That an expenditure from the 2024 Community Advisory Committee on Planning (CACP) Budget BE APPROVED, up to \$550.00, for security services and refreshments at the Stewardship Sub-Committee meeting, hosting the Western University Public History Program presentations; it being noted that the CACP has sufficient funds in its 2024 budget to cover this expense.

### 5.2 Heritage Planners' Report

That it BE NOTED that the Heritage Planners' Report, dated October 9, 2024, was received.

5.3 (ADDED) Listed Properties and City Hall - Discussion

That it BE NOTED that the Community Advisory Committee on Planning held a general discussion with respect to Listed Properties and City Hall.

**6. Adjournment**

The meeting adjourned at 6:20 PM.

## Report to Planning and Environment Committee

**To:** Chair and Members  
Planning and Environment Committee  
**From:** Scott Mathers, MPA, P. Eng.  
Deputy City Manager, Planning and Economic Development  
**Subject:** Quarterly Heritage Report – Q3 2024  
**Date:** October 22, 2024

## Recommendation

That, on the recommendation of the Director, Planning and Development, the following report **BE RECEIVED** for information.

## Executive Summary

Approval, or approval with terms and conditions, of alterations affecting heritage designated properties may be granted administratively pursuant to the Delegated Authority By-law for Heritage Designated Properties (By-law No. C.P.-1502-129, as amended). The purpose of this report is to provide Municipal Council with information regarding Heritage Alteration Permits that were processed pursuant to the Delegated Authority By-law during the third quarter of 2024 (July 1 – September 30, 2024).

## Linkage to the Corporate Strategic Plan

This recommendation supports the following 2023-2027 Strategic Plan areas of focus:

- London has safe, vibrant, and healthy neighbourhoods and communities.
  - Londoners have a strong sense of belonging and sense of place.
    - Create cultural opportunities that reflects arts, heritage, and diversity of community.

## Analysis

### 1.0 Background Information

Heritage Alteration Permit approval may be required to consent to or permit alterations to a heritage designated property. Pursuant to the Delegated Authority By-law, By-law No. C.P.-1502-129, as amended, staff may approve or approve with terms and conditions a Heritage Alteration Permit application. Only those Heritage Alteration Permit applications meeting a “condition for referral” defined by the Delegated Authority By-law are referred to the Community Advisory Committee on Planning (CACAP, the City’s municipal heritage committee) for consultation and require a decision to approve, approve with terms and conditions, or refuse by Municipal Council.

### 2.0 Discussion and Considerations

#### 2.1 Heritage Alteration Permits

The following Heritage Alteration Permits were processed pursuant to the Delegated Authority By-law in the third quarter of 2024:

- 135 Wortley Road (WV-OS HCD) – signage
- 585 Colborne Street (WW HCD) – porch alterations
- 34 Argyle Street (B/P HCD) – rear addition
- 428 English Street (OE HCD) – parking
- 106 Askin Street (WV-OS HCD) – detached accessory building
- 29 Bruce Street (WV-OS HCD) – accessibility alterations
- 36 Askin Street (WV-OS HCD) – parking
- 179 Wortley Road (WV-OS HCD) – signage
- 532 Dufferin Avenue (EW HCD) – windows

- 169 Wortley Road (WV-OS HCD) – porch alterations
- 850 Lorne Avenue (OE HCD) – porch alterations
- 380 Princess Avenue (WW HCD) – addition
- 63 Byron Avenue East (WV-OS HCD) – masonry
- 811 Princess Avenue (OE HCD) – porch alterations
- 529 Princess Avenue (Part IV) – porch alterations
- 85 Wilson Avenue (B/P HCD) – windows
- 720 Lorne Avenue (OE HCD) – porch alterations
- 310 Grosvenor Street (BH HCD) – rear addition and detached garage
- 129 Duchess Avenue (WV-OS HCD) – rear addition alterations
- 190 Wortley Road (WV-OS HCD) – signage
- 329-331 Richmond Street (DT HCD) – cast iron columns
- 106 Duchess Avenue (WV-OS HCD) – porch alterations
- 633 Wellington Street (WW HCD) – signage
- 472 Richmond Street (DT HCD/Part IV) – freestanding sign
- 550 Dufferin Avenue (EW HCD) – roof replacement, windows
- 130 King Street (DT HCD) – signage
- 239 Wortley Road (WV-OS HCD) – porch alterations
- 868 Waterloo Street (BH HCD) – storm windows
- 420 Talbot Street (DT HCD) – security shutter
- 63 Byron Avenue East (WV-OS HCD) – rear addition removal
- 99 Dundas Street (DT HCD) – signage

The review of 100% of these Heritage Alteration Permit applications was completed within the provincially mandated timeline. No Heritage Alteration Permit applications were referred to the CACP or Municipal Council for a decision (Table 1).

*Table 1: Summary of Heritage Alteration Permits (HAP) by review type and time period.*

	<b>Delegated Authority</b>	<b>Municipal Council</b>	<b>Total</b>
<b>HAP applications (Q3 2024)</b>	31	0	31
<b>HAP applications (year to date)</b>	68	1	69
<b>HAP applications (2023)</b>	97	8	105
<b>HAP applications (2022)</b>	89	14	103
<b>HAP applications (2021)</b>	70	16	86

## **Conclusion**

The purpose of this report is to provide Municipal Council with information regarding Heritage Alteration Permits that were processed pursuant to the Delegated Authority By-law during the third quarter of 2024.

**Prepared by:** Kyle Gonyou, RPP, MCIP, CAHP  
**Manager, Heritage and Urban Design**

**Submitted by:** Kevin Edwards, RPP, MCIP  
**Manager, Community Planning**

**Recommended by:** Heather McNeely, RPP, MCIP  
**Director, Planning and Development**

**Submitted by:** Scott Mathers, MPA, P. Eng.  
**Deputy City Manager, Planning and Economic Development**

## Report to Planning & Environment Committee

**To:** Chair and Members  
Planning & Environment Committee

**From:** Scott Mathers, MPA, P. Eng  
Deputy City Manager, Planning and Economic Development

**Subject:** Building Division Detailed Update: 2024 Year-To-Date

**Date:** September 25, 2024

## Recommendation

That, on the recommendation of the Deputy City Manager, Planning and Economic Development, the following report **BE RECEIVED**.

## Executive Summary

The Building Division is responsible for the administration and enforcement of the *Ontario Building Code Act* and the *Ontario Building Code*. Related activities undertaken by the Building Division include the processing of building permit applications and inspections of associated construction work. The Building Division also issues sign and pool fence permits. The purpose of this report is to provide Municipal Council with information related to permit issuance and inspection activities for the year until the end of August 2024.

As of August 31, 2024, the Building Division recorded growth in key metrics compared to the same period last year. A total of 2,677 new dwelling units were approved, representing a 5.4% increase. The construction value reached \$1.6 billion, reflecting a 87.9% increase. These figures highlight a rise in development activity within the municipality.

## Linkage to the Corporate Strategic Plan

Growing our Economy

- London is a leader in Ontario for attracting new jobs and investments.

Leading in Public Service

- The City of London is trusted, open, and accountable in service of our community.
- Improve public accountability and transparency in decision making.

## Analysis

### 1.0 Background Information

This report provides information on permit and associated inspection activities for the month of August 2024. Attached as Appendix “A” to this report is a “Summary Listing of Building Construction Activity for the Month of August 2024”, as well as respective “Principle Permits Reports”.

### 2.0 Discussion and Considerations

#### 2.1 Building Permit Data and Inspection Activities as of August 31, 2024

##### Year-to-date Permits Issued

As of August 31, 2024, a total of 2,645 permits were issued, with a construction value of \$1.6 billion, representing 2,677 new dwelling units. Compared to the same period in

2023, this represents a 5.38% increase in the number of building permits, with a 87.9% increase in construction value and an 124.77% increase in the number of dwelling units constructed.

### **Total permits to construct New Single and Semi-Dwelling Units**

As of the end of August 2024, the number of building permits issued for the construction of single and semi-detached dwellings was 190, representing a 14.5% increase over the same period in 2023.

### **Number of Applications in Process**

As of the end of August 2024, 800 applications are in process, representing approximately \$1.1 billion in construction value and an additional 2,677 dwelling units compared with 968 applications, with a construction value of \$917 million and an additional 1,167 dwelling units in the same period in 2023.

### **Rate of Application Submission**

Applications received up to August 31, 2024 averaged to 15.7 applications per business day, for a total of 2,645 applications. Of the applications submitted 190 were for the construction of single detached dwellings and 369 townhouse units.

### **Inspections – Building**

A total of 16,008 inspection requests were received with 17,668 inspections being conducted.

In addition, 143 inspections were completed related to complaints, business licenses, orders and miscellaneous inspections.

Of the 16,008 inspections requested, 100% were conducted within the provincially mandated 48 hour period.

### **Inspections - Code Compliance**

A total of 8,482 inspection requests were received, with 8,145 inspections being conducted.

An additional 1,388 inspections were completed relating to complaints, business licences, orders and miscellaneous inspections.

Of the 8,482 inspections requested, 100% were conducted within the provincially mandated 48 hour period.

### **Inspections - Plumbing**

A total of 7,542 inspection requests were received with 9,927 inspections being conducted related to building permit activity.

An additional 95 inspections were completed related to complaints, business licenses, orders and miscellaneous inspections.

Of the 7,542 inspections requested, 100% were conducted within the provincially mandated 48 hour period.

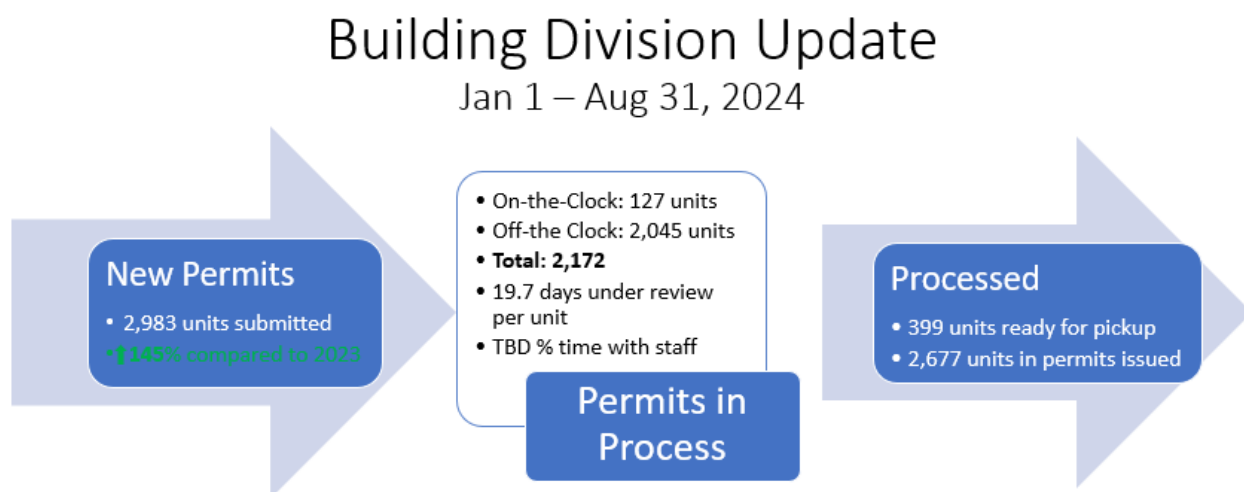


## 2019-2024 Permit Data

Additional permit data has been provided in Appendix “A” to reflect 2019 – 2024 permit data.

## New Housing Unit Activity

The following diagram provides a simplified summary of building permit activity beginning at the start of the calendar year. It was reported in the October of 2023 in a report titled “London’s Housing Pledge: A Path to 47,000 units by 2031 Update” to the Strategic Priorities and Policy Committee that this figure would be included in future Building Division update reports.



This figure provides a deeper dive into the Permits and Inspections Housing Unit Supply number discussed in the previous section. It shows the inflow and outflow of housing unit permits through the building area on a year-to-date basis and the volume of units in permits that are under review.

## Comparison to CHMC Targets

This section provides a comparison of actual progress towards these targets, both on an annual basis and cumulatively.

### **Annual Target vs. Actual**

Year	CMHC Target	Units Completed	Difference	% Difference
2023	3,447	1,726	-1,721	-49.9%
2024	3,917	2,677 (YTD)	-1,240 (YTD)	-31.6% (YTD)
2025	4,700	TBD	TBD	TBD

As of 2024 YTD, we are 1,240 units behind the CMHC target for the year, which represents a 31.6% deficit.

If we are to calculate the average rate of units completed per month so far this year and then extrapolate that over the remained of the year a projected estimate of 4,016 units may be hit by years end.

### **Cumulative Progress Towards 2031 Target**

Year	Cumulative Target	Cumulative Units	% of Target Achieved
2023	3,447	1,726	3.7%
2024	7,364	4,403 (YTD)	9.4%
2025	12,064	TBD	TBD
2031	47,000	TBD	TBD

As of 2024 YTD, we have achieved 9.4% of the overall target of 47,000 units for 2031. While we are ahead of 2023's performance, additional efforts will be required to close the gap and meet future targets.

### **3.0 Analysis**

Analysis of September 31, 2024 building permit data shows a strengthening in the housing and building marketplace over the same period in 2023. The City of London saw a 5.4% increase in building permits compared to the same period last year. Construction values have increased substantially by 87.9%. Single and semi-detached dwellings units in permits have increased by over last year by 15.6%; multi-unit (Duplex, Triplex, Quadplex, Apartment Buildings) have increased by over 392.5% resulting in an overall increase in units by over 124.8%.

### **Conclusion**

The purpose of this report is to provide Municipal Council with information regarding the building permit issuance and building & plumbing inspection activities for the month of May 2024. Attached as Appendix "A" to this report is a "Summary Listing of Building Construction Activity" as of May 31, 2024 as well as "Principle Permits Reports".

**Prepared by:** Alan Shaw  
Deputy Chief Building Official  
Planning and Economic Development

**Submitted by:** Scott Mathers, MPA, P.Eng.  
Deputy City Manager  
Planning and Economic Development

**Recommended by:** Scott Mathers, MPA, P.Eng.  
Deputy City Manager  
Planning and Economic Development

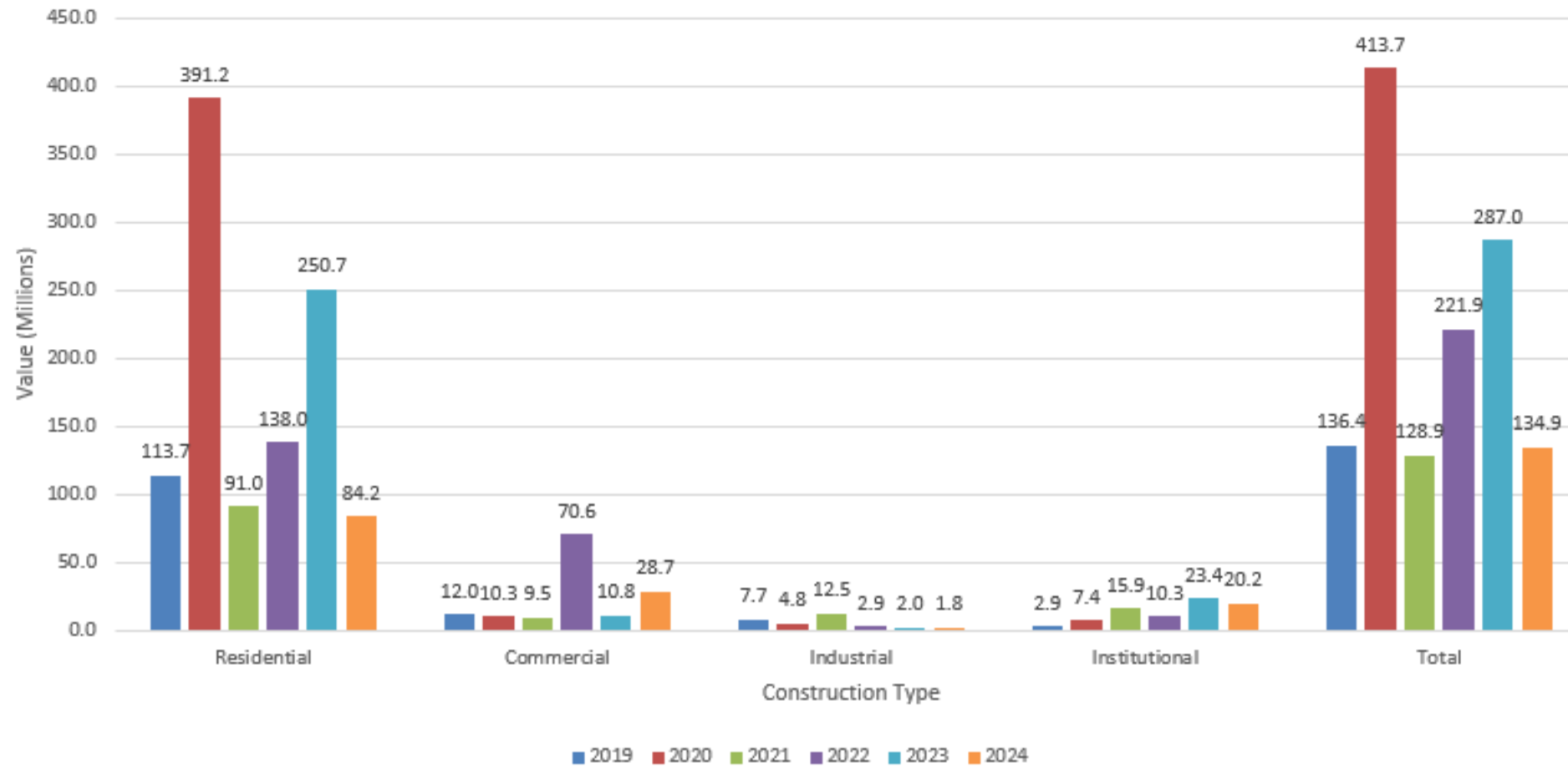
APPENDIX "A"

**CITY OF LONDON**  
**SUMMARY LISTING OF BUILDING CONSTRUCTION ACTIVITY FOR THE MONTH OF August 2024**

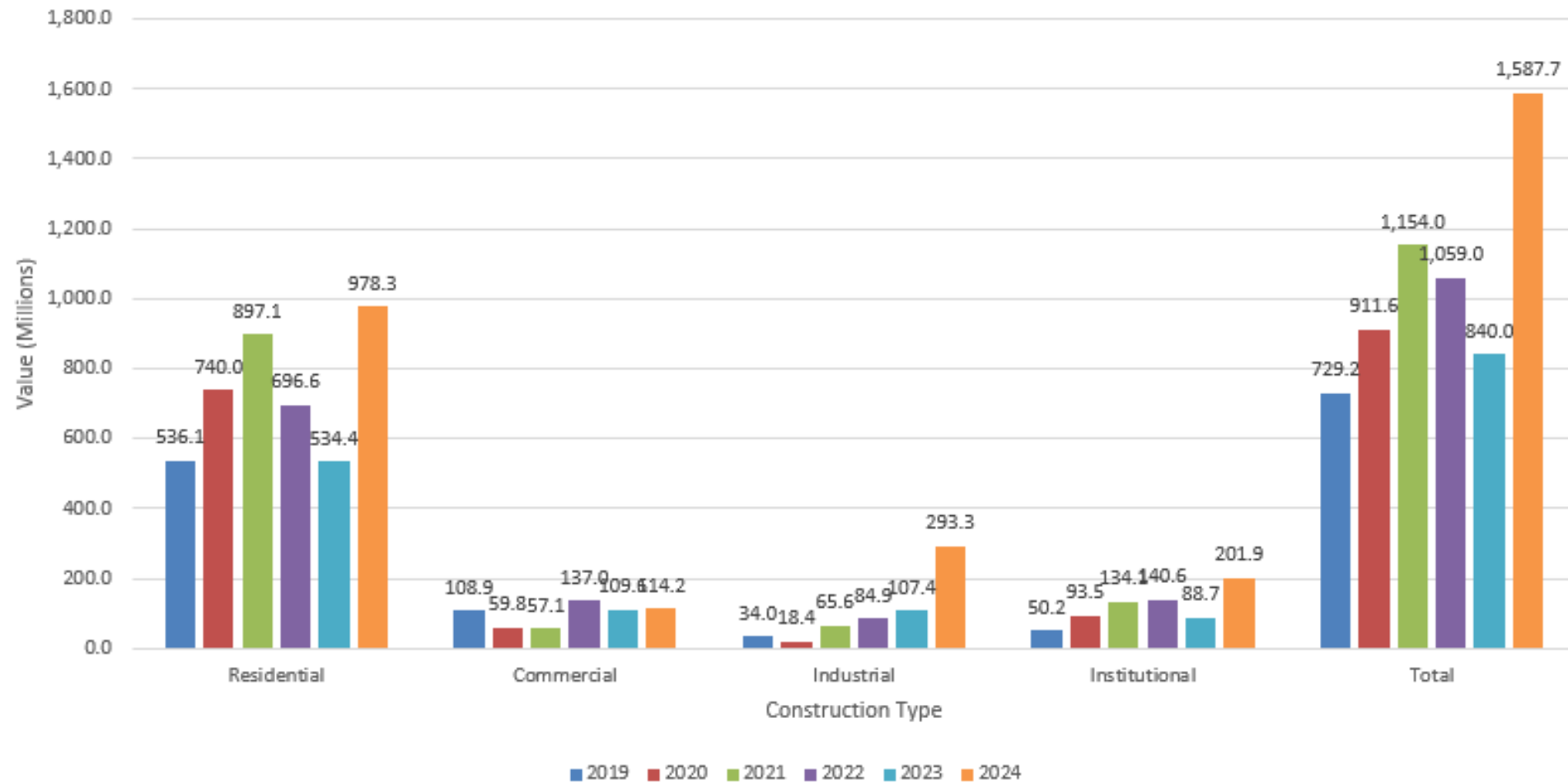
CLASSIFICATION	August 2024			to the end of August 2024			August 2023			to the end of August 2023			August 2022			to the end of August 2022		
	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS
SINGLE DETACHED DWELLINGS	18	15,019,336	18	188	119,652,713	189	30	16,976,648	30	164	90,256,055	164	58	30,238,847	58	480	243,461,189	480
SEMI DETACHED DWELLINGS	0	0	0	2	1,939,988	4	0	0	0	2	1,854,757	3	0	0	0	0	0	0
TOWNHOUSES	16	36,657,300	90	62	139,387,370	369	11	22,718,930	71	50	90,380,097	272	21	36,089,701	91	98	140,405,624	457
DUPLEX, TRIPLEX, QUAD, APT BLDG	4	22,209,064	72	30	610,996,243	1,778	2	49,696,221	74	13	128,611,951	361	4	59,165,143	175	12	244,442,763	675
RES-ALTER & ADDITIONS	172	10,305,876	33	1,343	106,357,165	331	209	161,354,383	260	1,285	223,271,394	391	228	12,535,924	26	1,351	68,291,507	134
COMMERCIAL -ERECT	2	10,000,000	0	9	26,002,837	2	0	0	0	6	38,580,370	0	2	1,600,000	0	11	39,508,279	0
COMMERCIAL - ADDITION	1	6,500,000	0	16	11,978,056	0	1	5,500,000	0	8	8,511,730	0	3	22,510,000	0	9	26,151,900	0
COMMERCIAL - OTHER	32	12,181,346	0	255	76,174,031	4	20	5,323,900	0	210	62,490,050	0	30	46,521,210	0	192	71,311,824	0
INDUSTRIAL - ERECT	0	574,989	0	6	107,288,507	0	0	0	0	4	46,321,468	0	1	1,199,606	0	2	29,580,184	0
INDUSTRIAL - ADDITION	1	1,273,000	0	9	135,826,255	0	0	0	0	7	45,228,428	0	0	0	0	7	52,657,871	0
INDUSTRIAL - OTHER	5	0	0	45	50,174,084	0	6	1,973,000	0	35	15,888,051	0	6	1,700,460	0	21	2,619,160	0
INSTITUTIONAL - ERECT	0	800,000	0	6	120,206,727	0	1	14,988,852	0	3	34,699,252	0	0	0	0	3	99,646,230	0
INSTITUTIONAL - ADDITION	2	17,821,700	0	7	9,913,871	0	2	2,005,000	0	14	12,209,743	0	0	0	0	2	2,379,000	0
INSTITUTIONAL - OTHER	21	1,600,000	0	163	71,777,608	0	20	6,451,800	0	149	41,744,016	0	40	10,311,000	0	108	38,547,536	0
AGRICULTURE	1	693,968	0	6	3,062,500	0	2	625,000	0	6	4,080,000	0	0	0	0	3	1,610,000	0
SWIMMING POOL FENCES	15	33,000	0	102	4,509,481	0	15	692,000	0	136	4,732,368	0	38	2,213,518	0	272	10,884,409	0
ADMINISTRATIVE	16	0	0	80	318,000	0	17	39,000	0	109	432,000	0	19	45,000	0	112	852,000	0
DEMOLITION	9	0	6	90	0	60	11	0	6	71	0	43	3	0	1	71	0	45
SIGNS/CANOPY - CITY PROPERTY	1	0	0	7	0	0	4	0	0	8	0	0	2	0	0	13	0	0
SIGNS/CANOPY - PRIVATE PROPERTY	32	0	0	219	0	0	20	0	0	230	0	0	40	0	0	259	0	0
<b>TOTALS</b>	<b>348</b>	<b>135,669,579</b>	<b>213</b>	<b>2,645</b>	<b>1,595,565,435</b>	<b>2,677</b>	<b>371</b>	<b>288,344,734</b>	<b>435</b>	<b>2,510</b>	<b>849,291,729</b>	<b>1,191</b>	<b>495</b>	<b>224,130,409</b>	<b>351</b>	<b>3,026</b>	<b>1,072,349,476</b>	<b>1,791</b>

CLASSIFICATION	August 2021			to the end of August 2021			August 2020			to the end of August 2020			August 2019			to the end of August 2019		
	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS
SINGLE DETACHED DWELLINGS	70	34,137,150	70	780	356,736,550	780	64	28,730,500	64	545	232,191,004	545	61	28,676,520	61	490	205,006,176	490
SEMI DETACHED DWELLINGS	0	0	0	1	223,500	1	0	0	0	1	354,000	2	0	0	0	0	0	0
TOWNHOUSES	14	15,291,300	56	162	149,539,000	606	7	7,150,200	24	80	72,293,685	266	14	14,780,360	54	164	122,479,869	490
DUPLEX, TRIPLEX, QUAD, APT BLDG	1	36,000,000	124	11	333,596,000	1,485	4	351,000,000	1,086	10	400,878,800	1,310	2	64,758,000	283	7	164,602,920	713
RES-ALTER & ADDITIONS	165	5,596,716	14	1,249	57,017,596	88	155	4,302,493	8	896	34,296,009	31	163	5,526,295	6	1,260	43,982,533	53
COMMERCIAL - ERECT	0	0	0	21	11,424,400	0	1	2,500,000	0	9	7,160,300	0	1	3,100,000	0	24	53,215,903	0
COMMERCIAL - ADDITION	1	55,000	0	5	455,000	0	0	0	0	2	791,800	0	1	8,000	0	12	9,650,718	0
COMMERCIAL - OTHER	34	9,423,200	0	234	45,261,304	0	29	7,806,692	0	235	51,812,827	0	46	8,889,330	2	308	46,082,989	0
INDUSTRIAL - ERECT	5	12,349,909	0	12	45,227,409	0	2	4,750,000	0	4	8,186,700	0	2	6,400,000	0	3	9,450,000	0
INDUSTRIAL - ADDITION	0	0	0	5	3,386,560	0	0	0	0	4	7,918,800	0	0	0	0	5	7,278,000	0
INDUSTRIAL - OTHER	3	142,000	0	24	16,961,980	0	2	5,300	0	25	2,277,907	0	8	1,329,200	0	42	17,242,804	0
INSTITUTIONAL - ERECT	0	0	0	1	12,000,000	0	0	0	0	3	32,825,000	0	0	0	0	0	0	0
INSTITUTIONAL - ADDITION	1	660,000	0	6	47,273,386	0	0	0	0	8	15,178,000	0	2	252,000	0	5	14,049,600	0
INSTITUTIONAL - OTHER	15	15,227,800	0	99	74,854,950	0	20	7,408,000	0	118	45,456,001	0	20	2,641,000	0	189	36,144,410	0
AGRICULTURE	1	200,000	0	2	350,000	0	0	0	0	1	100,000	0	0	0	0	3	210,000	0
SWIMMING POOL FENCES	44	1,416,400	0	307	8,679,140	0	58	1,637,071	0	278	6,724,570	0	24	512,050	0	175	3,757,520	0
ADMINISTRATIVE	5	12,000	0	64	256,000	0	8	5,000	0	34	100,000	0	19	52,000	0	125	404,100	0
DEMOLITION	11	0	6	57	0	41	12	0	11	52	0	39	9	0	7	62	0	26
SIGNS/CANOPY - CITY PROPERTY	1	0	0	8	0	0	0	0	0	2	0	0	2	0	0	10	0	0
SIGNS/CANOPY - PRIVATE PROPERTY	38	0	0	223	0	0	18	0	0	191	0	0	23	0	0	268	0	0
<b>TOTALS</b>	<b>409</b>	<b>130,511,475</b>	<b>264</b>	<b>3,271</b>	<b>1,163,242,774</b>	<b>2,960</b>	<b>380</b>	<b>415,295,256</b>	<b>1,182</b>	<b>2,498</b>	<b>918,545,403</b>	<b>2,154</b>	<b>397</b>	<b>136,924,755</b>	<b>406</b>	<b>3,152</b>	<b>733,557,542</b>	<b>1,746</b>

Construction Value of Building Permits (August)



Construction Value of Building Permits (Jan-August)





**City of London - Building Division**  
**Principal Permits Issued from August 1, 2024 to August 31, 2024**

Owner	Project Location	Proposed Work	No. of Units	Construction Value
NICHOLAS SCHREFF HANS SCHREFF	10 Kenneth Ave	(statcan) Add (Residential) - Four-Plex ADDITION TO REAR OF HOUSE ADDING (2) ADDITION DWELLING UNITS FOR A TOTAL OF 4 UNITS *****ADDITIONAL RESIDENTIAL UNITS AS PER SEC 4.37 OF ZONING BY-LAW Z-1***** *****TOTAL OF 5 BEDROOMS COMBINED AMONGST ALL 4 UNITS*****	2	319,072
IRONSTONE BUILDING COMPANY INC. IRONSTONE BUILDING COMPANY INC.	1030 Darnley Blvd	Install - Townhouse - Condo install site services		1,297,000
WONDERLAND POWER CENTRE INC. WONDERLAND POWER CENTRE INC.	1040 Wharnccliffe Rd S	(statcan) Alter - Recreation Centre ALTERATION TO CREATE A RECREATIONAL FACILITY ( SMASH PICKLEBALL COURTS). ***SHELL PERMIT***Sprinkler work not authorized. Provide sealed sprinkler shop drawings and hydraulic calculations. Provide stairs, guardrails and handrails shop drawings.	0	500,000
Wellington Commons Holdings Inc	1061 Hargrieve Rd	(statcan) Alter - Offices Interior Fit Out ***SHELL PERMIT***Provide sprinkler shop drawings	0	3,000,000
2261531 LTD. 2261531 ONTARIO LTD.	1103 Adelaide St N	(statcan) Alter - Restaurant INTERIOR ALTERATION TO EXISTING TIM HORTONS RESTAURANT *****ALL PROPOSED SIGNAGE ON SEPERATE PERMIT***** *****ALL PROPOSED EXTERIOR / FACADE WORK MUST BE CONDUCTED ON PRIVATE PROPERTY OTHERWISE A PERMIT OF APPROVED WORKS IS REQUIRED*****	0	458,987
DREWLO HOLDINGS INC DREWLO HOLDINGS INC	1147 Hobbs Dr	(statcan) Erect - Duplex ERECT NEW SDD 2 STOREYS, 1 CAR GARAGE, 4 BEDROOMS IN THE MAIN DWELLING, 1 BEDROOM ADDITIONAL RESIDENTIAL UNIT IN THE BASEMENT, SB-12 A1, LOT-157, Plan 33M-826, NO A/C, NO DECK, HRV & DWHR REQUIRED. **SOILS REPORT REQUIRED** *****TOTAL OF 5 BEDROOMS FOR BOTH UNITS COMBINED***** *****ADDITIONAL RESIDENTIAL UNITS AS PER SEC 4.37 OF ZONING BY-LAW Z-1***** *****AC MUST BE A MINIMUM OF 3 FEET FROM ANY PROPERTY LINE*****	2	527,472



**City of London - Building Division**  
**Principal Permits Issued from August 1, 2024 to August 31, 2024**

Owner	Project Location	Proposed Work	No. of Units	Construction Value
WESTERN UNIVERSITY WESTERN UNIVERSITY	1151 Richmond St	Install - Water service/main - University Infrastructure upgrade work along Perth Drive on campus. Scope of work involves preparing the underground utilities to support the construction of the forthcoming Western Commons Building.		3,500,000
WESTERN UNIVERSITY WESTERN UNIVERSITY	1151 Richmond St	Install - Water service/main - University Infrastructure upgrade work along Perth Drive on campus. Scope of work involves preparing the underground utilities to support the construction of the forthcoming Western Commons Building.	0	3,500,000
WESTERN UNIVERSITY WESTERN UNIVERSITY	1151 Richmond St	Install Site Services - University INSTALL SITE SERVICES		6,200,000
SIFTON LIMITED SIFTON PROPERTIES LIMITED	1175 Riverbend Rd D	(statcan) Erect - Townhouse - Condo ERECT NEW TOWNHOUSE BLOCK BLDG D - 8 UNITS, DPN 40, 42, 44, 46, 48, 50, 52 & 54, 2 STOREY, 1 CAR GARAGE, 3 BEDROOMS, UNFINISHED BASEMENT, W/ A/C, UNCOVERED DECK, HOT 2000, HRV & DWHR REQUIRED	8	2,887,371
CANADIAN COMMERCIAL DEVELOPMENT CORP	1225 Wonderland Rd N	(statcan) Alter - Libraries INTERIOR ALTERATION TO SHERWOOD MALL LIBRARY . ***SHELL PERMIT*** Provide Sprinkler drawings for review.	0	1,900,000
TONY MARSMAN CONSTRUCTION LTD TONY MARSMAN CONSTRUCTION LTD	1300 Fanshawe Park Rd E	Install Site Services - Site Services Site services for a commercial plaza consist of 6 buildings, and parking area		3,000,000
2555212 ONTARIO LTD. 2555212 ONTARIO LTD.	1525 Chickadee Trail A	(statcan) Erect - Street Townhouse - Condo ERECT NEW TOWNHOUSE BLOCK - BLOCK A, 5 UNITS, 2 STOREYS, 1 CAR GARAGE, 3 BEDROOMS, UNFINISHED BASEMENT, A/C, COVERED PORCH, UNCOVERED DECK, SB-12 A1, HRV & DWHR REQUIRED	5	2,015,527
CF REALTY HLDG INC., FAIRVIEW CORP CF REALTY HLDG INC., C/O CADILLAC FAIRVIEW CORP	1680 Richmond St	(statcan) Alter - Retail Store CARTER'S INTERIOR FIT UP OF VACANT RETAIL UNIT IN AN EXISTING MALL	0	450,000
IRONSTONE BUILDING COMPANY INC. IRONSTONE BUILDING COMPANY INC.	1870 Evans Blvd E	(statcan) Erect - Townhouse - Condo ERECT NEW TOWNHOUSE BLOCK BLDG E - 8 UNITS, UNIT 49, 51, 53, 55, 66, 64, 62, 60, 3-STOREY, 1 CAR GARAGE, 3 BEDROOMS, UNFINISHED BASEMENT, W/ A/C, DECK, SB-12 A1, HRV & DWHR REQUIRED	8	3,244,925





**City of London - Building Division**  
**Principal Permits Issued from August 1, 2024 to August 31, 2024**

Owner	Project Location	Proposed Work	No. of Units	Construction Value
IRONSTONE BUILDING COMPANY INC. IRONSTONE BUILDING COMPANY INC.	1870 Evans Blvd F	(statcan) Erect - Townhouse - Condo ERECT NEW TOWNHOUSE BLOCK BLDG F - 4 UNITS, UNIT 44, 46, 48, 50, 3 STOREY, 1 CAR GARAGE, 3 BEDROOMS, UNFINISHED BASEMENT, W/ A/C, DECK, SB-12 A1, HRV & DWHR REQUIRED	4	1,604,095
IRONSTONE BUILDING COMPANY INC. IRONSTONE BUILDING COMPANY INC.	1870 Evans Blvd G	(statcan) Erect - Townhouse - Condo ERECT NEW TOWNHOUSE BLOCK, BLDG G, 4 UNITS DPN 52, 54, 56, 58, 3 STOREYS, 1 CAR GARAGE, 3 BEDROOMS, NO BASEMENT, NO A/C, COVERED PORCH, UNCOVERED DECK, SB-12 A1, HRV & DWHR REQUIRED	4	1,604,095
IRONSTONE BUILDING COMPANY INC. IRONSTONE BUILDING COMPANY INC.	1870 Evans Blvd O	(statcan) Erect - Townhouse - Condo ERECT NEW TOWNHOUSE BLOCK BLDG O - 4 UNITS, UNIT 89, 91, 93, 95, 3 STOREY, 1 CAR GARAGE, 3 BEDROOMS, UNFINISHED BASEMENT, W/ A/C, DECK, SB-12 A1, HRV & DWHR REQUIRED	4	1,604,095
IRONSTONE BUILDING COMPANY INC. IRONSTONE BUILDING COMPANY INC.	1870 Evans Blvd P	(statcan) Erect - Townhouse - Condo ERECT NEW TOWNHOUSE BLOCK BLDG P - 8 UNITS, UNIT 73, 75, 77, 79, 81, 83, 85, 87, 3 STOREY, 1 CAR GARAGE, 3 BEDROOMS, UNFINISHED BASEMENT, W/ A/C, NO DECK, SB-12 A1, HRV & DWHR REQUIRED	8	3,183,700
IRONSTONE BUILDING COMPANY INC. IRONSTONE BUILDING COMPANY INC.	1870 Evans Blvd Q	(statcan) Erect - Townhouse - Condo ERECT NEW TOWNHOUSE BLOCK - BLDG Q, 8 UNITS DPN 57, 59, 61, 63, 65, 67, 69, 71, 3 STOREYS, 1 CAR GARAGE, 3 BEDROOMS, NO BASEMENT, NO A/C, COVERED PORCH, UNCOVERED DECK, SB-12 A1, HRV & DWHR REQUIRED	8	3,181,251
JACOB KATZ HYDE PARK VENTURES INC.	1875 Dalmagarry Rd QQ	(statcan) Erect - Townhouse - Condo ERECT NEW TOWNHOUSE BLOCK BLDG QQ - 4 UNITS, CATALPA SQUARE UNIT 819, 821, 823, 825, 2 STOREY, 1 CAR GARAGE, 3 BEDROOMS, UNFINISHED BASEMENT, W/ A/C, NO DECK, SB-12 A1, HRV & DWHR REQUIRED	4	1,728,994
JACOB KATZ HYDE PARK VENTURES INC.	1875 Dalmagarry Rd SS	(statcan) Erect - Townhouse - Condo ERECT NEW TOWNHOUSE BLOCK BLDG SS - 4 UNITS, 803, 805, 807 & 809 CATALPA SQUARE, 2 STOREY, 1 CAR GARAGE, 3 BEDROOMS, UNFINISHED BASEMENT, W/ A/C, NO DECK, S12 A1, HRV & DWHR REQUIRED	4	1,728,994



**City of London - Building Division**  
**Principal Permits Issued from August 1, 2024 to August 31, 2024**

Owner	Project Location	Proposed Work	No. of Units	Construction Value
NICHOLAS DI PARDO	2075 Trafalgar St	(statcan) Erect - Duplex ERECT 2 UNITS. 2 STOREY, 4 BEDROOM, UNFINISHED BASEMENT, SB-12 A5, HRV & DWHR REQUIRED. (2081 Trafalgar St Block A) *****AC MUST BE A MINIMUM OF 3 FEET FROM ANY PROPERTY LINE***** *****ADDITIONAL RESIDENTIAL UNITS AS PER SEC 4.37 OF ZONING BY-LAW Z-1***** *****TOTAL OF 4 BEDROOMS FOR BOTH UNITS COMBINED*****	2	673,992
SAM MANGAT CHARDI KALA INVESTMENTS LTD	2130 Kains Rd	(statcan) Alter - Restaurant INTERIOR FIT-UP FOR AUTHENTIC RESTAURANT (A2), COMBINING 2.5 UNITS	0	486,234
SAM MANGAT CHARDI KALA INVESTMENTS LTD	2136 Kains Rd	(statcan) Alter - Beauty Parlours Interior fit-up for a Nail Salon. *****A CITY OF LONDON BUSINESS LICENSE IS REQUIRED*****	0	598,745
ST JOSEPH'S HEALTH CARE LONDON ST JOSEPH'S HEALTH CARE LONDON	268 Grosvenor St	(statcan) Alter - Hospitals Addition of sprinkler system	0	600,000
1028147 Ontario Limited	311 Oxford St E	(statcan) Add (Non-Residential) - Private School - Elementary 2 STOREY 58M2 Addition to provide a new Elevator and Entrance Vestibule AND STAIRS	0	750,000
VINCENT O'DONOVAN WONDERLAND POWER CENTRE INC.	3245 Wonderland Rd S	(statcan) Erect - Retail Store Permit for building shell and interior of new retail home goods and fashion user.	0	2,000,000
Cieslak Paul	325 Edmonton St	(statcan) Erect - Apartment Building ERECT 2 STOREY SEMI-DETACHED BUILDING, LOTS 142 AND 143. 2 ADDITIONAL DWELLING UNITS IN EACH FOR A TOTAL OF 6 UNITS	6	1,694,600
VINCENT O'DONOVAN WONDERLAND POWER CENTRE INC.	3263 Wonderland Rd S	(statcan) Erect - Retail Store ERECT GROUP E BUILDING, INCLUDING INTERIOR FIT UP - ADONIS	0	8,000,000
YORK DEVELOPMENTS YORK DEVELOPMENTS	3325 Wonderland Rd S	(statcan) Alter - Retail Store Interior fit-up. *****ALL PROPOSED SIGNAGE ON SEPERATE PERMIT*****	0	650,000
DREWLO HOLDINGS INC DREWLO HOLDINGS INC	380 Asher Cres	(statcan) Erect - Apartment Building BLDG A - ERECT 6 STOREY APARTMENT BUILDING WITH 62 UNITS	62	19,313,000
1000171215 Ontario Inc	4102 Eastgate Cres	(statcan) Add (Non-Residential) - Warehousing Addition to an existing industrial building	0	574,989



**City of London - Building Division**  
**Principal Permits Issued from August 1, 2024 to August 31, 2024**

Owner	Project Location	Proposed Work	No. of Units	Construction Value
SIFTON LIMITED SIFTON PROPERTIES LIMITED	4209 Calhoun Way A	(statcan) Erect - Street Townhouse - Condo ERECT NEW TOWNHOUSE, BLOCK A - 4 UNITS, 4293, 4295, 4297, 4299, 2-STOREY, 1 CAR GARAGE, 3 BEDROOMS, UNFINISHED BASEMENT, W/ A/C, NO DECK, SB-12 A1, HRV & DWHR REQUIRED	4	1,613,891
1726201 Ontario Inc	456 Southdale Rd E	(statcan) Alter - Daycare Centres INTERIOR ALTERATION FOR DAYCARE	0	315,000
TALU PROPERTIES INC. TALU PROPERTIES INC.	460 Asher Cres	Install - Townhouse - Rental Application for permit to construct or demolish - install site services for three apartment buildings consisting of 176 units and a ten block townhouse development, consisting of 72 units.		2,491,453
TALU PROPERTIES INC. TALU PROPERTIES INC.	460 Asher Cres A	(statcan) Erect - Street Townhouse - Rental ERECT NEW TOWNHOUSE BLOCK - BLDG A, 8 UNITS DPN 1, 3, 5, 7 and 462, 466, 470, 474 Asher Cres. 3 STOREYS, 2-CAR GARAGE, 3 BEDROOMS, NO BASEMENT, COVERED PORCH, NO A/C, SB-12 A1, HRV & DWHR REQUIRED **SOILS REPORT REQUIRED.**	8	3,249,823
LISA BECKETT TALU PROPERTIES INC.	460 Asher Cres B	(statcan) Erect - Street Townhouse - Rental ERECT NEW TOWNHOUSE BLOCK - BLDG B, 8 UNITS DPN 9, 11, 13, 15 and 478, 482, 486, 490 Asher Cres. 3 STOREYS, 2-CAR GARAGE, 3 BEDROOMS, NO BASEMENT, COVERED PORCH, NO A/C, SB-12 A1, HRV & DWHR REQUIRED **SOILS REPORT REQUIRED.**	8	3,249,823
LISA BECKETT TALU PROPERTIES INC.	460 Asher Cres C	(statcan) Erect - Street Townhouse - Rental ERECT NEW TOWNHOUSE BLOCK - BLDG C, 6 UNITS DPN 17, 19, 21, 23, 25, 27 3 STOREYS, 1-CAR GARAGE, 3 BEDROOMS, NO BASEMENT, COVERED PORCH, NO A/C, SB-12 A1, HRV & DWHR REQUIRED **SOILS REPORT REQUIRED.**	6	2,375,530
LISA BECKETT TALU PROPERTIES INC.	460 Asher Cres J	(statcan) Erect - Street Townhouse - Rental ERECT NEW TOWNHOUSE BLOCK - BLDG J, 6 UNITS DPN 29, 31, 33, 35, 37, 39 3 STOREYS, 1-CAR GARAGE, 3 BEDROOMS, NO BASEMENT, COVERED PORCH, NO A/C, SB-12 A1, HRV & DWHR REQUIRED **SOILS REPORT REQUIRED.**	6	2,375,530
ST JOSEPH'S HEALTH CARE LONDON ST JOSEPH'S HEALTH CARE LONDON	550 Wellington Rd	(statcan) Alter - Care Facility Interior Renovations to an existing Kitchen and Laundry Room	0	450,000



**City of London - Building Division**  
**Principal Permits Issued from August 1, 2024 to August 31, 2024**

Owner	Project Location	Proposed Work	No. of Units	Construction Value
ST JOSEPH'S HEALTH CARE LONDON ST JOSEPH'S HEALTH CARE LONDON	550 Wellington Rd	Install - Hospitals This project will replace some of the existing 4" domestic water pipes within the existing building on Level 1. The new piping will run adjacent to the existing, and tie-in on the same floor.		465,000
700 Richmond Inc	700 Richmond St	(statcan) Alter - Offices ALTER 4TH FLOOR UNIT	0	1,229,015
SOUTHSIDE MANAGEMENT LTD SOUTHSIDE CONSTRUCTION MANAGEMENT LTD	720 Apricot Dr 8	(statcan) Erect - Townhouse - Cluster SDD ERECT NEW CLUSTER SDD. 2 STOREYS, 2 CAR GARAGE, 4 BEDROOMS, COVERED DECK, UNFINISHED BASEMENT, SB-12 A1, Unit 4, MVLCP. No. 972, NO A/C, HRV & DWHR REQUIRED. **SOILS REPORT REQUIRED.**	1	1,009,656
1985731 Ontario Inc	8 Kenneth Ave	(statcan) Add (Residential) - Four-Plex ADDITION TO REAR OF HOUSE EXISTING DUPLEX, CREATING (2) ADDITIONAL RESIDENTIAL UNITS	2	319,072
LONDON CITY	900 King St	(statcan) Alter - Food Processing Plant Shell fitup of spaces in existing building to be leased to future tenants for food processing purposes.	0	1,000,000
CORPORATION OF OF LONDON CORPORATION OF THE CITY OF LONDON	99 Dundas St	(statcan) Add (Non-Residential) - Arenas Budweiser Gardens Arena - INTERIOR ALTERATION TO 5 CONCESSIONS ON LEVEL 1&2 ***SHELL PERMIT***Provide sprinkler shop drawing, hood suppression shop drawing and GRCC.	0	6,500,000

Total Permits 47    Units 166    Value 109,420,931

*\* Includes all permits over \$250,000, except for single and semi-detached dwellings.*

## Report to Planning and Environment Committee

**To:** Chair and Members  
Planning and Environment Committee  
**From:** Scott Mathers, MPA, P.Eng.  
Deputy City Manager, Planning and Economic Development  
**Subject:** Development Pipeline – Developer of Lands within Open Site  
Plans, Open Condominium Plans, and Registered and  
Reference Plan (Subdivisions)  
**Date:** October 22, 2024

## Recommendation

That, on the recommendation of the Director, Planning and Development, the following report **BE RECEIVED** for information.

## Executive Summary

In April 2024, City Council approved the “Targeted Actions to Increase London’s Housing Supply: Supporting Council’s Pledge for 47,000 Units by 2031”. The Targeted Actions identified initiatives to facilitate the expedited delivery of new housing units. These Actions include the City actively pursuing development applications with approved units that are in various stages of the development approvals process (or “pipeline”). In support of this Targeted Action, Staff were requested to report on the developer of units which have been in the latter stages of the development pipeline for over two years, but which have not received building permit approval. In order to achieve this targeted action, it is important to understand how many units are in these latter stages of the pipeline and who is developing them.

The following report identifies number of units by developer within open site plans, open condominium plans, registered condominium plans, and units in registered and reference plans (for subdivisions).

Consistent with the Targeted Actions Report, this report also notes that inactive applications account for 51% of the units within the development pipeline categories noted above, and identifies next steps.

The purpose of this report is to help advance development applications towards building permit and construction of new housing units.

## Linkage to the Corporate Strategic Plan

This report will contribute to the advancement of Municipal Council’s 2023-2027 Strategic Plan and the following area of focus:

- **Strategic Plan Area of Focus: Housing and Homelessness**, by ensuring London’s growth and development is well-planned and consider use, intensity, and form.

## Discussion

### 1.0 Background Information

#### 1.1 Previous Reports Related to this Matter

- SPPC February 7, 2023, London’s Housing Pledge with respect to the City of London Municipal Housing Target of 47,000 units.

- February 16, 2023 – Mayor Morgan’s letter to the Province for the City of London’s Housing Pledge to facilitate and accelerate the housing supply of 47,000 units in our community by 2031
- SPPC October 31, 2023 London’s Housing Pledge: A Path to 47,000 units by 2031 Update
- SPPC April 16, 2024 Targeted Actions to Increase London’s Housing Supply: Supporting Council’s Pledge for 47,000 Units by 2031.

## **1.2 Units by Developer, Development Pipeline category, and Unit type**

In February 2023, The City of London pledged to accelerate the city’s housing supply by adopting the Ontario Ministry of Municipal Affairs housing target of 47,000 new units by 2031.

Through collaborative work of Civic Administration and Development Industry partners, a Targeted Actions Report to increase housing supply was brought forward to Council in April 2024 to support City Council’s housing pledge of 47,000 new units.

The Targeted Actions Report identifies six (6) categories of housing units based on their status within the planning and development application process (the “pipeline”). For each category, a range of initiatives are identified to support the housing pledge.

One action of the Targeted Actions report is for the City to actively pursue applications with units that are considered “inactive” because they have not proceeded to the next stage of the development application process in over two (2) years.

To facilitate opportunities for the advancement of these development applications, a table of units by developer and unit type is provided in Appendix ‘A’ to this report. Unit types are classified as: Low Density Residential (LDR), Medium Density Residential (MDR), or High Density Residential (HDR).

Appendix ‘A’ includes a table of all units by developer within the following development application categories:

- Open site plans,
- Open condominium plans,
- Registered condominium plans, and
- Subdivision registered plans and reference plans.

As of this report, there are 19,241 units within open site plans, condominium plans, and registered and reference subdivision plans across the city. Many are actively advancing towards permits and construction. However, 51% of units are defined as “inactive” because they have remained within the same stage of application for 2 years or longer.

These pipeline categories of units represent the latter stages of *Planning Act* processes, prior to building permit. These units therefore present potential for new unit creation over the short-term to help achieve the housing target.

## **2.0 Notes regarding unit information**

Some important notes regarding the unit counts and data include that:

- Development Industry partners were circulated unit tables through the Housing Supply Reference Group in September.
- Certain units within the “Other Developer”, “Other Individual” or “Numbered Company” categories may be associated with an identified land developer or

builder on the table in Appendix A; however, connections between these companies and a parent company are not included in the report.

- For the tracking of units in this report, units are identified by developer based on the development applications submitted to the City.
- Comments received from the development industry included that there have been subsequent land sales on certain sites (e.g. to a homebuilder), so the owner of certain properties may no longer be the developer identified.
- The data was retrieved in early September 2024.
- Subdivisions registered prior to the year 2000 are not included in the unit counts.

### **3.0 Next Steps to Promote Bringing Units Forward**

Next steps to promote advancement of units in latter stages of the planning application process may include:

- Civic Administration will continue to monitor Provincial legislation and Ontario Regulations changes such as Bill 185, the *Cutting Red Tape to Build More Homes Act, 2024*. Bill 185 identifies potential new “use it or lose it” lapsing conditions for certain subdivision and site plan applications.
- Outreach and engagement with landowners of inactive applications to provide details on the importance of bringing applications forward to construction and City resources to help advance applications.
- Further detail regarding ownership of inactive applications may also be provided at Council’s request.

## **Conclusion**

The Targeted Actions to Increase Housing Supply report identifies actions to facilitate delivery of new housing in support of Council’s pledge of 47,000 new units.

One such action is to actively pursue areas of approved units which are within latter stages of the development application pipeline but have not yet reached building permit issuance. Units which have not proceeded to a new stage in the pipeline within two (2) years are considered inactive. Inactive units account for almost 51% of the units within these pipeline categories representing the latter stages of planning processes.

Based on a request by Council, and in order to help facilitate advancement of inactive applications, developer information has been included in this report.

Separate consultations are ongoing with the Development Industry to help inform the City’s various housing supply initiatives, including the Official Plan Review under section 26 of the *Planning Act*.

Monitoring of units across all categories of the development application pipeline are also ongoing and take place during regular maintenance of the City’s vacant land inventory, which is available on the Open Data Portal of the City’s website.

**Prepared by:** Travis Macbeth, MCIP, RPP  
Manager, Planning Policy (Growth Management)

**Reviewed by:** Justin Adema, MCIP, RPP  
Manager, Long Range Planning

**Recommended by:** Heather McNeely, MICP, RPP  
Director, Planning and Development

**Submitted by:** Scott Mathers, MPA, P.Eng.  
Deputy City Manager, Planning and Economic  
Development

TM/tm



## Appendix A: Units by Land Developer and Unit Type

Developer	LDR	MDR	HDR	Unit Total
1515 Trossacks Ltd	0	0	99	99
1630 Hp Inc, Hyde Park Investments Inc	0	0	143	143
1635 Richmond (London) Corp	0	18	0	18
1674 HP Inc	0	0	46	46
604 Beaverbrook Developments Inc	0	32	0	32
608 Commissioners Inc	0	95	0	95
900 Jalna GP Ltd	0	78	0	78
Alora Homes Ltd	0	0	40	40
Auburn Developments Inc.	206	305	0	511
Banman Developments (Legendary) Inc.	4	0	0	4
Banman Developments Inc	0	57	0	57
Bluestone Properties Inc	11	28	579	618
Calloway REIT (Fox Hollow) Inc	0	168	0	168
Cardinal Homes Inc	0	8	0	8
Carvest Properties Ltd	0	0	135	135
Century Centre Developments inc	0	0	1272	1272
City of London	10	0	0	10
Claybar Developments Inc	0	79	0	79
Colborne Commercial Investments Limited	0	18	0	18
Corlon	17	0	0	17
Cridon Investments Limited	0	0	612	612
Crosko Land Corp	0	29	0	29
Crown Homes of London Inc	0	7	0	7
CSH-HCN (Royalcliffe) Inc C/O Chartwell Seniors Housing	0	0	163	163
Drewlo Holdings Inc.	521	448	266	1235
Drygas Properties Inc	0	0	15	15
East Village Holdings Limited	0	0	243	243
Encore At Upper Richmond Village Inc	0	0	107	107
Evepark London Gp Inc	0	42	0	42
Farhi Holdings Corporation	6	0	340	346
Forest Edge Commons Inc	0	32	0	32
Four Fourteen Inc	0	17	0	17
Foxwood Developments (London) Inc.	75	153	804	1032
Gantlett Contracting Inc	6	0	0	6
Goldfield Ltd	0	66	0	66
Grosvenor Development Corp	0	36	36	72
Hampton Group Inc.	8	0	0	8
HL General Partner Inc	0	0	284	284
Horizen Developments Gp Corp	0	0	129	129
Hyde Park Ventures Inc	0	279	0	279
Indwell Community Homes	0	139	0	139
Jing For Jing Ltd	0	0	12	12
Kenmore Homes (London) Inc.	15	0	0	15
La-Rosa Community Ltd	0	19	0	19
Legacy Homes of London	1	19	0	20
Ljm Developments (Halton Hills) Inc	0	0	181	181
London Realty Investment Corp Dky Investments Inc	0	0	46	46
Lux Homes Design & Build Inc	0	85	0	85
Magnificent Homes	0	39	0	39

Masar Development Inc	0	26	0	26
Meadowlily Developments Inc	36	52	0	88
Millstone Homes Inc	0	143	0	143
Norquay Developments	13	0	0	13
North Development Corp	0	7	0	7
Northwinds Village Inc	0	138	0	138
Parkit Enterprise Inc	0	0	436	436
Pulse Communities (Aria) Inc	0	197	0	197
Pulse Communities (Aria 2) Inc	0	122	0	122
Pulse Communities (Rhythm II) Inc	0	76	0	76
Rembrandt Developments (London) Inc.	10	0	0	10
Rembrandt Meadowlily Inc.	1	296	0	297
Residenza Affordable Housing	0	202	0	202
RC Holdings (London)	0	23	0	23
Richmond Hyland Centre Inc	0	0	256	256
Rockmount Homes Inc	0	22	0	22
Roman Catholic Episcopal Corporation London Diocese	0	73	0	73
RRW Holdings Inc	0	7	0	7
Rubies Estates Limited	0	9	0	9
Safegate Financing Inc	0	17	0	17
Sier Developments Corporation	4	0	0	4
Sifton Properties Limited	123	788	947	1858
Sofco Properties Limited	0	30	0	30
Southdale West Holdings Ltd	0	0	724	724
Southside	58	263	0	321
Space Rider Corporation	0	0	270	270
Summit Properties Inc	0	0	233	233
Sunnngindale Golf & Country Club Ltd.	0	19	0	19
Talu Properties Inc	0	52	0	52
The Canada Life Assurance Company	0	0	405	405
The Incorporated Synod of the Diocese of Huron	0	0	94	94
The Ironside Building Company Inc	0	213	0	213
Timeless Homes Corp	6	0	0	6
Topping	93	0	0	93
Townline Orchard Property Limited	11	0	0	11
Tricar Developments Inc.	0	10	188	198
Tridon	57	0	0	57
Unity Project For Relief Of Homelessness In London	0	0	41	41
Westchester Homes Ltd	0	17	0	17
Westhaven Homes (2008) Inc	4	0	0	4
Whiterock Village Inc.	5	0	0	5
Willow Bridge Construction Inc	0	0	39	39
York Developments Inc.	77	77	182	336
Youth Opportunities Unlimited	0	0	36	36
<b>Subtotal</b>	<b>1378</b>	<b>5175</b>	<b>9403</b>	<b>15956</b>
Numbered Companies	45	1155	1466	2666
Other Developers (2 units or less)	16	0	0	16
Other Individuals	47	530	26	603
<b>TOTAL</b>	<b>1486</b>	<b>6860</b>	<b>10895</b>	<b>19241</b>

# Dundas Streetscape Master Plan Argyle Core Area



Planning and Environment Committee Meeting

October 22, 2024

27

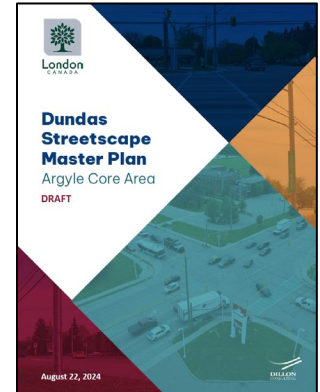
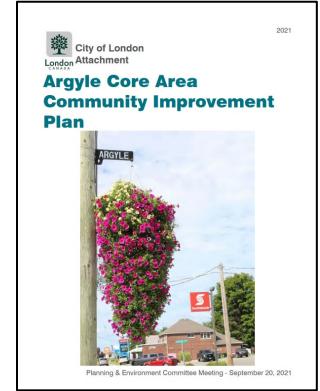
# Project Initiation & Purpose

The **2021 CIP** established the following goals & objectives for the Argyle Core Area:

- Developing a high-quality pedestrian realm
- Improving mobility and prioritizing active mobility

The **Dundas Streetscape Master Plan** will guide future public realm improvements by:

- Defining a vision and standards for the public realm
- Serving as an implementation tool to ensure future development and infrastructure projects provide high-quality design standards
- Providing a framework for access management improvements



## Three Public Update Meetings were provided

- Meeting 1: November 7, 2023 (Information Gathering)
- Meeting 2: January 31, 2024 (Preliminary Design)
- Meeting 3: June 26, 2024 (Final Design Concepts)



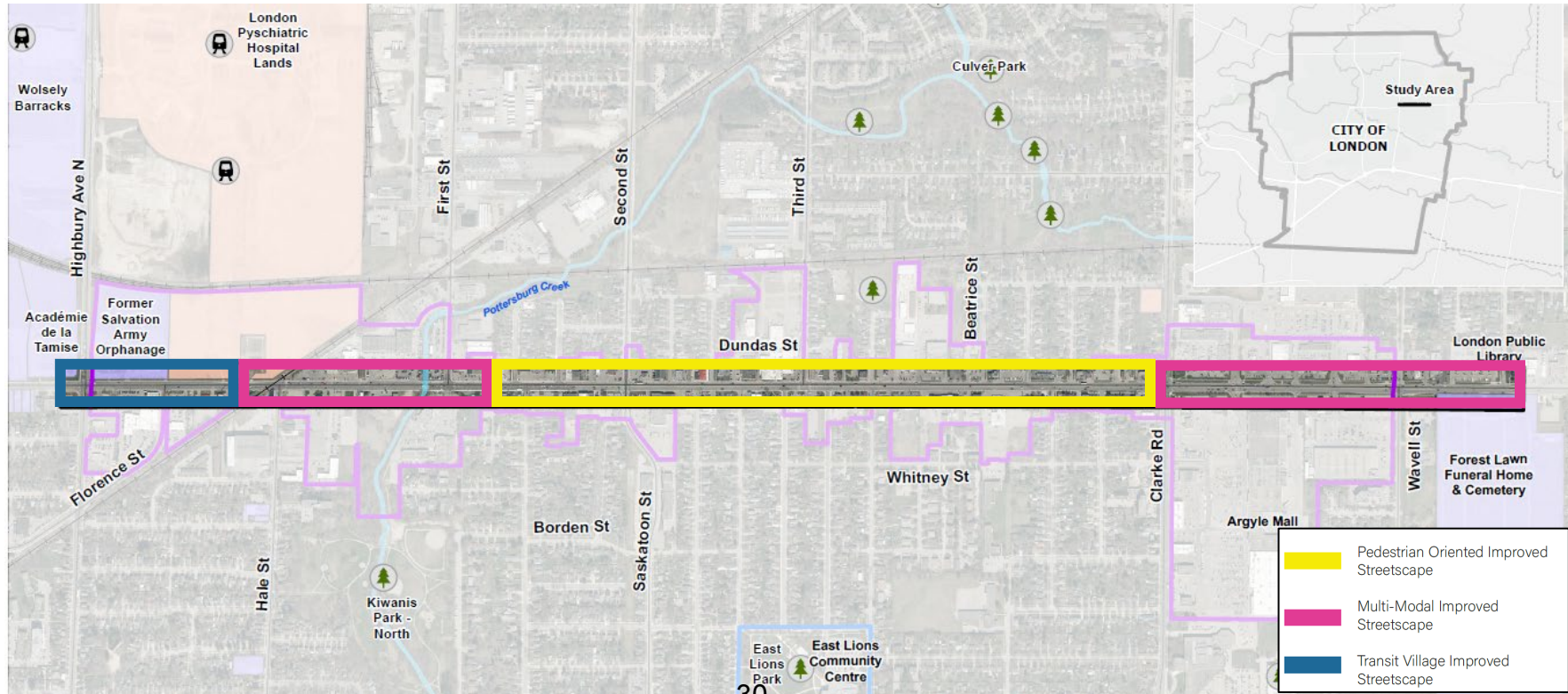
- Residents
- Businesses
- Accessibility Community Advisory Committee
- Integrated Transportation Community Advisory Committee





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# Recommendations

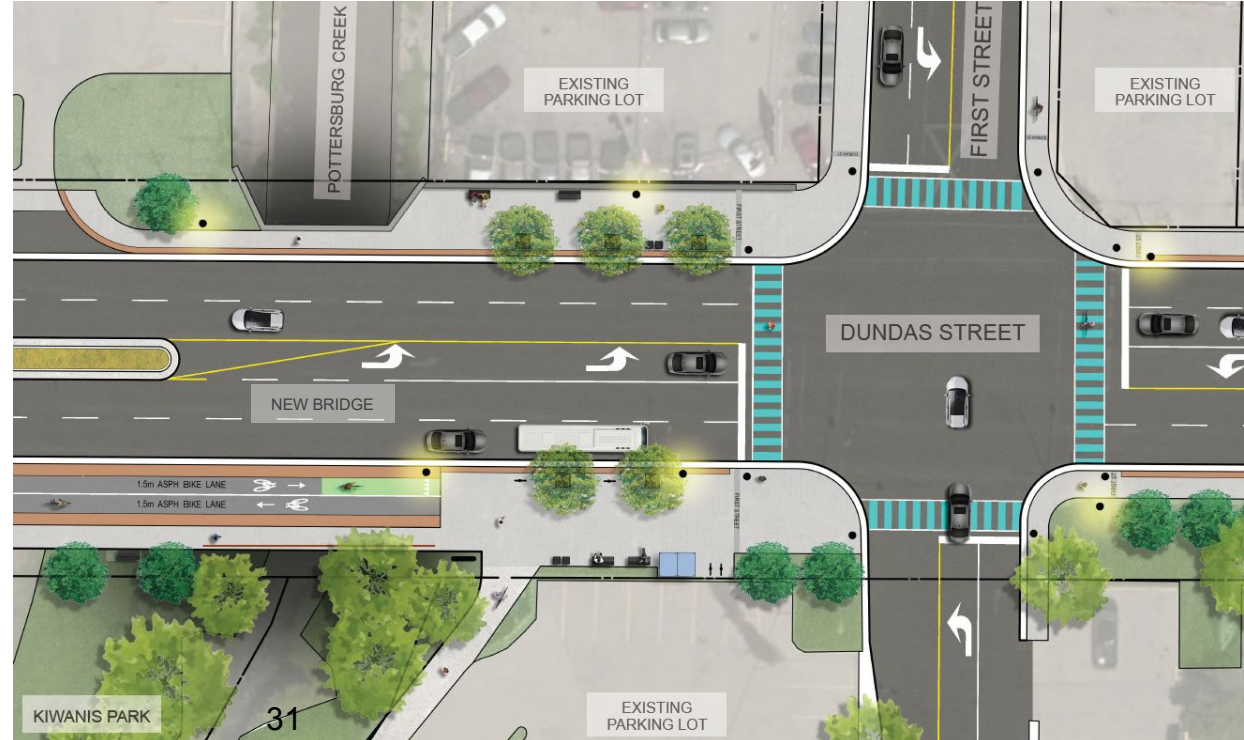


# Implementation

Public realm and streetscape improvements will be implemented during:

- **Redevelopment**
- Planned **capital programs**, such as:
  - Dundas Street East Bridge Replacement Project (Pottersburg Creek)
  - Dundas Street East Pavement Rehabilitation (Burdick to Beatrice)

## Dundas Street East over Pottersburg Creek Bridge Replacement



## Report to Planning and Environment Committee

**To:** Chair and Members  
Planning and Environment Committee

**From:** Kelly Scherr, P.Eng., MBA, FEC, Deputy City Manager,  
Environment and Infrastructure, and  
Scott Mathers, MPA, P.Eng., Deputy City Manager, Planning  
and Economic Development

**Subject:** Dundas Streetscape Master Plan for the Argyle Core Area

**Date:** October 22, 2024

## Recommendation

That, on the recommendation of the Deputy City Manager of Environment and Infrastructure and Deputy City Manager of Planning and Economic Development, the following actions be taken with respect to the Dundas Streetscape Master Plan for the Argyle Core Area:

- a) The Dundas Streetscape Master Plan for the Argyle Core Area and the Executive Summary from the Plan attached hereto as Appendix A, **BE RECEIVED** for information;
- b) The Dundas Streetscape Master Plan for the Argyle Core Area, **BE APPROVED** as a plan identifying infrastructure and urban design guidance for future capital projects and redevelopment; and,
- c) City Staff **BE DIRECTED** to initiate an Official Plan amendment to add the Dundas Streetscape Master Plan for the Argyle Core Area to the list of Council approved Urban Regeneration Guidelines in The London Plan.

## Executive Summary

The recommendation from this report is to initiate the process to add the Dundas Streetscape Master Plan for the Argyle Core Area into The London Plan Urban Regeneration Guidelines (Policy 1717) for the document to act as a tool to guide future infrastructure projects, access management within the corridor, and planning and development applications. The Master Plan was developed in response to the 2021 Community Improvement Plan recommendations and is applicable for properties along the Dundas Street corridor within the Argyle Core Area which extends from east of Highbury Avenue to Forest Lawn Avenue.

## Linkage to the Corporate Strategic Plan

City Council's 2023-2027 Strategic Plan identifies "Mobility and Transportation" and "Economic Growth, Culture and Prosperity" as strategic areas of focus. This report supports the Strategic Plan by identifying infrastructure that supports safe, integrated, connected, reliable and efficient transportation choices while creating an environment to support a vibrant business community along this urban corridor.

## Analysis

### 1.0 Background Information

#### 1.1 Previous Reports Related to this Matter

- December 14, 2020 – Planning and Environment Committee – Argyle Regeneration Study Recommendations



- June 21, 2021 – Planning and Environment Committee – Draft Argyle Core Area Community Improvement Plan
- September 20, 2021 – Planning and Environment Committee – Argyle Core Area Community Improvement Plan
- July 18, 2023 – Civic Works Committee – RFP-2023-097 Streetscape Master Plan for Dundas Street Appointment of Consulting Engineer – Irregular Result

## 1.2 Policy Context

The framework and recommendations set out by the City of London’s existing policy documents informed the outcomes and recommendations from the Dundas Streetscape Master Plan for the Argyle Core Area. Relevant documents include:

- The London Plan (2016, as amended)
- Provincial Planning Statement (2024)
- The City of London Zoning By-law (2011, consolidated)
- London Psychiatric Hospital (LPH) Secondary Plan (2016, as amended)
- Complete Streets Design Manual (2018)
- City of London Design Specifications and Requirements Manual (2019)
- Argyle Core Area Community Improvement Plan (2021)
- Mobility Master Plan (Ongoing)

The Dundas Streetscape Master Plan for the Argyle Core Area complies with the Urban Corridor place type policies and the tools for guideline documents of The London Plan. The Dundas Streetscape Master Plan for the Argyle Core Area will provide guidance for all future development along the Dundas Street corridor to implement the vision for the Argyle Core Area in accordance with the Urban Corridor Place Type. Adoption of the Dundas Streetscape Master Plan for the Argyle Core Area by City Council will include adding reference in the list of Urban Regeneration Guidelines (Policy 1717) of The London Plan.

The Dundas Streetscape Master Plan for the Argyle Core Area will be referenced in conjunction with the City Design policies of The London Plan to evaluate all planning and development proposals (i.e. The London Plan amendments, Zoning By-law amendments, Site Plan applications, Minor Variances and Consents) from both the public and the private sector, as well as all projects seeking available Argyle Core Area CIP financial incentives. It may be referenced by development proponents when contemplating their plans. Additionally, the Dundas Streetscape Master Plan will be considered for future infrastructure works to ensure designs align with the ultimate vision for the Dundas Street corridor.

The recommendations presented in the Dundas Streetscape Master Plan for the Argyle Core Area are also informed by urban design best practices and Accessibility for Ontarians with Disabilities Act (AODA) requirements.

## 1.3 Project Background

In 2021, the City completed the Argyle Core Area Community Improvement Plan (CIP) which established the following goals and objectives for the Argyle Core Area:

- Develop a high-quality pedestrian realm by providing a pedestrian-oriented streetscape and public spaces that are safe, clean, accessible and pleasant.
- Improve mobility by providing an interconnected community-wide transportation network that is safe, convenient, and prioritizes active mobility.

The CIP recommended that the City develop a streetscape master plan for Dundas Street, to support The London Plan vision for the Urban Corridor Place Type as high-quality spaces with neighbourhood amenities including parks, civic spaces, and attractive seating areas accessible to the public.

In 2023, Dillon Consulting Limited was appointed as the Consulting Engineer to complete the Dundas Streetscape Master Plan for the Argyle Core Area. The Dundas Streetscape Master Plan aims to guide future public realm improvements and redevelopment by:

- Implementing a key action from the CIP to develop a high-quality public realm;
- Setting a defined vision and standards for the public realm;
- Serving as an implementation tool to ensure that future development and infrastructure projects provide a high-quality design standard in accordance with The London Plan (City of London's Official Plan); and,
- Providing a framework to guide discussions with prospective developers and property owners regarding access management.

Streetscape guidelines for the Argyle Core Area define standards for boulevards, sidewalks, cycling lanes, transit stops, streetlighting, and green infrastructure for trees, planters and low impact design features.

A review of access management along Dundas Street was also completed to identify opportunities for improving traffic operations and safety for active transportation modes. The Dundas Streetscape Master Plan recommends future improvements to driveways and some access management opportunities consistent with City standards to minimize conflicts between various modes.

#### **1.4 Community Involvement**

Several public presentations and meetings have occurred during the background review, preliminary design, and final design phases of the project. Participation and feedback opportunities were provided to residents, the Argyle Business Improvement Association (BIA), the Argyle Community Association, Indigenous communities, the Accessibility Community Advisory Committee, and the Integrated Transportation Community Advisory Committee. Three public update meetings were held in-person at the East Lions Community Centre and presentation material was provided on the project's Get Involved webpage (<https://getinvolved.london.ca/argylestreetscape>). Notices for these engagement opportunities were advertised in the Londoner and through social media channels. Businesses along the corridor were provided copies directly and through the mail. Notices were also sent to members of the Argyle Community Association.

The first public update meeting occurred on November 7, 2023, which included a presentation about the project, and engagement activities focused on understanding area needs and collecting feedback on guiding principles, with 40 attendees and 30 surveys completed. City staff also attended the Argyle BIA Annual General Meeting on November 8, 2023, to provide information and engage with local businesses.

The second public update meeting was held on January 31, 2024, to present the draft streetscape concept and design elements, including typical corridor cross-sections and opportunities for landscape enhancements for areas within the public realm. Feedback on the draft streetscape concept was received, and surveys were collected in-person and online. The draft streetscape concept and findings from the access management review of entrance driveways was presented to the Argyle BIA during a board meeting on February 14, 2024.

The third public update meeting occurred on June 26, 2024, to present the Dundas Streetscape Master Plan, including conceptual designs for the street.

## 1.5 Master Plan Recommendations

The Dundas Streetscape Master Plan is a comprehensive design guideline that defines short, medium and long-term strategies and provides recommendations for streetscape improvements that enhance the public realm, improve safety and reinforce community identity.

Guiding principles are the core strategies in achieving the vision for the Dundas Streetscape Master Plan and reflected in the overall recommendations. The following principles were developed based on the feedback from the public consultation process and best practices:

- Improved Mobility
- Improve Pedestrian Realm and Safety
- Strengthen Argyle Core Community
- Promote Economic Vitality
- Improved Green Connections

The concept plan developed in the Master Plan identifies three distinct streetscape typologies or segments, which reflect the road function, planned active transportation routes and current corridor widths. This classification allows better understanding of the characteristics of different Dundas Street segments and facilitates informed decision-making for future development, improvement, or maintenance.

For Dundas Street within the Argyle Core Area, the streetscape typologies have been characterized as:

- Pedestrian Oriented Improved Streetscape, which is the central zone.
- Transit Village Streetscape, located at the west entrance to the corridor. The “Transit Village” terminology was selected for this streetscape type based on the adjacent land uses which are classified as “Transit Village” and “Rapid Transit Corridor” Place Types as identified in the London Plan.
- Multi-Modal Improved Streetscape, located at the west and east entrances to the corridor.

The full report can be found at <https://getinvolved.london.ca/argylestreetscape>.

## 2.0 Discussion and Considerations

As future capital works programs upgrade existing infrastructure and as redevelopment occurs in the Argyle Core Area, the recommendations identified in the Dundas Streetscape Master Plan will be considered to improve the streetscape and public realm.

Upcoming infrastructure projects that are currently planned include:

### 1) Dundas Street East Pavement Rehabilitation from Burdick Place to Beatrice Street

This near-term infrastructure renewal project is part of the City’s overall pavement management program to address deficient pavement conditions across the major road network. Working within the planned budget for this project, and recognizing constraints imposed by the narrow right-of-way and presence of existing utilities, there are limited opportunities to include elements as identified in the Master Plan. This project would include the introduction of centre medians in several locations as well as some hardscape and softscape elements to improve the public realm as identified in the Master Plan. Additional

engagement with several property and business owners to discuss access management opportunities within this section will be undertaken during detailed design. Further details and costs for improvements will be developed during detailed design.

Also within this section, a traffic study has been scheduled to support a new potential pedestrian signal at Beatrice Street as recommended in the CIP and Streetscape Master Plan.

## 2) Dundas Street East over Pottersburg Creek Bridge Replacement Project

This near-term project is funded through the major bridge upgrades capital program with a cost-sharing from the Investing in Canada Infrastructure Program (ICIP) Public Transit Stream (PTS) for active transportation improvements across bridges. The Master Plan identifies the importance of providing a high-quality pedestrian realm and active transportation facilities along Dundas connecting the LPH lands and Kiwanis Park. The design of the bridge will address these requirements and further opportunities to improve the public realm as recommended by the Master Plan will also be assessed during the planning and design of the bridge project.

In addition to these identified capital projects, the recommendations and guidance provided in the Master Plan will be considered as part of any redevelopment plans and as part of the planning application process within the corridor.

Civic Administration also continues to monitor information related to potential funding programs sponsored by senior levels of government and will leverage these opportunities as they become available. The recommendations from the Dundas Master Plan will support the city in the application process for future funding programs.

## Conclusion

The development of the Dundas Streetscape Master Plan has been informed through extensive engagement with residents, business owners and operators, community advisory communities as well as the involvement of the Argyle BIA and community association.

The recommendations of the Dundas Streetscape Master Plan are consistent with the Provincial interests of providing a well-designed built form and providing a sense of place and is consistent with both the Provincial Policy Statement and The London Plan. Adopting the Dundas Streetscape Master Plan as a Guideline Document in the London Plan will enable the use of this document as a tool for infrastructure and urban design guidance for all future infrastructure projects, planning applications, and potential Community Improvement Plan (CIP) incentive applications.

The recommendations of this report are to approve the plan and initiate an Official Plan amendment to adopt the Dundas Streetscape Master Plan for the Argyle Core Area into The London Plan.

**Prepared by:** **Garfield Dales, P.Eng.**  
**Division Manager, Transportation Planning and Design**

**Kyle Gonyou, RPP, MCIP, CAHP**  
**Manager, Heritage and Urban Design**

**Submitted by:** **Doug MacRae, P.Eng., MPA**  
**Director, Transportation and Mobility**

**Heather McNeely, MCIP, RPP**  
**Director, Planning and Development**

**Recommended by:** **Kelly Scherr, P.Eng., MBA, FEC**  
**Deputy City Manager, Environment and Infrastructure**

**Scott Mathers, MPA, P.Eng.**  
**Deputy City Manager, Planning and Economic Development**

**Appendix A:** **Dundas Streetscape Master Plan for the Argyle Core Area Executive Summary**



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# Dundas Streetscape Master Plan

**Final**

**October 8, 2024**



# Executive Summary

## Project Overview

The Dundas Streetscape Master Plan (Dundas Streetscape MP) is a key initiative under the Argyle Core Area Community Improvement Plan (CIP, 2021). This comprehensive plan addresses the need for an enhanced public realm along the Dundas Street corridor between Highbury Avenue North and Wavell Street, focusing on improving neighbourhood safety, supporting businesses, improving mobility and connections, and reinforcing the identity of the Argyle community. By developing short, medium, and long-term strategies, the Dundas Streetscape MP outlines a clear vision for the future of Dundas Street as a vibrant, multi-modal urban corridor.

## Vision

The vision for the Dundas Streetscape MP will guide future development within the Dundas Street corridor in the Argyle Core Area. By 2040, Dundas Street will be:

- an important multi-modal transportation corridor;
- a safe place to walk, shop, spend time and invest in a home or business;
- a multi-modal urban corridor that accommodates healthy living for a diverse group of people and for all ages, physical and mental abilities, and mobility needs; and,
- inspires a strong sense of community through a comfortable pedestrian experience that fosters daily interactions along the street and community celebrations in public spaces.

## Streetscape Typologies

To achieve this vision, the Dundas Streetscape MP outlines three distinct streetscape typologies, each tailored to different sections of the Dundas Street corridor, and improved street intersections:

### Streetscape Typology 1: Pedestrian-Oriented Improved Streetscape

The Pedestrian Oriented Improved Streetscape corridor is generally applicable to the areas east of First Street extending west of Clarke Road, focusing on:

- Prioritizing pedestrian movement and public realm enhancements along the central section of Dundas Street;
- Maintaining five-lane road with a center two-way left-turn lane, and 1.8- to 2.0-metre-wide sidewalks on both sides;
- Including a 1.0 to 1.5 metre curb-side Furnishing Zone with waste/recycling bins, planters, and bike racks, and,
- Updating street and pedestrian lighting, and planting trees where space allows, avoiding utility conflicts.

### Streetscape Typology 2: Transit Village Streetscape

The Transit Village Streetscape is in proximity to nearby Rapid Transit Corridor and Transit Village Place Types as detailed in The London Plan and marks the entrance into the Argyle Core Area. This typology focuses on improving the pedestrian experience near future rapid transit

stations, with the west entrance serving as a key gateway to the Argyle Core Area. Key design elements also include:

- 2.0 to 2.5 metre sidewalks with a Furnishing and/or Planting Zone on both sides of Dundas Street and a 1.0 to 1.5 metre sodded boulevard on the north side;
- Benches, waste/recycling bins, and street trees where feasible, coordinated with utilities, and,
- Updated pedestrian lighting and a center-raised median with decorative elements at the Argyle Core Area entrance.

### **Streetscape Typology 3: Multi-Modal Improved Streetscape**

The Multi-Modal Improved Streetscape is characterized by the integration of the proposed cycling routes as per The London Plan. A key feature of this typology is the 36-metre Right-of-Way (ROW), primarily located in the east corridor, east of Clarke Road, with some sections in the west corridor at the London Psychiatric Hospital lands. The wider ROW allows for an expanded public realm, creating more opportunities to enhance the pedestrian experience and improve walkability. Some of the main features include:

- A 3.0 metre wide two-way protected cycle track on the south side of Dundas Street (Florence St. to Kiwanis Park) and the north side (Hale St. to LPH entrance);
- 2.0 to 3.0 metre wide sidewalks as part of the Pedestrian Clear Zone, and,
- Furnishing and/or Planting Zones between sidewalks and property lines, featuring benches, waste/recycling receptacles, and sodded boulevards (1.0 to 1.5 metres).

## **Street Intersections**

The plan also highlights intersection improvements along the corridor, categorizing them into two distinct intersections typologies, including:

- **Multi-Modal Improved Intersections:** Enhancements focus on creating safer crossing points for pedestrians, cyclists, and vehicles. Typical design features include protected cycle tracks, improved lighting, and tactile walking surface indicators for accessibility;
- **Typical Street Intersections:** Focuses on improved pedestrian safety by providing better pedestrian infrastructure at all intersections crossings. Typical design features include high-contrast, white band crosswalks with graphic asphalt pedestrian crossing treatments at signalized crossings, tactile walking surface indicators, pedestrian audible call buttons at all intersections and reduced curb radii to minimize crossing distances, where feasible.

## **Streetscape Guidelines**

The Dundas Streetscape MP also provides streetscape guidelines for public realm enhancements, which includes the structures, materials, and design features within the public Right-of-Way.

To enhance the sense of place, distinctive "placemaking elements" like gateways, public art installations, and interpretive signage are strategically placed.

The streetscape guidelines section of the Dundas Streetscape MP provides detailed guidance on integrating public realm elements for the Dundas Street corridor in the Argyle Core Area. It outlines comprehensive strategies for improving the pedestrian realm and place design, cycling facilities, transit facilities, motor vehicle design, green infrastructure, and utilities and municipal services.



## Report to Planning & Environment Committee

**To:** Chair and Members  
Planning & Environment Committee  
**From:** Scott Mathers, MPA, P. Eng  
Deputy City Manager, Planning and Economic Development  
**Subject:** Reducing Off-the-Clock Permit Applications  
**Date:** October 22, 2024

## Recommendation

That the report titled "Reducing Off-the-Clock Permit Applications" **BE RECEIVED** for information.

## Executive Summary

This report addresses the issue of reducing off-the-clock building permit applications. Off-the-clock permit applications are those submitted with missing required drawings or without meeting all applicable laws, leading to delays and increased workloads. The report outlines the original intent of off-the-clock applications, their implications, and proposed administrative procedures to mitigate their occurrence.

## Linkage to the Corporate Strategic Plan

Growing our Economy

- London is a leader in Ontario for attracting new jobs and investments.

Leading in Public Service

- The City of London is trusted, open, and accountable in service of our community.
- Improve public accountability and transparency in decision making.

## Analysis

### 1.0 Background Information

#### 1.1 What is an off-the-clock Building Permit?

- **On the Clock:** A preliminary screening of the permit application confirms all applicable laws are met at the time of application, all required application documents have been submitted, allowing issuance within provincially legislated timelines (10, 15, 20, or 30 days).
- **Off the Clock:** A preliminary screening of the permit application finds missing documentation or key compliance information. Comments are provided to the applicant by the City's drawing reviewer requiring the applicant to submit revised drawings or further information. Once comments are provided by the City the permit is no longer subject to the provincially legislated deadlines as a permit cannot legally be issued.

#### 1.2 Industry Feedback

The length of time it takes to approve building permits and the number of off-the-clock permit applications has been a long-standing concern raised by the building and development industries. In February 2023 Council endorsed a pledge to accelerate the housing supply of 47,000 units in our community by 2031. To support this commitment, a Customer Service and Process Improvement Reference Group was created to improve collaboration with development industry partners focusing on continuously improvement of our processes. The need to better track and reduce the number of off-the-clock permits was highlighted early in the reference group's discussions. Over the course of several meetings,

off-the-clock data was prepared, analyzed and presented to the reference group. Out of this analysis, significant issues were identified. As well, a key data collection gap was identified and was incorporated into our standard processes.

As a continuation of the work of the reference group, an off-the-clock permit action was included in the April 2024's Targeted Actions to Increase London's Housing Supply:

### **Off-The-Clock Permit Focus| Q3-2024**

Review of the off-the-clock permits: provide a report to Council with recommendations and actions made to reduce the number of off-the-clock permits. Report off-the-clock permits as part of regular Building reporting.

This report provides a discussion on off-the clock permit applications and highlights the actions being taken to reduce the number of off-the-clock permit applications with the overall goal of reducing the time to build new housing in London.

## **2.0 Discussion and Considerations**

### **2.1 Purpose of Off-the-Clock Applications**

Off-the-clock permit applications allow for submission prior to demonstrating compliance with applicable laws (classifying them an incomplete application). This process was intended to provide flexibility but has resulted in additional workload, liability, and delays.

The goal is not to allow permits to remain off-the-clock indefinitely.

In some cases, an off-the-clock permit could be several years old. This can be due to a combination of factors including delays in staff providing comments on resubmission or delays in the applicant providing a resubmission. The original intent of off-the-clock permit applications was to expedite the review process by allowing submissions before all compliance was demonstrated. However, this has led to several issues:

- **Increased Workload:** Additional reviews and follow-ups are required.
- **Liability:** Potential for noncompliance to changing Building Codes as well as criticism due to perceived delays.
- **Process Delays:** Revisions/resubmissions required to meet compliance can slow down the overall timeline. Also, the addition of incomplete applications into the system ultimately takes resources away from complete applications delaying their issuance.

### **2.2 Off the Clock Permit Metrics**

#### **Analysis of Original Data vs. Current Status: Off-the-Clock Permits**

##### **Original Data Presented to the CSPI Group (03/13/2023):**

In the initial analysis presented to the CSPI group on March 13, 2023, there were 696 off-the-clock permits identified. A significant portion of these permits, 111, were over 2 years old. This backlog was primarily made up of single-family home permits, indicating substantial delays in processing. Additionally, 167 permits were aged between 1 to 2 years. The overall ratio of off-the-clock permits to the 3-year average of submitted permits was 17%, signaling ongoing inefficiencies in the permitting process.

##### **Current Status (08/22/2024 Data):**

In the more recent data analysis from August 22, 2024, there were 690 off-the-clock permits, a slight decrease from the original data. However, the number of permits over 2 years old increased to 173, highlighting a growing concern in this category. The ratio of off-the-clock permits to the 3-year average of submitted permits rose to 18%,

suggesting that while there have been efforts to address the backlog, the processing efficiency for new applications remains a challenge.

### **Implications for Current Processes and Strategies:**

The comparison between these two analyses reveals that, despite efforts, there has been little substantial change in the reduction of older permits, particularly those over 2 years old, which continue to predominantly involve single-family homes. The slight changes in the total number of off-the-clock permits and the minor increase in the ratio to submitted permits reinforce the pressing need to refine and improve current processes and strategies.

Additional application permit data has been provided in Appendix “A” to reflect the comparison between the 2023 and 2024 data.

### **2.3 Proposed Procedures and Targets**

#### Objectives

The primary objectives of refining the permit application process include:

- **Freeing up staff time:** Concentrating efforts on 'on-the-clock' permits to ensure efficient use of resources and faster processing times.
- **Reducing overall permit issuance times:** Accelerating both on-the-clock and off-the-clock permit acceptance through clear instructions to ensure applicants know the requirements prior to submission.
- **Minimizing 'Inactive Permit Applications':** Decreasing the number of permits that become abandoned, as defined by section 4.6 of the City of London's B-7 Building By-law, by addressing the causes of prolonged inactivity and ensuring timely responses and updates.

#### Strategies and Targets

To achieve these objectives, the following strategies are proposed:

##### **1. Inactivity Administrative Procedure:**

- **Implementation:** Applications will be canceled after 6 months of inactivity to prioritize active projects and maintain a streamlined system.
- **Process:** Include a pre-cancellation notification allowing the applicant an opportunity to demonstrate ongoing activity or intent to proceed, ensuring fair communication and opportunity to comply.

##### **2. Automatic Cancellation:**

- **Implementation:** Automatically cancel any applications that remain in the system for more than two years.
- **Rationale:** This administrative procedure aims to limit complications due to potential changes in the building code, reducing the risk of non-compliance over time.

##### **3. Concurrent Review Administrative Procedure with Planning:**

- **Implementation:** Accept applications for concurrent review with site plan approvals if no major issues are unresolved, requiring a sign-off from Planning.
- **Goal:** Prevent premature applications from congesting the system, while allowing for an expedited review process for less complex applications.

#### 4. Foundation/Site Servicing Permit Administrative Procedure:

- **Implementation:** Issue partial permits based on a principal agreement to the site plan, thus allowing construction to start prior to the full building permit issuance.
- **Collaborative Development:** Develop specific criteria for partial permits through consultation with the building and development industry as part of the Customer Service Process Improvement (CSPI) rapid improvement group.
- **Advantages:** This administrative procedure minimizes the liability typically associated with conditional permits and facilitates earlier commencement of construction.

#### Metrics for Success

To effectively measure the success of the refined building permit application process, we propose the following metrics categorized based on application processing statuses:

##### Category 1: Pending Compliance Applications (Incomplete Applications)

**Definition:** Applications considered incomplete due to missing required documentation necessary for a full application review or where all "applicable law" as indicated within the Ontario Building Code is not presently met. This determination is made during the prescreening of submitted application documents.

##### Target:

- **Monitoring Only:** No specific quantitative target is set for this category due to the complexities involved. Regular monitoring will ensure these applications progress appropriately and receive the necessary support to achieve compliance efficiently.

##### Evaluation Criteria:

- **Average Compliance Resolution Time:** Track and report the average time taken to address the incomplete aspects from the date of application submission.
- **Industry Partner Feedback:** Collect feedback from applicants and internal departments to identify areas for improvement in the process of completing and complying with all applicable laws.

##### Category 2: Conditionally Accepted Incomplete Applications

**Definition:** Applications accepted as "complete" based on a mutual agreement between the site plan approval, zoning, and building departments, despite certain aspects being incomplete. This acceptance is determined during the prescreening process and must meet an agreed-upon list of criteria. Once accepted, these applications are subject to Ontario Building Code legislated timelines.

##### Target:

- **Criteria Development:** Collaboratively develop and implement criteria for accepting incomplete applications as complete. Ensure all involved departments agree and understand the criteria.
- **Compliance with Timelines:** Once accepted, these applications will be processed within the Ontario Building Code's legislated timelines.

##### Evaluation Criteria:

- **Inter-Departmental Agreement:** Measure the effectiveness of the criteria and how consistently they are applied across departments.

- **Timeliness:** Ensure that all applications in this category meet the legislated timelines without exception.

### Category 3: Deficiency-Flagged Applications

**Definition:** Applications that experience delays due to prolonged response times from either the City or the applicant, resulting in a slowdown or suspension of the review process. These applications can also become "off-the-clock" when deficiencies are indicated on the drawings and the deficiency list is provided to the applicant. The prescreening of application documents also plays a role in identifying potential issues early on, helping to mitigate delays.

#### Criteria and Targets:

##### 1. City Response Time

- **Criteria:** Average response time by the City for each submission round.
- **Target:** Achieve an average response time of **10 business days or less** per submission.
- **Rationale:** Ensures timely processing and demonstrates the City's commitment to efficiency and responsiveness.

##### 2. Applicant Response Time

- **Criteria:** Average response time by applicants for each resubmission or requested information.
- **Target:** Encourage applicants to maintain an average response time of **15 business days or less** per submission.
- **Rationale:** Promotes proactive engagement from applicants and reduces overall application processing durations.

##### 3. Proportion of Off-the-Clock Applications

- **Criteria:** Percentage of applications classified as off-the-clock (due to either City or applicant delays, including deficiencies identified in drawings) relative to all open applications.
- **Target:** Maintain off-the-clock applications at **15% or lower** of all open applications monthly.
- **Rationale:** Reflects effective management of the application pipeline and identifies areas needing attention to prevent backlog. Shows a commitment from both the applicant and city to resolve deficiencies to allow permit issuance.

#### Evaluation Methods:

- **Monthly Reporting (Internal):** Generate detailed reports outlining performance against these targets, enabling timely identification of issues and implementation of corrective actions.
- **Seasonal Reporting (External):** Provide comprehensive reports summarizing application, permit and associated inspection activities for the specified period. These reports, which include detailed summaries like the "Summary Listing of Building Construction Activity" and "Principle Permits Reports," enable external partners to assess performance, identify trends, and collaboratively address any challenges.
- **Process Audits:** Conduct periodic audits to assess the effectiveness of implemented strategies and identify opportunities for further improvement.

- **Industry Partner Engagement:** Hold regular meetings with internal teams and industry partners to discuss performance metrics, gather feedback, and collaboratively develop solutions for observed challenges.

### Implementation Plan

- **Timeline:** Detailed timeline for the phased implementation of these administrative procedures, including pilot testing and full-scale rollout.
  - **Strategy 1** – Q4 2024
  - **Strategy 2** – Q1 2025
  - **Strategy 3** – Q2 2025
  - **Strategy 4** – Q2 2025
- **Monitoring:** *Integration with Data and KPI Initiatives:* The monitoring of these administrative procedures will be fully integrated into our ongoing data and Key Performance Indicator (KPI) enhancements. This integration ensures that administrative procedure effectiveness is continually measured against predefined metrics, facilitating regular updates and refinements based on both quantitative data and qualitative feedback from staff and industry partners. This approach will enable a dynamic response to evolving needs and enhance strategic decision-making.
- **Reporting:** Regular updates to Council and stakeholders on progress towards targets and effectiveness of the new Administrative Procedures.

### 3.0 Analysis

The proposed adjustments to the permit application process are rooted in a comprehensive analysis of the current challenges and inefficiencies within the City of London's Building Division. This analysis has highlighted the significant impact of 'off-the-clock' and abandoned permits on the city's ability to meet strategic goals, particularly those related to housing development and economic growth.

#### Key Findings:

- **Increased Processing Times:** Analysis of permit data revealed that off-the-clock permits significantly increase overall processing times due to repeated reviews and extended communication cycles.
- **Resource Allocation:** A substantial portion of departmental resources is currently devoted to managing incomplete or non-compliant applications, which detracts from the ability to efficiently process compliant, on-the-clock permits.
- **Economic Impact:** Delays in permit processing directly affect the city's economic development initiatives by slowing down construction projects, which in turn impacts job creation and investment in the local economy.
- **Partner Feedback:** Feedback from industry partners through the Customer Service Process Improvement (CSPI) group has consistently emphasized the need for more streamlined processes and clearer communication from the city to reduce delays and uncertainties.

#### Strategic Importance:

- **Supporting Economic Development:** By reducing the time and complexity involved in obtaining building permits, the city can become more attractive to developers and investors, which supports broader economic growth and job creation.

- **Enhancing Service Delivery:** Improving the permit process aligns with the city's strategic goals of enhancing public service delivery by making the city more responsive, efficient, and user-friendly.
- **Fulfilling Housing Objectives:** Accelerating the permit process is essential for meeting the city's housing pledges, particularly the commitment to accelerate the housing supply of 47,000 units by 2031.

**Anticipated Outcomes:**

- **Reduction in Permit Processing Times:** The introduction of partial permits, along with streamlined and digitized processes, is expected to cut down processing times significantly.

## Conclusion

By implementing these targeted strategies, we aim to significantly enhance the efficiency and effectiveness of the permit application process. This will not only reduce delays but also improve overall service delivery, contributing to the City's goals of increasing housing supply and maintaining a high standard of public service. These changes are aligned with our strategic commitment to streamline city operations and improve responsiveness to the needs of our residents and developers.

**Prepared by:** Alan Shaw  
 Chief Building Official  
 Planning and Economic Development

**Submitted by:** Scott Mathers, MPA, P.Eng.  
 Deputy City Manager  
 Planning and Economic Development

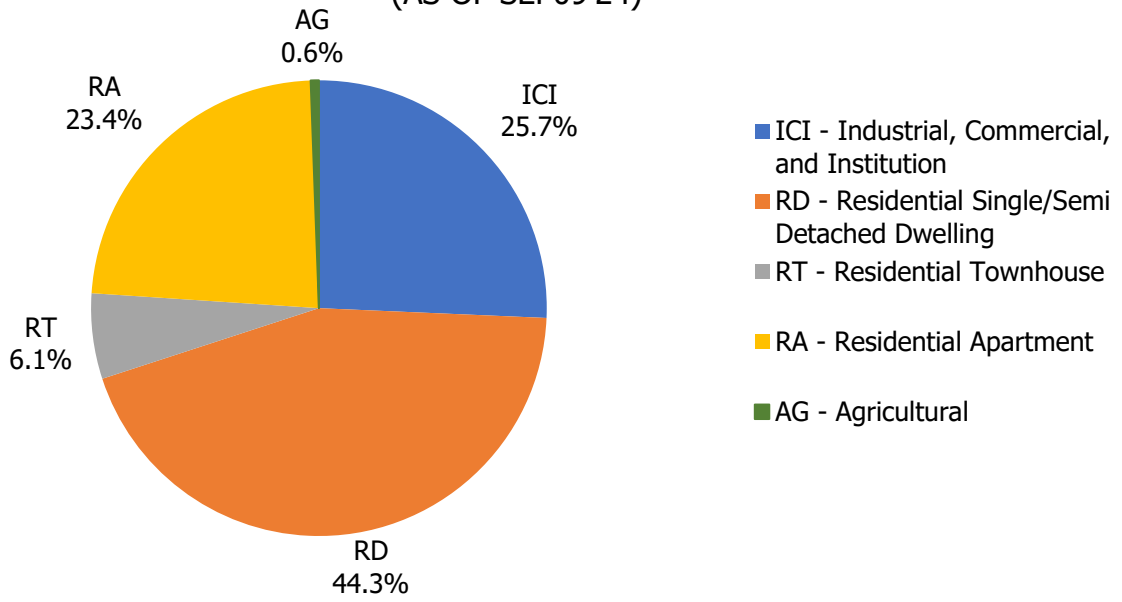
**Recommended by:** Scott Mathers, MPA, P.Eng.  
 Deputy City Manager  
 Planning and Economic Development

## APPENDIX "A"



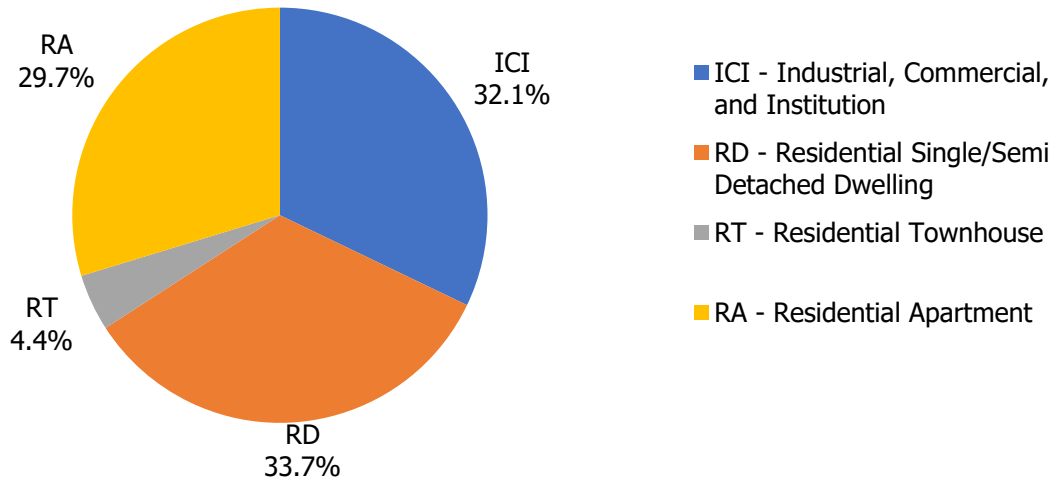
### ALL OFF THE CLOCK APPLICATIONS

(AS OF SEP09'24)

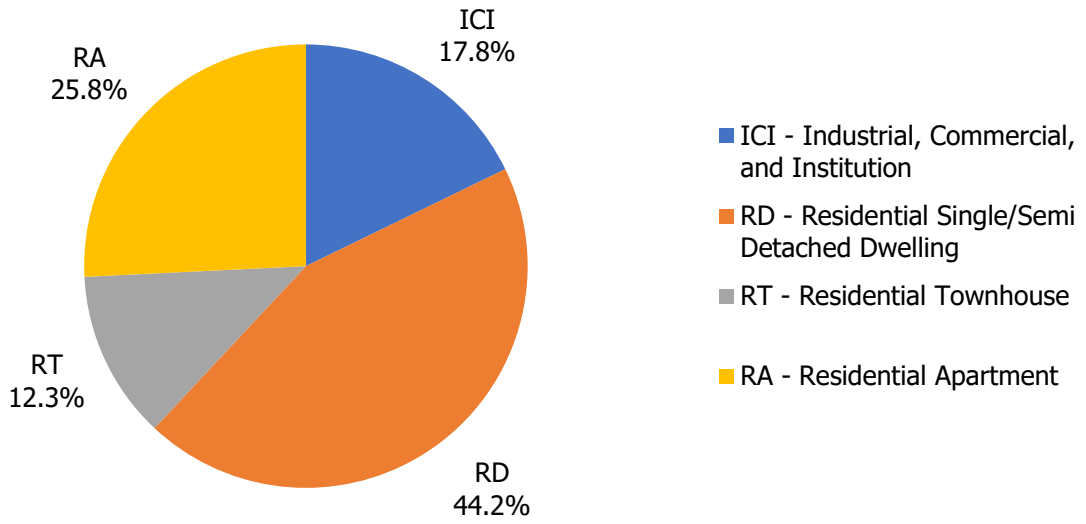


### 2024 OFF THE CLOCK APPLICATIONS

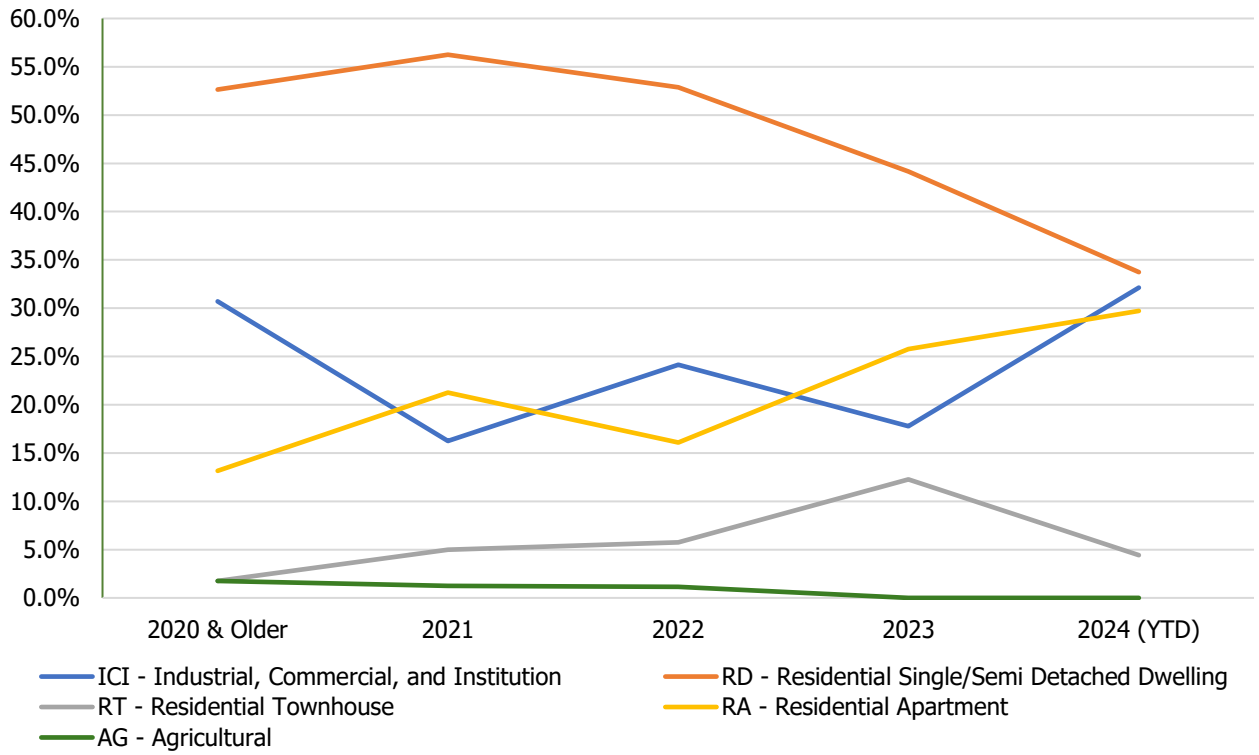
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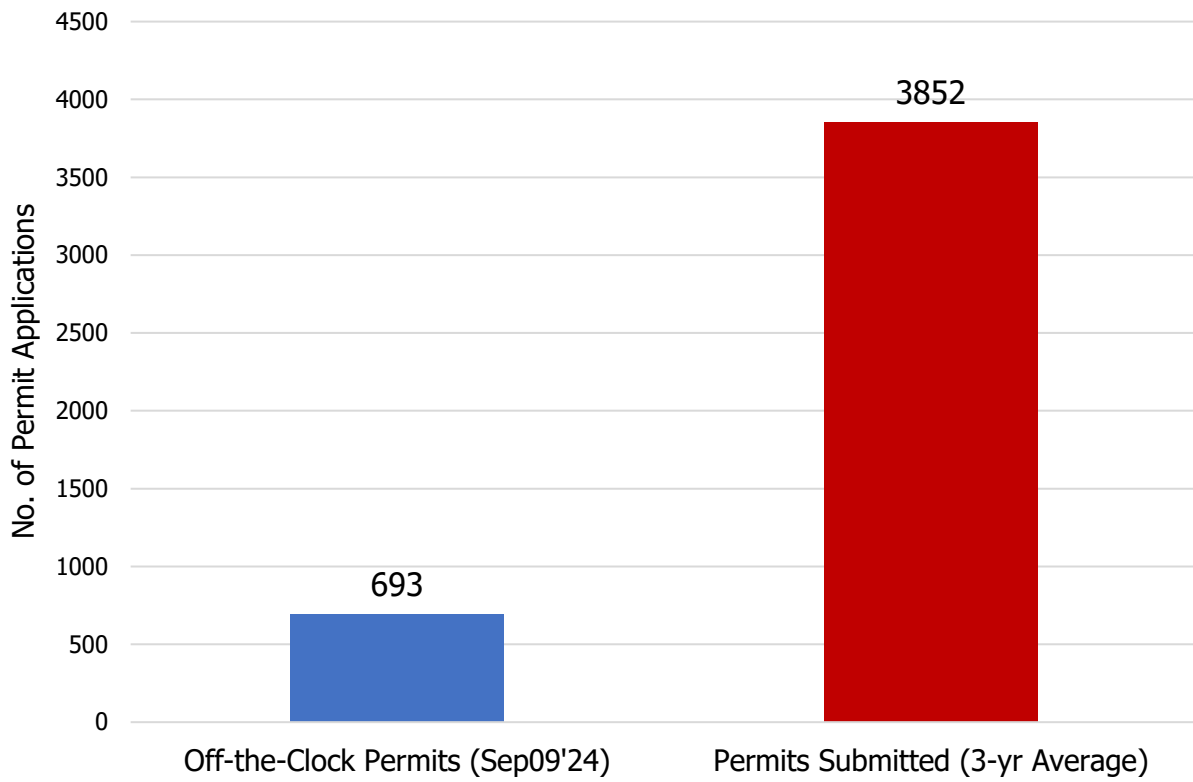
**2023 OFF THE CLOCK APPLICATIONS**  
(AS OF SEP09'24)



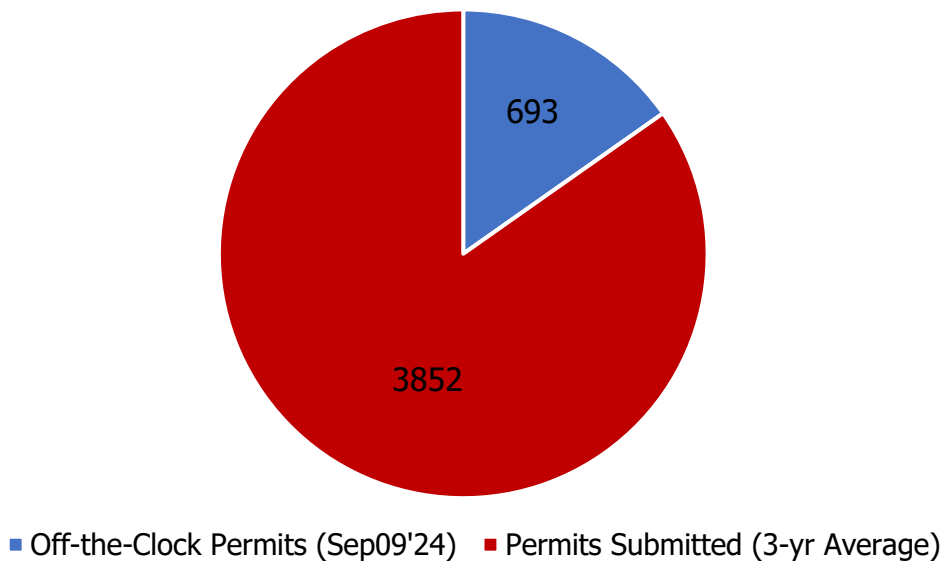
**YEARLY PERCENTAGE OF OFF THE CLOCK PERMITS BY PERMIT TYPE**  
(AS OF SEP09'24)



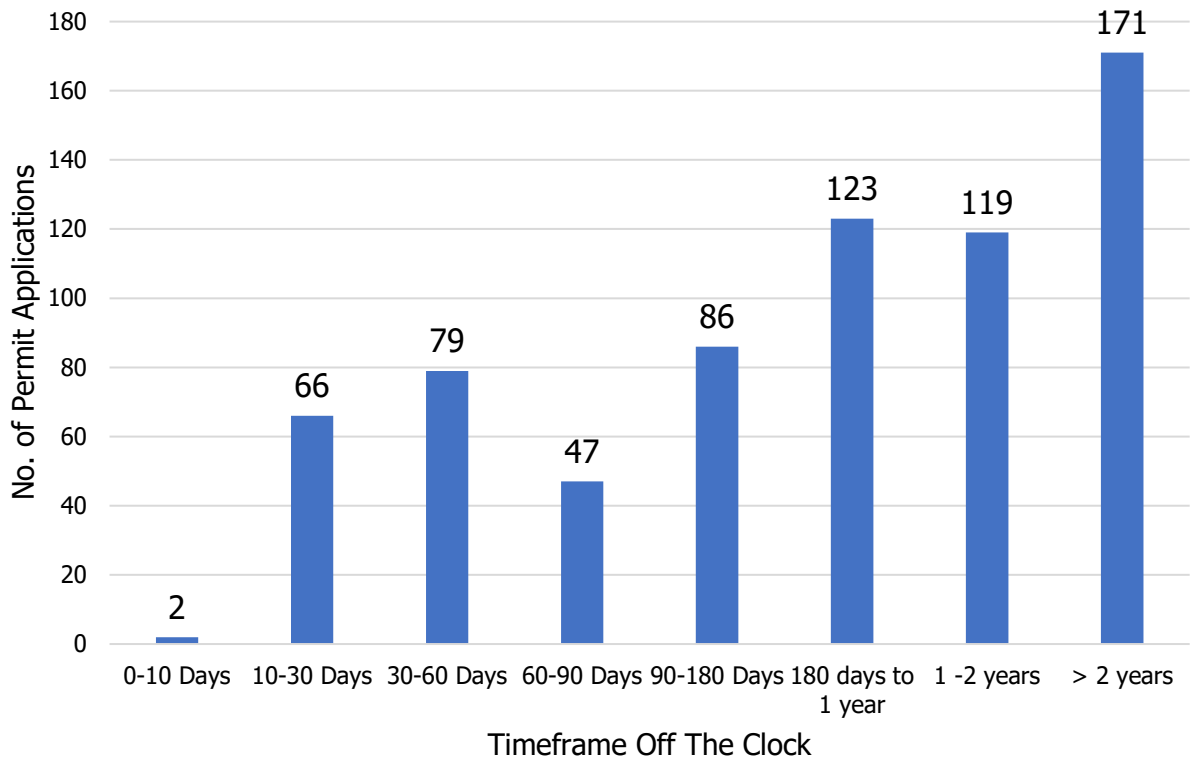
### COMPARISON OF OFF THE CLOCK PERMITS VERSUS ANNUAL PERMITS SUBMITTED



As of September 09, 2024, off-the-clock permit application comprise 18% of the average number of permit applications submitted over the past 3 years.

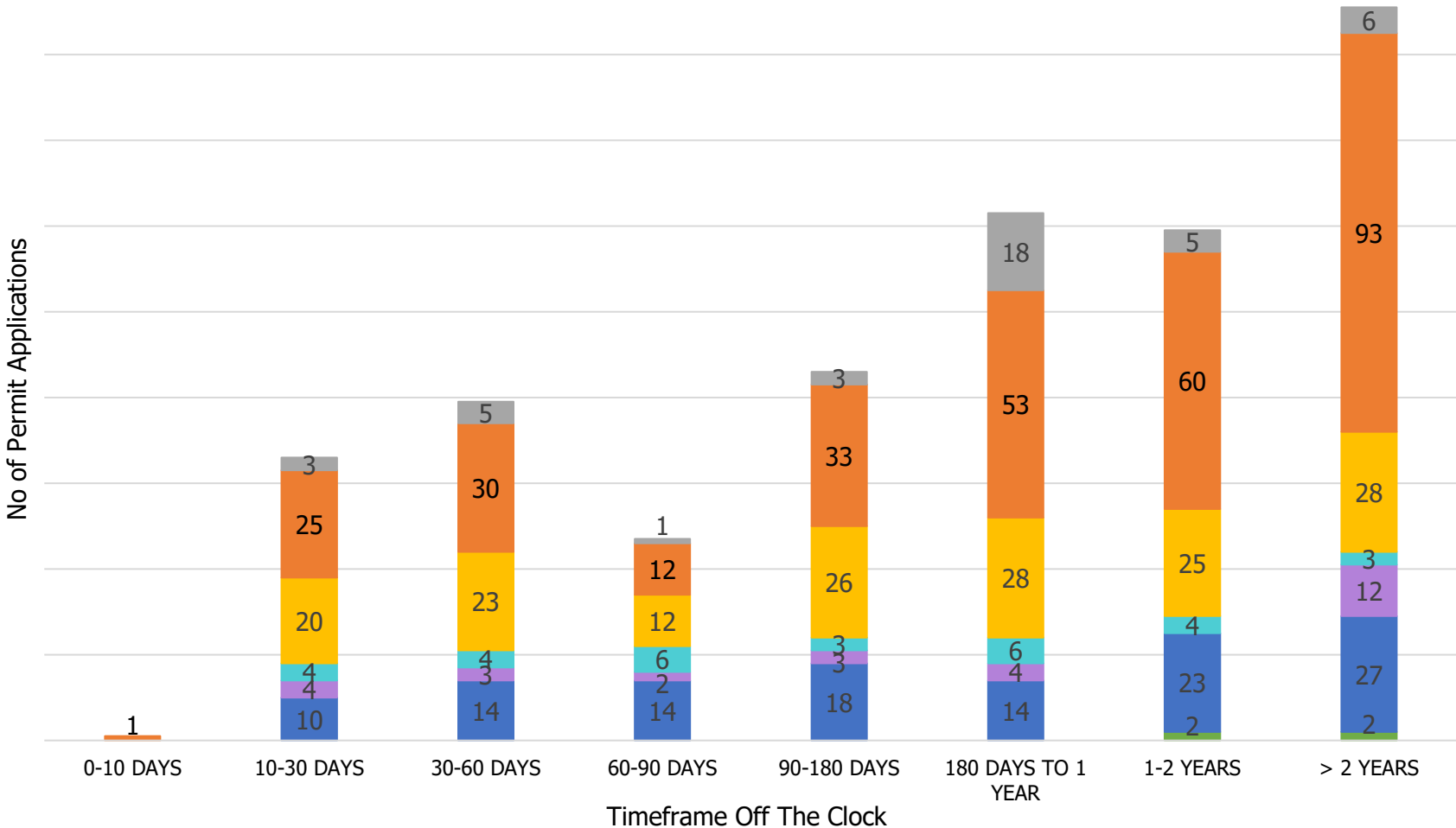


**OFF THE CLOCK PERMIT APPLICATIONS BY DURATION**  
(AS OF SEP09'24)

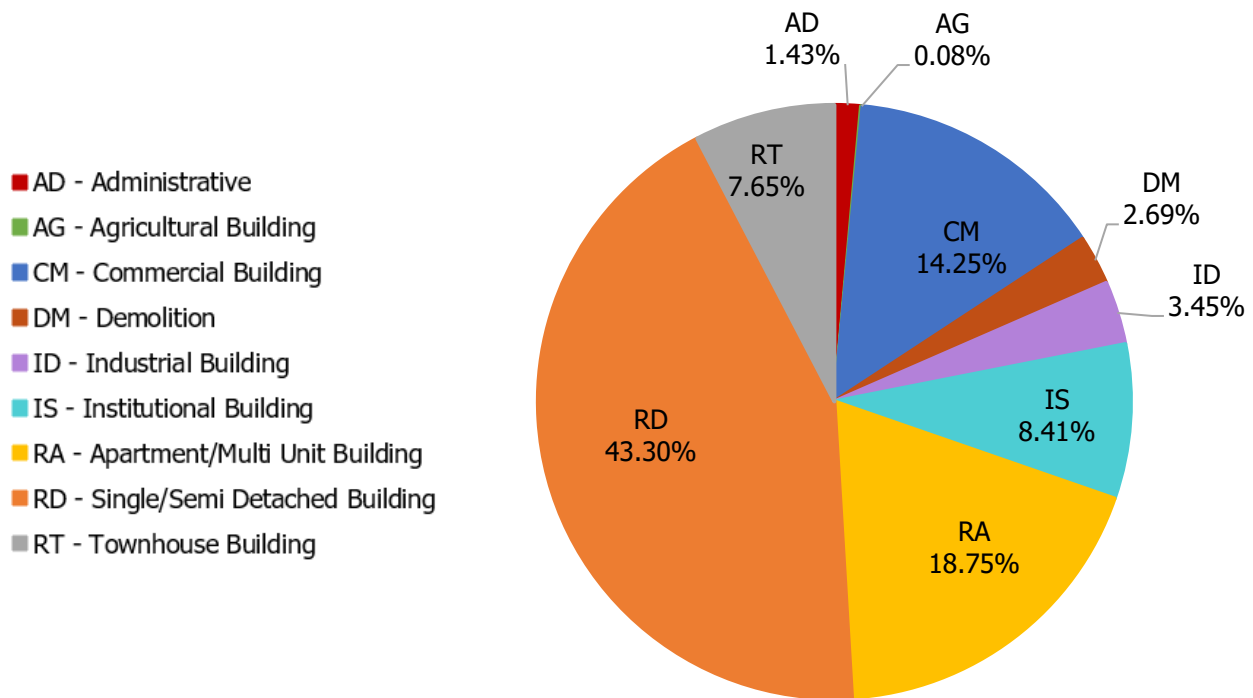
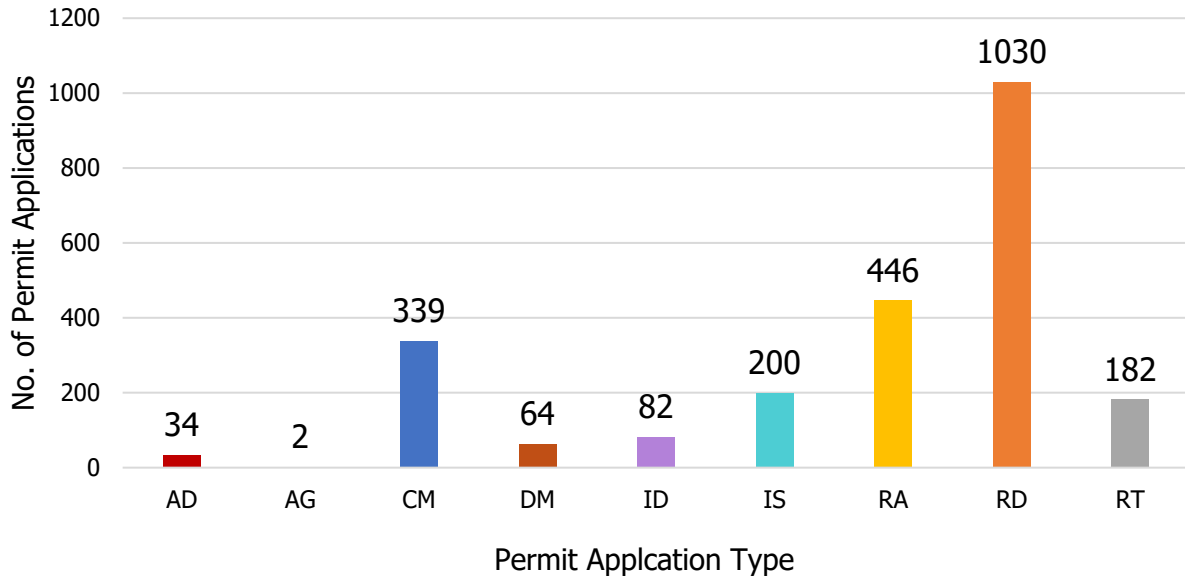


### TIMEFRAME OFF THE CLOCK BY PERMIT TYPE (AS OF SEP09'24)

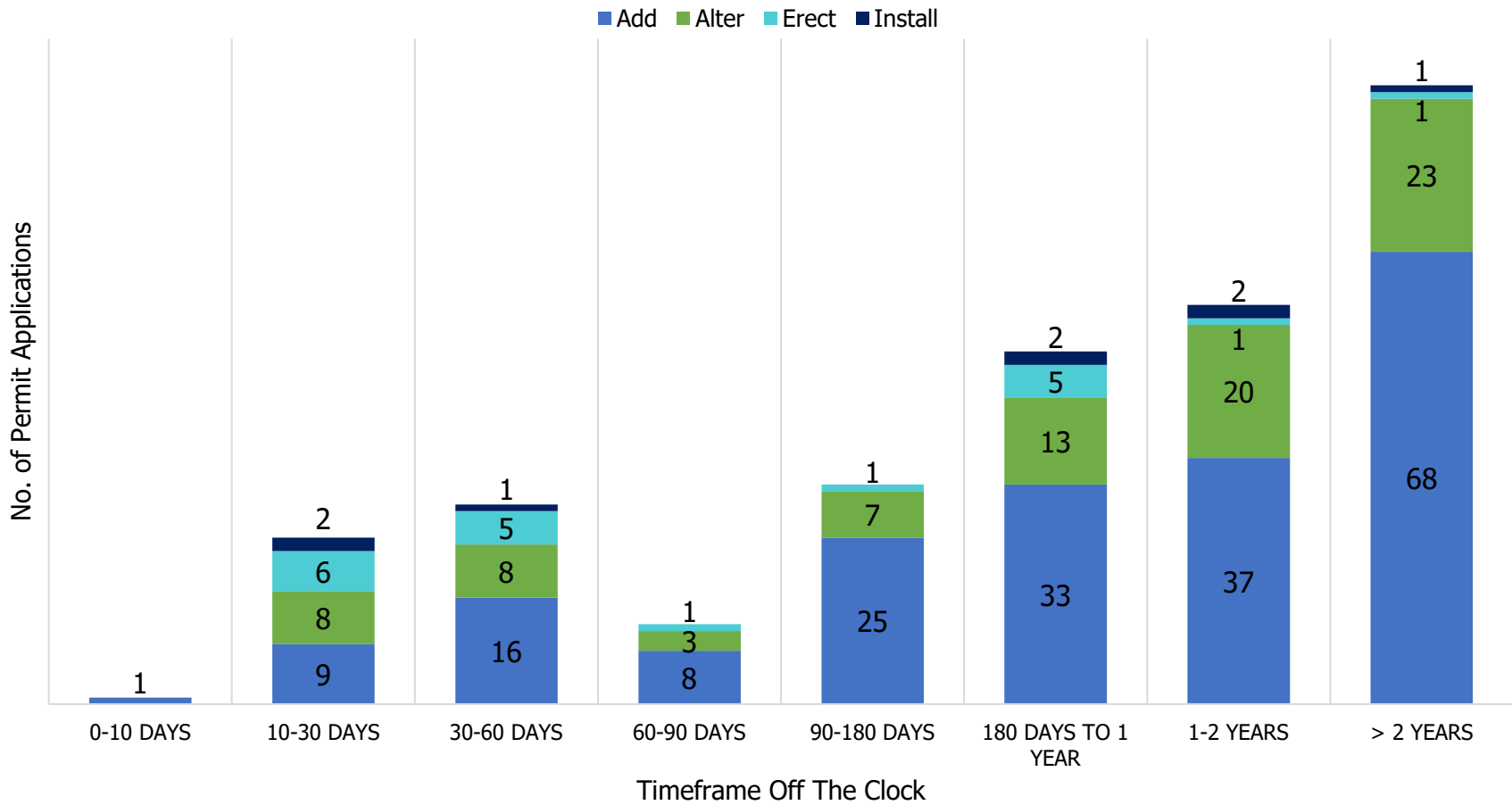
- Permit Type: AG (Agricultural)
- Permit Type: CM (Commercial)
- Permit Type: ID (Industrial)
- Permit Type: IS (Institutional)
- Permit Type: RA (Apartment/Multi Unit)
- Permit Type: RD (Single/Semi Detached)
- Permit Type: RT (Townhouse)



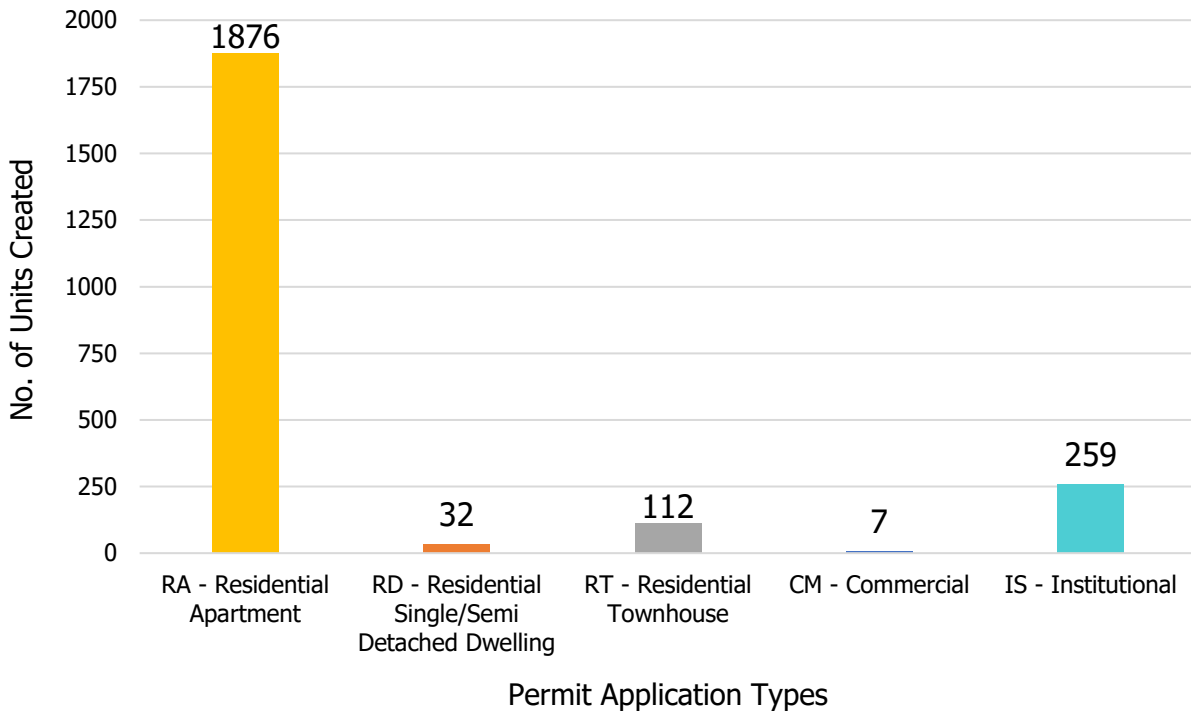
**TOTAL No. OF PERMIT APPLICATIONS SUBMITTED IN 2024**  
(AS OF SEP09'24)



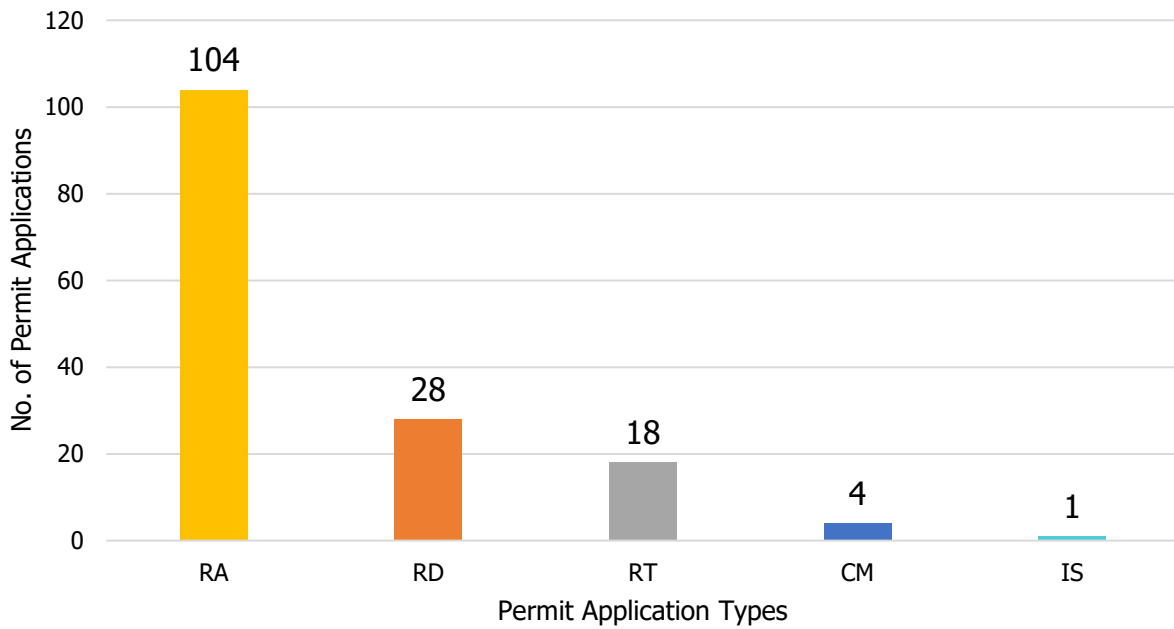
## TIMEFRAME OF OFF THE CLOCK RD PERMITS BY WORK TYPE (AS OF SEP09'24)



**NEW UNITS - OFF THE CLOCK PERMITS  
(AS OF SEP09'24)**



**No. OF OFF THE CLOCK PERMIT APPLICATIONS w/ NEW UNITS CREATED  
(AS OF SEP09'24)**





## Report to Planning and Environment Committee

**To:** Chair and Members  
Planning and Environment Committee  
**From:** Scott Mathers, MPA, P.Eng.  
Deputy City Manager, Planning and Economic Development  
**Subject:** London Cross Cultural Learner Centre  
763-773 Dundas Street  
File Number: Z-9777, Ward 4  
Public Participation Meeting  
**Date:** October 22, 2024

## Recommendation

That, on the recommendation of the Director, Planning and Development, the following actions be taken with respect to the application of London Cross Cultural Learner Centre (c/o Valerian Marochko) (c/o Zelinka Priamo Ltd.) relating to the property located at 763-773 Dundas Street:

- (a) the proposed by-law attached hereto as Appendix "A" **BE INTRODUCED** at the Municipal Council meeting November 5, 2024, to amend Zoning By-law No. Z.-1, in conformity with the Official Plan, The London Plan, to change the zoning of the subject property **FROM** a Business District Commercial Special Provision (BDC(19)\*D250\*H46) Zone **TO** a Holding Business District Commercial Special Provision ((h-18\*BDC(\_)\*D550\*H82) Zone;
- (b) The Site Plan Approval Authority **BE REQUESTED** to consider the following design issues through the site plan process:
  - i) Provide second paratransit lay-by for south tower or relocate proposed paratransit lay-by to a central location with a protected waiting area.
  - ii) Investigate renewable sources of energy such as solar for the roof and sides of the building, and geothermal for interior heating and cooling;
  - iii) Investigate air source heat pump options;
  - iv) Include a minimum of 5% EV charging spots roughed in;
  - v) Utilize bird friendly policies using the CSA standard.

**IT BEING NOTED**, that the above noted amendment is being recommended for the following reasons:

- i) The recommended amendment is consistent with the *Provincial Planning Statement, 2024 (PPS)*;
- ii) The recommended amendment conforms to the Old East Village Dundas Street Corridor Secondary Plan, including but not limited to the Land Use, Built Form, Public Realm and Housing Options policies.
- iii) The recommended amendment conforms to The London Plan, including but not limited to the Key Directions, City Building policies, the Neighbourhoods Place Type policies, Old East Village Specific-Segment, and the Zoning to the Upper Maximum Height;
- iv) The recommended amendment would permit an appropriate form of redevelopment at an intensity that can be accommodated on the subject lands and is considered compatible with the surrounding neighbourhood; and
- v) The recommended amendment support's Council's commitment to increase housing supply and affordability, and initiatives related to the Housing Accelerator Fund that will support the creation of affordable housing units.

## Executive Summary

### Summary of Request

The applicant has requested an amendment to Zoning By-law Z.-1 to rezone the subject

lands from a Business District Commercial Special Provision (BDC(19)\*D250\*H46) Zone to a Holding Business District Commercial Special Provision (h-18\*BDC(\_)\*D550\*H82) Zone.

### **Purpose and the Effect of Recommended Action**

The recommended action will permit **247 residential units** (with a minimum 40% proposed to be affordable) within a 6-storey mixed-use building fronting Dundas Street, consisting of 34 residential units and approximately 300m<sup>2</sup> of gross-floor commercial space, and a 24-storey mixed-use building consisting of 213 units and ground-floor multi-use space.

Staff are recommending approval of the requested Zoning By-law Amendment with special provisions that will facilitate the development while ensuring mitigation measures are incorporated into the design.

### **Linkage to the Corporate Strategic Plan**

This recommendation will contribute to the advancement of Municipal Council's 2023-2027 Strategic Plan in the following ways:

1. **Strategic Plan Area of Focus: Wellbeing and Safety**, by promoting neighbourhood planning and design that creates safe, accessible, diverse, walkable, healthy, and connected communities.
2. **Strategic Plan Area of Focus: Housing and Homelessness**, by supporting faster/ streamlined approvals and increasing the supply of housing with a focus on achieving intensification targets.
3. **Housing and Homelessness**, by increasing access to a range of quality, affordable, and supportive housing options that meet the unique needs of Londoners.

### **Analysis**

#### **1.0 Background Information**

##### **1.1 Property Description and Location**

The subject lands, municipally known as 763-773 Dundas Street, are located on the south side of Dundas Street midblock between Hewitt Street to the west and Rectory Street to the east, in the East London Planning District. The subject lands are comprised of two irregularly shaped parcels, which once consolidated would have a total area of approximately 0.48 hectares (4,8000 square metres), 40.3 metres of frontage along Dundas Street and a depth of 90.6 metres.

The subject lands are currently developed with two 2-storey mixed use buildings, one of which at 765 and 769 Dundas Street is currently vacant. The rear (southerly) portion of the subject lands known as 763 Dundas Street are vacant.

Vehicular access is currently provided via a driveway from Dundas Street located between the two existing buildings. An existing rear lane provides access from Hewitt Street to Rectory Street and also provides vehicular access to the subject lands. This portion of Dundas Street consists of two lanes, with a dedicated bicycle lane. Pedestrian sidewalks are also located on both the north and south sides of Dundas Street.

The subject lands are part of an established neighbourhood consisting primarily of low-rise commercial and residential uses. 773 Dundas Street is also identified as a Heritage Listed property and the subject lands are adjacent to several other properties listed on the City of London Register of Cultural Resources.

##### **Site Statistics:**

- Current Land Use: Two 2-storey active and vacant mixed-use buildings
- Frontage: 40.3 metres (Dundas Street)
- Depth: 90.6 metres

- Area: 0.48 hectares
- Shape: Irregular
- Located within the Built Area Boundary: Yes
- Located within the Primary Transit Area: Yes

**Surrounding Land Uses:**

- North: Business District Commercial/Residential
- East: Business District Commercial/Office Residential/Regional Facility
- South: Residential/ Restricted Service Commercial
- West: Business District Commercial

**Existing Planning Information:**

- The London Plan Place Type: Urban Corridor Place Type fronting a Main Street/Civic Boulevard
- Rapid Transit and Urban Corridor Specific-Segment Policies: Old East Village Specific-Segment
- Secondary Plans: Old East Village Dundas Street Corridor Secondary Plan
- Special Planning Areas: Primary Transit Area
- Existing Zoning: Business District Commercial Special Provision (BDC(19)\*D250\*H46) Zone

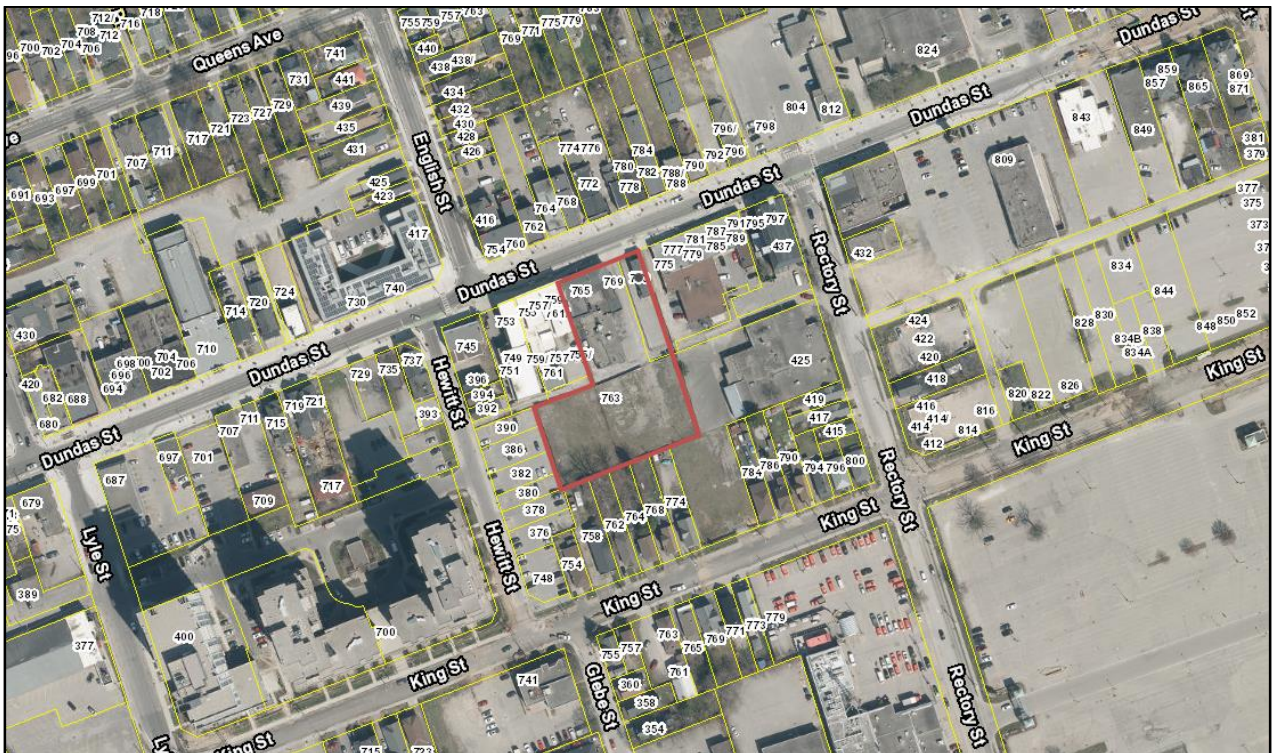


Figure 1- Aerial Photo of the subject lands and surrounding area.



Figure 2 - Streetview of the subject lands (view looking south from Dundas Street)

**2.0 Discussion and Considerations**

**2.1 Development Proposal**

The applicant is proposing to redevelop the subject lands with a 6-storey mixed-use

building fronting Dundas Street, consisting of 34 residential units and approximately 300m<sup>2</sup> of gross-floor commercial space; and a 24-storey mixed-use building located to the rear, consisting of 213 units and ground-floor multi-use space. A total of 247 residential units, with a minimum of 40% proposed to be affordable, are proposed at a mixed-use density of 550 units per hectare. Primary vehicular access to the subject lands is provided from Dundas Street with a secondary vehicular access provided via the existing laneway from Hewitt Street. The secondary laneway access is proposed to be one-way only, directing traffic from Hewitt Street and exiting to Dundas Street. A total of 85 surface parking spaces are proposed (16 surface spaces and 69 underground spaces), as well as short-term and long-term bicycle parking spaces at a rate of 0.1 spaces per unit and 0.9 spaces per unit, respectively. The conceptual site plan is shown below as Figure 3 and a massing model shown as Figure 4.

The proposed development includes the following features:

- Land use: Mixed-use (residential and commercial)
- Form: Two (2) apartment buildings
- Height: 6-storeys and 24-storeys
- New Residential Units: 247
- Density: 550 units per hectare
- Lot Coverage: 50.7%
- Landscape Open Space: 24%
- Parking Spaces: 85 surface parking spaces (0.4 spaces/unit)
- Bicycle Parking Spaces: Long-term (0.9 spaces/unit) and short-term (0.1 spaces/unit)

Additional information on the development proposal is provided in Appendix B.

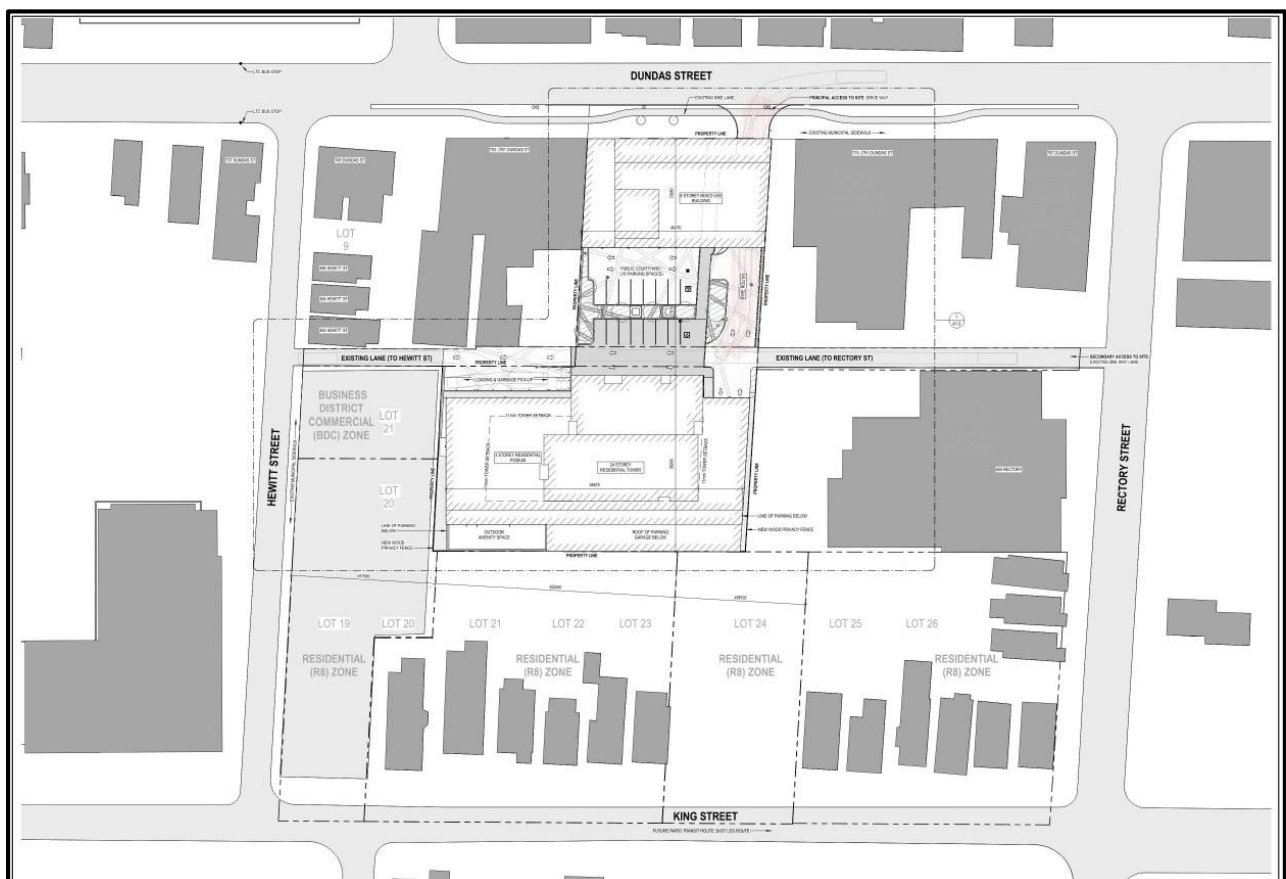


Figure 3 - Conceptual Site Plan

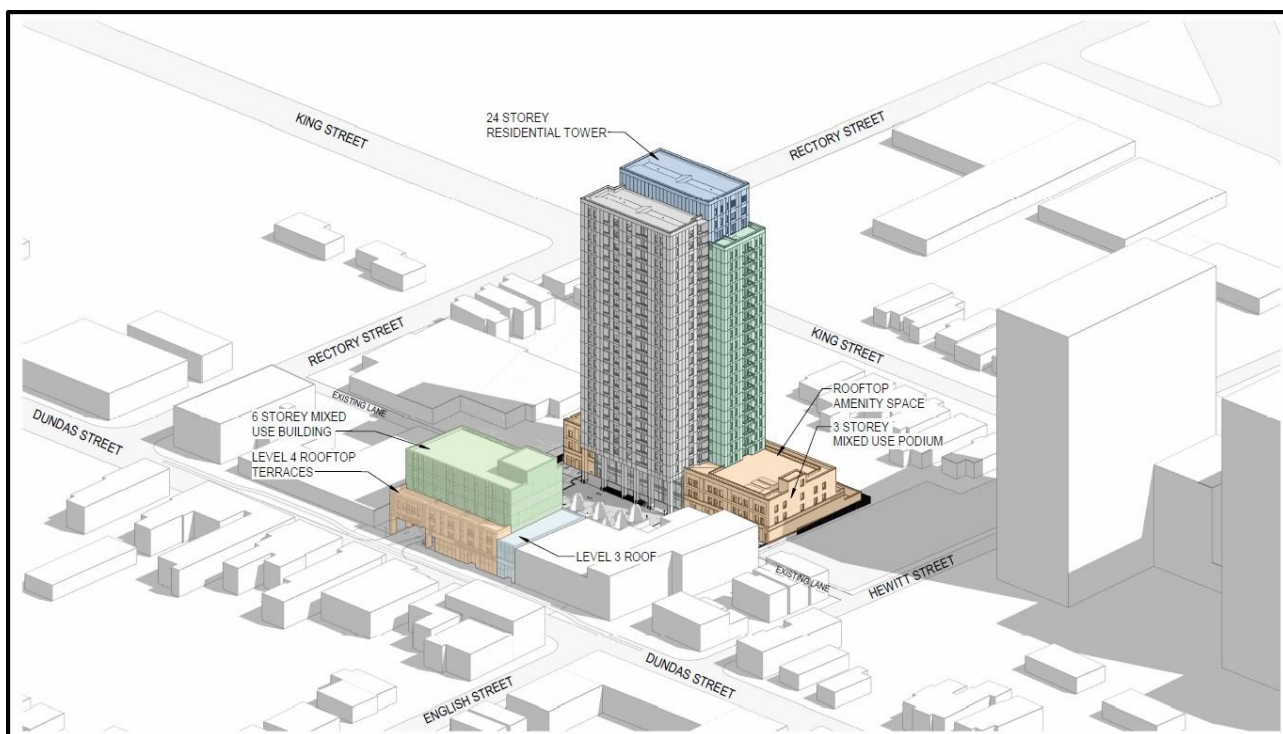


Figure 4 – Massing Model

Additional plans and drawings of the development proposal are provided in Appendix D.

## 2.2 Requested Amendment(s)

The applicant has requested an amendment to Zoning By-law Z.-1 to rezone the subject lands from a Business District Commercial Special Provision (BDC(19)\*D250\*H46) Zone to a Business District Commercial Special Provision ((h-18\*BDC(\_)\*D550\*H82) Zone.

The following table summarizes the special provisions that have been proposed by the applicant and those that are being recommended by staff.

Regulation (BDC(_)) Zone)	Required	Proposed	Recommended
Additional Permitted Uses	N/A	<ul style="list-style-type: none"> <li>Institutions</li> <li>Social Service Establishment</li> </ul>	As requested
Stepback Above the 3rd Storey – Street Facing Façade (minimum)	N/A	N/A	5.0 metres
West Interior Side Yard Depth Abutting a Residential Zone (minimum):	32.0 metres	1.8 metres	1.5 metres
Rear Yard Depth (minimum)	32.0 metres	0.0 metres	As requested
Rear Yard Depth Above the 1st Storey (minimum)	32.0 metres	N/A	6.0 metres
Rear Yard Depth Above the 2nd Storey (minimum)	32.0 metres	N/A	9.0 metres
Tower Floorplate (maximum)	N/A	N/A	1,000 square metres
Tower Setback - East and South (minimum)	N/A	N/A	11.0 metres
Tower Setback –	N/A	N/A	15.0 metres

Regulation (BDC( ) Zone)	Required	Proposed	Recommended
West (minimum)			
Height (maximum)	46 metres	82 metres	As requested
Density (maximum)	250 uph	550 uph	As requested

### 2.3 Internal and Agency Comments

The application and associated materials were circulated for internal comments and public agencies to review. Comments received were considered in the review of this application and are addressed in Section 4.0 of this report.

Key issues identified by staff and agencies included:

- High-rise buildings should be designed with slender towers that reduce shadow impact, minimize the obstruction of views, and are less massive to neighbouring properties. Provide a 15-metre setback to the tower from interior property lines to give consideration for future development on adjacent properties and provide confirmation that the proposed tower is 1,000 square metres or less.
- A second paratransit lay-by or a relocated of the proposed paratransit lay-by is required.
- An h-18 holding provision is require until archaeological matter have been satisfied.

Detailed internal and agency comments are included in Appendix D of this report.

### 2.4 Public Engagement

On August 30, 2024, Notice of Application was sent to 722 property owners and residents in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on September 5, 2024. A “Planning Application” sign was also placed on the site.

There were **no** responses received during the public consultation period. Comments received were considered in the review of this application and are addressed in Section 4.0 of this report.

Detailed public comments are included in Appendix E of this report.

### 2.5 Policy Context

#### ***The Planning Act and the Provincial Planning Statement, 2024***

The Provincial Planning policy framework is established through the *Planning Act* (Section 3) and the *Provincial Planning Statement, 2024 (PPS)*. The *Planning Act* requires that all municipal land use decisions affecting planning matters shall be consistent with the *PPS*.

The *Provincial Planning Statement (PPS), 2024* provides policy direction on matters of provincial interest related to land use planning and development. The PPS is issued under the authority of Section 3 of the *Planning Act* and came into effect on October 24, 2024. The Provincial Policy Statement, 2024, applies to all decisions that effects a planning matter made on or after October 20, 2024.

The proposed development meets the intent of the *Provincial Policy Statement, 2024*, by introducing residential intensification in the form of mixed-use apartment buildings within the urban growth boundary and near public-transit and other services.

The mechanism for implementing Provincial policies is through the Official Plan, The London Plan. Through the preparation, adoption and subsequent Ontario Land Tribunal (OLT) approval of The London Plan, the City of London has established the local policy framework for the implementation of the Provincial planning policy framework. As such,

matters of provincial interest are reviewed and discussed in The London Plan analysis below.

As the application for a Zoning By-law amendment complies with The London Plan, it is staff's opinion that the application is consistent with the *Planning Act* and the *PPS*.

### ***The London Plan, 2016***

The London Plan (TLP) includes evaluation criteria for all planning and development applications with respect to use, intensity and form, as well as with consideration of the following (TLP 1577-1579):

1. Consistency with the Provincial Policy Statement and all applicable legislation.
2. Conformity with the Our City, Our Strategy, City Building, and Environmental policies.
3. Conformity with the Place Type policies.
4. Consideration of applicable guideline documents.
5. The availability of municipal services.
6. Potential impacts on adjacent and nearby properties in the area and the degree to which such impacts can be managed and mitigated.
7. The degree to which the proposal fits within its existing and planned context.

Staff are of the opinion that all the above criteria have been satisfied.

### ***Old East Village Dundas Street Corridor Secondary Plan***

The *Old East Village Dundas Street Corridor Secondary Plan (2020)* has been reviewed in its entirety and it is staff's opinion that the proposed Zoning By-law amendment is consistent with it. The subject lands are located within the Old East Village Dundas Street Corridor Secondary Plan Area boundary in an area that permits up to high-rise forms.

## **3.0 Financial Impact/Considerations**

### **3.1 Financial Impact**

None.

## **4.0 Key Issues and Considerations**

### **4.1 Land Use**

The proposed mixed-use building is supported by the policies of the PPS 2024. The site is in the Urban Corridor Place Type, as identified on Map 1 of The London Plan. The proposed mixed-used apartment buildings are contemplated in the Urban Corridor Place Type, which permits a range of residential, retail, service, office, cultural, recreational, and institutional uses (TLP 837\_). Mixed-use buildings are encouraged, and where there is a mix of uses within an individual building, retail and service uses are encouraged to front the street at grade (TLP 837\_4). The proposed mixed-used apartment buildings align with the vision of the Urban Corridor Place Type by contributing to a mix of residential and a range of other uses at grade along corridors that help establish demand for rapid transit services (TLP 830\_4). The vision for the Urban Corridor Place Type will also be implemented by supporting the development of a variety of residential types, with varying locations, size, affordability, tenure, design, and accessibility so that a broad range of housing requirements are satisfied (TLP 830\_11). Further, the subject lands are also located within the Primary Transit Area (PTA) which will be a focus of residential intensification and transit investment within London that will be a good fit within existing neighbourhoods (TLP 90\_).

The Old East Village Dundas Street Corridor Secondary Plan contemplates a broad range of residential, retail, service, office, cultural, recreational, and institutional uses (3.2.b). Mixed-use buildings are encouraged as the preferred form of development within the Secondary Plan area (3.2.1). Dundas Street properties, between Adelaide

Street North and Burbrook Place, shall provide street-oriented active uses on the ground floor for the majority of the Dundas Street frontage (3.2.c) with residential uses encouraged above the ground floor to increase the residential population and provide a variety of housing options.

Institutions and Social Service Establishment uses are also being recommended as additional permitted uses which are also contemplated in the Urban Corridor Place Type and Old East Village Dundas Street Corridor Secondary Plan as a means of continued provision of neighbourhood services (3.2.h).

## **4.2 Intensity**

The proposed residential intensity is consistent with the policies of the PPS that encourage all types of residential intensification, including redevelopment which results in a net increase in residential uses (2.2.1.2), compact form (2.4.1.3.c), and an appropriate mix of housing options and densities (2.3.1.3). While the proposed 6-storey mixed-use building fronting Dundas Street conforms with Table 9 – Maximum Height in the Rapid Transit and Urban Corridor Place Types of The London Plan, which contemplates a maximum height of 8-storeys (upper maximum of 10-storeys), the 24-storey mixed-use building located to the rear does not. Where there is a conflict or inconsistency between the parent policies or maps of The London Plan and the policies or maps of a secondary plan, the secondary plan policies or maps will prevail. Otherwise, the parent policies and maps of The London Plan will be read together and in conjunction with the secondary plan (TLP 1558\_). The proposed 24-storey mixed-use building conforms with the Old East Village Dundas Street Corridor Secondary Plan which permitted a maximum height of up to 24-storeys on properties such as this that allow for high-rise forms (3.3.1.g). As such, the proposed intensity is in conformity with the Old East Village Dundas Street Corridor Secondary Plan.

When zoning to the upper maximum, development should include features required to mitigate the impacts of the additional height and densities to ensure the increase in building height represents good planning within its context (TLP 1640\_, 1641\_). As proposed, the mixed-used apartment buildings include design features such as a continuous street wall consistent with the established character of the Old East Village (3.3.2.a), the proposed high-rise building delineates the base, middle, and top through the use of stepbacks and architectural materials, and the tower is appropriately scaled to provide a slender tower with a floorplate of under 1,000 square metres. The proposed intensity will facilitate an appropriate form and scale of residential intensification that is designed to mitigate the impact of new development on adjacent neighbourhood areas on a property of a sufficient size and configuration to accommodate the proposed development and to help mitigate planning impacts on adjacent uses (TLP 839\_4 and 841\_13). The development will also facilitate the efficient use of land and existing municipal services, as existing services are available and sufficient to adequately service the proposed number of units.

## **4.3 Form**

The proposed built form is supported by the policies of the PPS 2024 and is contemplated in accordance with the Urban Corridor Place Type of The London Plan. The London Plan encourages compact forms of development for planning new growth, including “inward and upward” compact forms of development (TLP 7\_, 59\_2, 66\_, 79\_). The form of development will foster a well-designed built form, be a good fit and compatible with its context, support a positive pedestrian environment and support all types of active mobility and universal accessibility (TLP, 193\_1 to 193\_5). Buildings should be sited so that they maintain and reinforce the prevailing street wall or street line of existing buildings and minimize the visual exposure of parking areas to the street (TLP 256\_ & 269\_).

In accordance with the Old East Village Dundas Street Corridor Secondary Plan, mid-rise buildings (described as buildings 4-storeys and up to and including 8-storeys in height) should be designed to express the following components (3.3.3):



- Should be designed to provide a sensitive transition of built form to the establishing established built form; and
- Shall include a setback of a minimum of five metres along public rights-of-way to mitigate downward wind shear and support the existing street character at street level.

The mid-rise building is proposed to be adjacent to the Dundas Street frontage to help reinforce the existing urban street wall and is designed with ground floor commercial space and a 5.0 metre setback above the third storey. The proposed mid-rise building, therefore, is consistent with the policies of the Old East Village Dundas Street Corridor Secondary Plan as well as the City Design policies of The London Plan by facilitating an appropriate form and scale of residential intensification that is designed to mitigate the impact of new development on adjacent neighbourhood areas (TLP 841\_13). Further, the proposed mid-rise building supports a positive pedestrian environment, a mix of housing types to support aging in place and affordability and is designed to be a good fit and compatible within its context/neighbourhood character (TLP 193\_).

As well, high-rise buildings should be designed to express the following components (3.3.4):

- A podium designed to support a pedestrian-scaled environment at street level;
- Shall setback a minimum of 5 metres along public right-of-way;
- Should be designed with slender towners that reduce shadow impact, minimum the obstruction of views, and are less massive to neighboring properties with floorplates of approximately 1,000 square metres or less is a reasonable target to achieve this goal;
- The top portions of the towers shall be articulated through the use of a small setback, difference in articulation, or the use of an architectural feature; and
- Should have a minimum separation distance of 30 metres between towers and a setback of a minimum distance of 15 metres from interior property lines.

The high-rise building is proposed to be located to the rear of the subject lands and includes ground-floor multi-use spaces to serve the community and provide amenity area for residents. The building is comprised of a 3-storey podium along the northerly, easterly, and westerly elevations, with a 1-story podium along the southerly elevation where the proposed development interfaces with the rear yard of existing single detached dwellings, and a minimum 11.0 metre setback on all sides of the building. The proposed high-rise development also provides an appropriately scaled, slender tower with a floorplate of 840 square metres. The proposed built form and massing of the high-rise building has consideration for the surrounding land uses and are appropriate within the existing and future context of the neighbourhood (TLP 953\_2). The proposed setbacks to the podium and tower separation from the southern and eastern properties lines are also considered appropriate from adjacent properties (TLP 841\_2), minimizing potential impacts of shadowing and loss of sunlight access, and ensuring a reasonable level of privacy. It should be noted that while the Secondary Plan encourages towers of high-rise buildings to be setback a minimum distance of 15 metres from interior property lines, staff are satisfied the proposed 11 metres provides sufficient separation to protect opportunities for future high-rise development on adjacent sites.

Primary vehicle access to the subject lands is provided from Dundas Street with a secondary vehicle access provided via the existing land way from Hewitt Street promoting connectivity and safe movement for pedestrians, cyclists, and motorists (TLP 255\_). The secondary laneway access is proposed to be one-way only, directing traffic from Hewitt Street and existing to Dundas Street (3.3.2.f). Parking for the proposed development will be located internal to the site integrated in a way that does not detract from the character of the street (3.3.2.f). A total of 85 surface and underground parking spaces will be provided, as well as both short-term (0.1 space per unit) and long-term (0.9 spaces per unit) bicycle parking spaces. The site is well connected with public transit, and the Dundas Street commercial node has a high proportion of existing retail/commercial, service and employment use within convenient walking distance to

reduce single vehicle trips and encourage more active transportation such as walking or cycling. The proposed number of vehicle parking spaces is reasonable given the active mobility and public transit options and will contribute to achieving the overall intent of the Urban Corridor. It is also noted that the subject lands are in an area of the City with no minimum residential parking requirement.

Through the review of the application the following site layout and built form features have been acknowledged and should be carried forward:

- Street orientation and the continuation of the street wall with ground floor active uses for the building located on Dundas Street.
- A 5 metre setback above the 3<sup>rd</sup> storey for the building located on Dundas Street.
- Active uses fronting the internal courtyard/shared outdoor amenity space for the ground floor of the 24-storey tower.

Through the review and consideration of concerns raised, staff recommended matters such providing a second paratransit lay-by for south tower or relocate proposed paratransit lay-by to a central location with a protected waiting area be considered by the Site Plan Approval Authority. Staff are satisfied that these matters can be sufficiently addressed through a subsequent Site Plan application.

The proposed mixed-used apartment buildings are supported by the policies of the Provincial Policy Statement, contemplated in the Urban Corridor Place Type (TLP 841\_), in keeping with the City Building policies of The London Plan, and conform to the Old East Village Dundas Street Corridor Secondary Plan.

#### **4.4 Affordable Housing**

In the case of rental housing, The London Plan defines *affordable housing* as either; “a) A unit for which the rent does not exceed 30 percent of gross annual household income for low- and moderate-income households; or b) A unit for which the rent is at or below the average market rent of a unit in the City of London” (TLP, 1795\_). For the purposes of this application, the proposed affordable housing development aims to supply housing at lower-than-average rent which will contribute to the range of options for housing affordability in a well-connected neighbourhood with convenient access to employment opportunities, recreational areas and goods and services within walking distance.

Confirmation of the number of units and any City funding provided to support the development would be addressed through a future municipal contribution agreement. Contact has been made by the applicant with the City’s Municipal Housing Development group who administer funding and associated agreements to support affordable housing.

#### **4.4 Holding Provisions**

##### *Archaeological Assessment*

As part of the complete application a Stage 1-2 Archaeological Assessment was required and submitted. However, the city has not yet received confirmation of acceptance of this report by the Ministry of Citizenship and Multiculturalism (MCM). As such, the archaeological matters for this property have not yet been addressed. The h-18 holding provision is recommended to ensure that the outstanding archaeological matters are addressed prior to soil disturbance.

#### **4.5 Zoning**

The applicant has requested an amendment to Zoning By-law Z.-1 to rezone the subject lands from a Business District Commercial Special Provision (BDC(19)\*D250\*H46) Zone to a Holding Business District Commercial Special Provision (h-18\*BDC(\_)\*D550\*H82) Zone. The following summarizes the special provisions that

have been proposed by the applicant and those that are being recommended by staff.

*A maximum height of 82 metres and a maximum density of 550 units per hectare.*

A special provision to permit a maximum height of 82 metres and a maximum density of 550 units per hectares are proposed by the applicant and are being recommended by staff. The Business District Commercial Zone regulations do not specify a maximum building height nor density of apartment buildings, rather heights and densities shall be applied on a site-specific basis to allow for flexibility and to ensure that the potential height impacts are mitigated on the abutting properties. As discussed in Section 4.1 of this report, the subject site is located within a section the Old East Village Dundas Street Corridor Secondary Plan which permits a maximum height of up to 24-storeys (3.3.1.g). Further, the applicant has requested a maximum residential density of 550 units per hectare to permit the proposed mixed-use development within appropriately scaled buildings and floorplates. As such, the proposed maximum height and density are considered appropriate and will allow for the implementation of the proposed development, facilitating an appropriate form and scale of residential intensification on a property of a sufficient size and configuration to accommodate the proposed development and to help mitigate planning impacts on adjacent uses (TLP 839\_4).

*A minimum west interior side yard depth of 1.8 metres and a rear yard depth of 0.0 metres.*

Special provisions to permit a minimum west interior side yard depth of 1.8 metres and a rear yard depth of 0.0 metres are proposed by the applicant with staff recommending an interior side yard depth of 1.5 metres (for flexibility) and a rear yard depth of 0.0 metres. The intent of regulating yard depths is to ensure sufficient space between buildings and lot lines to accommodate onsite functions while mitigating potential impacts on adjacent properties. In this case, the special provisions relate to the podium of the high-rise building which is comprised of a 3-storey podium along the northerly, easterly, and westerly elevation and a 1-story podium along the southerly elevation where the proposed development interfaces with the rear yard of existing single detached dwellings. Based on the development concept and existing and future neighbourhood context, staff have no concerns with the reduced yard setbacks. Additional mitigation measures will be considered through the Site Plan Approval process.

#### *Staff Recommended Special Provisions*

Staff are recommending the following special provisions to implement the proposed development in accordance with the policies of The London Plan and Old East Village Dundas Street Corridor Secondary Plan, and to provide additional certainty that potential impacts of the proposed height and density will be mitigated. The special provisions, supported by the applicant, are as follows:

- Stepback Above the 3rd Storey – Street Facing Façade (minimum) – Staff are recommending a minimum stepback of 5.0 metres above the podium, consistent with the form policies of the Old East Village Dundas Street Corridor Secondary Plan and the current design of Building A and B. The special provision will ensure that the proposed podium-rooftop amenity spaces are functional.
- Tower Setback (minimum) – Staff are recommending special provisions to ensure sufficient separation between the tower-portion of the development and the property lines at 763 Dundas Street. A 11.0 metre setback from the easterly and southerly property lines are being recommended by staff. Staff are also recommending a minimum tower setback of 15.0 metres from the westerly property line to ensure appropriate tower separation.
- Tower Floorplate (maximum) – Staff are recommending a maximum floorplate for the tower to ensure an appropriately scaled development that minimizes the mass of the building to reduce shadow impact, minimum the obstruction of views, and are less massive to neighboring properties (3.3.4.c). The proposed building has a maximum tower floorplate of approximately 840m<sup>2</sup>. Staff recommended a maximum tower floorplate of 1,000m<sup>2</sup>.

## Conclusion

The applicant has requested an amendment to Zoning By-law Z.-1 to rezone the subject lands from a Business District Commercial Special Provision (BDC(19)\*D250\*H46) Zone to a Holding Business District Commercial Special Provision (h-18\*BDC( )\*D550\*H82) Zone. Staff are recommending approval of the requested Zoning By-law amendment with special provisions to permit a 6-storey mixed-use building with 34 residential units and approximately 300m<sup>2</sup> of gross-floor commercial space; and a 24-storey mixed-use building with 213 residential units and ground-floor multi-use space, for a total of 247 residential units and a mixed-use density of 550 units per hectare.

The recommended action is consistent with the Provincial Planning Statement, 2024 (PPS), conforms to The London Plan and Old East Village Dundas Street Corridor Secondary Plan, contributes to a mix of housing types, will facilitate a form of residential development at an appropriate scale and intensity, and which support's Council's commitment to increase housing supply and affordability, and initiatives related to the Housing Accelerator Fund that will support the creation of affordable housing units.

**Prepared by:**                    **Michaella Hynes**  
**Planner, Planning Implementation**

**Reviewed by:**                **Catherine Maton, MCIP, RPP**  
**Manager, Planning Implementation**

**Recommended by:**        **Heather McNeely, MCIP, RPP**  
**Director, Planning and Development**

**Submitted by:**              **Scott Mathers, MPA, P.Eng.**  
**Deputy City Manager, Planning and Economic**  
**Development**

**Copy:**  
Britt O'Hagan, Manager, Current Development  
Mike Corby, Manager, Site Plans  
Brent Lambert, Manager, Development Engineering

## Appendix A – Zoning By-law Amendment

Bill No. (number to be inserted by Clerk's Office)  
2024

By-law No. Z.-1-

A by-law to amend By-law No. Z.-1 to rezone an area of land located at 763-773 Dundas Street.

WHEREAS this amendment to the Zoning By-law Z.-1 conforms to the Official Plan;

THEREFORE, the Municipal Council of The Corporation of the City of London enacts as follows:

1. Schedule "A" to By-law No. Z.-1 is amended by changing the zoning applicable to lands located at 763-773 Dundas Street, as shown on the attached map **FROM** a Business District Commercial Special Provision (BDC(19)\*D250\*H46) Zone **TO** a Holding Business District Commercial Special Provision (h-18\*BDC(\_)\*D550\*H82) Zone.

2. Section Number 25.4 of the Business District Commercial Special Provision (BDC(\_)) Zone is amended by adding the following Special Provisions:

BDC(\_) 763-773 Dundas Street

a. Additional Permitted Uses

- a. Institutions
- b. Social Service Establishment

b. Regulations

- a. West Interior Side Yard Depth Abutting a Residential Zone (minimum): 1.5 metres
- b. Front Yard Depth Above the 3<sup>rd</sup> Storey (minimum): 5.0 metres
- c. Rear Yard Depth (minimum): 0.0 metres
- d. Rear Yard Depth Above the 1<sup>st</sup> Storey (minimum): 6.0 metres
- e. Rear Yard Depth Above the 2<sup>nd</sup> Storey (minimum): 9.0 metres
- f. Tower Floorplate (maximum) – 1,000 square metres
- g. Height (maximum): 82 metres
- h. Tower Setback – East and South (minimum): 11.0 metres
- i. Tower Setback – West (minimum): 15.0 metres
- j. Density (maximum): 550 units per hectare

3. This Amendment shall come into effect in accordance with Section 34 of the *Planning Act, R.S.O. 1990, c. P13*, either upon the date of the passage of this by-law or as otherwise provided by the said section.

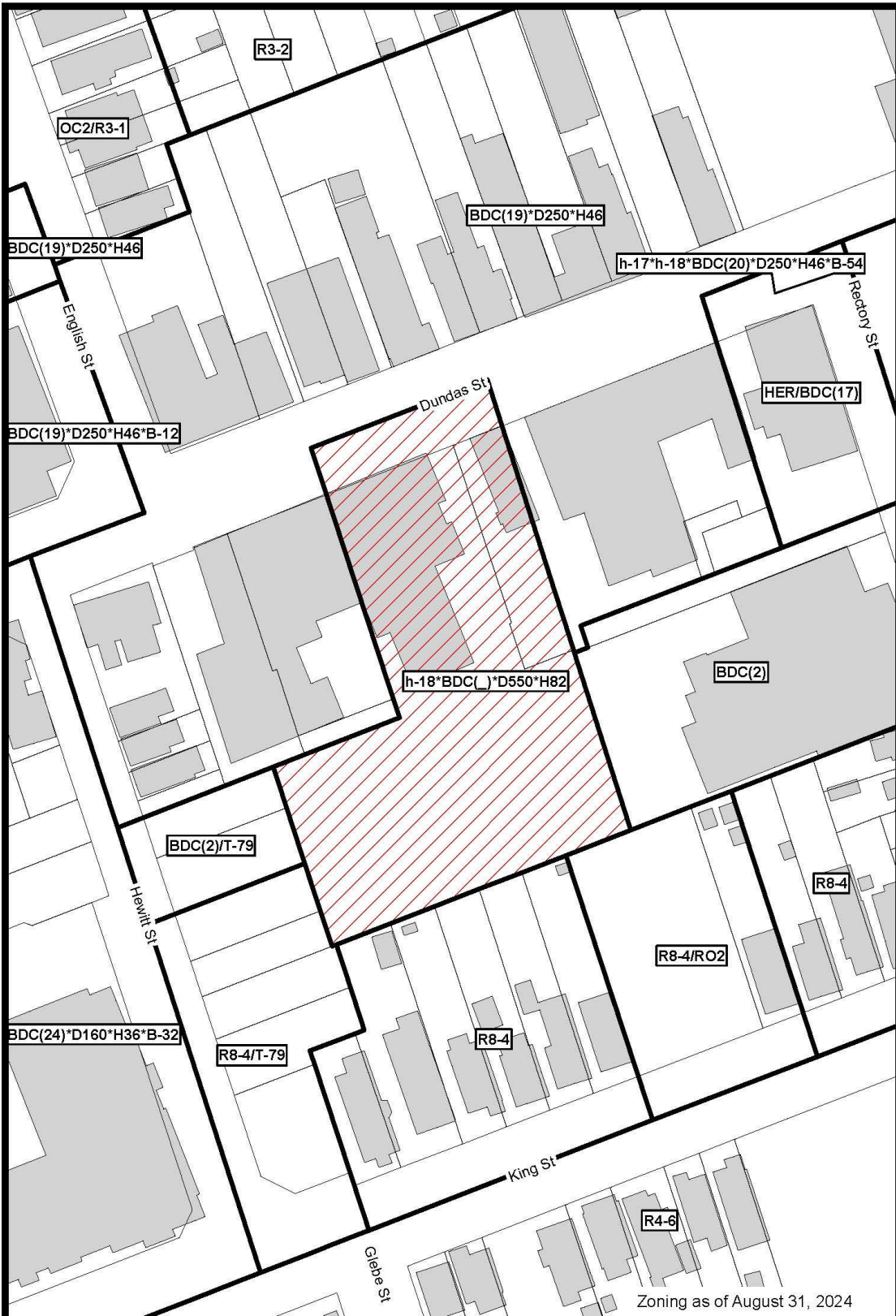
PASSED in Open Council on November 5, 2024, subject to the provisions of PART VI.1 of the *Municipal Act, 2001*.

Josh Morgan  
Mayor

Michael Schulthess  
City Clerk

First Reading – November 5, 2024  
Second Reading – November 5, 2024  
Third Reading – November 5, 2024

AMENDMENT TO SCHEDULE "A" (BY-LAW NO. Z.-1)



Zoning as of August 31, 2024

File Number: Z-9777

Planner: MH

Date Prepared: 2024/10/01

Technician: JI

By-Law No: Z.-1-

SUBJECT SITE 

1:1,000

0 5 10 20 30 40 Meters



Geodatabase

## Appendix B - Site and Development Summary

### A. Site Information and Context

#### Site Statistics

Current Land Use	Two 2-storey active and vacant mixed-use buildings
Frontage	40.3 metres (Dundas Street)
Depth	90.6 metres
Area	0.48 hectares (4,800 square metres)
Shape	Irregular
Within Built Area Boundary	Yes
Within Primary Transit Area	Yes

#### Surrounding Land Uses

North	Business District Commercial/Residential
East	Business District Commercial/Office Residential/Regional Facility
South	Residential/ Restricted Service Commercial
West	Business District Commercial

#### Proximity to Nearest Amenities

Major Intersection	Dundas Street and Adelaide Street North (470 metres)
Dedicated cycling infrastructure	Dundas Street (on site)
London Transit stop	Routes 2/20 East/West Dundas Street
Public open space	Queens Park (360 metres)
Commercial area/use	On site
Food store	FreshCo Trafalgar & Highbury (2,250 metres)
Community/recreation amenity	Cross Cultural Learner Centre (780 metres)

### B. Planning Information and Request

#### Current Planning Information

Current Place Type	Urban Corridor Place Type fronting a Main Street/Civic Boulevard
Current Special Policies	Rapid Transit and Urban Corridor Specific-Segment Policies: Old East Village Specific-Segment
Current Zoning	Business District Commercial (BDC(19)*D250*H46) Zone

#### Requested Designation and Zone

Requested Place Type	N/A
Requested Special Policies	N/A
Requested Zoning	Holding Business District Commercial Special Provision (h-18*BDC(_)*D550*H82) Zone

#### Requested Special Provisions

Regulation (BDC(_)) Zone)	Required	Proposed	Recommended
Additional Permitted Uses	N/A	<ul style="list-style-type: none"> <li>Institutions</li> <li>Social Service Establishment</li> </ul>	As requested
Stepback Above the 3rd Storey – Street Facing Façade	N/A	N/A	5.0 metres



Regulation (BDC( ) Zone)	Required	Proposed	Recommended
(minimum)			
West Interior Side Yard Depth Abutting a Residential Zone (minimum):	32.0 metres	1.8 metres	1.5 metres
Rear Yard Depth (minimum)	32.0 metres	0.0 metres	As requested
Rear Yard Depth Above the 1st Storey (minimum)	32.0 metres	N/A	6.0 metres
Rear Yard Depth Above the 2nd Storey (minimum)	32.0 metres	N/A	9.0 metres
Tower Floorplate (maximum)	N/A	N/A	1,000 square metres
Tower Setback - East and South (minimum)	N/A	N/A	11.0 metres
Tower Setback – West (minimum)	N/A	N/A	15.0 metres
Height (maximum)	46 metres	82 metres	As requested
Density (maximum)	250 uph	550 uph	As requested

## C. Development Proposal Summary

### Development Overview

The recommended action will permit a 6-storey mixed-use building fronting Dundas Street, consisting of 34 residential units and approximately 300m<sup>2</sup> of gross-floor commercial space; and a 24-storey mixed-use building located to the rear, consisting of 213 units and ground-floor multi-use space at a density of 550 units per hectare. A total of 247 residential units with a minimum of 40 percent affordable are proposed.

### Proposal Statistics

Land use	Mixed-use
Form	Two (2) Apartment Buildings
Height	6-storeys and 24-storeys
New Residential units	247 units
Density	550 units per hectare
Building coverage	50.7%
Landscape open space	24%
New use being added to the local community	Yes

### Mobility

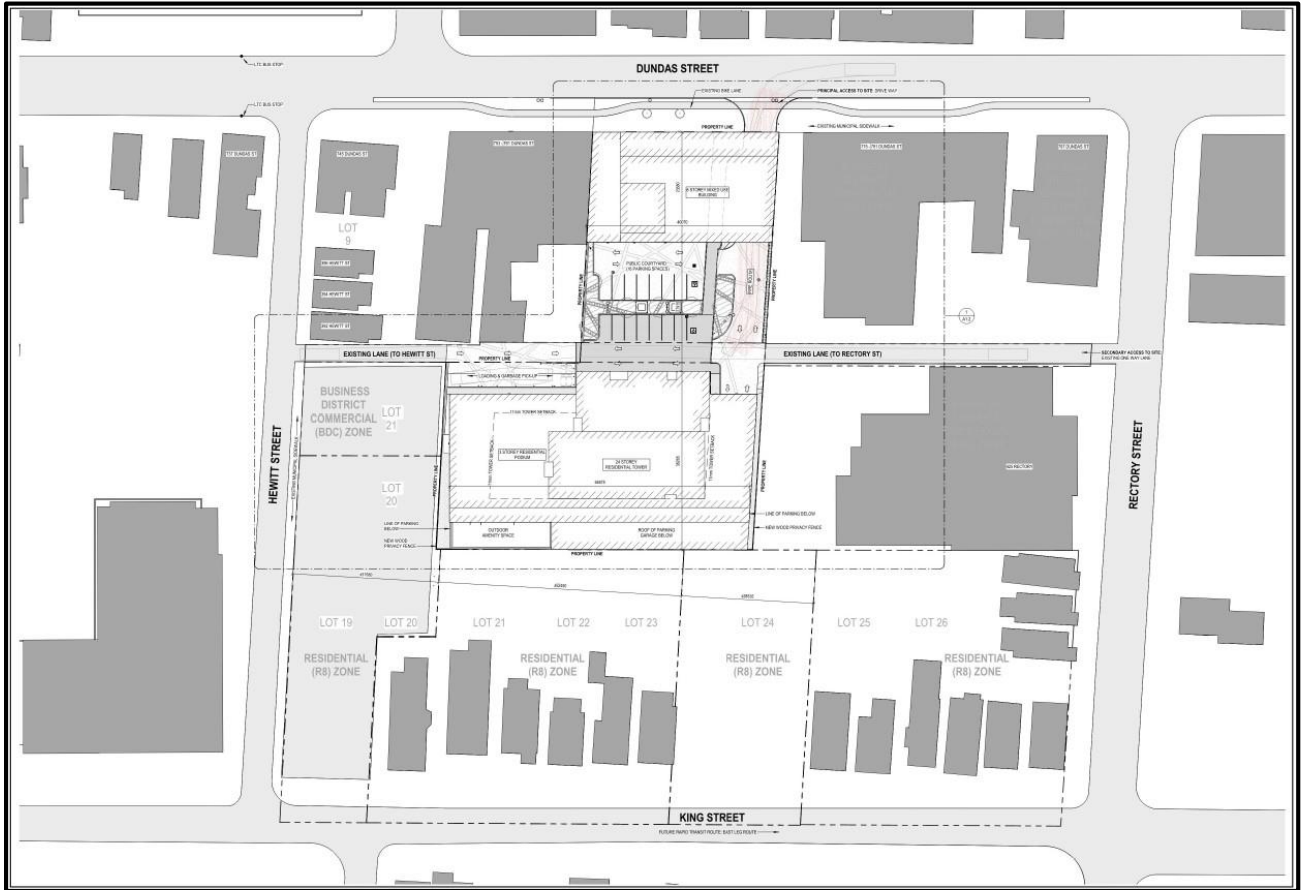
Parking spaces	85 parking spaces (16 at grade and 69 underground)
Vehicle parking ratio	0.4 space/unit
New electric vehicles charging stations	Unknown
Secured bike parking spaces	Yes
Secured bike parking ratio	1.0/unit
Completes gaps in the public sidewalk	N/A
Connection from the site to a public sidewalk	Yes
Connection from the site to a multi-use path	No

## Environment

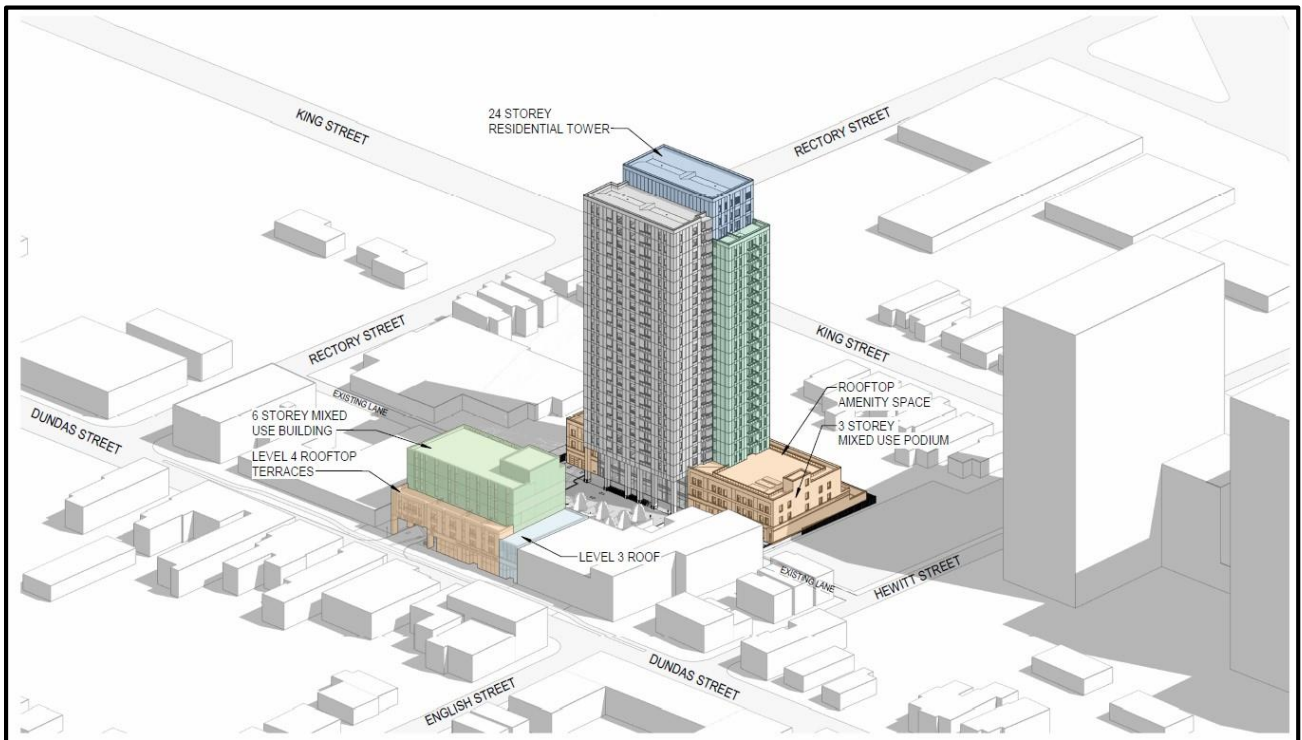
Tree removals	No
Tree plantings	Yes
Tree Protection Area	No
Loss of natural heritage features	No
Species at Risk Habitat loss	No
Minimum Environmental Management Guideline buffer met	N/A
Existing structures repurposed or reused	Yes
Green building features	Unknown

# Appendix C – Additional Plans and Drawings

## Conceptual Site Plan



## Massing Model



## Appendix D – Internal and Agency Comments

### UTRCA

- The subject lands **are not** affected by any regulations (Ontario Regulation 41/24) made pursuant to Section 28 of the *Conversation Authorities Act*.
- The UTRCA has **no objections** to the application, and we have no Section 28 approval requirements.

### Site Plan

#### Major Issues

- Provide second paratransit lay-by for south tower or relocate proposed paratransit lay-by to a central location with a protected waiting area.

#### Matters for Site Plan

- Provide raised pedestrian walkway from Dundas Street to south tower.
- Ensure all accessible parking spaces comply with Figure 7.1 or Figure 7.2 of the Site Plan Control By-law.
- Locate all underground accessible parking spaces close to elevators and barrier-free entrances.
- Ensure barrier-free access from underground parking areas to elevators.
- Label all main entrances for both buildings. Include barrier-free descriptor where applicable.
- Provide measurements from property lines to buildings.
- Indicate direction of traffic.

### CN Rail

- Thank you for consulting CN on the application mentioned in subject. It is noted that the subject site is located within 1000 meters to CN's rail yard facilities. CN has concerns of developing/densifying residential uses in proximity to railway operations. Development of sensitive uses in proximity to railway operations cultivates an environment in which land use incompatibility issues are exacerbated. The Guidelines for New Development in Proximity to Railway Operations reinforce the safety and well-being of any existing and future occupants of the area. Please refer to these guidelines for the development of sensitive uses in proximity to railway operations. These policies have been developed by the Railway Association of Canada and the Federation of Canadian Municipalities. CN encourages the municipality to pursue the implementation of the following criteria as conditions of an eventual project approval:
  - The Owner shall engage a consultant to undertake an analysis of noise. Subject to the review of the noise report, the CN may consider other measures recommended by an approved Noise Consultant.
  - The following clause should be inserted in all development agreements, offers to purchase, and agreements of Purchase and Sale or Lease of each dwelling unit within 1000m of the railway right-of-way:
    - "Warning: Canadian National Railway Company or its assigns or successors in interest has or have a rights-of-way within 1000 metres from the land the subject hereof. There may be alterations to or expansions of the railway facilities on such rights-of-way in the future including the possibility that the railway or its assigns or successors as aforesaid may expand its operations, which expansion may affect the living environment of the residents in the vicinity, notwithstanding the inclusion of any noise and vibration attenuating measures in the design of the development and individual dwelling(s). CNR will not be responsible for any complaints or claims arising from use of such facilities and/or

- operations on, over or under the aforesaid rights-of-way.”
  - The Owner shall be required to grant CN an environmental easement for operational noise and vibration emissions, registered against the subject property in favour of CN.
- CN anticipates the opportunity to review a noise study taking into consideration the Guidelines for New Development in Proximity to Railway Operations developed by the Federation of Canadian Municipalities and the Railway Association of Canada.
- We request that CN rail and the proximity@cn.ca email be circulated on any and all public notices and notice of decisions with respect to this and future land use planning applications with respect to the subject site.

## Urban Design

- The proposed development is located within the Old East Village Core of the Old East Village Dundas Corridor Secondary Plan, the Urban Corridor Place Type in The London Plan along a Main Street. Urban Design is generally supportive of the development proposed at this location. Please see below for specific Urban Design comments.
- The following site layout and built form features have been acknowledged and should be carried forward:
  - Street orientation and the continuation of the street wall with ground floor active uses for the building located on Dundas Street.
  - A 5m stepback above the 3<sup>rd</sup> storey for the building located on Dundas Street.
  - Active uses fronting the internal courtyard/shared outdoor amenity space for the ground floor of the 24-storey tower.

## Matters for ZBA

- High Rise Building (tower):
  - High-rise buildings should be designed with slender towers that reduce shadow impact, minimize the obstruction of views, and are less massive to neighbouring properties. Provide a **15-metre** setback to the tower from interior property lines to give consideration for future development on adjacent properties. Refer to the Old East Village Dundas Street Corridor Secondary Plan, Policy 3.3.4.g).
  - Ensure the tower is designed to reduce shadow impact, minimize obstruction of views, and are less massive to neighbouring properties. Provide confirmation that the proposed tower is **1,000 square metres** or less. Refer to the Old East Village Dundas Street Corridor Secondary Plan, Policy 3.3.4.c).

## Matters for Site Plan

- Site Layout:
  - Explore opportunities to create a midblock connection to King Street to directly connect to future rapid transit. Refer to The London Plan, Policy 255.
  - Ensure a safe and comfortable pedestrian connection from the City sidewalk on Dundas Street and Hewitt Street to the main entrance to the 24-storey tower. Refer to The London Plan, Policy 255.
- High Rise Building (podium):
  - Differentiate the main residential entrance to the 24-storey tower building through an increased proportion of glazing and wayfinding elements such as signage, and canopies/awnings to increase pedestrian safety and security. Refer to The London Plan, Policy 291.
- Overall Building Design:
  - All development fronting onto Dundas Street should be consistent with the Old East village Commercial Corridor Urban Design Manual to coordinate the façade and built form with the existing character of Dundas Street

## **Ecology**

- This e-mail is to confirm that there are currently no ecological planning issues related to this property and/or associated study requirements.

## **Heritage**

- Heritage staff have received and reviewed the following reports submitted as a part of Z-9777:
  - Zelinka Priamo Ltd., Heritage Impact Assessment, 763-773 Dundas Street, August 22, 2024
  - TMHC Inc., Stage 1 Archaeological Assessment, 773 Dundas Street (P324-0910-2024) January 22, 2024
  - TMHC Inc., Stage 1-2 Archaeological Assessment, 763-769 Dundas Street (P324-0694-2021 and P324-0704-2021) December 6, 2021

## **Heritage Impact Assessment**

- Heritage staff have reviewed and agree with the findings and conclusions of the Heritage Impact Assessment. The HIA identifies the design strategies including the use of a continued street wall, step-backs and appropriate materials that mitigate the potential impacts of the proposed development on the adjacent heritage-listed properties. Please note, as a part of the proposed development the building at 763-769 Dundas Street will require demolition. The building is directly connected to the adjacent heritage-listed property at 761 Dundas Street. Staff will be seeking a demonstration through the Site Plan process that appropriate site plan controls and demolition/construction activities will not directly impact the adjacent property at 761 Dundas Street.

## **Archaeological Assessment**

- Staff have reviewed the archaeological assessments submitted as a part of this application. Please note, the City has not yet received the Ministry's acceptance letters for these assessment reports. In order to accept these reports and clear the properties of their archaeological potential, the applicant must also submit the Ministry's review and acceptance letter. Heritage staff recommend that the h-18 holding provision be applied until the archaeological matters have been satisfied.

## **Additional Notes**

- Please note, the properties at 763-769 Dundas Street were previously removed from the Register of Cultural Heritage Resources in 2023. The property at 773 Dundas Street is currently subject to a demolition request. A Public Participation Meeting was held at PEC on August 26, 2024. The demolition request will be included on the PEC Report to Council at Council's meeting scheduled for September 24, 2024.
- Please also note, following the demolition request process for 763-769 Dundas Street the property owner for the heritage-listed property at 761 Dundas Street contacted heritage staff to express concerns about potential demolition impacts to his property. At that time we did not have an active planning application, but I advised him that he would have an opportunity to comment during the ZBA process.

## **Landscape Architecture**

- No comments.

## **Engineering**

- Engineering has no further comments at this time regarding this application.

## **Matter for Site Plan**

- All engineering related comments provided during SPC 23-130 are still Applicable.
- Additional comments below are to be addressed during the site plan application Stage.

### *Stormwater*

- For the benefit of the project, please ensure the applicant is informed about the additional SWM issues/requirements to be considered by the applicant's consultant engineer when preparing the storm servicing strategy for this land during the Site Plan Application stage:
- As stated in response to SPC23-130, although the proposed development does not contain 29 or more at grade parking spaces, the site is subject to Case 4 of the PPS (CofL DSRM 6.9) which states the on-site private stormwater system must provide 100% of the quality control for the lands to be developed (normal, 70% TSS removal), as there are no downstream quality controls in place. The consultant is to address stormwater quality control as part of the stormwater servicing strategy for the site.
- Grading and Disposal of Storm, Surface and Wastewater shall be in accordance with Section 12 of the Site Plan Control By-Law (SPCB). A grading plan is to be designed by a Professional Engineer and certified by a designated professional, and include the following considerations:
  - The sites grading and drainage shall conform to the overall drainage pattern of the adjacent lands
  - Storm water is contained within the site to the greatest extent practical, and directed to an internal storm drainage system, thereby generally preventing drainage onto adjacent properties.
  - No surface ponding should develop under a 5-year design storm event.
  - The major storm event for the development shall be safely conveyed via overland flow route to Dundas Street, to the greatest extent practical.
  - The proposed shall comply with riparian right (common) law.

### *Wastewater*

- The subject lands are allocated 430uph equivalent to approximately 181units, with the proposed for 247units (+66u) from the drainage area plan. SED previously reviewed a capacity analysis based on 235units and was amicable to the increase as it was roughly 1L/s and there is surplus capacity in the system.
- The applicant's engineer is to provide the updated maximum population and peak flow based on the increase in units and mixed use and provide inspection manhole entirely on private property but as close to the street line as possible as part of the site plan application drawings.

### **Parks Long Range Planning and Design**

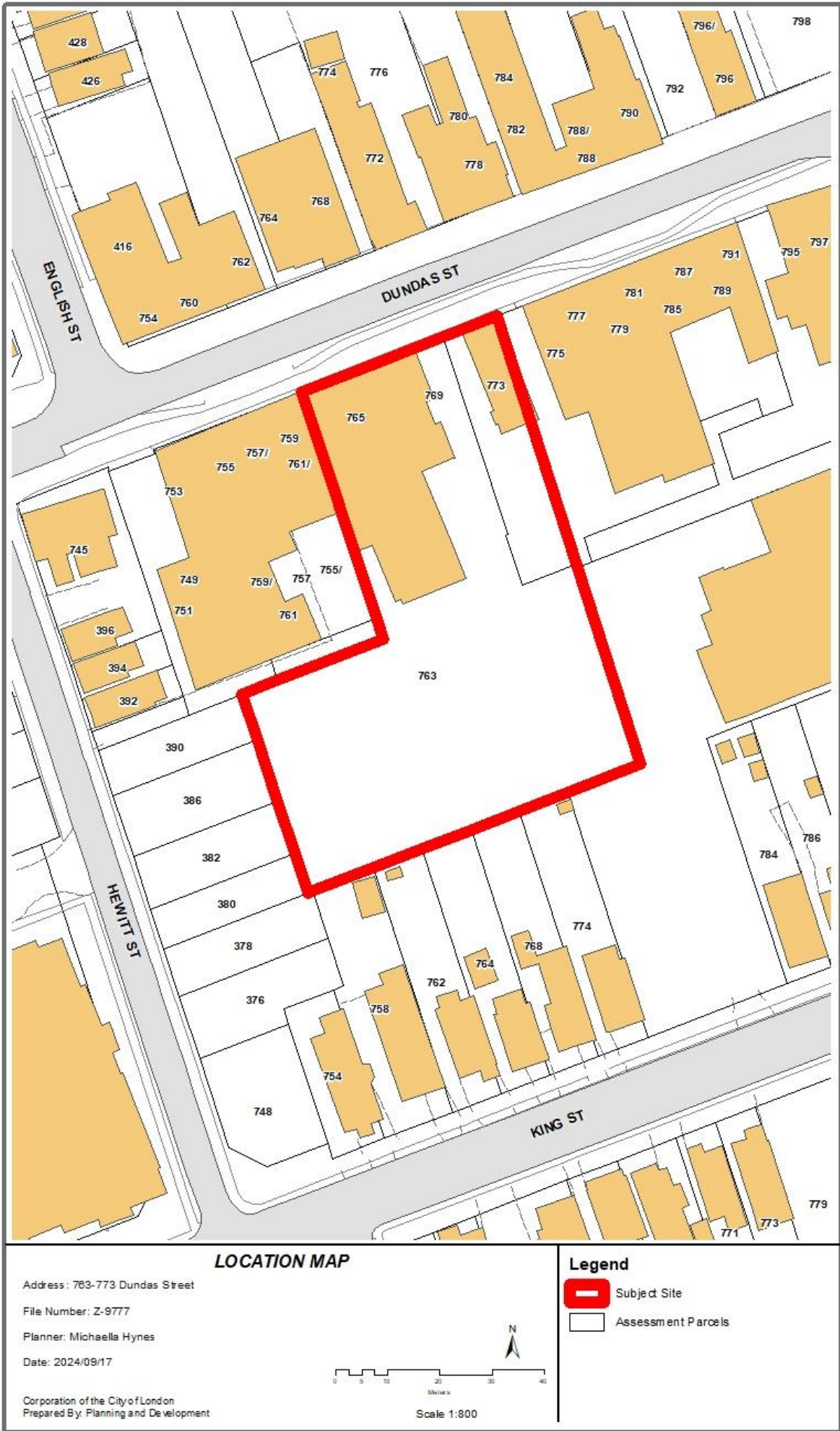
#### *Matters for Site Plan*

- Parkland dedication has not been taken for this site. It is to be noted that the applicant, as a condition of site plan approval, will be required to provide parkland dedication in the form of cash-in-lieu pursuant to By-law CP-25.

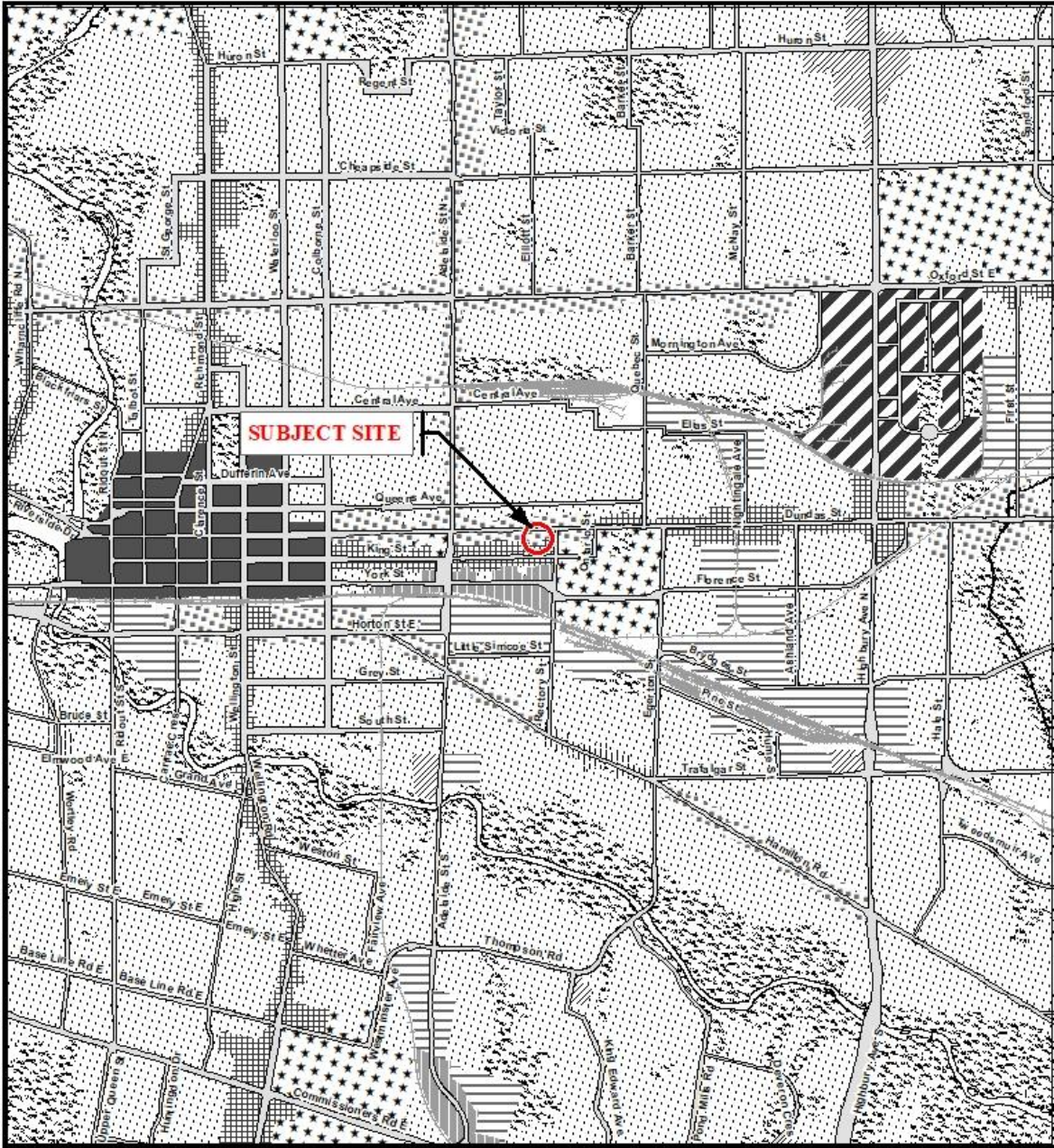
### **London Hydro**

- Servicing the above proposal should present no foreseeable problems. Any new and/or relocation of existing infrastructure will be at the applicant's expense, maintaining safe clearance from L.H. infrastructure is mandatory. Note: Transformation lead times are minimum 16 weeks. Contact the Engineering Dept. to confirm requirements & availability.
- London Hydro has no objection to this proposal or possible official plan and/or zoning amendment. Any new or relocation of the existing service will be at the expense of the owner.

# Appendix F – Relevant Background







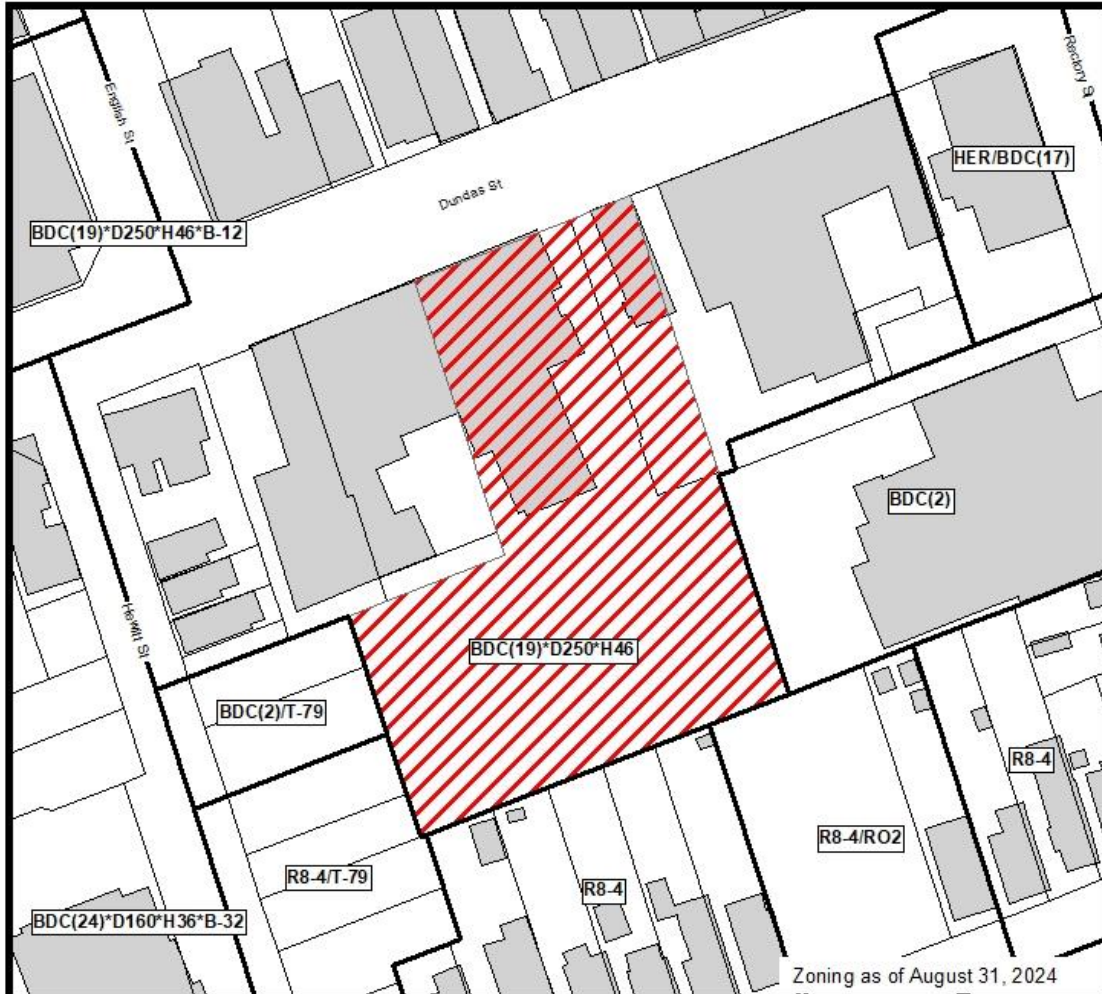
**Legend**

- |                        |                          |   |
|------------------------|--------------------------|---|
| Downtown               | Future Community Growth  | Environmental Review                    |
| Transit Village        | Heavy Industrial         | Farmland                                |
| Shopping Area          | Light Industrial         | Rural Neighbourhood                     |
| Rapid Transit Corridor | Future Industrial Growth | Waste Management Resource Recovery Area |
| Urban Corridor         | Commercial Industrial    | Urban Growth Boundary                   |
| Main Street            | Institutional            |   |
| Neighbourhood          | Green Space              |   |

*This is an excerpt from Planning & Development's working consolidation of Map 1 - Place Types of the London Plan, with added notations.*

<p><b>CITY OF LONDON</b>  <b>Official Plan</b>  <b>LONDON PLAN MAP 1</b>  <b>- PLACE TYPES -</b>          PREPARED BY: Planning &amp; Development</p>	 Scale 1:30,000 Meters	<p><b>File Number:</b> Z-9777  <b>Planner:</b> MH  <b>Technician:</b> JI  <b>Date:</b> 2024/09/17</p>
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Project Location: E:\Planning\Projects\lp\_officialplan\work\consolid\0\excerpts\_LondonPlan\mxd\Z-9777\_Map1\_PlaceTypes.mxd



**COUNCIL APPROVED ZONING FOR THE SUBJECT SITE: BDC(19)\*D250\*H46**

1) **LEGEND FOR ZONING BY-LAW Z-1**

- |   |                                   |
|---|-----------------------------------|
| R1 - SINGLE DETACHED DWELLINGS            | RF - REGIONAL FACILITY            |
| R2 - SINGLE AND TWO UNIT DWELLINGS        | CF - COMMUNITY FACILITY           |
| R3 - SINGLE TO FOUR UNIT DWELLINGS        | NF - NEIGHBOURHOOD FACILITY       |
| R4 - STREET TOWNHOUSE                     | HER - HERITAGE                    |
| R5 - CLUSTER TOWNHOUSE                    | DC - DAY CARE                     |
| R6 - CLUSTER HOUSING ALL FORMS            | OS - OPEN SPACE                   |
| R7 - SENIOR'S HOUSING                     | CR - COMMERCIAL RECREATION        |
| R8 - MEDIUM DENSITY/LOW RISE APTS.        | ER - ENVIRONMENTAL REVIEW         |
| R9 - MEDIUM TO HIGH DENSITY APTS.         |                                   |
| R10 - HIGH DENSITY APARTMENTS             | OB - OFFICE BUSINESS PARK         |
| R11 - LODGING HOUSE                       | LI - LIGHT INDUSTRIAL             |
| DA - DOWNTOWN AREA                        | GI - GENERAL INDUSTRIAL           |
| RSA - REGIONAL SHOPPING AREA              | HI - HEAVY INDUSTRIAL             |
| CSA - COMMUNITY SHOPPING AREA             | EX - RESOURCE EXTRACTIVE          |
| NSA - NEIGHBOURHOOD SHOPPING AREA         | UR - URBAN RESERVE                |
| BDC - BUSINESS DISTRICT COMMERCIAL        |                                   |
| AC - ARTERIAL COMMERCIAL                  | AG - AGRICULTURAL                 |
| HS - HIGHWAY SERVICE COMMERCIAL           | AGC - AGRICULTURAL COMMERCIAL     |
| RSC - RESTRICTED SERVICE COMMERCIAL       | RRC - RURAL SETTLEMENT COMMERCIAL |
| CC - CONVENIENCE COMMERCIAL               | TGS - TEMPORARY GARDEN SUITE      |
| SS - AUTOMOBILE SERVICE STATION           | RT - RAIL TRANSPORTATION          |
| ASA - ASSOCIATED SHOPPING AREA COMMERCIAL |                                   |
| OR - OFFICE/RESIDENTIAL                   | "h" - HOLDING SYMBOL              |
| OC - OFFICE CONVERSION                    | "D" - DENSITY SYMBOL              |
| RO - RESTRICTED OFFICE                    | "H" - HEIGHT SYMBOL               |
| OF - OFFICE                               | "B" - BONUS SYMBOL                |
|   | "T" - TEMPORARY USE SYMBOL        |

**CITY OF LONDON**

PLANNING SERVICES / DEVELOPMENT SERVICES

**ZONING  
BY-LAW NO. Z.-1  
SCHEDULE A**



THIS MAP IS AN UNOFFICIAL EXTRACT FROM THE ZONING BY-LAW WITH ADDED NOTATIONS

FILE NO:

Z-9777

MH

MAP PREPARED:

2024/09/17

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## Report to Planning and Environment Committee

**To:** Chair and Members  
Planning and Environment Committee  
**From:** Scott Mathers, MPA, P.Eng.  
Deputy City Manager, Planning and Economic Development  
**Subject:** Encore at Upper Richmond Inc.  
2118 Richmond Street  
File Number: OZ-9770, Ward 5  
**Date:** September 10, 2024

## Recommendation

That, on the recommendation of the Director, Planning and Development, the following actions be taken with respect to the application of Encore at Upper Richmond Inc. relating to the property located at 2118 Richmond Street:

- (a) the proposed by-law attached hereto as Appendix "A" **BE INTRODUCED** at the Municipal Council meeting on November 5, 2024, to amend the Official Plan, The London Plan, to amend specific policy 1069 of the Neighbourhoods Place Type;
- (b) the proposed by-law attached hereto as Appendix "B" **BE INTRODUCED** at the Municipal Council meeting on November 5, 2024, to amend Zoning By-law No. Z.-1, in conformity with the Official Plan, The London Plan, as amended in part (a) above, to change the zoning of the subject property **FROM** a Holding Residential R6/Residential R8 Bonus (h\*5-h-11\*h-183\*R6-5/R8-4\*B-30) Zone, **TO** a Holding Residential R9 Special Provision (h-183\*R9-7(\_)\*H40\*D211) Zone, and to delete the B-30 Bonus Zone in its entirety;
- (c) The Site Plan Approval Authority **BE REQUESTED** to consider the following design issues through the site plan process:
  - i) Incorporation of underground parking;
  - ii) Street oriented design and safe and accessible pedestrian connections;
  - iii) Enhanced landscaping to include a minimum 50% native species, with no invasive species planted;
  - iv) Nine mature trees (minimum diameter at breast height (dbh) of 11cm) be incorporated as part of the landscaping plan between the south building face and Sunningdale Road East;
  - v) Investigate renewable sources of energy such as solar for the roof and sides of the building, and geothermal for interior heating and cooling;
  - vi) Investigate air source heat pump options; and,
  - vii) Apply bird friendly policies using the CSA standard.
- (d) pursuant to Section 34(17) of the *Planning Act*, as determined by the Municipal Council, no further notice **BE GIVEN** in respect of the proposed by-law as changes made to the application are technical in nature;

**IT BEING NOTED**, that the above noted amendments are being recommended for the following reasons:

- i) The recommended amendment is consistent with the *Provincial Planning Statement 2024*;
- ii) The recommended amendment conforms to *The London Plan*;
- iii) The recommended amendment will permit development that is considered appropriate and compatible with the existing and future land uses surrounding the subject lands.

## Executive Summary

### Summary of Request

The Applicant has requested an amendment to The London Plan to amend specific policy 1069 in the Neighbourhoods Place Type to: remove the Bonus Zone Provisions;

increase the permitted residential density from 123 units per hectare to 211 units per hectare, to allow for a total of 158 residential dwelling units; and, to remove the required three (3) storey townhouses along the base of the building.

The Applicant has requested an amendment to the Zoning By-law Z.-1 to rezone the property from a Holding Residential R6/Residential R8 Bonus (h\*h-5\*h-11\*h-183\*R6-5/R8-4\*B-30) Zone, to a Residential R9 Special Provision (R9-7(□)) Zone.

### **Purpose and the Effect of Recommended Action**

The recommended action will permit the development of an eleven (11) storey residential apartment building with a total of 158 units, an increase of **51 units** from the previously approved development.

Staff are recommending approval of the requested Official Plan Amendment and Zoning By-law Amendment with special provisions to permit increased height, increased density, reduced setbacks, reduced lot coverage, and increased landscaped open space.

Staff are recommending approval with retention of the existing h-183 holding provision, which will ensure the development will not occur until such time as a Hydrogeological Study has been prepared and submitted, and that the development will not have any negative impacts on the groundwater in the area.

### **Linkage to the Corporate Strategic Plan**

This recommendation will contribute to the advancement of Municipal Council's 2023-2027 Strategic Plan in the following ways:

- **Housing and Homelessness**, by ensuring London's growth and development is well-planned and considers use, intensity, and form; and,
- **Wellbeing and Safety**, by promoting neighbourhood planning and design that creates safe, accessible, diverse, walkable, healthy, and connected communities.

### **Analysis**

#### **1.0 Background Information**

##### **1.1 Previous Reports Related to this Matter**

**O-6284-** On June 16, 2003, Planning and Environment Committee - Official Plan Amendments in accordance with the *Uplands North Area Plan*.

**OZ-7602-** On February 3, 2009, Planning and Environment Committee – Recommending refusal of amendments to the Official Plan and Zoning By-law Z.-1 to permit retail/pharmacy and office uses.

**PL090268-** On October 26, 2009, the Ontario Municipal Board dismissed the appeal by the Applicant, confirming Council's decision to refuse retail/pharmacy and office uses.

**OZ-7890-** On June 13, 2011, Planning and Environment Committee – Amendments to the Official Plan and Zoning By-law to allow for a 260-unit, fifteen (15) storey tall (50 metres) apartment building. At the public participation meeting, the Applicant presented an alternative proposal which would permit the construction of a 113-unit, twelve (12) storey tall (40 metres) apartment building and an eight (8) unit, thirteen (13) metres tall townhouse building.

**OZ-7890** – On February 27, 2012, Planning and Environment Committee – Amendments to the Official Plan and Zoning By-law to allow for a 113-unit, twelve (12) storey (40 metres) apartment building and an eight (8) unit, thirteen (13) metre tall townhouse building.

## 1.2 Planning History

In February of 2002, landowners in the area initiated the *Uplands North Area Plan* for the lands bounded by Richmond Street to the west, Adelaide Street to the east, Sunningdale Road to the south and the municipal boundary to the north. The *Area Plan* was intended to serve as a guideline for future development of the area and to address issues including: mix of dwelling types, road configuration, school locations, servicing and the location of parkland. The subject lands were designated Multi-family, Medium Density residential under the *Uplands North Area Plan*.

In 2008, an application was made for an Official Plan and Zoning By-law Amendment to permit retail uses and pharmacy offices. Staff recommended approval of the requested amendments, but Council refused the Application on February 2, 2009. This decision was appealed to the Ontario Municipal Board, who upheld the decision of Council.

A new application was submitted in 2011, requesting to amend the Official Plan and the Zoning By-law to permit a fifteen (15) storey (50 metres) apartment building containing 260 units (300 units per hectare). A public participation meeting was held on June 13, 2011, where the Applicant proposed an alternative proposal for a twelve (12) storey, (40 metre) apartment building with 113 units. Council referred this proposal back to Staff for review through the planning and public consultation process. Another public meeting was held on February 27, 2021, following review and consultation of the twelve (12) storey proposal. Council directed Civic Administration and Staff to meet with the Applicant to request further revisions to the development form and site layout, as well as prepare a traffic study for the area of Richmond Street and Sunningdale Road and widen Sunningdale Road.

Meetings were held with City Staff and the Applicant in April of 2013. It was agreed to reduce the height to ten (10) storeys and to require that the townhouse component be constructed at the same time as the apartment component to ensure both buildings are constructed over, and can share, the underground parking. A revised application was submitted in February of 2014 and accepted as complete in June of 2014 following the provision of additional information. On July 22, 2014, a public meeting was held for the Revised Application. Staff recommended approval of amendments to the Official Plan to add a Specific Area Policy and to change the zoning from Urban Reserve UR4 to Holding Residential R6/Residential R8 Bonus (h•h-5•h-11•R6-5/R8-4•B-30) Zone. The Official Plan and Zoning By-law Amendments allowed for a ten (10) storey apartment building containing 107 units (123 units per hectare) with a three (3) storey townhouse component integrated into the base of the building adjacent to Sunningdale Road.

## 1.3 Property Description and Location

The subject property is located at the northeast corner of the intersection of Richmond Street and Sunningdale Road East. The lot is irregular in shape and approximately 0.75 hectares (1.85 acres) in area. The site has approximately 62 metres of frontage on Sunningdale Road and is approximately 90 metres deep. The lands are currently vacant and are accessed by a gravel driveway from Richmond Street. An Imperial Oil pipeline is located along the frontage of Sunningdale Road and has an associated easement over the front portion of the lands, some of which have been dedicated for road widening. All new buildings on the subject lands are required to be setback a minimum of 20 metres from the centreline of this pipeline.

### Site Statistics:

- Current Land Use: Vacant
- Frontage: 62 metres on Sunningdale road
- Depth: 90 metres
- Area: 0.75 hectares
- Shape: Irregular
- Located within the Built Area Boundary: No
- Located within the Primary Transit Area: No

### Surrounding Land Uses:

- North: Vacant agricultural lands planned for future residential development

- East: Vacant agricultural lands planned for future residential development
- South: Low Density Residential
- West: Commercial and Medium Density Residential

**Existing Planning Information:**

The London Plan Place Type: Neighbourhoods Place Type on an Urban Thoroughfare (Richmond Street) and a Civic Boulevard (Sunningdale Road East)

Existing Special Policies: Specific Policy Area 16 (Policy 1069 of *The London Plan*)

Existing Zoning: Holding Residential R6/Residential R8 Bonus (h\*h-5\*h-11\*h-183\*R6-5/R8-4\*B-30) Zone

Additional site information and context is provided in Appendix “C”.



Figure 1- Aerial Photo of 2118 Richmond Street and surrounding lands



Figure 2 - Streetview of 2118 Richmond Street (view looking east)

**2.0 Discussion and Considerations**

**2.1 Development Proposal**

The requested Official Plan and Zoning By-law Amendments would facilitate the development of an eleven (11) storey apartment building, in a “L” shaped configuration,

comprised of a three (3) storey base and an eight (8) storey tower with stepbacks incorporated above the third and eighth storeys. A total of 158 units are proposed, an increase of 51 units from the previously approved development.

The proposal includes outdoor common amenity space on the east side of the site, as well as two internal amenity areas. An amenity room and lounge are included on the ground floor, and rooftop indoor and outdoor community amenity areas are proposed for the eleventh floor. Access is proposed to be provided from a right-in/right-out driveway on Richmond Street, located at the rear of the building. The majority of parking will be provided in two levels of underground parking and surface parking is provided towards to rear of the site for visitor and accessible parking spaces. Walkways are proposed throughout the site to connect building and unit entrances to the public sidewalk and enhanced landscaping is proposed to provide wayfinding and an aesthetic promenade.

The proposed development includes the following features:

- Land use: Residential
- Form: Apartment building
- Height: 11 storeys (40 m)
- Residential units: 158
- Density: 211 units per hectare
- Building coverage: 25.7%
- Parking spaces: 152 underground / 18 surface
- Bicycle parking spaces: 152 long-term / 16 short-term
- Landscape open space: 58.7 %

Additional information on the development proposal is provided in Appendix “C”.

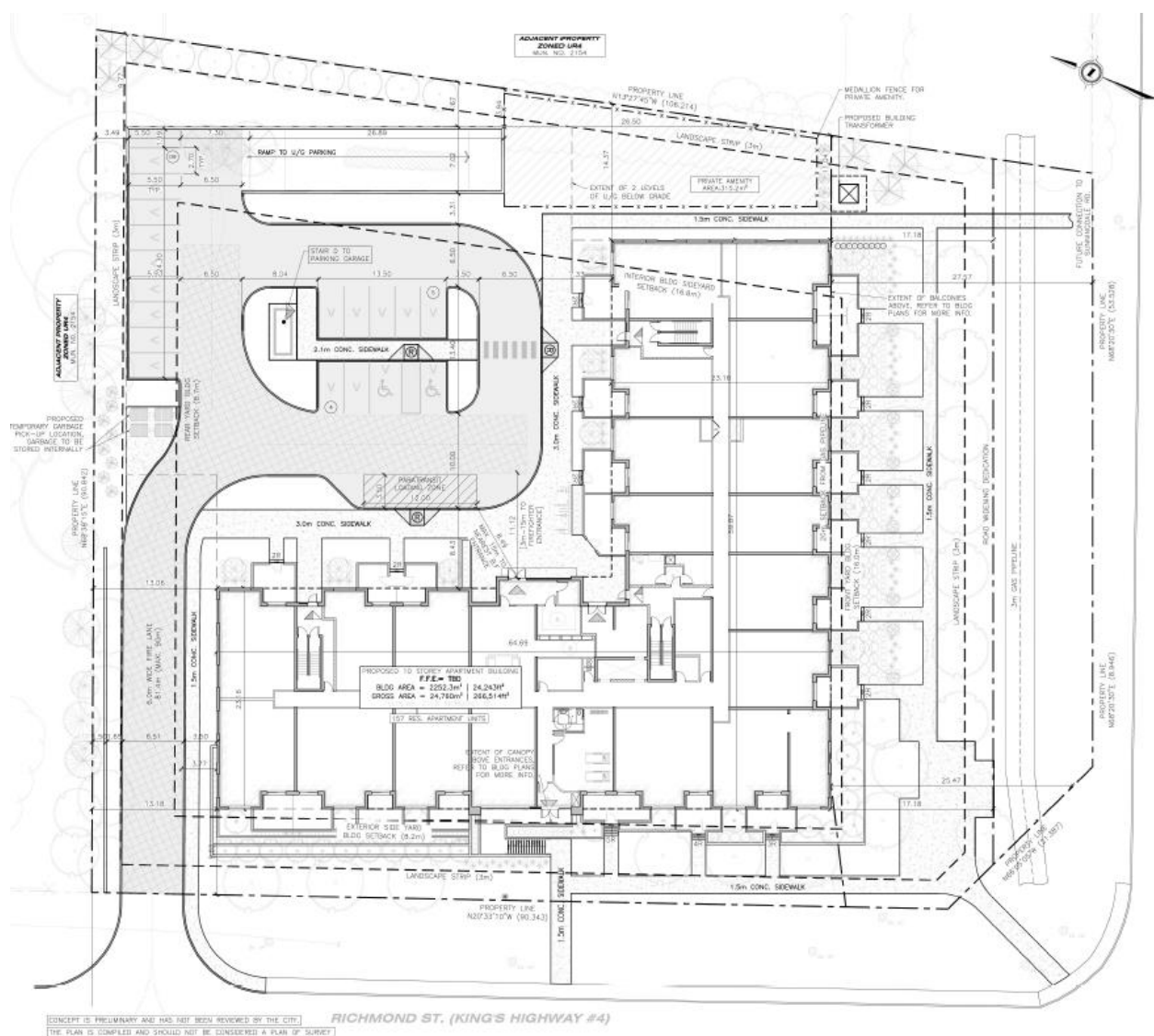


Figure 3 - Conceptual Site Plan (August 2024)



Figure 4 –Rendering looking Northeast (August 2024)

Additional plans and drawings of the development proposal are provided in Appendix “D”.

## 2.2 Requested Amendments

The Applicant has requested to amend specific policy 1069 of the Neighbourhoods Place Type of The London Plan (Specific Area Policy 16 on Map 7) to: remove references to the Bonus Zone provisions; increase the permitted residential density from 123 units per hectare to 211 units per hectare, to allow for a total of 158 residential dwelling units; and, to remove the required three (3) storey townhouses along the base of the building.

The Applicant has requested an amendment to the Zoning Bylaw Z.-1 To change the zoning from a Holding Residential R6/Residential R8 Bonus (h\*h-5\*h-11\*h-183\*R6-5/R8-4\*B-30) to a Residential R9 Special Provision (R9-7(\_)) Zone with special provisions to implement the proposed development.

Road widening dedication was taken along Richmond Street and Sunningdale Road East as part of Site Plan Application SPA19-038, which was submitted to implement the previous proposal for a 10-storey apartment building with 3 storey townhouses integrated into the base of the building. When this Application was submitted and circulated, those dedicated lands were included in the site design and statistics. Section 4.14 of the Z.-1 Zoning By-law permits lands required for road widening dedication to included in site statistic calculations, except where changes are made subsequent to that dedication. The Applicant has revised their requests to reflect the lot, as reduced by road widening dedication, and Staff have recommended that no further notice be required as the changes are technical in nature, and the height and unit count are not changing as a result.

The following table summarizes the special provisions that have been proposed by the Applicant.

<b>Regulation (R9-7)</b>	<b>Required</b>	<b>Proposed</b>
Exterior Site Yard Depth (Minimum)	12 Metres	9 Metres
Interior side Yard Depth (Minimum)	16 Metres	12 Metres
Rear Yard Depth (Minimum)	16 Metres	13 Metres
Landscaped Open Space (Minimum)	30 %	48.4 %
Lot Coverage (Maximum)	40 %	28 %
Height (Maximum)	n/a	40 Metres



<b>Regulation (R9-7)</b>	<b>Required</b>	<b>Proposed</b>
Density (Maximum)	150 units per hectare	211 units per hectare
Building setback from Sunningdale Road East – Above the Third Floor (Minimum)	n/a	21 Metres
Building setback from Sunningdale Road East – Above the Eighth Floor (Minimum)	n/a	29 Metres
Encroachments for Balconies and Canopies	1.5 Metres	Unlimited

### **2.3 Internal and Agency Comments**

The Application and associated materials were circulated for internal comments and public agencies to review. Comments received were considered in the review of this Application and are addressed in Section 4.0 of this report.

Key issues identified by Staff and agencies included:

- This site has an h-183 zoning provision; a Hydrogeological Study shall be prepared by a qualified professional and submitted to the City to evaluate the potential impact of the proposed development to area private wells and provide recommendations for monitoring post construction impacts and possible mitigation measures to the satisfaction of the City Engineer.
- There is currently no municipal sanitary sewer fronting the subject lands. The intended outlet for the site is ultimately tributary to the 450mm diameter on Villagewalk Blvd. The extension is to be at the cost of the developer and consistent in tributary area and populations from accepted area plans. The proposed populations are approximately 3 times over the allocated amount.
- Appropriate setbacks from the Imperial Oil Pipeline.

Detailed internal and agency comments are included in Appendix “E” of this report.

### **2.4 Public Engagement**

On August 21, 2024, Notice of Application was sent to 14 property owners and residents in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on August 29, 2024. A “Planning Application” sign was also placed on the site.

There was one response received during the public consultation period. Comments received were considered in the review of this application and are addressed in Section 4.0 of this report.

Concerns expressed by the public relate to:

- Removal of the townhouses;
- Privacy;
- Shadowing; and,
- Building Design.

Detailed public comments are included in Appendix “F” of this report.

### **2.5 Policy Context**

#### ***The Planning Act and the Provincial Planning Statement, 2024***

The Provincial planning policy framework is established through the *Planning Act* (Section 3) and the *Provincial Planning Statement, 2024 (PPS)*. The *Planning Act* requires that all municipal land use decisions, affecting planning matters, shall be consistent with the *PPS*.

The mechanism for implementing Provincial policies is through the Official Plan, The London Plan. Through the preparation, adoption and subsequent Ontario Land Tribunal (OLT) approval of The London Plan, the City of London has established the local policy framework for the implementation of the Provincial planning policy framework. As such, matters of provincial interest are reviewed and discussed in The London Plan analysis below.

Section 2.1 subsection 6 and Section 2.2 speak to create complete communities by: accommodating an appropriate range and mix of land uses, housing options, transportation options with multimodal access, employment, public service facilities and other institutional uses, recreation, parks and open space, and other uses to meet long-term needs; improving accessibility for people of all ages and abilities by addressing land use barriers which restrict their full participation in society; improving social equity and overall quality of life for people of all ages, abilities, and incomes, including equity-deserving groups, promote densities for new housing which efficiently use land, resources, and infrastructure; and, requiring transit supportive development and promoting active transportation. Section 2.3 notes that settlement areas shall be the focus of growth and development. This contributes to resilient development and the creation of healthy, livable, and safe communities. This proposal will develop a vacant site within the settlement area that will have access to municipal services, as well as provide a range of housing in compact form for current and future residents. The subject lands are designated and intended, over the long term, to be used for multiple-dwelling, low to medium density residential uses.

The compact form and density of the proposal will result in efficient and resilient development, and this will encourage the use of public and active transportation options. This will help to support energy conservation and help to improve air quality, which is consistent with Section 2.9 of the *PPS*. An archaeological study was completed for the subject site and determined there would no impacts to archaeological or cultural resources, which is consistent with Section 4.6 of the *PPS*. The site is also located outside of any natural or man-made hazards, which helps to protecting public health and safety as prioritized in Section 5.0 of the *PPS*.

As the Application for an Official Plan and Zoning By-law Amendments complies with The London Plan, it is staff's opinion that the application is consistent with the *Planning Act* and the *PPS*.

### ***The London Plan, 2016***

The London Plan (TLP) includes evaluation criteria for all planning and development applications with respect to use, intensity and form, as well as with consideration of the following (TLP 1577-1579):

1. Consistency with the Provincial Policy Statement and all applicable legislation.
2. Conformity with the Our City, Our Strategy, City Building, and Environmental policies.
3. Conformity with the Place Type policies.
4. Consideration of applicable guideline documents.
5. The availability of municipal services.
6. Potential impacts on adjacent and nearby properties in the area and the degree to which such impacts can be managed and mitigated.
7. The degree to which the proposal fits within its existing and planned context.

Staff are of the opinion that all the above criteria have been satisfied.

The London Plan also includes conditions for evaluating the appropriateness of Specific Area Policies where the applicable place type policies would not accurately reflect the intent of City Council with respect to a specific site or area (TLP 1729-1734).

The following conditions apply when considering a new Specific Area Policy:

1. The proposal meets all other policies of the Plan beyond those that the specific

policy identifies.

2. The proposed policy does not have an adverse impact on the integrity of the place type policies or other relevant parts of this Plan.
3. The proposed use is sufficiently unique and distinctive such that it does not establish an argument for a similar exception on other properties in the area.
4. The proposed use cannot be reasonably altered to conform to the policies of the place type.
5. The proposed policy is in the public interest and represents good planning.

An existing specific policy applies to the subject lands and staff are of the opinion that the requested amendments to this policy satisfy all of the above conditions.

### **3.0 Financial Impact/Considerations**

None.

### **4.0 Key Issues and Considerations**

#### **4.1 Land Use**

The proposed residential use is supported by the policies of the PPS 2024 and aligns with the vision of the Neighbourhoods Place Type by contributing to a diversity and mix of different housing types, intensities, and forms (TLP 918\_2). The subject lands are in the Neighbourhoods Place Type along a Civic Boulevard (Sunningdale Road East) and an Urban Thoroughfare (Richmond Street) in accordance with Maps 1 and 3 of The London Plan. This Place Type at this location, based on Street Classification, permits single detached, semi-detached, duplex, converted dwellings, townhouses, low-rise apartments and group homes (Table 10). Specific policy 1069 permits an apartment building.

As the proposed use is contemplated by specific policy 1069, staff is satisfied the recommended amendment is in conformity with The London Plan and is appropriate within the existing and planned context.

#### **4.2 Intensity**

The proposed residential intensity is consistent with the policies of the PPS 2024 that encourage all types of residential intensification, including redevelopment which results in a net increase in residential uses (2.2.1.2), compact form (2.4.1.3.c), and an appropriate mix of housing options and densities (2.3.1.3). In the Neighbourhoods Place Type, the range of permitted building heights are identified in Table 11. At this location, a standard maximum building height of four (4) storeys is permitted, or an upper maximum height of six (6) storeys may be permitted in conformity with the Our Tools policies of this Plan relating to Zoning to the Upper Maximum Height (Policies 1638 to 1641). Policies contained in Specific Area Policies augment, and prevail, over the more general policies of the Place Type. Specific Area policy 1069 permits a maximum height of 10 storeys, or 33 metres, and the Applicant has requested amendments to allow for 11 storeys, or 40 metres. Similar heights and heights greater than that requested have been contemplated and permitted in the surrounding area. Heights ranging from a maximum of 38 metres is permitted at 1985 Richmond Street and 48 metres are permitted at 30, 100, and 230 Villagewalk Boulevard. Figure 3 below shows the similar permitted heights on adjacent lands.

Staff are satisfied the subject lands are of a sufficient size and configuration to accommodate the proposed apartment development, and that the proposed intensity is consistent with the existing and planned context of the area as demonstrated on Figure 3 below. On this basis, staff is satisfied the recommended amendment is in conformity with the intensity policies of The London Plan.

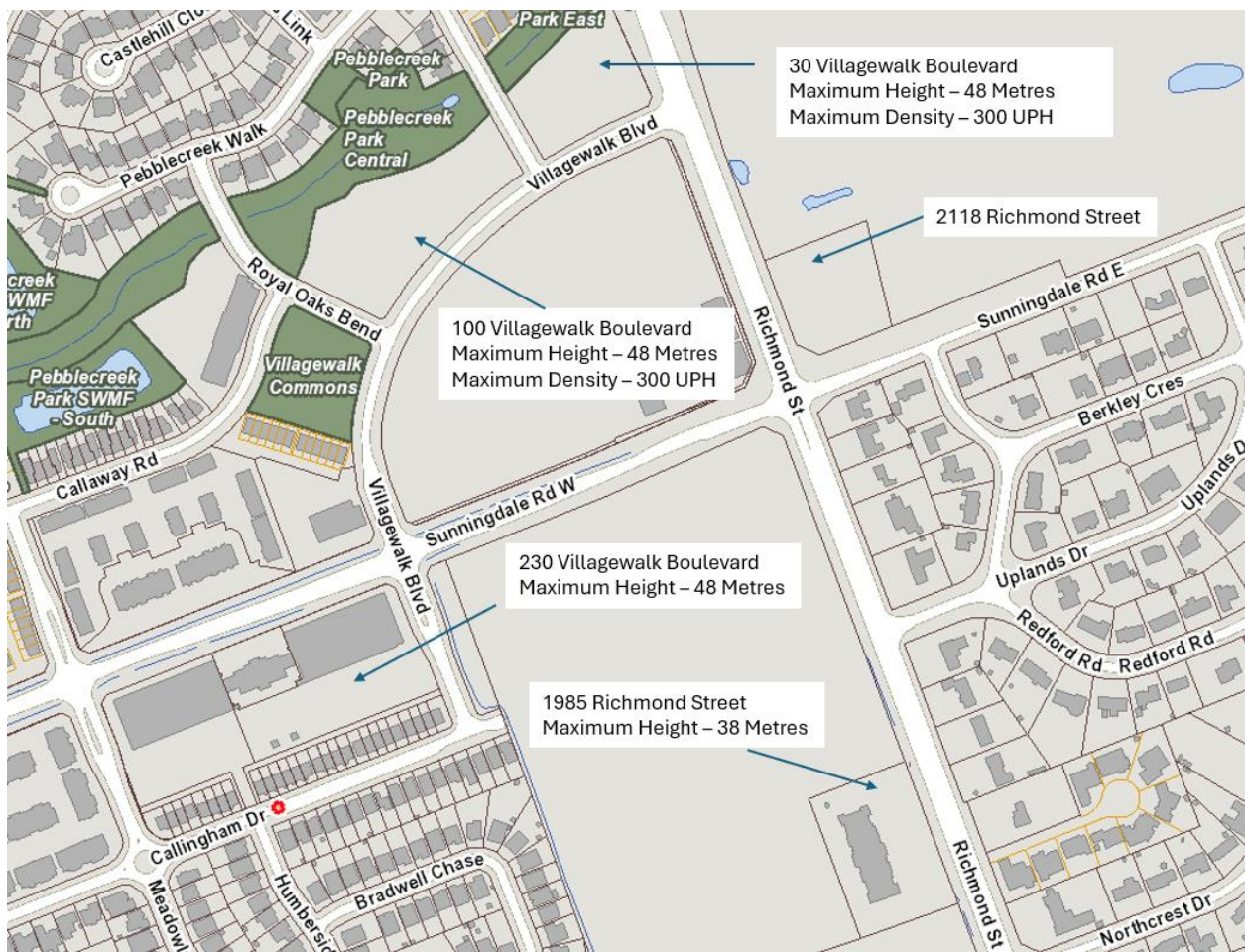


Figure 3 – Permitted Heights and Densities on adjacent lands

### 4.3 Form

The proposed built form is supported by the policies of the PPS 2024 and is consistent with the Neighbourhoods Place Type policies and the City Design policies of The London Plan by facilitating an appropriate form and scale of residential intensification that is compatible with the existing neighbourhood character (TLP 953\_2). As previously noted, the recommended zoning would permit medium density residential development, which can be accommodated on the lands. The recommended zoning would facilitate the development of mid-rise development, which aligns with the form identified as appropriate in The London Plan and is designed with street and pedestrian orientation in mind to promote connectivity. This connectivity could contribute to walkability to support lands to the northwest and northeast in the Shopping Area and Transit village Place Types.

The policies relating to buildings promote an active street front at a human scale to support pedestrian activity and safety (TLP 285 and 286). The built form, site layout, key entrances and streetscape should be designed to establish a sense of place and character consistent with the planning vision of the Place Type and the surrounding area (TLP 197, 202, 221 and 252). These policies are addressed through the requested reduced front yard and exterior side yard setbacks, which would site the building close to the street to create an active street front at a human scale. The Applicant’s Planning Justification Report and Urban Design Brief have outlined how building and unit entrances will be oriented towards Richmond Street and Sunningdale Road East to contribute to street and pedestrian oriented design. A network of pathways and a promenade are also proposed to contribute to pedestrian oriented design and connectivity throughout and beyond the subject lands. Enhanced landscaping will be considered during the Site Plan stage at the main entrances and gateways will be incorporated to create a sense of place and character, as well as a variety of materials will be included in the building materials and colours.

Policies 289 to 293 of The London Plan speak to the components and design of mid- and high-rise buildings. Policy 289 notes that these buildings should be designed to express three different components: a base, middle, and top. The base should establish a human-scale façade with active frontages and the top should provide a

finishing treatment to hide or integrate mechanical penthouses into the overall building design. The middle is to be cohesive, but distinct from the base and top. The proposed development incorporates these three components and meets their intent. Policies 290 to 293 speak to: located building at corner sites, where possible; orient principal building entrances and transparent windows to the public right-of-way and public spaces to reinforce the public realm; the incorporation of a podium or other design elements into the base of the building to reduce the apparent height and mass of buildings on the pedestrian environment; and, design that minimizes massing, shadowing, visual and obstruction of views from the street, public spaces and neighbouring properties. The proposal has considered these policies, siting the building near the corner of the property, orienting main building entrances and ground floor unit entrances to the public realm, contributing to the public realm and pedestrian environment through podium design, and locating the greatest height at the at the north end of the site to help mitigate shadowing and privacy impacts.

#### 4.4 Zoning

The subject lands are currently zoned a Holding Residential R6/Residential R8 Bonus (h-h-5-h-11-h-183-R6-5/R8-4\*B-30) Zone, which permits a ten (10) storey apartment building with a density of 123 units per hectare. The Applicant has requested an amendment to change the zoning of the subject lands to a Residential R9 Special Provision (R9-7(\_)) Zone to permit an eleven (11) storey apartment building with a density of 211 units per hectare.

The current zoning contains the following holding provisions:

- **h Purpose:** To ensure the orderly development of lands and the adequate provision of municipal services, the “h” symbol shall not be deleted until the required security has been provided for the development agreement or subdivision agreement, and Council is satisfied that the conditions of the approval of the plans and drawings for a site plan, or the conditions of the approval of a draft plan of subdivision, will ensure a development agreement or subdivision agreement is executed by the applicant and the City prior to development. Permitted Interim Uses: Model homes are permitted in accordance with Section 4.5(2) of the By-law; (Z.-1-122078) (Z.-1-142245)
- **h-5 Purpose:** To ensure that development takes a form compatible with adjacent land uses, agreements shall be entered into following public site plan review specifying the issues allowed for under Section 41 of the Planning Act, R.S.O. 1990, c. P.13, prior to the removal of the "h-5" symbol. Notwithstanding this, residential developments of 10 or fewer units are exempt from Public Site Plan Meetings, as per Bill 23, More Homes Built Faster Act, 2022. Permitted Interim Uses: Residential Developments of 10 units or fewer. (Z.-1-94236 deleted and replaced by Z.-1-233100)
- **h-11 Purpose:** To ensure the orderly development of lands and the adequate provision of municipal services, the "h-11" symbol shall not be deleted until a development agreement associated with a site plan which provides for appropriate access arrangements to the satisfaction of Council is entered into with the City of London. Permitted Interim Uses: Existing uses.(O.M.B. File #R 910387 - Appeal #3004 May 19, 1994)(Z.-1-92066)
- **h-183 Purpose:** To ensure that development will not have any negative impacts on the groundwater in the area, with specific attention given to any negative impacts on existing wells, a Hydrogeological Study shall be prepared by a qualified professional and submitted to the City to evaluate the potential impact of the proposed development to area private wells and provide recommendations for monitoring post construction impacts and possible mitigation measures to the satisfaction of the City Engineer prior to the removal of the h-183 symbol. Any recommendations contained therein shall be incorporated into the development agreement to the satisfaction of the City of London. (Z.-1-142350)

The Applicant has requested the removal of all holding provisions, with the exception of the h-183. Staff support removal of the h, h-5 and h-11, but recommend the h-183 remain on the site. Site Plan Approval considers matters of a highly technical nature

which involves review from subject matter experts. Staff are satisfied that the feedback received through the public processes for the Official Plan and Zoning By-law Amendment applications pertaining to the site can be sufficiently addressed through Staff's technical review of the future Site Plan application without an additional public meeting as required by the h-5 holding provision. The orderly development of lands, adequate provision of services, execution of a development agreement, and appropriate access arrangements can be addressed through the future Site Plan Approval Application, satisfying the removal of the h and h-11. In lieu of submitting the required Hydrogeological Study as part of this Application, the Applicant has requested to defer it to a future Site Plan stage and has agreed to retain the h-183 holding provision until that time.

A number of Special Provisions have been requested as follows:

Reduced Exterior Side Yard Setback of 9 metres (Minimum)

Exterior Side Yard Setbacks are intended to ensure there is sufficient separation between new development and the public right-of-way to potentially mitigate negative impacts, while also providing adequate space to ensure there are adequate sight lines, landscaping, and space to accommodate future road-widening, should it be required. The requested reduced exterior side yard setback helps to facilitate development that is street and pedestrian oriented by helping to establish a strong street edge and an active street front, while still allowing sufficient space for sight lines and landscaping.

Reduced Interior Side Yard Setback of 12 metres (Minimum)

Interior Side Yard Setbacks are intended to ensure there is sufficient separation between new and existing development to potentially mitigate negative impacts, while also providing access to the interior yard space. The proposed setback would provide sufficient distance between the proposed future development to the west and allows for private amenity space to be accommodated on the lands. There is sufficient access to the interior side yard.

Reduced Rear Yard Setback of 13 metres (Minimum)

Rear Yard Setbacks are intended to ensure there is sufficient separation between new and existing development, to potentially mitigate negative impacts while also provided access to the rear and interior yard space. The requested reduced rear yard setback allows sufficient space for sight lines and access to the lands.

Landscaped Open Space of 48.4 per cent (Minimum)

The Zoning By-law defines Landscaped Open Space as open space which is used for the growth and maintenance of grass, flowers, shrubbery, and other landscaping and includes any surfaced walk, patio, swimming pool or similar area, but does not include any access driveway or ramp, parking area, bus parking area, roof-top area or any open space beneath or within any building or structure. A minimum of 30 per cent is required under the Residential R9 (R9-7) Zone Variation. The requested special provision provides for a greater amount of landscaped open space than is required under the standard regulations.

Maximum Height

Specific policy 1067 in The London Plan permits a maximum height of 10 storeys or 33 metres, and the Applicant has requested amendments to allow for 11 storeys or 40 metres. Similar heights, and heights greater than that requested, have been contemplated and permitted in the surrounding area. Heights ranging from a maximum of 38 metres is permitted at 1985 Richmond Street and 48 metres are permitted at 30, 100, and 230 Villagewalk Boulevard. The requested height for the subject lands will help to establish this transition in uses and intensity of development, and locates density close to amenities and commercial areas. The recommended zoning and height are considered appropriate for the lands and in keeping with heights permitted on adjacent lands.

#### Maximum Density of 211 Units Per Hectare

A maximum density of 150 units per hectare is permitted in the Residential R9 (R9-7) Zone Variation and a special provision to permit a maximum density of 211 units per hectare has been requested. Similar densities have been considered and permitted in the R9-7 Zone Variation. In addition, lands to the west at 30, 100 and 115 Villagewalk Boulevard and 58 Sunningdale Road West permit densities ranging from 100 to 300 units per hectare. The requested density for the subject lands will help to establish this transition in uses and intensity of development, and locates density close to amenities and commercial areas. The recommended zoning and density are considered appropriate for the lands and in keeping with densities permitted on adjacent lands.

#### **4.5 Specific Policy Area Requirements and Bonus Zone Provisions**

Both the Specific Policy Area and Bonus Zone included criteria that must be met to achieve the increased height and density they permit. Under specific policy 1069, the increased height and density were subject to the integration of a three-storey townhouse form into the base of the building adjacent to Sunningdale Road East and the proposed form of development shall address the City Design chapter and the Our Tools part of The London Plan. The site was bonused for: enhanced urban design features; underground parking and related open space; and, the planting of nine mature trees (minimum diameter at breast height (dbh) of 11cm) as part of the landscaping plan between the south building face and Sunningdale Road East.

The Applicant has requested to remove the requirement to integrate a three-storey townhouse form into the base of the building. The intent of this requirement is locate the maximum intensity of development away from the single-detached dwellings within the residential neighbourhood south of Sunningdale Road in order to mitigate potential impacts on shadowing and privacy, as well as provide a transition in intensity of development. The proposal maintains the three-storey built form along Sunningdale Road East, but in the form of apartment units rather than townhouse units. Unit entrances and pathway connections to the sidewalk are proposed to replicate a townhouse-built form and improve connectivity throughout the site. Staff are satisfied that the proposal still meets the intent of the requirement and the difference in appearance of the built form will be minimal. Staff support the amendment that removes the requirement for three-storey townhomes in favour of a three-storey apartment building form.

To ensure the requirements for enhanced urban design features, underground parking, additional open space and the planting of nine mature trees along Sunningdale Road East are still met, they have been included as items for the Site Plan Approval Authority to consider through the Site Plan review process.

#### **4.6 Public Comments**

##### **Removal of Townhouse Requirements**

As noted above, the Applicant has requested to remove the requirement for townhouses to be integrated into the base of the building adjacent to Sunningdale Road East. Staff are satisfied that the proposal still meets the intent of the requirement and the difference in the appearance of the built form will be minimal.

##### **Privacy and Shadowing**

As discussed above, the proposal maintains the three-storey built form component along Sunningdale Road East to locate the greatest density away from the single detached dwellings within the residential neighbourhood south of Sunningdale Road East and will help to mitigate potential impacts on shadowing and privacy.

##### **Building Design**

Staff have reviewed the proposed development and provided comments to ensure the development incorporates good urban design principles. Although some building design features may be considered in Zoning Amendments, site layout and parking orientation will be dealt with through a Site Plan Approval Application at a later date.

## Conclusion

The Applicant has requested an amendment to The London Plan to amend specific policy 1069 in the Neighbourhoods Place Type to: remove the Bonus Zone Provisions; increase the permitted residential density from 123 units per hectare to 211 units per hectare, to allow for a total of 158 residential dwelling units; and, to remove the required three (3) storey townhouses along the base of the building. The Applicant has also requested an amendment to the Zoning By-law Z.-1 to rezone the property from a Holding Residential R6/Residential R8 Bonus (h\*5\*h-11\*h-183\*R6-5/R8-4\*B-30) Zone, to a Residential R9 Special Provision (R9-7(\_)) Zone.

Staff are recommending approval of the requested Official Plan Amendment and Zoning By-law Amendment with special provisions and a holding provision. The recommended action is consistent with the PPS 2024, conforms to The London Plan and will permit development that is appropriate and compatible within the existing and planned context.

**Prepared by:** Alison Curtis, MCIP, RPP  
Planner, Planning and Development

**Reviewed by:** Catherine Maton, MCIP, RPP  
Manager, Planning Implementation

**Recommended by:** Heather McNeely, MCIP, RPP  
Director, Planning and Development

**Submitted by:** Scott Mathers, MPA, P.Eng.  
Deputy City Manager, Planning and Economic  
Development

CC: Britt O'Hagan, Manager, Current Development  
Mike Corby, Manager, Site Plans  
Brent Lambert, Manager, Development Engineering

HM/MC/AC/ac

Y:\Shared\DEVELOPMENT SERVICES\11 - Current Planning\DEVELOPMENT  
APPS\2024 Applications 9691 to\Applications\Richmond Street 2118 (AC) OZ-9770



## Appendix A – Official Plan Amendment

Bill No. (number to be inserted by  
Clerk's Office)  
2024

By-law No. C.P.-XXXX-

A by-law to amend the Official Plan, *The  
London Plan* for the City of London, 2016  
relating to 2118 Richmond Street

The Municipal Council of The Corporation of the City of London enacts as follows:

1. Amendment No. (to be inserted by Clerk's Office) to the Official Plan, *The London Plan* for the City of London Planning Area – 2016, as contained in the text attached hereto and forming part of this by-law, is adopted.
2. This Amendment shall come into effect in accordance with subsection 17(27) of the *Planning Act, R.S.O. 1990, c.P.13*.

PASSED in Open Council on November 5, 2024

Josh Morgan  
Mayor

Michael Schulthess  
City Clerk

First Reading – November 5, 2024  
Second Reading – November 5, 2024  
Third Reading – November 5, 2024

**AMENDMENT NO.**  
**to the**  
**OFFICIAL PLAN, THE LONDON PLAN, FOR THE CITY OF LONDON**

**A. PURPOSE OF THIS AMENDMENT**

The purpose of this Amendment is to amend the Specific Policy 1069 for the Neighbourhoods Place Type to permit an 11 storey apartment building containing 158 units (211 units per hectare) and to remove references to Bonus Zone Provisions and the requirement to integrate three storey townhouses into the base of the building adjacent to Sunningdale Road East.

**B. LOCATION OF THIS AMENDMENT**

This Amendment applies to lands located at 2118 Richmond Street in the City of London.

**C. BASIS OF THE AMENDMENT**

The site-specific amendment to the existing specific policy would allow for an 11 storey apartment building comprised of a three (3) storey base and an eight (8) storey tower with a total of 158 units (211 units per hectare). The requested Official Plan Amendment is consistent with the PPS 2024, conforms to The London Plan and will permit development that is appropriate and compatible within the existing and planned context.

**D. THE AMENDMENT**

*The London Plan* for the City of London is hereby amended as follows:

Specific Policies for the Neighbourhoods Place Type of Official Plan, The London Plan, for the City of London is amended by deleting and replacing policy 1069 with the following text:

1069\_ 2118 Richmond Street

For the property located at 2118 Richmond Street, a maximum height of eleven storeys, including a rooftop amenity level, and a maximum density of up to 211 units per hectare, subject to the following:

1. The permitted form of development shall be an apartment building with three-storey base adjacent to Sunningdale Road East. The apartment building component of the structure shall be restricted to the north portion of the property, thereby locating the maximum intensity away from the single detached dwellings within the residential neighbourhood south of Sunningdale Road East.
2. The proposed form of development shall address the City Design chapter and the Our Tools part of this Plan with respect to, at a minimum, enhanced urban design, the provision of underground parking, and encouraging aesthetically attractive residential developments through the enhanced provision of landscaped open space.

**“Schedule 1”**



## Appendix B – Zoning Bylaw Amendment

Bill No.(number to be inserted by Clerk's Office)  
2024

By-law No. Z.-1-

A by-law to amend By-law No. Z.-1 to rezone an area of land located at 2118 Richmond Street

WHEREAS upon approval of Official Plan Amendment Number (number to be inserted by Clerk's Office) this rezoning will conform to the Official Plan;

THEREFORE the Municipal Council of The Corporation of the City of London enacts as follows:

1. Section Number 4.3 is amended to delete the Bonus (B-30) Zone in its entirety;
2. Schedule "A" to By-law No. Z.-1 is amended by changing the zoning applicable to lands located at 2118 Richmond Street as shown on the attached map comprising part of Key Map No. A102, **FROM** a Holding Residential R6 and R8 Bonus (h-h-5-h-11-h-183-R6-5/R8-4-B-30) Zone **TO** a Holding Residential R9 Special Provision (h-183-R9-7(\_)-H40) Zone;
3. Section Number 13.4 of the Residential R9 (R9-7) Zone is amended by adding the following Special Provisions:

R9-7(\_) 2118 Richmond Street

a. Regulations

- i) Exterior Side Yard Depth (Minimum) – 9 metres
  - ii) Interior Side Yard Depth (Minimum) – 12 metres
  - iii) Rear Yard Depth (Minimum) – 13 Metres
  - iv) Landscaped Open Space (Minimum) – 48.4%
  - v) Lot Coverage (Maximum) – 28%
  - vi) Height (Maximum) – 40 metres
  - vii) Density (Maximum) – 211 units per hectare
  - viii) Building Setback from Sunningdale Road East Above the Third Floor (Minimum) – 21 metres
  - ix) Building Setback from Sunningdale Road East Above the Eighth Floor (Minimum) – 29 metres
4. This By-law shall come into force and be deemed to come into force in accordance with Section 34 of the Planning Act, R.S.O. 1990, c. P13, either upon the date of the passage of this by-law or as otherwise provided by the said section.

PASSED in Open Council on November 5, 2024 subject to the provisions of PART VI.1 of the *Municipal Act*, 2001.

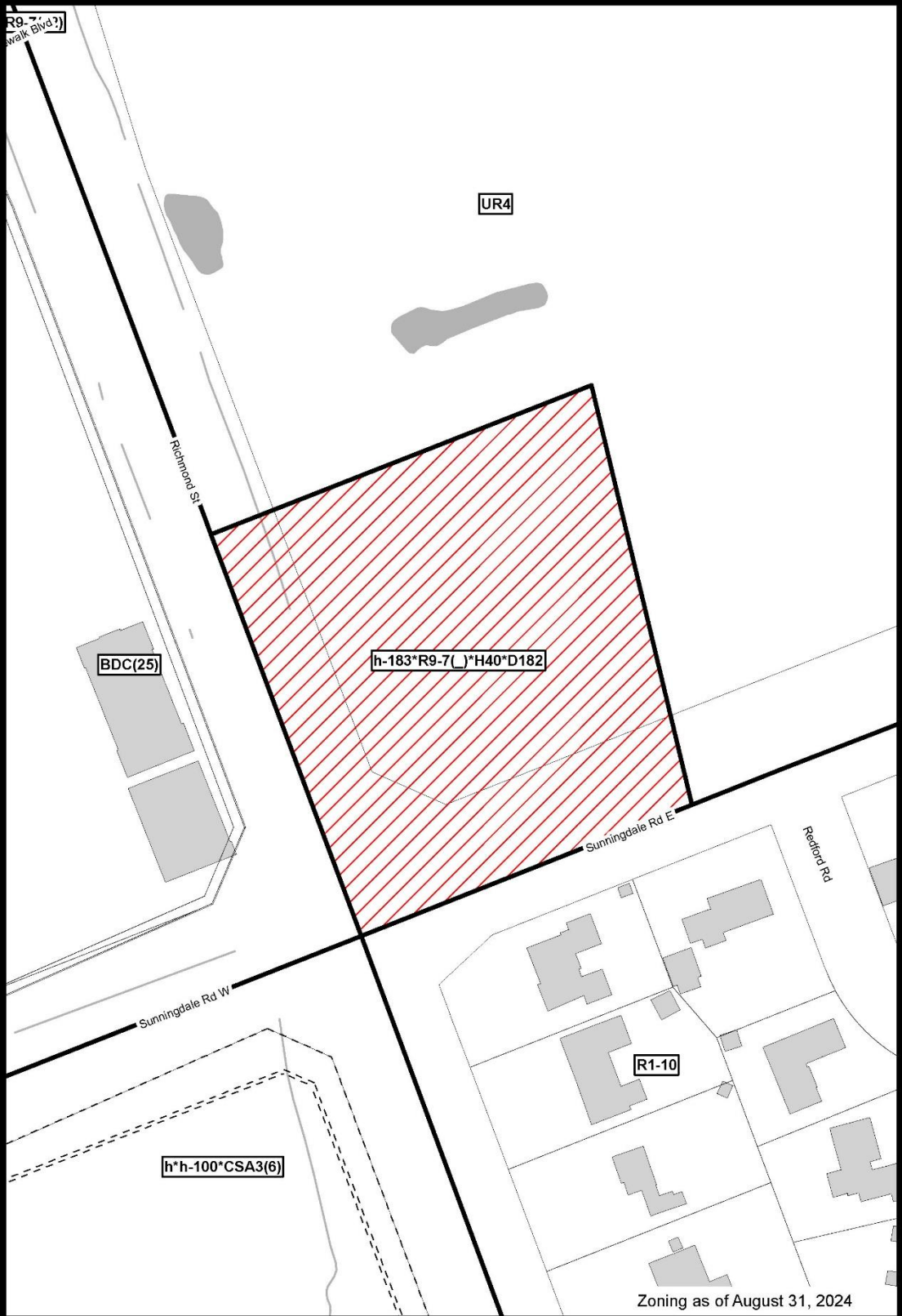
Josh Morgan


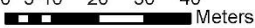

Mayor

Michael Schulthess  
City Clerk

First Reading – November 5, 2024  
Second Reading – November 5, 2024  
Third Reading – November 5, 2024

AMENDMENT TO SCHEDULE "A" (BY-LAW NO. Z.-1)



<p>File Number: OZ-9770                  Planner: AC                  Date Prepared: 2024/09/27                  Technician: RC                  By-Law No: Z.-1-</p>	<p>SUBJECT SITE </p> <p>1:1,250</p> <p>0 5 10 20 30 40 Meters </p> <p></p>
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Geodatabase

## Appendix C - Site and Development Summary

### A. Site Information and Context

#### Site Statistics

Current Land Use	Vacant
Frontage	62 metres (203.4 feet)
Depth	90 metres (295 feet)-
Area	0.75 hectares (1.85 acres)
Shape	Irregular
Within Built Area Boundary	No
Within Primary Transit Area	No

#### Surrounding Land Uses

North	Vacant agricultural lands planned for future residential development
East	Vacant agricultural lands planned for future residential development
South	Low Density Residential
West	Commercial and Medium Density Residential

#### Proximity to Nearest Amenities

Major Intersection	Richmond Street and Sunningdale Road East, 24 metres
Dedicated cycling infrastructure	Future Cycling Infrastructure on Richmond Street and Sunningdale Road East, 24 metres
London Transit stop	Sunningdale Road East, 80 metres
Public open space	Villagewalk Commons, 356 metres
Commercial area/use	Upper Richmond Village, 45 metres
Food store	Loblaws, 1262 metres
Primary school	Masonville Public School, 1465 metres
Community/recreation amenity	Masonville Library, 1857 metres

### B. Planning Information and Request

#### Current Planning Information

Current Place Type	Neighbourhoods Place Type on an Urban Thoroughfare (Richmond Street) and a Civic Boulevard (Sunningdale Road East)
Current Special Policies	Special Policy Area 16 (Policy 1069 of <i>The London Plan</i> )
Current Zoning	Holding Residential R6/Residential R8 Bonus (h*h-5*h-11*h-183*R6-5/R8-4*B-30) Zone

#### Requested Designation and Zone

Requested Place Type	N/A
Requested Special Policies	Amended Special Policy Area 16 (Policy 1069 of <i>The London Plan</i> )
Requested Zoning	Holding Residential R9 (h-183*R9-7( ) Zone

#### Requested Special Provisions

Regulation (R9-7)	Required	Proposed
Exterior Site Yard Depth (Minimum)	12 Metres	9 Metres
Interior side Yard Depth (Minimum)	16 Metres	12 Metres
Rear Yard Depth (Minimum)	16 Metres	13 Metres
Landscaped Open Space (Minimum)	30 %	48.4 %

Regulation (R9-7)	Required	Proposed
Lot Coverage (Maximum)	40 %	28 %
Height (Maximum)	n/a	40 Metres
Density (Maximum)	150 units per hectare	211 units per hectare
Building setback from Sunningdale Road East – Above the Third Floor (Minimum)	n/a	21 Metres
Building setback from Sunningdale Road East – Above the Eighth Floor (Minimum)	n/a	29 Metres

## C. Development Proposal Summary

### Development Overview

The requested Official Plan and Zoning By-law Amendments would facilitate the development of an eleven (11) storey apartment building, in a “L” shaped configuration, comprised of a three (3) storey base and an eight (8) storey tower with setbacks incorporated above the third and eighth storeys. A total of 158 units are proposed, an increase for 51 units from the previously approved development.

The proposal includes outdoor common amenity space on the east side of the site, as well as two internal amenity areas. An amenity room and lounge are included on the ground floor, and rooftop indoor and outdoor community amenity areas are proposed for the eleventh floor. Access is proposed to be provided from a right-in/right-out driveway on Richmond Street, located at the rear of the building. The majority of parking will be provided in two levels of underground parking and surface parking is provided towards to rear of the site for visitor and barrier free parking spaces. Walkways are proposed through the site to building and unit entrances to the public sidewalk and enhanced landscaping is proposed to provide wayfinding and an aesthetic promenade.

### Proposal Statistics

Land use	Residential
Form	Apartment Building
Height	11 Storeys (40 metres)
Residential units	158 (increase of 51 units)
Density	211 Units per hectare
Building coverage	25.7%
Landscape open space	58.7%
New use being added to the local community	No

### Mobility

Parking spaces	152 underground, 18 surface
Vehicle parking ratio	1.44 Spaces per unit
New electric vehicles charging stations	Unknown
Secured bike parking spaces	168
Secured bike parking ratio	1 Spaces per unit
Completes gaps in the public sidewalk	Yes
Connection from the site to a public sidewalk	No
Connection from the site to a multi-use path	NA



## Environmental Impact

Tree removals	Approximately 8
Tree plantings	24 recommended; to be determined through SPA
Tree Protection Area	No
Loss of natural heritage features	N/A
Species at Risk Habitat loss	N/A
Minimum Environmental Management Guideline buffer met	N/A
Existing structures repurposed or reused	N/A
Green building features	Unknown

**Appendix D – Additional Plans and Drawings**



**WEST ELEVATION (RICHMOND ST)**  
SCALE: 1/4"=1'-0"



**SOUTH ELEVATION (SUNNINGDALE RD)**  
SCALE: 1/4"=1'-0"

### Internal Department Comments

#### **Parks Planning and Design**

Parks Planning and Design staff have reviewed the submitted notice of application and offer the following comments:

**1. Major Issues**

- None.

**2. Matters for OPA/ZBA**

- None.

**3. Matters for Site Plan**

- Parkland dedication has not been taken for this site. It is to be noted that the applicant, as a condition of site plan approval, will be required to provide parkland dedication in the form of cash-in-lieu pursuant to By-law CP-25.

#### **Urban Design**

##### Major Issues:

- Urban Design is generally supportive of the proposed 11-storey apartment building and acknowledges the applicant for proposing an ‘L’ shaped building located close to both public streets, for locating the majority of the parking underground, and for including step-backs in the building design along Sunningdale Road E to transition to the lower intensity uses to the south.

##### Matters for OPA/ZBA:

- Urban Design recommends the following Special Provisions be incorporated into the proposed R9-7( ) Zone, in addition to the Special Provisions already proposed by the applicant:
  - Minimum setback of 33.0m above the 10<sup>th</sup> storey from the southerly property line (as currently proposed).
  - Minimum step-back of 3.0m above the 3<sup>rd</sup> storey along Richmond Street.
  - The principal building entrance shall be located at Richmond Street, or at the corner of Richmond Street and Sunningdale Road E.
  - Urban Design is supportive of the other Special Provisions proposed by the applicant.

##### Matters to be Addressed at Site Plan:

- The applicant is acknowledged for providing the following site and building design features:
  - Individual unit entrances for ground floor units with direct walkway access to the public sidewalk.
  - Weather protection (canopy), signage and changes in massing/materials to identify the building entrance facing toward Richmond Street.
  - Terraced landscaping to soften the appearance of the retaining wall along Richmond Street.
  - Changes in materials, massing and articulation of the building façade facing toward the public streets.
- Design the individual unit entrances on the ground floor along Richmond Street and Sunningdale Road E to function and appear as principal entrances [TLP Policy 291]:

- Include lockable front-door style accesses, as opposed to sliding patio doors.
- Provide weather protection, such as a porch or canopy above these entrances.
- Consider designing the terraced retaining wall along Richmond Street to match the style of the retaining wall along the Fanshawe Park Road W frontage at 545 & 549 Fanshawe Park Road W, to soften its appearance (materials, landscaping, etc.) [TLP Policy 285].
- Include a walkway for the individual unit entrances located north of the building entrance along Richmond Street to provide direct access to the public sidewalk [TLP Policy 255, 268].
- Consider integrating the parking ramp entrance into the ground floor level of the proposed built form to provide additional communal amenity space for the anticipated number of residents [TLP 275, 295].
- To promote pedestrian connectivity and safety, delineate pedestrian pathways through the parking (e.g., painted lines, inset concrete) [TLP Policy 255].
- Ensure that the development is “future ready”. Once parking requirements have been achieved, consider including charging stations for ebikes and electric vehicles [TLP Policy 729].
- Consider making the roof strong enough to hold solar panels and/or green roof infrastructure [TLP Policy 666, 475(7)].
- To promote passive surveillance and CPTED principles, provide adequate lighting throughout the site to mitigate safety concerns

## **Ecology**

OZ-9770 OPA/ZBA Notice of Application – 2118 Richmond Street  
ZBA to allow an 11 Storey residential apartment building

This e-mail is to confirm that there are currently no ecological planning issues related to this property and/or associated study requirements.

### Major Issues Identified

- None

### Matters for OPA/ZBA

- None

### Matters for Site Plan

- None

### Ecology – Complete Application Requirements

- None

### Notes

- Avoid tree removal within the active bat roosting period (April 30 – September 1) to reduce potential interactions with Endangered bat species, to avoid contravention of the Endangered Species Act.
- Avoid vegetation removal within the active breeding bird period (April 1 – August 30) to avoid disturbing nesting birds and contravening the Migratory Bird Convention Act.

## **Landscape Architecture**

1. Major Issues
  - No potential grounds for refusal, or issues that could require significant changes to the proposal.
2. Matters for OPA/ZBA
  - No matters that will influence the OP/ZBL mapping, designation/zone, regulations, special provisions, holding provisions, etc.
3. Matters for Site Plan

- Replacement trees to be recommendation to Site Plan Review based on total dbh removed. 246 cm dbh is proposed for removal, in accordance with LP Policy 399, 24 replacement trees are required. However, the city is in the process of developing a bylaw to implement Policy 399, so the follow ratio will be applied: 1 replacement tree for a trunk diameter of 50.99 cm, 2 replacement trees for a trunk diameter between 51cm and 60cm, and 1 additional replacement tree for every 10cm of trunk diameter thereafter to a maximum of 11 replacement trees. To this end 2 replacement trees would be required. Tree planting required as part of the planning and development approvals process may be counted as replacement trees as required by these policies.

### **Heritage and Archaeology Comments**

OZ-9770 – 2118 Richmond Street

*OPA and ZBA to allow 11 storey residential apartment*

There are no cultural heritage or archaeological concerns with this application.

### **Engineering Comments**

**Engineering has no concerns to rezoning as the engineering related matters are being reviewed as part of the site plan application 2019 and recommended the Holding provisions to site will remain until engineering matters are addressed.**

The following major issues still needs to be addressed by the applicant at the site plan application stage.

- Please note: This site has a h-183 zoning provision; Hydrogeological Study shall be prepared by a qualified professional and submitted to the City to evaluate the potential impact of the proposed development to area private wells and provide recommendations for monitoring post construction impacts and possible mitigation measures to the satisfaction of the City Engineer.
- There is currently no municipal sanitary sewer fronting the subject lands. The intended outlet for the site is ultimately tributary to the 450mm diameter on Villagewalk Blvd. The extension is to be at the cost of the developer and consistent in tributary area and populations from accepted area plans. The proposed populations are approximately 3 times over the allocated amount.
- Dimension the required Richmond St widening which is a 5.712 meter widening to achieve the required 24.0m from centerline.
- 6.0mx6.0m daylight triangle required at the intersection corner.

### **Matters for Site Plan**

#### **Sewer**

- There is currently no municipal sanitary sewer fronting the subject lands. The intended outlet for the site is a future sanitary extension at no cost to the City ultimately tributary to the 450mm diameter sewer on Villagewalk Blvd adequately sized and sloped to take in all tributary lands as per accepted area plans and design sheets. The subject lands are allocated approximately 120ppl/ha equivalent to approximately 75ppl for the proposed block. The proposed population is approximately 3.5 times over the allocated accepted population.
- The applicant's engineer is to provide the maximum population and peak flow, and demonstrate all tributary external lands consistent with accepted area plans to the future extension on Richmond Street at no Cost to the City that are ultimately tributary to the 450mm diameter on Villagewalk Blvd.
- Holding Provisions are required until servicing has been demonstrated and extended in standard location in the ROW at no cost to the City for the proposed development and consistent with accepted area plans for tributary areas and populations and to the satisfaction of SED.

#### **Stormwater:**

- the Necessary SWM servicing and drainage requirements/controls to service this site will be implemented as part of a forthcoming SPA and development

agreement. Record of SPC for this development has been provided under SPC23-088.

**Water:**

- Water service is available via the municipal 400mm PVC High Level watermain on Sunningdale Rd or the 300mm PVC high level watermain on Richmond Street.
- A water servicing design brief will be required addressing proposed watermain layout and domestic demands, fire flows and water quality that all meet City Standards.
- Further comments provided at site plan stage.
- All Water servicing to the site shall be to City Standards

**Transportation:**

- No additional widening is required from the Sunningdale frontage.
- The Richmond St frontage is subject to a 5.712 metre widening to achieve the required 24.0m from centreline.
- A Transportation Impact Assessment (TIA) will be required, the TIA will evaluate the impact the development will have on the transportation infrastructure in the area and provide recommendations for any mitigation measures. The TIA will need to be scoped with City staff prior to undertaking and be undertaken in general conformance with the City's TIA guidelines
- 6.0mx6.0m daylight triangle required at the intersection corner.
- Detailed comments regarding access design and location will be made through the site plan process.

**Site Plan**

1. Major Issues

- none

2. Matters for OPA/ZBA

- Confirm the extent of the landscaped open space. Exclude any lands that have already been dedicated as road widening along Sunningdale as it appears city-owned lands are forming part of the calculation. The density bonus provided by the R9-7 may capture the 211uph requested without a special provision, however accurate landscaped open space details are required for this calculation.
- There is an imperial oil gas line along Sunningdale Rd E, provide the dimension of the setback from the centre of the pipeline to the nearest wall of building. The requirement is 20m and it appears to be slightly less than the minimum required by s.4.20 of Z.-1.
- The proposal is for an R9-7 zone, though some of the required regulations referenced are from the B-30 zone (lot area, frontage, landscaped open space etc). Confirm proposed zone request with applicant. Zoning data sheet and zoning data table details do not match.
- Special provisions required for: exterior side yard depth, rear yard depth, site-specific height, \*density, and pipeline setback TBD

3. Matters for Site Plan

- Parking opposite ramp to underground parking should be removed to avoid maneuvering conflict.
- Other commentary to be addressed at the site plan application stage

**External Agency Comments**

**UTRCA**

The Upper Thames River Conservation Authority (UTRCA) has reviewed this application with regard for the policies within the Environmental Planning Policy Manual for the Upper Thames River Conservation Authority (June 2006), Section 28 of the *Conservation Authorities Act*, the *Planning Act*, the Provincial Policy Statement (2020), and the Upper Thames River Source Protection Area Assessment Report.

#### CONSERVATION AUTHORITIES ACT

The subject lands *are not* affected by any regulations (Ontario Regulation 41/24) made pursuant to Section 28 of the *Conservation Authorities Act*.

#### RECOMMENDATION

The UTRCA has *no objections* to the application and we have no Section 28 approval requirements.

#### **London Hydro**

Servicing the above proposal should present no foreseeable problems. Any new and/or relocation of existing infrastructure will be at the applicant's expense, maintaining safe clearances from London Hydro infrastructure is mandatory. Note: Transformation lead times are minimum 16 weeks. Contact the engineering Dept. to confirm requirements and availability.

London Hydro has no objection to this proposal or possible official plan and/or zoning amendment, Any new or relocation of the existing service will be at the expense of the owner.

### Community Engagement

**Public Liaison:** Information regarding the requested Zoning By-law Amendment application and opportunities to provide comments were provided to the public as follows:

- Notice of Public Participation Meeting was sent to property owners within 120 metres of the subject property and on published in the Public Notices and Bidding Opportunities section of The Londoner August 21, 2024.
- Notice of Application was sent to property owners within 120 metres of the subject property on August 21, 2024.
- Information about the Application were posted on the website on August 21, 2024.

### Notice of Application – Londoner August 29, 2024

**2118 Richmond Street** – The purpose and effect of this Official Plan and Zoning By-law Amendment is to permit an eleven (11) storey residential apartment building with a total of 158 residential units, 227 parking spaces, and 168 bicycle parking spaces. Possible amendment to the Official Plan to amend Specific Area Policy 16 (Policy 1069 of The London Plan) to remove references to the Bonus Zone Provisions; increase the permitted residential density from 123 units per hectare to 211 units per hectare, and to remove the required three (3) storey townhouses along the base of the building. Possible change to the Zoning By-law Z.-1 **FROM** a Holding Residential R6 and R8 Bonus (h\*h-5\*h-11\*h-183\*R6-5/R8-4\*B-30) Zone **TO** a Residential R9 Special Provision (R9-7(\_)) Zone. Special Provisions are requested for increased density, increased height, reduced setbacks, minimum landscape open space and maximum lot coverage. The City may also consider additional considerations, such as a different base zone, the use of holding provisions, and/or additional special provisions.

File: OZ-9770 Planner: A. Curtis X. 4497

### **Public Comments**

Dear Ms. Curtis,

Please include the following comments in future considerations regarding 2118 Richmond Street.

I would encourage the planning committee to refuse this revised application, instead encouraging York Developments to return to terms upon which the neighbouring Uplands community initially agreed. The previous proposal from York developments, itself the result of years worth of input from neighbours, had transferred most of the building mass to the northern interior side of the property, leaving only a three-storey section along Sunningdale Road and Richmond Street. This decision prevented the single-family residences along Sunningdale Road and in the interior of the Uplands neighbourhood from having a tower looming so close to the front of their properties. While this new proposal retains the three-storey section along Sunningdale, I decision I applaud, I would like to see the highest point of the tower located farther into the interior of the property, along the north property line; I have included a photo of York's initial proposal below, which demonstrates this more completely.





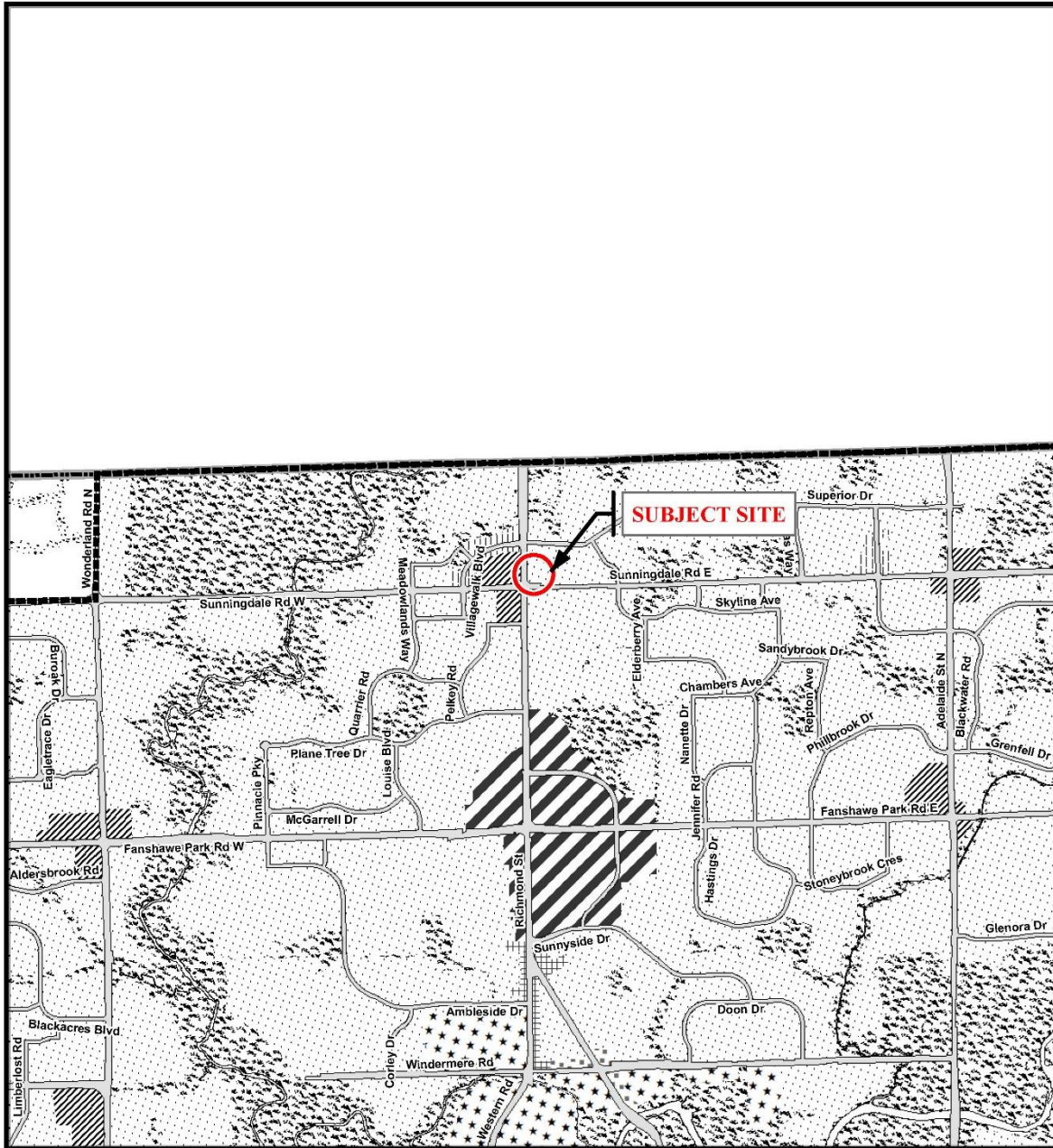
Finally, although I realise the City has little say in the design of the building itself, I would like to see the use of brick in place of the unattractive faux wood trim York Developments has now decided to use. In so many ways, the initial proposal was vastly more aesthetically attractive, and I urge York to return to this plan.

Thank you so much for your consideration,

Arthur

# Appendix G – Planning Information

## The London Plan Excerpt



### Legend

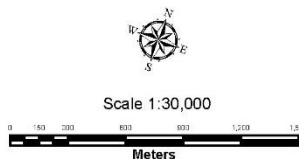
Downtown	Future Community Growth	Environmental Review
Transit Village	Heavy Industrial	Farmland
Shopping Area	Light Industrial	Rural Neighbourhood
Rapid Transit Corridor	Future Industrial Growth	Waste Management Resource Recovery Area
Urban Corridor	Commercial Industrial	Urban Growth Boundary
Main Street	Institutional	
Neighbourhood	Green Space	

*This is an excerpt from Planning & Development's working consolidation of Map 1 - Place Types of the London Plan, with added notations.*

### CITY OF LONDON Official Plan

#### LONDON PLAN MAP 1 - PLACE TYPES -

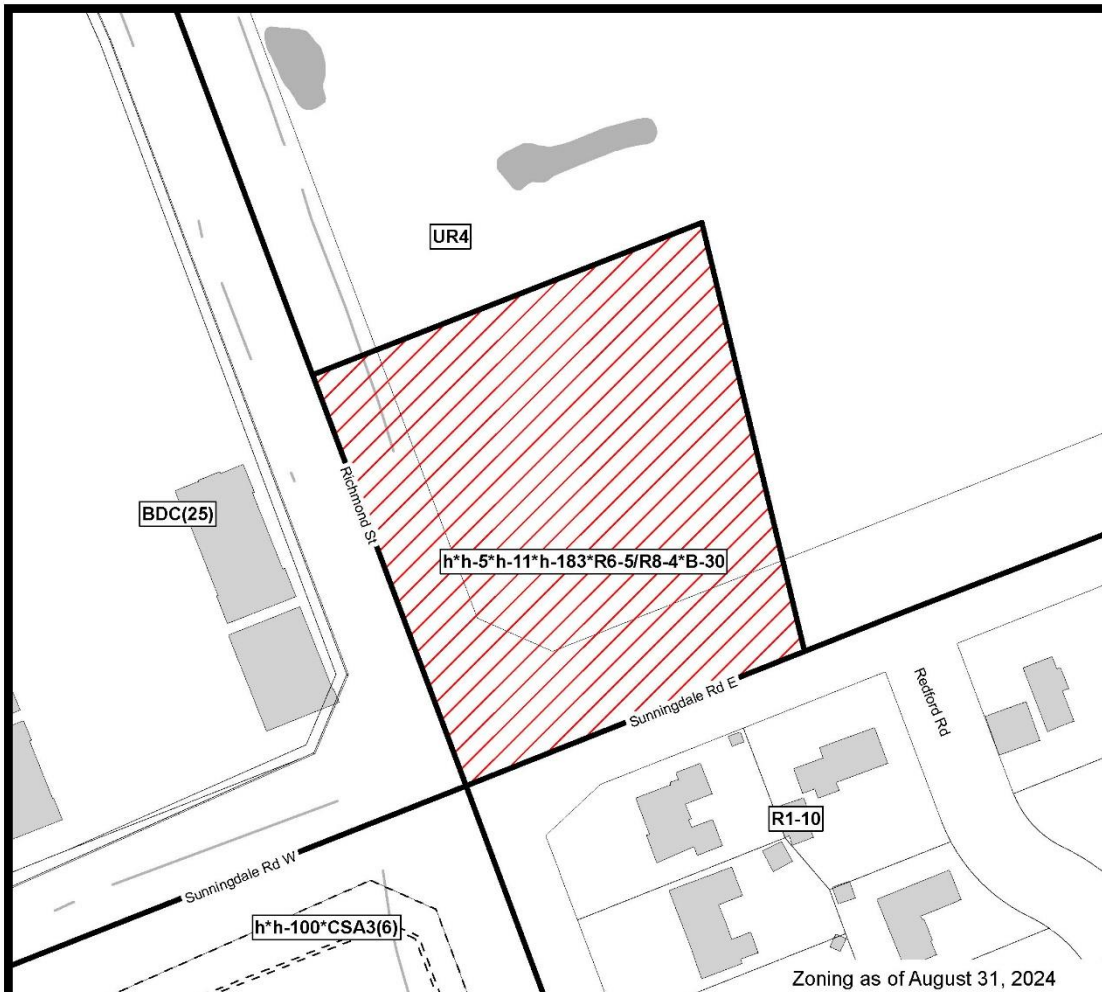
PREPARED BY: Planning & Development



**File Number:** OZ-9770  
**Planner:** AC  
**Technician:** RC  
**Date:** 2024/09/19

Project Location: E:\Planning\Projects\p\_officialplan\workconsolid00\excerpts\_LondonPlan\mxds\OZ-9770\_Map1\_PlaceTypes.mxd

# Zoning By-law Excerpt



 COUNCIL APPROVED ZONING FOR THE SUBJECT SITE: **h\*h-5\*h-11\*h-183\*R6-5/R8-4\*B-30**

1) **LEGEND FOR ZONING BY-LAW Z-1**

- |   |                                   |
|---|-----------------------------------|
| R1 - SINGLE DETACHED DWELLINGS            | RF - REGIONAL FACILITY            |
| R2 - SINGLE AND TWO UNIT DWELLINGS        | CF - COMMUNITY FACILITY           |
| R3 - SINGLE TO FOUR UNIT DWELLINGS        | NF - NEIGHBOURHOOD FACILITY       |
| R4 - STREET TOWNHOUSE                     | HER - HERITAGE                    |
| R5 - CLUSTER TOWNHOUSE                    | DC - DAY CARE                     |
| R6 - CLUSTER HOUSING ALL FORMS            |                                   |
| R7 - SENIOR'S HOUSING                     | OS - OPEN SPACE                   |
| R8 - MEDIUM DENSITY/LOW RISE APTS.        | CR - COMMERCIAL RECREATION        |
| R9 - MEDIUM TO HIGH DENSITY APTS.         | ER - ENVIRONMENTAL REVIEW         |
| R10 - HIGH DENSITY APARTMENTS             |                                   |
| R11 - LODGING HOUSE                       | OB - OFFICE BUSINESS PARK         |
|   | LI - LIGHT INDUSTRIAL             |
| DA - DOWNTOWN AREA                        | GI - GENERAL INDUSTRIAL           |
| RSA - REGIONAL SHOPPING AREA              | HI - HEAVY INDUSTRIAL             |
| CSA - COMMUNITY SHOPPING AREA             | EX - RESOURCE EXTRACTIVE          |
| NSA - NEIGHBOURHOOD SHOPPING AREA         | UR - URBAN RESERVE                |
| BDC - BUSINESS DISTRICT COMMERCIAL        |                                   |
| AC - ARTERIAL COMMERCIAL                  | AG - AGRICULTURAL                 |
| HS - HIGHWAY SERVICE COMMERCIAL           | AGC - AGRICULTURAL COMMERCIAL     |
| RSC - RESTRICTED SERVICE COMMERCIAL       | RRC - RURAL SETTLEMENT COMMERCIAL |
| CC - CONVENIENCE COMMERCIAL               | TGS - TEMPORARY GARDEN SUITE      |
| SS - AUTOMOBILE SERVICE STATION           | RT - RAIL TRANSPORTATION          |
| ASA - ASSOCIATED SHOPPING AREA COMMERCIAL |                                   |
| OR - OFFICE/RESIDENTIAL                   | "h" - HOLDING SYMBOL              |
| OC - OFFICE CONVERSION                    | "D" - DENSITY SYMBOL              |
| RO - RESTRICTED OFFICE                    | "H" - HEIGHT SYMBOL               |
| OF - OFFICE                               | "B" - BONUS SYMBOL                |
|   | "T" - TEMPORARY USE SYMBOL        |

**CITY OF LONDON**

PLANNING SERVICES / DEVELOPMENT SERVICES

**ZONING  
BY-LAW NO. Z-1  
SCHEDULE A**



THIS MAP IS AN UNOFFICIAL EXTRACT FROM THE ZONING BY-LAW WITH ADDED NOTATIONS

FILE NO:

OZ-9770

AC

MAP PREPARED:

2024/09/19

RC

1:1,500

0 5 10 20 30 40  
Meters

## Report to Planning and Environment Committee

**To:** Chair and Members  
Planning and Environment Committee  
**From:** Scott Mathers, MPA, P.Eng.  
Deputy City Manager, Planning and Economic Development  
**Subject:** Victor Anastasiadis (c/o Zelinka Priamo Ltd.)  
566, 568, 572 and 578 Colborne Street  
File Number: Z-9775, Ward 13  
Public Participation Meeting  
**Date:** October 22, 2024

## Recommendation

That, on the recommendation of the Director, Planning and Development, the following actions be taken with respect to the application of Victor Anastasiadis (c/o Zelinka Priamo Ltd.) relating to the property located at 566, 568, 572 and 578 Colborne Street:

- (a) the proposed by-law attached hereto as Appendix "A" **BE INTRODUCED** at the Municipal Council meeting November 26, 2024, to amend Zoning By-law No. Z.-1, in conformity with the Official Plan, The London Plan, to change the zoning of the subject property **FROM** a Residential R3/Residential R11 (R3-2/R11) Zone **TO** a holding Residential R5 Special Provision (h-18\*R5-7(\_)) Zone;

**IT BEING NOTED**, that the above noted amendment is being recommended for the following reasons:

- i) The recommended amendment is consistent with the Provincial Planning Statement, 2024 (PPS), which encourages the regeneration of settlement areas and land use patterns within settlement areas that provide for a range of uses and opportunities for intensification and redevelopment. The PPS directs municipalities to permit all forms of housing required to meet the needs of all residents, present and future;
- ii) The recommended amendment conforms to The London Plan, including but not limited to the Key Directions, City Design and Building policies, and the Neighbourhoods Place Type policies.
- iii) The recommended amendment would permit an appropriate form of infill development at an intensity that is appropriate for the site and surrounding neighbourhood

## Executive Summary

### Summary of Request

The applicant has requested an amendment to the Zoning By-law Z.-1 to rezone the property from a Residential R3/Residential R11 (R3-2/R11) Zone to a Residential R5 Special Provision (R5-7(\_)) Zone.

### Purpose and the Effect of Recommended Action

The recommended action would permit the addition of two (2), 3-storey stacked townhouse buildings to the rear of the existing buildings at 572 and 578 Colborne Street, containing two (2) and six (6) units respectively. The recommended action would also permit the conversion of the existing lodging houses to three (3) units, an increase of two (2) units per building. The result would be an increase of **12 residential units** between the two buildings, in addition to the existing duplexes at 566 and 568 Colborne Street, for a total of 18 units on site.

Staff are recommending a holding provision that will ensure the development will not occur until such time as the City is in receipt of the Ministry of Citizenship and Multiculturalism's (MCM) compliance review letter for the Stage 1-2 Archaeological Assessment.

## Linkage to the Corporate Strategic Plan

This recommendation will contribute to the advancement of Municipal Council's 2023-2027 Strategic Plan in the following ways:

- **Strategic Plan Area of Focus: Wellbeing and Safety**, by promoting neighbourhood planning and design that creates safe, accessible, diverse, walkable, healthy, and connected communities.
- **Strategic Plan Area of Focus: Economic Growth, Culture, and Prosperity** by increasing residential occupancy and livability in the Core Area.

## Analysis

### 1.0 Background Information

#### 1.1 Previous Reports Related to this Matter

None.

#### 1.2 Planning History

A.054/17 – Variance application to permit a fourplex dwelling. The applicant was seeking to permit zero parking spaces whereas 4 are required. The Committee of Adjustment granted the request on May 1, 2017.

#### 1.3 Property Description and Location

The subject lands are located on the east side of Colborne Street, between Central Avenue and Waverly Place, in the Central London Planning District. The subject lands are rectangular in shape and have combined area of approximately 2,011 square metres, approximately 45 metres of frontage along Colborne Street, and a lot depth of approximately 45 metres. The lands are currently occupied by four (4) existing buildings, including two (2) lodging houses (572 & 578 Colborne Street) and two (2) duplexes (566 & 568 Colborne Street).

#### Site Statistics:

- Current Land Use: Residential
- Frontage: 45 metres
- Depth: 45 metres
- Area: 0.20 hectares
- Shape: Regular
- Located within the Built Area Boundary: Yes
- Located within the Primary Transit Area: Yes

#### Surrounding Land Uses:

- North: Residential
- East: Residential
- South: Residential
- West: Residential

#### Existing Planning Information:

- The London Plan Place Type: Neighbourhoods Place Type fronting a Neighbourhood Connector
  - Existing Zoning: Residential R3/Residential R11 (R3-2/R11) Zone



Figure 1- Aerial Photo of 566, 568, 572, 578 Colborne Street and surrounding lands.



Figure 2 - Streetview of 566, 568, 572, 578 Colborne Street (view looking east on Colborne Street)

## 2.0 Discussion and Considerations

### 2.1 Development Proposal

The subject lands are to be intensified with the addition of two (2), 3-storey stacked townhouse buildings attached to the rear of the existing lodging houses (572 and 578 Colborne Street). The lodging houses themselves are each proposed to be converted from one unit to 3 units. No changes are proposed to the duplex buildings at 566 and 568 Colborne Street, but the rear of all properties will support a common parking area. The lands currently consist of three separate parcels which are proposed to be consolidated.

Notably, there will be no meaningful change to how each of the buildings look from Colborne Street as the additions are proposed to only the rear of the two existing buildings.

The proposed development includes the following features:

- Land use: Residential
- Form: two (2) stacked townhouses, two (2) converted 3-unit dwellings, two (2) duplexes.
- Height: 3 storeys (11.5 metres)
- Residential units: 18
- Density: 78 units per hectare
- Building coverage: 36%
- Parking spaces: 9 surface spaces
- Bicycle parking spaces: 0 proposed
- Landscape open space: 37%
- Functional amenity space: 170 square metres

Additional information on the development proposal is provided in Appendix “B”.

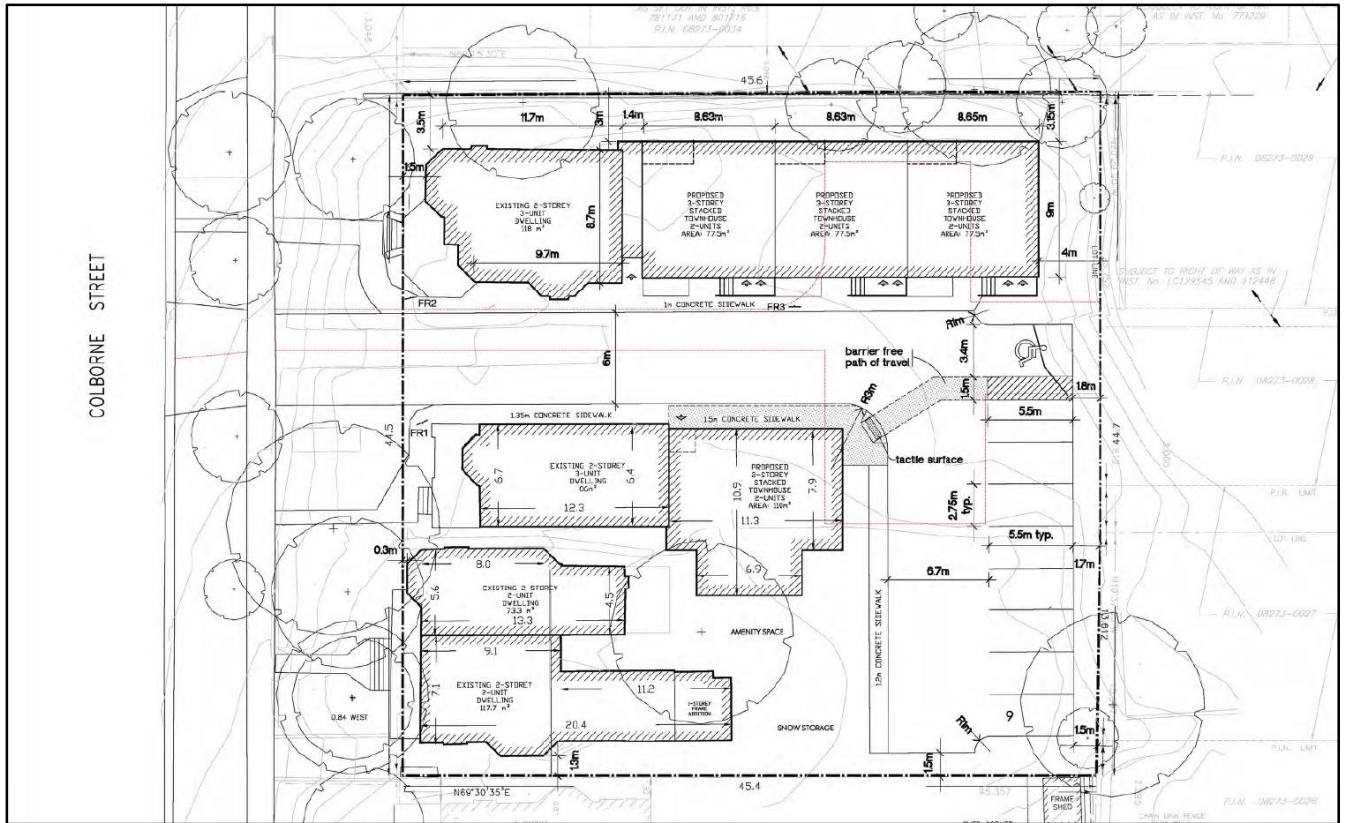


Figure 3 - Conceptual Site Plan (Received July 15, 2024)



Figure 4 – Elevations of proposed extension of 572 Colborne Street.





Figure 5 – Elevations of proposed extension of 578 Colborne Street.

Additional plans and drawings of the development proposal are provided in Appendix “C”.

## 2.2 Requested Amendment(s)

The applicant has requested an amendment to the Zoning Bylaw Z.-1 to rezone the property from Residential R3/Residential R11 (R3-2/R11) Zone to a Residential R5 Special Provision (R5-7(\_)) Zone.

The following table summarizes the special provisions that have been proposed by the applicant and those that are being recommended by staff.

Regulation (R5-7(_))	Required	Proposed
Front Yard Setback (min)	6.0 metres	0.3 metres (as existing)
Rear Yard Setback (min)	6.0 metres	4.0 metres
Permitted Uses	Cluster Stacked Townhouses	Converted Dwellings, Duplex Dwellings
South Interior Side Yard Setback (min)	6.0 metres	1.3 metres (as existing)
Density (min)	60 Units Per Hectare	80 Units Per Hectare
North Interior Side Yard Setback (min)	6.0 metres, including walls to windows to habitable rooms	3.0 metres, including walls to windows to habitable rooms

## 2.3 Internal and Agency Comments

The application and [associated materials](#) were circulated for internal comments and public agencies to review. Comments received were considered in the review of this application and are addressed in Section 4.0 of this report.

Key issues identified by staff and agencies included:

- Ministry Letter, Archaeological Assessment.

Detailed internal and agency comments are included in Appendix “D” of this report.

## 2.4 Public Engagement

On August 30, 2024, Notice of Application was sent to 276 property owners and residents in the surrounding area. Notice of Application was also published in the *Public*

*Notices and Bidding Opportunities* section of *The Londoner* on September 5, 2024. A “Planning Application” sign was also placed on the site.

There were zero responses received during the public consultation period.

## **2.5 Policy Context**

### ***The Planning Act and the Provincial Planning Statement, 2024***

The Provincial planning policy framework is established through the *Planning Act* (Section 3) and the *Provincial Planning Statement, 2024 (PPS)*. The *Planning Act* requires that all municipal land use decisions affecting planning matters shall be consistent with the *PPS*.

The mechanism for implementing Provincial policies is through the Official Plan, The London Plan. Through the preparation, adoption and subsequent Ontario Land Tribunal (OLT) approval of The London Plan, the City of London has established the local policy framework for the implementation of the Provincial planning policy framework. As such, matters of provincial interest are reviewed and discussed in The London Plan analysis below.

Both properties are Heritage Designated Properties under Part V of the *Ontario Heritage Act* as part of the *West Woodfield Heritage Conservation District*. The PPS directs that protected heritage property, which may contain built heritage resources or cultural heritage landscapes, shall be conserved. (4.6.1). The existing buildings are proposed to be retained, with the additions located at the rear to maintain character. Further, planning authorities shall not permit development and site alteration on adjacent lands to protected heritage property unless the heritage attributes of the protected heritage property will be conserved. (4.6.3).

The applicant submitted a Heritage Impact Assessment (HIA) as part of the complete application. Heritage staff have reviewed and agree with the research and conclusions of the Heritage Impact Assessment.

As the application for a Zoning By-law amendment complies with The London Plan, it is staff’s opinion that the application is consistent with the *Planning Act* and the *PPS*.

### ***The London Plan, 2016***

The London Plan (TLP) includes evaluation criteria for all planning and development applications with respect to use, intensity and form, as well as with consideration of the following (TLP 1577-1579):

1. Consistency with the Provincial Policy Statement and all applicable legislation.
2. Conformity with the Our City, Our Strategy, City Building, and Environmental policies.
3. Conformity with the Place Type policies.
4. Consideration of applicable guideline documents.
5. The availability of municipal services.
6. Potential impacts on adjacent and nearby properties in the area and the degree to which such impacts can be managed and mitigated.
7. The degree to which the proposal fits within its existing and planned context.

Staff are of the opinion that all the above criteria have been satisfied.

## **3.0 Financial Impact/Considerations**

None.

## 4.0 Key Issues and Considerations

### 4.1 Land Use

The proposed residential use is supported by the policies of the PPS 2024. The site is located within the Neighbourhoods Place Types of The London Plan with frontage on a Neighbourhood Connector (Colborne Street), in accordance with Map 1 – Place Types and Map 3 – Street Classifications.

Table 10 – Range of Permitted Uses provides the range of primary and secondary permitted uses that may be allowed within the Neighbourhoods Place Type by street classification (TLP 921\_). At this location, Table 10 permits a range of low-rise residential uses including: single detached dwellings, semi-detached dwellings, duplex dwellings, converted dwellings, townhouses, additional residential units, home occupations, and group homes. Within the Central London Planning District, additional uses are permitted including fourplexes, stacked townhouses and low-rise apartments. Staff is satisfied that the proposed stacked townhouse, duplex and converted dwelling uses are appropriate for the subject lands.

### 4.2 Intensity

The proposed residential intensity is consistent with the policies of the PPS 2024 that encourage all types of residential intensification, including redevelopment which results in a net increase in residential uses (2.2.1.2), compact form (2.4.1.3.c), and an appropriate mix of housing options and densities (2.3.1.3). The proposed 3-storey intensity is in conformity with the Neighbourhoods Place Type in The London Plan, as established in Table 11 – Range of Permitted Heights and the height increase is not a factor on the aesthetic of the existing heritage dwelling. Servicing is available for the proposed number of units and no concerns were raised regarding traffic, noise, parking or other negative impacts.

The specific policies for the Woodfield Neighbourhood provide for infill and intensification only where such development is clearly compatible with the character, scale and intensity of the low-rise residential neighbourhood in this area (TLP 1036\_). Area-specific zoning regulations such as, but not limited to, maximum floor area ratio, maximum dwelling size and on-site parking limitations may be applied to ensure that future development meets this objective. In this instance, the increase in density on the subject lands is compatible with the existing context of the neighbourhood, being that the subject lands are near the Central Avenue and Colborne Street intersection, which allows for greater intensification. The consolidated parcel allows for more comprehensive development and staff are satisfied no special provisions based on maximum floor area ratio, maximum dwelling size and on-site parking limitations are required to protect the site from over-intensification. Staff are therefore satisfied the recommended amendments are in conformity with the Woodfield Neighbourhood policies.

The subject lands are also located within a Near-Campus Neighbourhood which is subject to specific policies regarding intensification and increases in residential intensity in the Neighbourhoods Place Type. The following criteria apply when considering residential intensification within Near-Campus Neighbourhoods, in accordance with policy 968\_:

1. The proposed development is in conformity with the vision and planning goals for Near-Campus Neighbourhoods.
2. The proposed development is consistent with Tables 10 to 12 in the Neighbourhoods Place Type.
3. The development conforms to the Residential Intensification policies of this Plan, where those policies do not conflict with Near-Campus Neighbourhoods Policies.
4. The development conforms to any relevant Specific Policies of this chapter.
5. The development provides for an adequate amenity area that is appropriately shaped, configured, and located.
6. Mitigation measures are incorporated into the proposed building(s) and site

design which ensure that the amenity of surrounding residential land uses is not negatively impacted.

7. Significant heritage resources are protected and conserved where appropriate and necessary according to the Cultural Heritage policies of this Plan.
8. The proposal establishes a positive and appropriate example for similar locations within the Near-Campus Neighbourhoods areas.

Staff are satisfied that the use and associated residential intensification is appropriate for the subject lands. The proposed additions would allow for a moderate increase in intensity, from a total of 6 units spread out between 3 parcels to 18 units total for one consolidated parcel, without detracting from the character of the existing neighbourhood. Staff are therefore satisfied that recommended amendment and proposed development are in conformity with the Near-Campus Neighbourhoods policies of The London Plan.

The *West Woodfield Heritage Conservation District Plan* includes policies regarding land use and development patterns, including maintaining the residential amenity and human scale by ensuring that the low-density residential land use character remains dominant (4.1(a)). Where intensification is proposed, the policies within the plan promote adaptive reuse of the existing heritage building (4.1(d)). The recommended amendments facilitate the preservation and re-use of the existing heritage buildings, which are currently vacant and have fallen into disrepair. Further, the existing streetscape along Colborne Street is maintained by locating the proposed additions at the rear of the existing buildings.

### **4.3 Form**

The proposed built form is supported by the policies of the PPS 2024 and is consistent with the Neighbourhoods Place Type policies and the City Design policies of The London Plan by facilitating an appropriate form and scale of residential intensification that is compatible with the existing neighbourhood character (TLP 953\_2). The proposed rear additions are sensitive to the heritage buildings both on site and within the context of the broader West Woodfield Heritage Conservation District. One of the goals of the West Woodfield Heritage Conservation District Plan is to maintain the low-density residential character of the district as the predominant land use, while recognizing that certain areas of the District already have or are intended for a wider range of uses (3.1). The recommended amendments provide for rear additions to facilitate a total of 8 units, while converting the existing lodging houses that front the street into 3-unit converted dwellings. The two duplexes located at 566 and 568 Colborne Street will be unchanged, but the rear of all properties will be amalgamated to provide parking for the proposed additional units in the rear of the property. The Plan also guides infill development to be compatible with the heritage character and pedestrian scale of the District, and ensuring additions are subordinate to the original structure to allow the original heritage features and built form to take visual precedence on the street (3.1 and 4.2.1(c)).

The rear additions provide an opportunity for infill development that is compatible with the heritage character. The additions match the height of the existing buildings and will not detract from the pedestrian scale along Colborne Street. No significant modifications to the front facades of the existing buildings along Colborne Street are proposed, which will ensure the existing streetscape and pedestrian scale is maintained.

## **Conclusion**

The applicant has requested an amendment to the Zoning By-law Z.-1 to rezone the property from a Residential R3/Residential R11 (R3-2/R11) Zone to a holding Residential R5 Special Provision (h-18\*R5-7(\_)). Staff are recommending approval of the requested Zoning Bylaw amendment with special provisions.

The recommended action is consistent with the PPS 2024, conforms to The London Plan and will permit three storey rear additions to the existing buildings, and

conversions of the existing buildings, to increase the number of units on site from six (6) to 18.

**Prepared by:** Brent House  
Planner, Site Plans

**Reviewed by:** Catherine Maton, MCIP, RPP  
Manager, Planning and Development

**Recommended by:** Heather McNeely, MCIP, RPP  
Director, Planning and Development

**Submitted by:** Scott Mathers, MPA, P.Eng.  
Deputy City Manager, Planning and Economic  
Development

Copy:

Britt O'Hagan, Division Manager, Planning and Development

Mike Corby, Manager, Site Plans

Brent Lambert, Manager, Development Engineering

## Appendix A – Zoning Bylaw Amendment

Bill No. (number to be inserted by Clerk's Office)  
2024

By-law No. Z.-1-

A by-law to amend By-law No. Z.-1 to  
rezone an area of land located at 566,  
568, 572 & 578 Colborne Street

WHEREAS this amendment to the Zoning By-law Z.-1 conforms to the Official Plan;

NOW THEREFORE the Municipal Council of The Corporation of the City of London enacts as follows:

1. Schedule "A" to By-law No. Z.-1 is amended by changing the zoning applicable to lands located at 566, 568, 572, 578 Colborne Street, as shown on the attached map **FROM** a Residential R3/Residential R11 (R3-2/R11) Zone **TO** a holding Residential R5 (h-18\*R5-7(\_)) Zone.

2. Section Number 9.4 of the Residential R5 Zone is amended by adding the following Special Provisions:

R5-7(\_) 566, 568, 572, 578 Colborne Street

a. Permitted Uses:

1. Converted Dwellings
2. Duplex Dwellings

b. Regulations

1. Front Yard Setback (minimum) – 0.3 metres (as existing on the date of passing this by-law).
2. Rear Yard Setback (minimum) – 4.0 metres, including walls with windows to habitable rooms.
3. South Interior Side Yard Setback (minimum) – 1.3 metres (as existing on the date of passing this by-law).
4. North Interior Side Yard Setback (minimum) – 3.0 metres, including walls with windows to habitable rooms.
5. Density (maximum) – 80 Units Per Hectare.

3. This Amendment shall come into effect in accordance with Section 34 of the *Planning Act, R.S.O. 1990, c. P13*, either upon the date of the passage of this by-law or as otherwise provided by the said section.

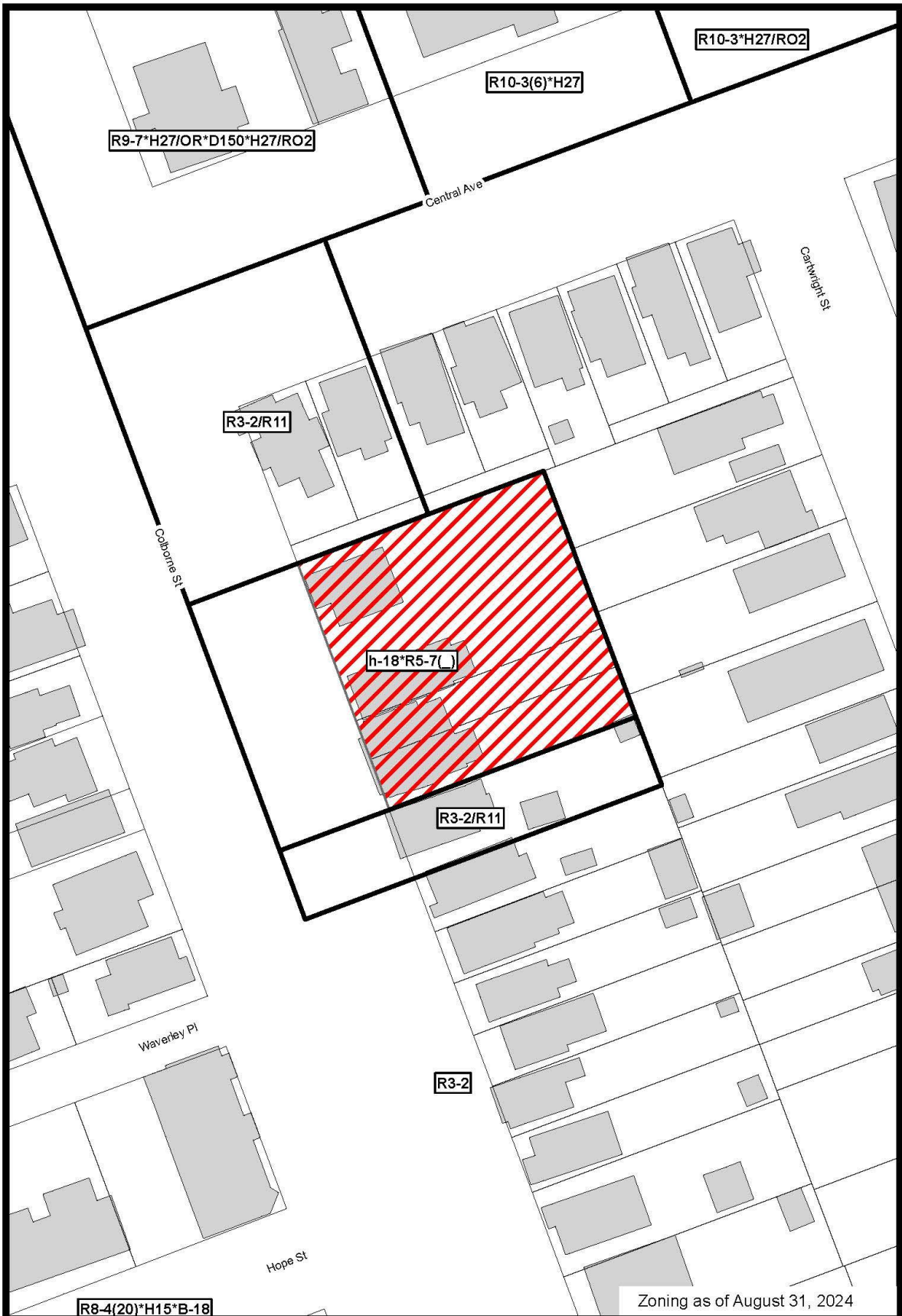
PASSED in Open Council on November 5, 2024 subject to the provisions of PART VI.1 of the *Municipal Act, 2001*.

Josh Morgan  
Mayor

Michael Schulthess  
City Clerk

First Reading – November 5, 2024  
Second Reading – November 5, 2024  
Third Reading – November 5, 2024

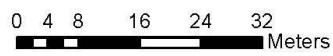
AMENDMENT TO SCHEDULE "A" (BY-LAW NO. Z.-1)



File Number: Z-9775  
 Planner: BH  
 Date Prepared: 2024/09/18  
 Technician: JI  
 By-Law No: Z.-1-

SUBJECT SITE 

1:800



Geodatabase



## Appendix B - Site and Development Summary

### A. Site Information and Context

#### Site Statistics

Current Land Use	Residential
Frontage	45 metres
Depth	45 metres
Area	0.20 hectares
Shape	Regular (rectangle)
Within Built Area Boundary	Yes
Within Primary Transit Area	Yes

#### Surrounding Land Uses

North	Residential
East	Residential
South	Residential
West	Residential

#### Proximity to Nearest Amenities

Major Intersection	Colborne Street/Central Avenue, ~51.9 metres
Dedicated cycling infrastructure	Colborne Street, ~19.6 metres
London Transit stop	Colborne at Central SB - #433, ~37.8 metres
Public open space	Victoria Park, ~525 metres
Commercial area/use	Oxbury Centre, ~3,800 metres
Food store	Food basics, ~3,800 metres
Community/recreation amenity	Carling Heights Community Centre, ~1,000 metres

### B. Planning Information and Request

#### Current Planning Information

Current Place Type	Neighbourhoods Place Type fronting a Neighbourhood Connector (Colborne Street)
Current Special Policies	Woodfield Neighbourhood
Current Zoning	Residential R3/Residential R11 (R3-2/R11) Zone

#### Requested Designation and Zone

Requested Place Type	N/A
Requested Special Policies	N/A
Requested Zoning	Residential R5 Special Provision (R5-7(_)) Zone

#### Requested Special Provisions

Regulation (R5-7(_))	Required	Proposed
Front Yard Setback (min)	6.0 metres	0.3 metres (as existing)
Permitted Uses	Cluster Stacked Townhouses	Converted Dwellings, Duplex Dwellings
South Interior Side Yard Setback (min)	6.0 metres	1.3 metres (as existing)
Density (min)	60 Units Per Hectare	80 Units Per Hectare
North Interior Side Yard Setback (min)	6.0 metres, including walls to windows to habitable rooms	3.0 metres, including walls to windows to habitable rooms

## C. Development Proposal Summary

### 1.1 Development Overview

The subject lands are to be intensified with the addition of two (2), 3-storey stacked townhouse buildings that are extensions of the existing lodging houses (572 and 578 Colborne Street). The lodging houses themselves are to be converted into 3-unit dwellings. There are no proposed changes to the attached duplex buildings (566 and 568 Colborne Street), but the rear of all properties will support a common parking area. While the lands currently comprise 3 properties, it is intended that they will be consolidated as a single parcel upon development.

Notably, there will be no meaningful change to how each of the buildings look from Colborne Street as the additions are proposed to only the rear of the two existing buildings.

### Proposal Statistics

Land use	Residential
Form	Two (2) stacked townhouses, two (2) 3-unit converted dwellings, two (2) duplexes
Height	3 Storeys (11.5 metres)
Residential units	18
Density	80 UPH
Building coverage	36%
Landscape open space	37%
Functional amenity space	~170 Metres <sup>2</sup>
New use being added to the local community	Yes

### Mobility

Parking spaces	9 above ground spaces
Vehicle parking ratio	0.5 Spaces per unit
New electric vehicles charging stations	Not included
Secured bike parking spaces	Not included
Secured bike parking ratio	N/A
Completes gaps in the public sidewalk	N/A
Connection from the site to a public sidewalk	Yes
Connection from the site to a multi-use path	N/A

### Environment

Tree removals	16
Tree plantings	Not included
Tree Protection Area	No
Loss of natural heritage features	No
Species at Risk Habitat loss	No
Minimum Environmental Management Guideline buffer met	NA
Existing structures repurposed or reused	Yes
Green building features	Unknown

## Appendix C – Additional Plans and Drawings



Figure 6 – 572 Colborne Street North and South Elevations



Figure 7 – 572 & 578 Colborne Street East and West Elevations

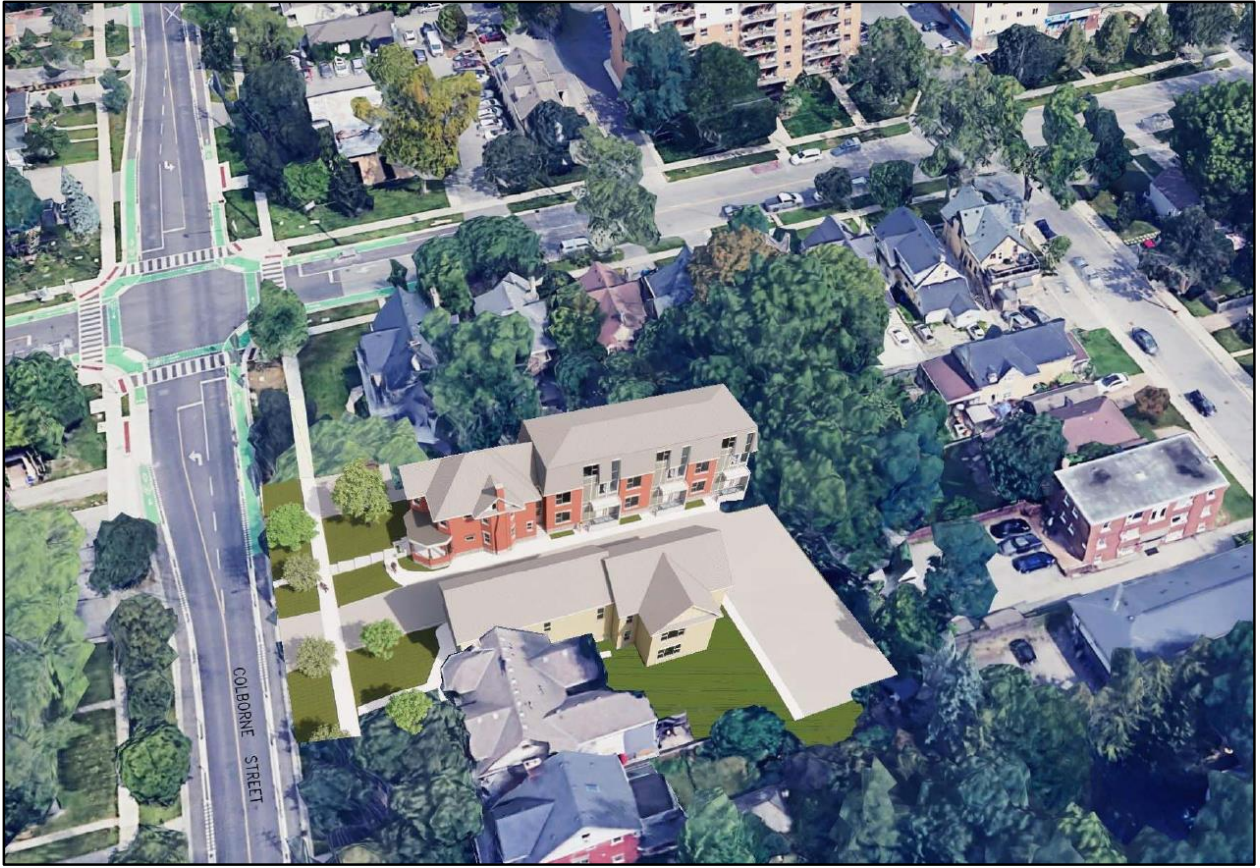
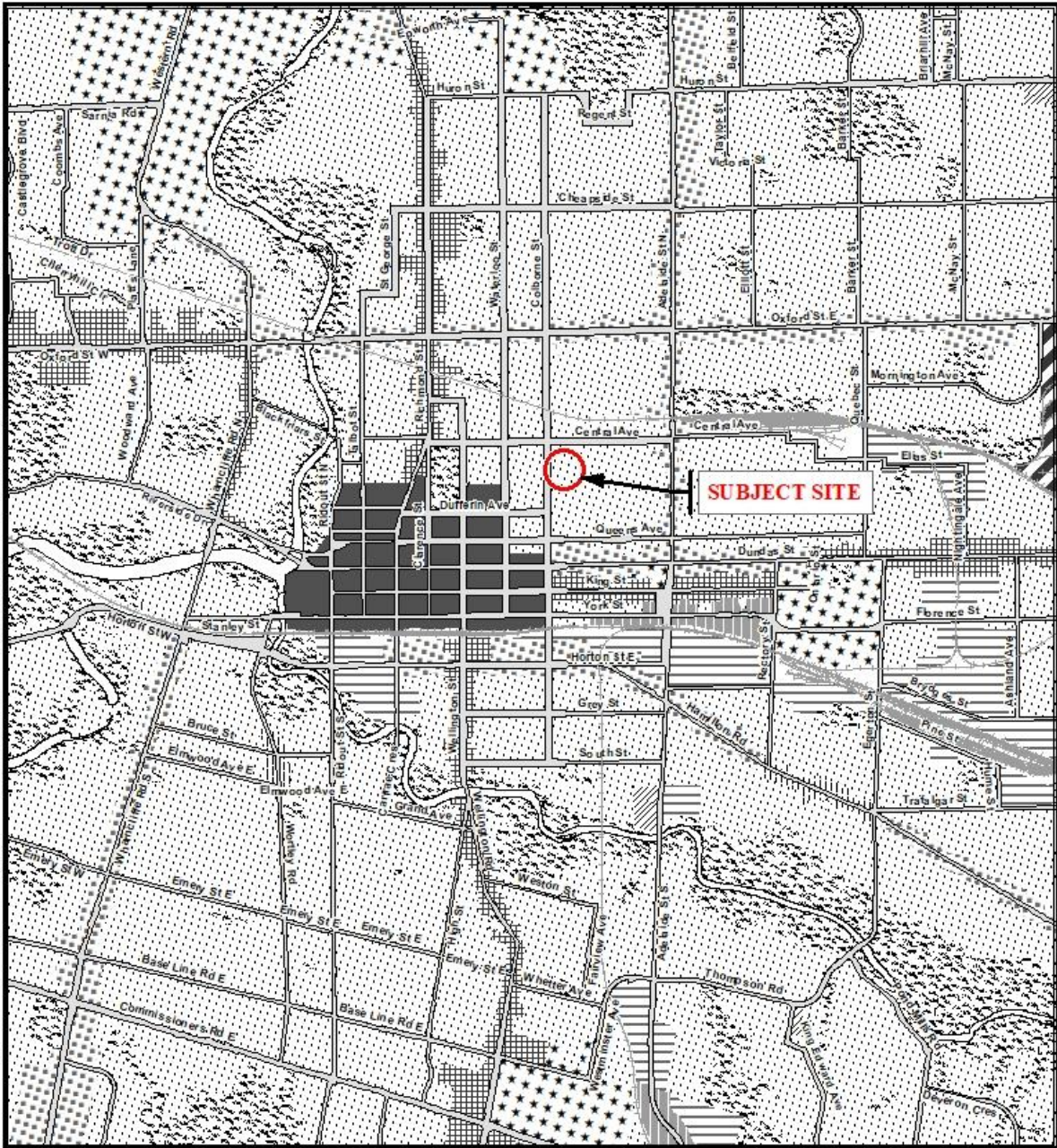


Figure 8 – Massing Model of proposed additions to 572 & 578 Colborne Street.



Figure 9 – Perspective View of proposed addition at 578 Colborne Street



**Legend**

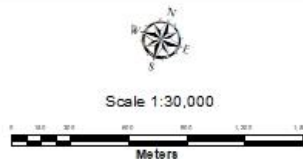
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|------------------------|--------------------------|---|
| Downtown               | Future Community Growth  | Environmental Review                    |
| Transit Village        | Heavy Industrial         | Farmland                                |
| Shopping Area          | Light Industrial         | Rural Neighbourhood                     |
| Rapid Transit Corridor | Future Industrial Growth | Waste Management Resource Recovery Area |
| Urban Corridor         | Commercial Industrial    | Urban Growth Boundary                   |
| Main Street            | Institutional            |   |
| Neighbourhood          | Green Space              |   |

*This is an excerpt from Planning & Development's working consolidation of Map 1 - Place Types of the London Plan, with added notations.*

**CITY OF LONDON  
Official Plan**

**LONDON PLAN MAP 1  
- PLACE TYPES -**

PREPARED BY: Planning & Development



**File Number:** Z-9775

**Planner:** BH

**Technician:** JI

**Date:** 2024/09/18

Project Location: E:\Planning\Projects\p\_officialplan\work\consolid00\excerpts\_LondonPlan\mxd\Z-9775\_Map1\_PlaceTypes.mxd



**COUNCIL APPROVED ZONING FOR THE SUBJECT SITE: R3-2/R11**

1) **LEGEND FOR ZONING BY-LAW Z-1**

- |  |   |
|--|---|
| <ul style="list-style-type: none"> <li>R1 - SINGLE DETACHED DWELLINGS</li> <li>R2 - SINGLE AND TWO UNIT DWELLINGS</li> <li>R3 - SINGLE TO FOUR UNIT DWELLINGS</li> <li>R4 - STREET TOWNHOUSE</li> <li>R5 - CLUSTER TOWNHOUSE</li> <li>R6 - CLUSTER HOUSING ALL FORMS</li> <li>R7 - SENIOR'S HOUSING</li> <li>R8 - MEDIUM DENSITY/LOW RISE APTS.</li> <li>R9 - MEDIUM TO HIGH DENSITY APTS.</li> <li>R10 - HIGH DENSITY APARTMENTS</li> <li>R11 - LODGING HOUSE</li> <br/> <li>DA - DOWNTOWN AREA</li> <li>RSA - REGIONAL SHOPPING AREA</li> <li>CSA - COMMUNITY SHOPPING AREA</li> <li>NSA - NEIGHBOURHOOD SHOPPING AREA</li> <li>BDC - BUSINESS DISTRICT COMMERCIAL</li> <li>AC - ARTERIAL COMMERCIAL</li> <li>HS - HIGHWAY SERVICE COMMERCIAL</li> <li>RSC - RESTRICTED SERVICE COMMERCIAL</li> <li>CC - CONVENIENCE COMMERCIAL</li> <li>SS - AUTOMOBILE SERVICE STATION</li> <li>ASA - ASSOCIATED SHOPPING AREA COMMERCIAL</li> <br/> <li>OR - OFFICE/RESIDENTIAL</li> <li>OC - OFFICE CONVERSION</li> <li>RO - RESTRICTED OFFICE</li> <li>OF - OFFICE</li> </ul> | <ul style="list-style-type: none"> <li>RF - REGIONAL FACILITY</li> <li>CF - COMMUNITY FACILITY</li> <li>NF - NEIGHBOURHOOD FACILITY</li> <li>HER - HERITAGE</li> <li>DC - DAY CARE</li> <br/> <li>OS - OPEN SPACE</li> <li>CR - COMMERCIAL RECREATION</li> <li>ER - ENVIRONMENTAL REVIEW</li> <br/> <li>OB - OFFICE BUSINESS PARK</li> <li>LI - LIGHT INDUSTRIAL</li> <li>GI - GENERAL INDUSTRIAL</li> <li>HI - HEAVY INDUSTRIAL</li> <li>EX - RESOURCE EXTRACTIVE</li> <li>UR - URBAN RESERVE</li> <br/> <li>AG - AGRICULTURAL</li> <li>AGC - AGRICULTURAL COMMERCIAL</li> <li>RRC - RURAL SETTLEMENT COMMERCIAL</li> <li>TGS - TEMPORARY GARDEN SUITE</li> <li>RT - RAIL TRANSPORTATION</li> <br/> <li>"h" - HOLDING SYMBOL</li> <li>"D" - DENSITY SYMBOL</li> <li>"H" - HEIGHT SYMBOL</li> <li>"B" - BONUS SYMBOL</li> <li>"T" - TEMPORARY USE SYMBOL</li> </ul> |
|--|---|

**CITY OF LONDON**

PLANNING SERVICES / DEVELOPMENT SERVICES

**ZONING  
BY-LAW NO. Z.-1  
SCHEDULE A**



THIS MAP IS AN UNOFFICIAL EXTRACT FROM THE ZONING BY-LAW WITH ADDED NOTATIONS

FILE NO:

Z-9775

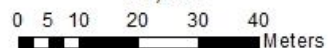
BH

MAP PREPARED:

2024/09/18

JJ

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## Appendix D – Internal and Agency Comments

### Heritage – Received September 5, 2024

The properties at 566-578 Colborne Street are designated pursuant to Part V of the Ontario Heritage Act, as they are included within the West Woodfield Heritage Conservation District. In addition, the properties are identified as having archaeological potential on the City's Archaeological Management Plan.

Heritage staff have received and reviewed the following reports submitted as a part of Z-9775:

- Zelinka Priamo Ltd., Heritage Impact Assessment – 566-578 Colborne Street, July 12, 2024
- Lincoln Environmental Consulting Corp., Stage 1-2 Archaeological Assessment of 572 Colborne Street, London, Ontario (P1289-0451-2023) August 21, 2024

### Heritage Impact Assessment

Heritage staff have reviewed and agree with the research and conclusions of the Heritage Impact Assessment. Staff are in agreement that the proposed development helps to “demonstrate that demand for new housing can be accommodated while remaining sensitive to important heritage assets.” Heritage staff are in agreement with the recommendations included within Section 6 of the HIA which include:

- “During the detailed design stage, it is recommended that material details and architectural features be used to further differentiate the proposed additions from the existing heritage buildings. For example, the transition from old to new brick could be interrupted by a “reveal”, or a recess dividing two parts. This element could be a stone or concrete seam, or a groove in the masonry veneer. Similar treatment should be applied at the foundation or building base, and at the roof if possible.”
- “For the rear extension to 578 Colborne Street, it is recommended that the portions of the building not finished in red brick (at and above unit entrances) be simplified with less interruptions and geometric complexity at the cornice line, and few mullions on the glazed façade. Spandrel panels could be replaced with glass or brick if possible. The height and geometric complexity of these sections may make them more visually prominent from Colborne Street.”

### Archaeological Assessment

Heritage staff have received and reviewed the archaeological assessment and note the findings of the report: “No archaeological resources were identified during the Stage 2 archaeological assessment and as such no further archaeological assessment of the study area is recommended.” Please note, the City requires the Ministry's review letter confirming acceptance of the report into the Ontario Public Register of Archaeological Reports before the property can be cleared of its archaeological potential.

### Additional Comments

Heritage staff are supportive of the application which seeks to retain the existing heritage buildings and intensify the properties through rear additions to the properties. Heritage have the following recommendations:

- The mitigation recommendations identified in Section 6 of the Heritage Impact Assessment should be implemented. Staff will be seeking implementation of these recommendations during the Site Plan process for this development;
- Heritage Alteration Permit approval will be required for the construction of the new rear additions, and any alterations to the exteriors of the existing buildings. HAP approval should be obtained during the Site Plan process;
- The h-18 holding provision should be applied to the property until such time that the Ministry has accepted the Stage 1-2 Archaeological Assessment.

### Ecology – Received September 9, 2024

#### Major Issues Identified

- None

#### Matters for OPA/ZBA

- None

#### Matters for Site Plan

- None

#### Ecology – Complete Application Requirements

- None

#### Notes

- Avoid tree removal within the active bat roosting period (April 30 – September 1) to reduce potential interactions with Endangered bat species, to avoid contravention of the Endangered Species Act.
- Avoid vegetation removal within the active breeding bird period (April 1 – August 30) to avoid disturbing nesting birds and contravening the Migratory Bird Convention Act.

### **Engineering – Received September 10, 2024**

**Engineering has no concerns and support the rezoning to this site.** However, the following with regards to the reports are to be addressed at the future site plan application Stage.

#### Stormwater

1. The submitted pre-development Tc appears to be incorrect. Figure 5.3 shall be used to establish the time of concentration for any site uses other than single family residential. (Please note, if the c-value is outside of the range of the curve on Figure 5.3, i.e. 0.39 or lower, a Tc of 23 minutes should be used.) The Consultant is to review and resubmit the SWM calculations for further review.
2. The Consultant is to update any stormwater servicing elements (storage, orifice control, etc.) within the strategy which may occur as a result of the updated calculations.
3. The post development calculation sheet contains typos where it identified all findings to be pre-development. The reports and drawings submitted should be thoroughly reviewed by the consulting engineer for errors.

#### Water

4. As indicated within the Planning and Design Report, the lands shall be consolidated as a single parcel upon development. As the City of London permits one water service pipe from the municipal watermain to the property line, the site shall service the proposed townhouse development utilizing a single connection to the municipal distribution system. The multiple connections proposed within the Servicing Brief are not permitted by Water Engineering.
5. The Owner's Engineer shall ensure that the existing water services to the structures are decommissioned to City Standards. Additionally, ensure that the decommissioned services are noted on the civil drawings for the proposed development.
6. Water servicing to the units shall be internal to the site. Direct connection to the municipal watermain on Colborne Street is not permitted.
7. Water servicing to the townhouse development shall be to City Standard 7.9.4.
8. A water servicing report will be required, with hydraulic modelling results addressing residential water demands, water quality, fire flows and resulting pressures.

#### Matters for Site Plan

- Please note that all engineering related comments provided during SPC 24-006, and 21-day review are still Applicable. However, As indicated within the Planning and Design Report, the lands shall be consolidated as a single parcel upon development, Water engineering direction is the site shall service the proposed townhouse development utilizing a single connection to the municipal distribution system. **The multiple connections proposed within the Servicing Brief are not permitted by Water Engineering.**



## UTRCA – Received September 3, 2024

- The subject lands **are not** affected by any regulations (Ontario Regulation 157/06) made pursuant to Section 28 of the *Conservation Authorities Act*.

## Urban Design – Received September 4, 2024

Urban Design is generally supportive of the proposed form of development and has **no zoning related comments**.

### Matters for Site Plan

Provide adequate buffering and screening (e.g., landscaping, board on board fencing etc.) to avoid noise and privacy issues and headlights shining into the units that are facing the driveway and the parking area to the north. TLP 235

## Parks Planning – Received September 9, 2024

1. Major Issues
  - None.
2. Matters for OPA/ZBA
  - None.
3. Matters for Site Plan
  - Parkland dedication has not been taken for this site. It is to be noted that the applicant, as a condition of site plan approval, will be required to provide parkland dedication in the form of cash-in-lieu pursuant to By-law CP-25.

## Landscape Architecture – Received September 9, 2024

The Development and Planning Landscape Architect has reviewed the Tree Preservation Report for 566-578 Colborne Street prepared for Anast Holdings by MTE Consultants Inc. No issues were identified in the format of the report.

4. Major Issues
  - No potential grounds for refusal, or issues that could require significant changes to the proposal.
5. Matters for Site Plan
  - One boundary tree was identified, #8, in the tree preservation plan, consent to injure or remove boundary trees is a requirement of Site Plan approval. A recommendation for approval will be forwarded for Site Plan Review. . If consent cannot be obtained from co-owner, then a non-disturbance setback will need to be established at the tree's critical root zone limit as determined by dbh,. 10m
  - A recommendation for proof of payment for the coordinated removal of city trees will be forwarded for Site Plan review for the removal of three CoL trees #27, 28 and 32. Proof of payment issued by Forestry Operations requirement of Site Plan approval.
  - Replacement trees to be recommendation to Site Plan Review based on total dbh removed. 446cm dbh is proposed for removal, in accordance with LP Policy 399, 45 replacement trees are required. However, the city is in the process of developing a bylaw to implement Policy 399, so the follow ratio will be applied: 1 replacement tree for a trunk diameter of 50.99 cm, 2 replacement trees for a trunk diameter between 51cm and 60cm, and 1 additional replacement tree for every 10cm of trunk diameter thereafter to a maximum of 11 replacement trees. To this end 11 replacement trees would

be required. Tree planting required as part of the planning and development approvals process may be counted as replacement trees as required by these policies. Where there is insufficient space on the same Site to plant all the number of Replacement Trees, a cash-in-lieu fee will be charge calculated by multiplying the number of Replacement Trees that could not be planted on site due to insufficient space by \$350 per tree

#### **Site Plan – Received September 9, 2024**

##### **Major Issues:**

- None.

##### **Matters for OPA/ZBA:**

- Special Provisions required for Front Yard Depth, Interior Yard Depth, and Density.
- Demonstrate that all properties are merged on title, via an opinion from your lawyer. Should they not be merged, additional considerations for special provisions are required. In addition, easements for the part lots 566 & 568 Colborne St may be required.

##### **Matters for Site Plan:**

- Provide 1.5m pedestrian walkways on both sides of the drive-aisle for the new areas of development (Site Plan Control By-law Table 7.1).
- Show barrier free signage and details (Site Plan Control By-law, Figure 7.3).
- Show fire route signage and details (Site Plan Control By-law, Figure 6.4).
- Separate snow storage and amenity space (Site Plan Control By-law, Section 1.5).
- Continue to provide 1.5m between parking areas and property lines.
- Specify the waste collection method, screening, storage location and collection point (Site Plan Control By-law), 10.2.

#### **London Hydro – Received September 3, 2024**

- London Hydro has no objection to this proposal or possible official plan and/or zoning amendment. Any new or relocation of the existing service will be at the expense of the owner.

## **Appendix E – Public Engagement**

On August 30, 2024, Notice of Application was sent to 276 property owners and residents in the surrounding area. Notice of Application was also published in the Public Notices and Bidding Opportunities section of The Londoner on September 5, 2024. A “Planning Application” sign was also placed on the site.

There were zero responses received during the public consultation period. Comments received were considered in the review of this application and are addressed in Section 4.0 of this report.

## Report to Planning and Environment Committee

**To:** Chair and Members  
Planning and Environment Committee  
**From:** Scott Mathers, MPA, P.Eng.  
Deputy City Manager, Planning and Economic Development  
**Subject:** Mike Tkaczyk (c/o Zelinka Priamo Ltd.)  
313-323 Horton Street E  
File Number: Z-9616, Ward 13  
Public Participation Meeting  
**Date:** October 22, 2024

## Recommendation

That, on the recommendation of the Director, Planning and Development, the following actions be taken with respect to the 313-323 Horton Street East:

- (a) the proposed by-law attached hereto as Appendix "A" **BE INTRODUCED** at the Municipal Council meeting November 5, 2024 to amend Zoning By-law No. Z.-1, in conformity with the Official Plan, The London Plan, to change the zoning of the subject property **FROM** an Arterial Commercial (AC4) Zone, **TO** a Holding Business District Commercial Special Provision (h-18\*BDC(\_)) Zone;
- (b) The Site Plan Approval Authority **BE REQUESTED** to consider the following design issues through the site plan process:
  - i) Retain the vehicular passageway with openings at grade allowing light and ventilation into the spaces and alleviating potential CPTED issues;
  - ii) Retain the terraces on the 7<sup>th</sup> storey along the north and east facades;
  - iii) Retain a step-down to 6 and 7-storeys to the rear of the building;
  - iv) Consider replacing the single door with double door for the principal residential lobby entrance facing Horton Street East to promote accessibility and wayfinding;
  - v) Provide an adequate all-season landscape buffer along the south property line to avoid any negative impacts on the private amenity spaces of the adjacent residential uses;
  - vi) Add all or a portion of the long-term bicycle parking on the ground floor to provide convenient access for cyclists without needing to use elevators between the storage areas and ground floor;
  - vii) Ensure permission is received from the Transportation Division to allow the proposed lay-by in the City right-of-way; and
  - viii) Incorporate green infrastructure and/or features for Low Impact Development (LID) into the site design.

**IT BEING NOTED**, that the above noted amendment is being recommended for the following reasons:

- i) The recommended amendment is consistent with the Provincial Planning Statement, 2024 (PPS), which encourages the regeneration of settlement areas and land use patterns within settlement areas that provide for a range of uses and opportunities for intensification and redevelopment. The PPS directs municipalities to permit all forms of housing required to meet the needs of all residents, present and future;
- ii) The recommended amendment conforms to The London Plan, including, but not limited to the Key Directions, City Design and Building policies, the Urban Corridor Place Type policies, and the SoHo Main Street Segment policies;
- iii) The recommended amendment conforms to the policies of the SoHo Community Improvement Area Plan;
- iv) The recommended amendment facilitates an appropriate form of development at an intensity that is appropriate for the context of the site and surrounding neighbourhood;

- v) The recommended amendment facilitates intensification within the Built-Area Boundary and the Primary Transit Area with an appropriate form of infill development; and
- vi) The recommended amendment supports Council's commitment to increase housing supply and affordability

## Executive Summary

### Summary of Request

The applicant has requested an amendment to Zoning By-law Z.-1 to rezone the property from an Arterial Commercial (AC4) Zone to a Business District Commercial Special Provision (BDC( )) Zone. Requested special provisions include a maximum height of 27.0 metres, a maximum density of 530 unit per hectare, and a minimum rear yard setback of 4.5 metres.

### Purpose and the Effect of Recommended Action

Staff are recommending approval of the requested Zoning By-law amendment with special provisions to permit the development of the 8-storey mixed-use apartment building with **107 residential units** and 179 square metres of ground floor commercial space.

## Linkage to the Corporate Strategic Plan

This recommendation supports the following Strategic Areas of Focus:

- **Housing and Homelessness**, by ensuring London's growth and development is well-planned and considers use, intensity, and form.
- **Wellbeing and Safety**, by promoting neighbourhood planning and design that creates safe, accessible, diverse, walkable, healthy, and connected communities.
- **Housing and Homelessness**, by increasing access to a range of quality, affordable, and supportive housing options that meet the unique needs of Londoners.

## Analysis

### 1.0 Background Information

#### 1.1 Previous Reports Related to this Matter

None.

#### 1.2 Planning History

None.

#### 1.3 Property Description and Location

The subject lands are located on the south side of Horton Street East, between Wellington Street and Waterloo Street, in the Central London Planning District. The site has a total area of approximately 2.0 hectares, with 40.3 metres of frontage along Horton Street East. The subject lands are currently occupied by vacant commercial buildings with surface parking areas.

The surrounding neighbourhood includes commercial and industrial uses across Horton Street East to the north; commercial, office and services uses to the east; low density residential development to the south; and commercial/office and low-to-medium density residential uses to the west.

Horton Street East is classified as a Civic Boulevard on Map 3 – Street Classifications of The London Plan. Horton Street East is a four-lane road with sidewalks on both sides, with a traffic volume of approximately 25,000 vehicles per day, and access to LTC transit routes, with several bus stops located at the intersection of Wellington Street and Horton Street East, and Waterloo Street and Horton Street East. Further, there is a pedestrian crosswalk provided across Horton Street E at the intersection of Wellington Street and Horton Street East.

### Site Statistics:

- Current Land Use: Vacant commercial buildings
- Frontage: 40.3 metres
- Depth: 50.7 metres
- Area: 2.0 hectares
- Shape: Regular (rectangle)
- Located within the Built Area Boundary: Yes
- Located within the Primary Transit Area: Yes

### Surrounding Land Uses:

- North: Commercial and industrial
- East: Commercial, offices and service
- South: Low density residential
- West: Commercial/office and low-to-mid rise residential

### Existing Planning Information:

- The London Plan Place Type: Urban Corridor Place Type on a Civic Boulevard
- Existing Special Policies: SoHo Main Street Specific Segment
- Existing Zoning: Arterial Commercial (AC4) Zone

Additional site information and context is provided in Appendix “B”.



Figure 1- Aerial Photo of Subject Site and surrounding lands



Figure 2 - Streetview of the Existing Vacant Commercial Building at 323 Horton Street East



Figure 3 - Streetview of the 2 Vacant Commercial Buildings at 313 and 317 Horton Street East

## 2.0 Discussion and Considerations

### 2.1 Original Development Proposal

The original development consisted of a conversion of the existing building at 323 Horton Street East into a mixed-use building with residential dwelling units on all floors. Specifically, the exterior of the building was proposed to be updated with modern masonry and stone finishes. The interior of the building was proposed to be converted to residential uses, while retaining approximately 66.9 square metres of the ground floor for commercial uses. A total of 15 residential units and one (1) commercial unit were proposed to be accommodated within the existing building.

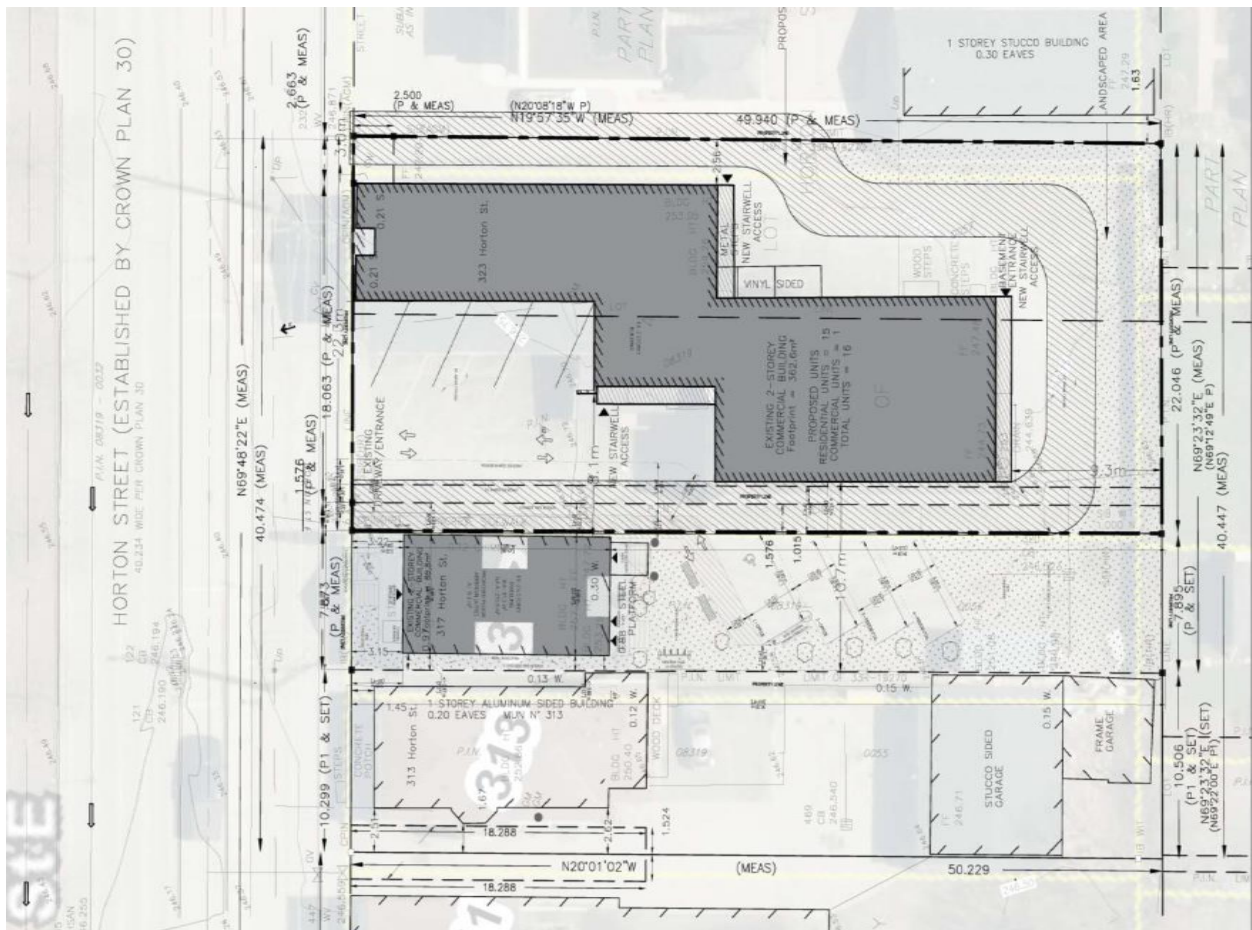


Figure 4 - Site Plan of Original Proposed Development



Figure 5 – Rendering of Proposed Original Development

## 2.2 Revised Development Proposal

Following discussions with City staff, a revised development proposal was submitted which includes the adjacent properties at 313 and 317 Horton Street East for a more comprehensive redevelopment. The proposed development consists of an 8-storey mixed-use building with 107 dwelling units, 179 square metres of commercial space on the ground floor, landscaped areas, indoor and outdoor amenity spaces, 2 surface accessible parking spaces, and bicycle parking in the interior of the building. The primary entrance is oriented to Horton Street East, with a secondary entrance at the rear of the building. Vehicular access is provided by a full turn driveway from Horton Street East under a cantilevered portion of the proposed building.

The proposed development includes the following features:

- Land use: Mixed-use building
- Form: Apartment building
- Height: 8-storeys (27.0 metres)



- Residential units: 107
- Density: 530 units per hectare
- Building coverage: 42%
- Parking spaces: 2 surface (accessible spaces)
- Bicycle parking spaces: 97 long-term spaces (0.9 spaces per unit), 11 short-term spaces (0.1 spaces per unit)
- Landscape open space: 31.2%

Additional information on the development proposal is provided in Appendix “B”.

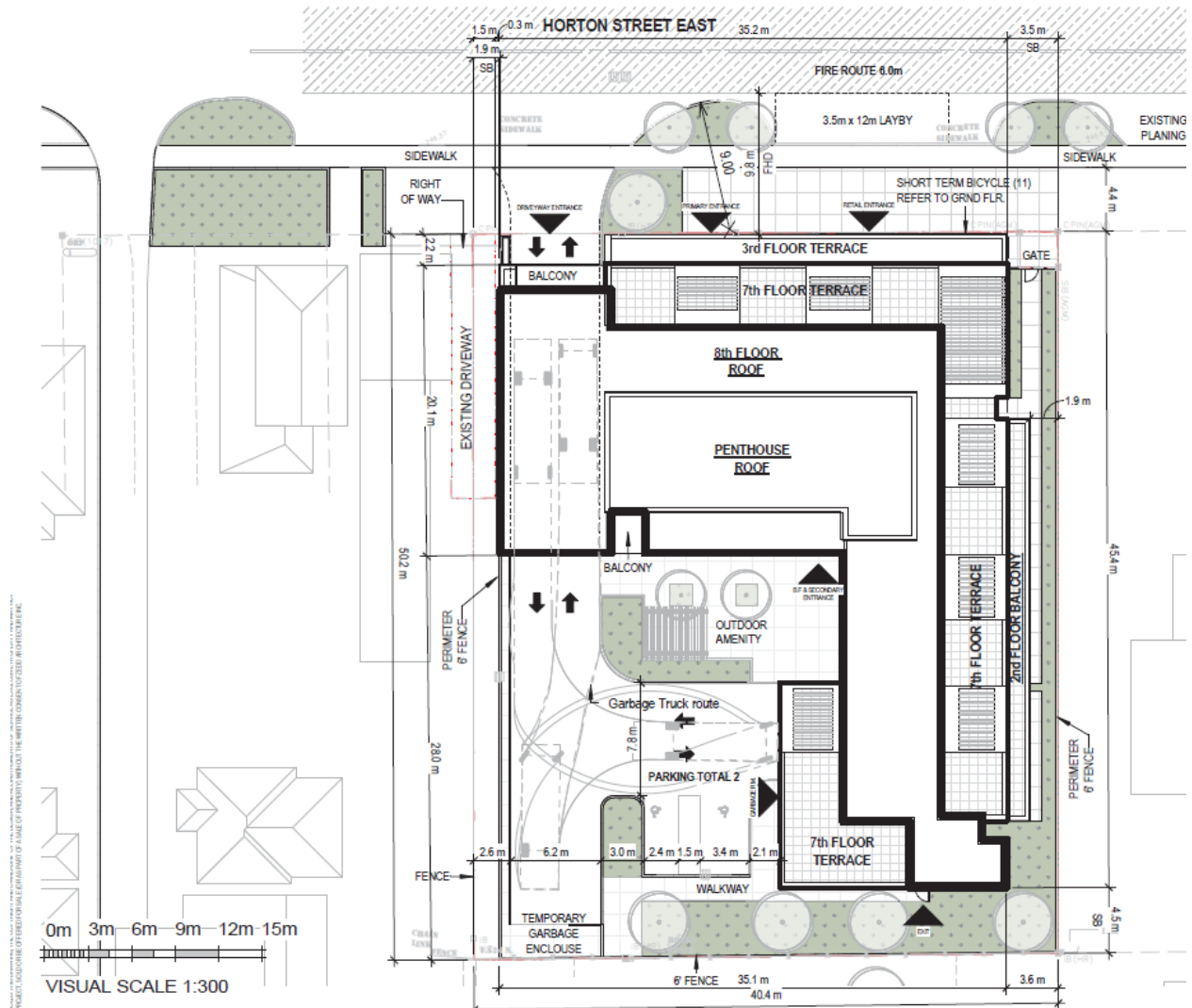


Figure 6 - Conceptual Site Plan (April 2024)



Figure7 – Rendering of proposed building – southwest view from Horton St E (April 2024)



Figure 8 – Rendering of proposed building – southeast view from Horton St E (April 2024)



Figure 9 – Rendering of proposed building – rear view (April 2024)

Additional plans and drawings of the development proposal are provided in Appendix “C”.

## 2.2 Requested Amendment(s)

The applicant has requested an amendment to the Zoning By-law Z.-1 to rezone the property from an Arterial Commercial (AC4) Zone, to a Business District Commercial Special Provision (BDC(\_)) Zone.

The following table summarizes the special provisions that have been proposed by the applicant.

Regulation (BDC Zone)	Required	Proposed	Recommended
Height (maximum)	10.0m	27.0m	27.0m
Density (maximum)	N/A	530uph	530uph
Rear Yard Setback (minimum)	12.6m	4.5m	4.5m

The following table summarizes the special provisions that are being recommended by staff.

Regulation (BDC Zone)	Required	Proposed	Recommended
East Interior Side Yard Setback (minimum)	0.0m	3.5m	3.5m
West Interior Side Yard Setback (minimum)	0.0m	1.9m	1.9m
Encroachments for balconies and canopies	A maximum of 1.5m projection no closer than 3.0m to the lot line for balconies and 0.5m maximum	N/A	To permit unlimited encroachments for balconies and canopies.

Regulation (BDC Zone)	Required	Proposed	Recommended
	projection for canopies		
Front Yard Setback Above the 3 <sup>rd</sup> Storey	N/A	0.0m	2.0m

### 2.3 Internal and Agency Comments

The application and associated materials were circulated for internal comments and public agencies to review. Comments received were considered in the review of this application; however, no major concerns were identified by staff.

Detailed internal and agency comments are included in Appendix “D” of this report.

### 2.4 Public Engagement

On May 31, 2023, Notice of Application was sent to property owners and residents in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on May 31, 2023. A “Planning Application” sign was also placed on the site.

On July 18, 2024, a Revised Notice of Application was sent to property owners and residents in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on July 18, 2024. A “Planning Application” sign was also placed on the site.

There was one (1) response received during the public consultation. Detailed public comments are included in Appendix “E” of this report. Concerns expressed including parking, lack of privacy, and intensity.

### 2.5 Policy Context

#### ***The Planning Act and the Provincial Planning Statement, 2024***

The Provincial planning policy framework is established through the *Planning Act* (Section 3) and the *Provincial Planning Statement, 2024 (PPS)*. The *Planning Act* requires that all municipal land use decisions affecting planning matters shall be consistent with the *PPS*.

The mechanism for implementing Provincial policies is through the Official Plan, The London Plan. Through the preparation, adoption and subsequent Ontario Land Tribunal (OLT) approval of The London Plan, the City of London has established the local policy framework for the implementation of the Provincial planning policy framework. As such, matters of provincial interest are reviewed and discussed in The London Plan analysis below.

As the application for a Zoning By-law amendment complies with The London Plan, it is staff’s opinion that the application is consistent with the *Planning Act* and the *PPS*.

#### ***The London Plan, 2016***

The London Plan (TLP) includes evaluation criteria for all planning and development applications with respect to use, intensity and form, as well as with consideration of the following (TLP 1577-1579):

1. Consistency with the Provincial Policy Statement and all applicable legislation.
2. Conformity with the Our City, Our Strategy, City Building, and Environmental policies.
3. Conformity with the Place Type policies.
4. Consideration of applicable guideline documents.
5. The availability of municipal services.
6. Potential impacts on adjacent and nearby properties in the area and the degree to which such impacts can be managed and mitigated.

7. The degree to which the proposal fits within its existing and planned context.

Staff are of the opinion that all the above criteria have been satisfied.

### ***SoHo Community Improvement Plan***

Community Improvement Plans are intended to provide City Council with the necessary tools to stimulate reinvestment and redevelopment, inspire appropriate infill and intensification, coordinate planning efforts, improve the physical infrastructure, support community economic development, preserve neighbourhood and cultural heritage values, and lead to the establishments of an improved neighbourhood.

The SoHo Community Improvement Plan area includes lands along the edges of the downtown and the Thames River. This plan has a vision, principals and strategic directions to help provide opportunities to meet both existing and future challenges and to provide building blocks that will rest on the neighbourhood's already strong foundation.

The two main transportation corridors of Wellington Street and Horton St E form a focal point in the Mixed-use Mainstreet District in the Neighbourhood Places component of this plan. These places form the heart of every community promoting improved opportunities for retail/commercial and greater connections to public transit. New development along these frontages will implement greater mixed-use zoning and an expanded range of permitted uses.

The SoHo Community Improvement Plan has been reviewed and it is staff's opinion that the proposed Zoning By-law amendment is consistent with it.

## **3.0 Financial Impact/Considerations**

### **3.1 Financial Impact**

There are no direct municipal financial expenditures with this application.

## **4.0 Key Issues and Considerations**

### **4.1 Land Use**

The subject lands are in the Urban Corridor Place Type of The London Plan, which contemplates a range of residential, retail, service, office, cultural, recreational, and institutional uses (TLP 837\_1). The Urban Corridor Place Type is intended to allow for a wide range of uses and intensities of development. Mixed-use buildings are encouraged along the Corridors. (837\_2). Urban Corridors are intended to support the development of a variety of residential types, with varying size, affordability, tenure, design, and accessibility so that a broad range of housing requirements are satisfied (830\_4 and 830\_11).

The site is also within the boundary of the SoHo Main Street Specific Segment in the Corridor Place Type (TLP 844\_). A broad range of uses at a walkable neighbourhood scale will be permitted (TLP 845\_).

The proposed mixed-use building is supported by policies of the PPS, is contemplated in the Urban Corridor Place Type and Main Street Segment in The London Plan. The proposed development and facilitates a higher-density, mixed-use building in keeping with the vision of the Urban Corridor Place Type. The proposed mixed-use building is sited close to the street with accessible parking spaces located at the rear of the building. Commercial uses are proposed on the ground floor of the building, providing an active street frontage along Horton Street East in keeping with the goals of Main Street segments.

### **4.2 Intensity**

The proposed development is consistent with the policies of the PPS that encourage all

types of residential intensification, including redevelopment which results in a net increase in residential uses (2.2.1.2), compact form (2.4.1.3.c), and an appropriate mix of housing options and densities (2.3.1.3). The proposed intensity conforms with Table 9 – Maximum Height in the Rapid Transit Corridor and Urban Corridor Place Type of The London Plan, which permits a minimum height of 2-storeys (or 8 metres) and a standard maximum height of 8-storeys. In the SoHo Main Street Specific Segment Policy, a standard maximum height of 12-storeys is permitted. Buildings up to 16-storeys may be permitted in conformity with the Our Tools part of the plan. (TLP 847\_).

Staff are satisfied the proposed 8-storey intensity is in conformity with the Urban Corridor Place Type of The London Plan. The proposed intensification is appropriate within the surrounding context of neighbouring commercial and residential uses, and will expand the range of housing options.

### **4.3 Form**

In accordance with policy 841\_, the following form policies apply within the Rapid Transit and Urban Corridor Place Types:

- Buildings should be sited close to the front lot line, to create a pedestrian-oriented street wall along Corridors and provide appropriate setback from properties that are adjacent to the rear lot line.
- The mass of large buildings fronting the street should be broken down and articulated at grade so that they support a pleasant and interesting pedestrian environment.
- Large expanses of blank wall will not be permitted to front the street, and windows, entrances, and other building features that add interest and animation to the street will be encouraged.
- Development should be designed to implement transit-oriented design principles.
- While access to development along Corridors may be provided from “side streets”, such accesses to development will be located and directed in a manner that minimizes impacts on the internal portions of adjacent neighbourhoods.
- Underground parking and structured parking integrated within the building design. Surface parking areas should be located in the rear and interior side yard.

All planning and development applications will conform with the City Design policies of The London Plan (TLP, 194\_). These policies direct all planning and development to foster a well-designed building form, and ensure development is designed to be a good fit and compatible within its context (TLP, 193\_1 and 193\_2). The site layout of new development should be designed to respond to its context, the existing and planned character of the surrounding area, and promote connectivity and safe movements for pedestrians, cyclists, and motorists between and within sites (TLP, 252\_ and 255\_). In terms of built form, buildings should be sited so that they maintain and reinforce the prevailing street wall or street line of existing buildings and minimize the visual exposure of parking areas to the street (TLP 256\_ & 269\_).

The Urban Corridor policies direct height and intensity within the corridors to be located close to the street to support transit usage and active transportation. The proposed building is oriented towards Horton Street East, a higher order street, with appropriate buffering and setbacks towards the existing residential uses to the south. The proposed built form and massing have consideration for the adjacent low density residential uses and is appropriate within the context of the surrounding neighbourhood. The mixed-use development will make a positive contribution to the streetscape with commercial uses on the ground floor and residential units above. The proposed building height is compatible with nearby building heights in the surrounding area and a good fit within its context.

In accordance with policy 289\_, high and mid-rise buildings should be designed to express three defined components: a base, middle, and top (TLP, 289\_). Alternative design solutions that address the following intentions may be permitted:

1. The base should establish a human-scale façade with active frontages including, where appropriate, windows with transparent glass, forecourts, patios, awnings, lighting, and the use of materials that reinforce a human-scale.
2. The middle should be visually cohesive with, but distinct from, the base and top.
3. The top should provide a finishing treatment, such as roof or a cornice treatment, to hide and integrate mechanical penthouses into the overall building design.

### Base

The base of the building was designed with positive design features that were recommended by Urban Design staff. These include the street-design to create an active streetscape along Horton Street East, design of the vehicle passageway with columns to allow for light and ventilation and mitigate potential safety issues, and a reduced front yard setback to encourage street-orientation. The principal entrance faces the public street to reinforce the public realm with a strong and vibrant street vibrance, establish an active frontage and provide for convenient pedestrian access (TLP, 291\_). As shown in Figure 10 below, the base contains predominantly glass to provide ample natural lighting, metal/wood style paneling and a grey metal siding intended to draw attention to the principal entrance.



Figure 10: North Elevation

### Middle

The middle of the proposed building corresponds to levels 4-6 and is visually cohesive with, but distinct from the base and top with a proposed step back, grey fibre cement cladding material with wood paneling in the recessed balconies.

### Top

For levels 7-8 a further step back is proposed with wood paneling material is proposed to reduce the perceived impact of the massing of the building and to ensure the top is differentiated from the middle component of the building.

It should be noted that the subject lands are exempt from minimum parking standards to support intense and walkable environments with a focus on providing residents with employment, services, shopping and transit within convenient walking distances, in accordance with Figure 4.19 of Zoning By-law Z.-1. Two accessible parking spaces are proposed, which are located internal to the site and are visually screened from the street. The proposed development will be accessible directly from Horton Street East with the principle entrance abutting the public realm to provide a safe, comfortable, and direct pedestrian connection, facilitating a pedestrian-oriented and transit-oriented development.

The proposed built form is consistent with the Urban Corridor Place Type policies, Main Street Specific Segment Policies, and the City Design policies of The London Plan by facilitating a compatible form of development that will help support the growing demands of London residents including an opportunity to enhance an important part of the Horton streetscape and public realm.

#### **4.4 Zoning Provisions**

The applicant has requested an amendment to Zoning By-law Z.-1 to rezone the subject site an Arterial Commercial (AC4) Zone, to a Holding Business District Commercial Special Provision (h-18\*BDC(\_)) Zone. The following summarizes the special provisions that have been proposed by the applicant and recommended by staff.

##### *Interior Side Yard Depths*

Staff are recommending a special provision to permit an interior (east) side yard setback of 3.5 metres, whereas a minimum of 0.0 metres is required, to allow for privacy and to ensure future redevelopment of adjacent properties is not hindered. Staff are also recommending a special provision to permit an interior (west) side yard setback of 1.9 metres, whereas a minimum of 0.0 metres is required, to accommodate access and maintenance.

##### *Rear Yard Setback*

The applicant is requesting a special provision to permit a minimum rear yard setback of 4.5 metres, whereas 12.6 metres is the minimum required. Staff are supportive of the reduced setback as it is appropriate for the subject site given it will assist with avoiding potential negative impacts on the low-rise residential uses to the south.

##### *Increased Height*

The applicant is requesting a special provision to permit a maximum building height of 8-storeys or 27.0 metres, whereas 12.0 metres is the maximum permitted. Staff are supportive of the increased height, as it is appropriate for the subject site given the street classification in Table 9 of The London Plan. Further, the site is located in proximity to other mid-rise buildings therefore the proposed height aligns with existing context of the surrounding area.

##### *Increased Density*

The applicant is requesting a special provision to permit a maximum density of 530 units per hectare. The increased density will allow for the implementation of the proposed redevelopment, facilitating an appropriate scale of development that is compatible within the existing neighbourhood character. Further, the proposed development is located in proximity of existing transit routes, which will support the use of transit by future residents. On this basis, staff are supportive of the proposed density of 530 units per hectare.

##### *Front Yard Setback Above the 3<sup>rd</sup> Storey*

Staff are recommending a special provision to permit minimum step back of 2.0m above the 3<sup>rd</sup> storey to create a pedestrian-scale environment along Horton Street East.

##### *Encroachments for Balconies and Canopies*

Staff are recommending a special provision to permit unlimited encroachments for balconies and canopies as since they are so close to the property boundary.

#### **4.5 Holding Provision**

A Stage 1-2 Archaeological Assessment was completed for the subject lands. The assessment identifies no further archaeological assessment of the property is required. However, the City has not received the Ministry's acceptance of this assessment; therefore, it is recommended an h-18 holding provision be applied until the City receives confirmation that the Ministry has accepted this archaeological assessment.



## Conclusion

The applicant has requested an amendment to the Zoning By-law Z.-1 to rezone the property from an Arterial Commercial (AC4) Zone, to a Holding Business District Commercial Special Provision (h-18.BDC(\_)) Zone. Staff are recommending approval of the requested Zoning By-law amendment with a holding provision and additional special provisions.

The recommended action is consistent with the PPS 2024, conforms to The London Plan, the SoHo Community Improvement Plan and will permit the development of an 8-storey mixed-use apartment building containing 107 residential units and 66.9 square metres of ground floor commercial space.

**Prepared by:** Alanna Riley, MCIP, RPP  
Senior Planner, Planning Implementation

**Reviewed by:** Catherine Maton, MCIP, RPP  
Manager, Planning Implementation

**Recommended by:** Heather McNeely, MCIP, RPP  
Director, Planning and Development

**Submitted by:** Scott Mathers, MPA, P.Eng.  
Deputy City Manager, Planning and Economic  
Development

Copy:  
Britt O'Hagan, Manager, Current Development  
Mike Corby, Manager, Site Plans  
Brent Lambert, Manager, Development Engineering

## Appendix A – Zoning Bylaw Amendment

Bill No. (number to be inserted by Clerk's Office)  
2024

By-law No. Z.-1-

A by-law to amend By-law No. Z.-1 to rezone an area of land located at 313-323 Horton Street East.

THEREFORE the Municipal Council of The Corporation of the City of London enacts as follows:

1. Schedule "A" to By-law No. Z.-1 is amended by changing the zoning applicable to lands located at 313-323 Horton Street E, as shown on the attached map **FROM** an Arterial Commercial (AC4) Zone, to a Holding Business District Commercial Special Provision (h-18\*BDC(\_)) Zone.
2. Section Number 25.4 of the Business District Commercial (BDC) Zone is amended by adding the following Special Provisions:  
  
BDC(\_) 313-323 Horton Street E
  - a. Regulations
    - i. Interior (East) Side Yard Setback (minimum): 3.5 metres
    - ii. Interior (West) Side Yard Setback (minimum): 1.9 metres
    - iii. Rear Yard Setback (minimum): 4.5 metres
    - iv. Front Yard Setback above the 3<sup>rd</sup> storey (minimum): 2.0m
    - v. Building Height (maximum): 27.0 metres
    - vi. Density (maximum): 530 units per hectare
    - vii. Unlimited encroachments for balconies and canopies along Horton St E
3. This Amendment shall come into effect in accordance with Section 34 of the *Planning Act, R.S.O. 1990, c. P13*, either upon the date of the passage of this by-law or as otherwise provided by the said section.

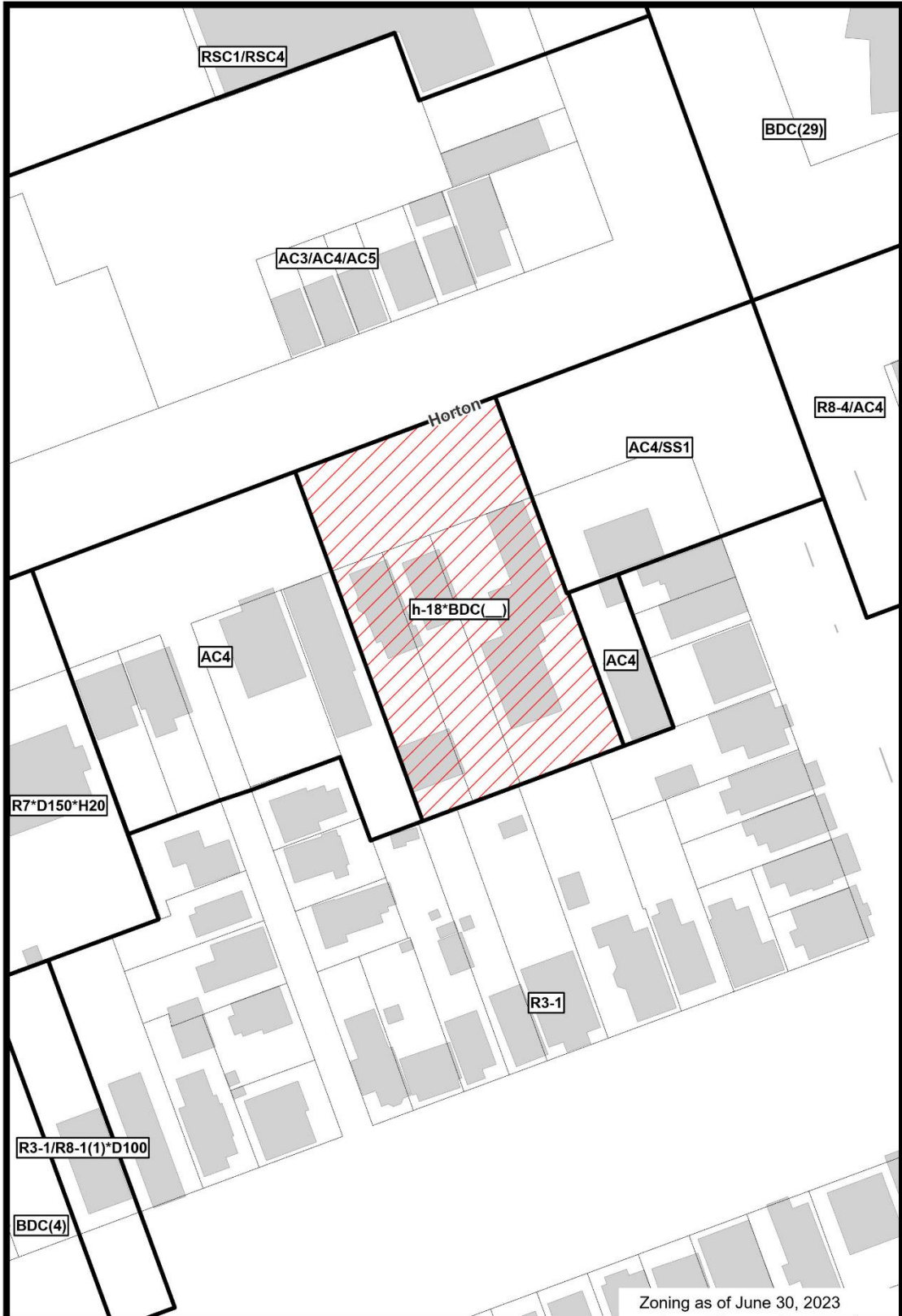
PASSED in Open Council on November 5, 2024, subject to the provisions of PART VI.1 of the *Municipal Act, 2001*.



Josh Morgan  
Mayor

Michael Schulthess  
City Clerk

First Reading – November 5, 2024  
Second Reading – November 5, 2024  
Third Reading – November 5, 2024

AMENDMENT TO SCHEDULE "A" (BY-LAW NO. Z.-1)



<p>File Number: Z-9616 Planner: AR Date Prepared: 2024/10/08 Technician: RC By-Law No: Z.-1-</p>	<p>SUBJECT SITE </p> <p>1:886</p> <p>0 25 50 Meters</p> 
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Geodatabase

## Appendix B - Site and Development Summary

### A. Site Information and Context

#### Site Statistics

Current Land Use	Vacant Commercial
Frontage	40.3 metres
Depth	50.7 metres
Area	2.0 hectares
Shape	Regular (rectangle)
Within Built Area Boundary	Yes
Within Primary Transit Area	Yes

#### Surrounding Land Uses

North	Commercial and Industrial
East	Commercial, Offices and Service
South	Low Density Residential (single detached dwellings, semi-detached dwellings, duplexes, triplexes, fourplexes and low-rise apartment buildings)
West	Commercial/Office and Residential (mid-rise apartments, townhouses and single detached dwellings)

#### Proximity to Nearest Amenities

Major Intersection	Wellington Street and Horton Street East, 139 metres
Dedicated cycling infrastructure	Horton Street East – bike lane, 6 metres
London Transit stop	Horton Street East – LTC stop, 50 metres
Public open space	Hill Street Park, 530 metres
Commercial area/use	10 metres
Food store	Convent Garden Market, 949 metres
Primary school	Aberdeen Public School, 779 metres
Community/recreation amenity	YMCA Community Centre, 425 metres

### B. Planning Information and Request

#### Current Planning Information

Current Place Type	Urban Corridor Place Type, fronting a Civic Boulevard
Current Special Policies	SoHo Main Street Specific Segment
Current Zoning	Arterial Commercial (AC4) Zone

#### Requested Designation and Zone

Requested Place Type	N/A
Requested Special Policies	N/A
Requested Zoning	Business District Commercial (BDC( )) Zone

#### 1.1 Requested Special Provisions

Regulation (BDC)	Required	Proposed	Recommended
Height (metres) (maximum)	10.0m	27.0m	27.0m
Density (uph) (maximum)	N/A	530uph	530uph

Rear Yard Setback (minimum)	12.6m	4.5m	4.5m
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## 1.2 Development Proposal Summary

### Development Overview

The purpose and effect of the recommended action is to permit the development of an 8-storey mixed-use building with 107 dwelling units, 179m<sup>2</sup> of commercial on the ground floor, landscaped areas, indoor and outdoor amenity spaces, 2 surface accessibility parking spaces, and bicycle parking in the interior of the building. Pedestrian access is proposed at locations facing both outward towards the streets and internal to the site facing the parking area. Vehicular access is provided by a full turn driveway from Horton Street East under a cantilevered portion of the proposed building.

### Proposal Statistics

Land use	Commercial/Residential
Form	Mixed-use apartment building
Height	8-storeys (27.0 metres)
Residential units	107
Density	503 units per hectare
Gross floor area	N/A
Building coverage	42%
Landscape open space	31.2%
Functional amenity space	N/A
New use being added to the local community	No

### Mobility

Parking spaces	2 surface parking spaces
Vehicle parking ratio	0.018 spaces per unit
New electric vehicles charging stations	N/A
Secured bike parking spaces	97
Secured bike parking ratio	0.9 spaces per unit
Completes gaps in the public sidewalk	N/A
Connection from the site to a public sidewalk	Yes
Connection from the site to a multi-use path	N/A

### Environmental Impact

Tree removals	N/A
Tree plantings	N/A
Tree Protection Area	No
Loss of natural heritage features	N/A
Species at Risk Habitat loss	N/A
Minimum Environmental Management Guideline buffer met	N/A
Existing structures repurposed or reused	N/A
Green building features	Unknown

**Appendix C – Additional Plans and Drawings**



*Building Rendering: northeast corner view along Horton ST E*



*Building Rendering: rear view of amenity and parking*



*Building Rendering: rear view of site*



*Building Rendering: close up of primary entrance and proposed commercial area*



*Building Rendering: proposed rooftop amenity area*



## Appendix D – Internal and Agency Comments

### Upper Thames River Conservation Authority – Received July 25, 2024

- The subject lands are not affected by any regulations (Ontario Regulation 41/24) made pursuant to Section 28 of the *Conservation Authorities Act*.

### Site Plan – Received July 23, 2024

#### 1. Major Issues

- *Permission from transportation required to allow lay-by in right of way*

#### 2. Matters for ZBA

- *Special provisions required for rear yard depth, site-specific building height, and density*
- *Applicant to confirm details for any balcony or canopy encroachments, and if a reduction is requested for short-term bicycle parking spaces.*

#### 3. Matters for Site Plan

- *Add all or a portion of the long-term bicycle parking on the ground floor to provide convenient access for cyclists without needing to use elevators between the storage areas and ground floor.*

*\*If you don't get confirmation back for the balcony/canopy encroachments, it would be best to include a special provision for 'unlimited' projection in the event that they do encroach since they are so close to the property boundary.*

*\*The revised application is based on what was reviewed through the Site Plan Consultation SPC24-054, so I've attached a copy of those comments and the zoning referral form as well.*

### Heritage – Received August 8, 2024

I have received and reviewed the Stage 1-2 Archaeological Assessment (P1131-0088-2024) for the properties at 313, 317, and 323 Horton Street East as a part of the submission for Z-9616. The assessment identifies no further archaeological assessment of the property is required. However the City has not received the Ministry's acceptance of this assessment.

Heritage recommends applying the h-18 holding provision until the City receives confirmation that the Ministry has accepted this archaeological assessment.

### Parks – Received December 8, 2023

#### Major Issues

- None.

#### Matters for OPA/ZBA

- None

#### Matters for Site Plan

- Parkland dedication is required in the form of cash in lieu, pursuant to By-law CP-25 and will be finalized at the time of site plan approval.

#### Complete Application Requirements

- None.

### Urban Design – Received July 30, 2024

***The following building design features are supported and should be retained for the future site plan application:***

- The vehicular passageway with openings at grade allowing light and ventilation into the space and alleviating potential CPTED issues
- The terraces on the 7th storey along the north and east facades
- A step-down to 7 storeys to the rear of the building

#### Matters for Zoning

1. **Provide a minimum interior side yard (east) setback of 3.5m** to allow for privacy and not hinder the redevelopment of adjacent properties. TLP 252, 253
2. **Provide a minimum interior side yard (west) setback of 1.9m** to accommodate access and maintenance. TLP 252, 253
3. **Provide a minimum rear yard (south) setback of 4.5m** to avoid potential negative impacts on the low-rise residential uses to the south. TLP 253
4. **Provide a minimum height of 4.5m for the commercial uses at grade facing Horton Street East** to create an active frontage along Horton Street East. TLP 291
5. **Provide a step-back of minimum 2m above 4th storey** to create a pedestrian-scale environment along the Urban Thoroughfare. TLP 286, 292, 848\_4
6. **Provide a maximum height of 27m or 8 storeys**

#### Matters for Site Plan

1. Consider replacing the single door with double door for the principal residential lobby entrance facing Horton Street East to promote accessibility and wayfinding. TLP 291
2. Provide adequate all-season landscape buffer along the south property line to avoid any negative impacts on the private amenity spaces of the adjacent residential uses. TLP 253
3. Ensure the east and west elevations are correctly labelled.

### **Landscape Architecture – Received August 8, 2024**

#### Major Issues

- a. No potential grounds for refusal, or issues that could require significant changes to the proposal.

#### 1. Matters for Site Plan

- a. Replacement trees to be recommendation to Site Plan Review based on total dbh removed. The summation of tree diameter to be destroyed shall correspond to the number of Replacement Trees required in accordance with London Plan Policy 399; all trees over 10cm in diameter, measured at a height of 1.4m above the ground, shall be replaced at a rate of 1 tree for every 10cm diameter removed. Trees required as part of the planning application process may be counted as replacement trees.
- b. If boundary trees are identified in the tree preservation plan, consent to injure or remove boundary trees is a requirement of Site Plan approval. A recommendation for approval will be forwarded for Site Plan Review. If consent cannot be obtained from co-owner, then a non-disturbance setback will need to be established at each tree's critical root zone limits as determined by dbh. Critical Root Zone" means the area of land within a radius of ten (10) cm from the trunk of a tree for every one (1) cm of trunk diameter
- c. A tree preservation plan will be required to:
  - o Establish the ownership of trees growing along property lines , including the identification of boundary trees that are protected by the province's Forestry Act 1998, c. 18, Sched. I, s. 21. It is the responsibility of the developer to adhere to the Forestry Act legislation and to resolve any tree ownership issues or disputes. Use Total Station to locate trees in close proximity to property lines. GPS location not acceptable due to errors caused by canopy coverage. If consent cannot be obtained from co-owner, then a non-disturbance setback will need to be established at each tree's critical root zone limits as determined by dbh.
  - o Identify critical root zones of boundary trees and those up to 3m outside of property lines.

- *Determine total dbh proposed for removal to determine tree replacement. London Plan Policy 399 requires 1 tree for every cm dbh removed.*

## **Ecology**

- No comments

## **Engineering – Received September 6, 2024**

**Our transportation and Sewer Engineering divisions have reviewed the attached reports and have no further comments.**

## **Engineering – Received August 9, 2024**

### **Zoning application comments**

- A Transportation Impact Assessment (TIA) will be required, the TIA will evaluate the impact the development will have on the transportation infrastructure in the area and provide recommendations for any mitigation measures. The TIA will need to be scoped with City staff prior to undertaking and be undertaken in general conformance with the City's TIA guidelines.

**The following items are to be considered during a future site plan application stage:**

#### ***Wastewater:***

- The municipal sanitary sewer to service the subject lands is the 450mm on Horton St. City Plan No. 23920 shows information pertaining to the sanitary sewer. The sanitary drainage area plan #22700 shows a density of 100 people per Ha for the property.
- A sanitary brief outlining intended population count and peak flow from the proposed development is required.

#### ***Water:***

- Water is available to the subject site via the municipal 200mm DI watermain on the south side of Horton Street East. Connection to the municipal 900mm CONC watermain on the north side of Horton Street East is not permitted.
- As the proposal includes the demolition of the existing structures, the Owner's Engineer shall provide confirmation that all the existing water services to the structures have been decommissioned to City Standards (cut and capped at the main).
- A water servicing report will be required addressing commercial and domestic water demands, water quality, fire flows and resulting pressures.
- The site is in the City's low-level service area, which has a hydraulic grade line of 301.8m.
- Water servicing to the site will be to City Standard 7.9.4.
- Water servicing shall be configured in a way to avoid the creation of a regulated drinking water system.
- Further comments to be provided during site plan application.

#### ***Stormwater:***

- The proposed land use of medium/high density residential will trigger the application of design requirements of Permanent Private Storm System (PPS) as approved by Council resolution on January 18, 2010. A standalone Operation and Maintenance manual document for the proposed SWM system is to be included as part of the system design and submitted to the City for review.
- As per the City of London's Design Requirements for Permanent Private Systems, the proposed application falls within the Central Subwatershed (case 4), therefore the following design criteria should be implemented:

- the flow from the site must be discharged at a rate equal to or less than the allowable flow;
- the discharge flow from the site must not exceed the capacity of the stormwater conveyance system;
- the design must account the sites unique discharge conditions (velocities and fluvial geomorphological requirements);
- “normal” level water quality is required (70% TSS removal) as per the MECP guidelines; and
- shall comply with riparian right (common) law.
- As per as-con 22699, the site (at C=0.80) is tributary to the existing 1200mm storm sewer on Horton Street. For proposed development in exceedance of the approved C-value of the downstream storm sewer design, the site is to store volumes in excess of the allowable release rate. On-site SWM controls design should include, but not be limited to required.
- The Developer shall be required to provide a Storm/drainage Servicing Report demonstrating that the proper SWM practices will be applied to ensure the maximum permissible storm run-off discharge from the subject site will not exceed the peak discharge of storm run-off under pre-development conditions up to and including 100-year storm events.
- As per as-constructed Drawing No (23920), a 300mm storm PDC exists to service each of the three properties. As per the Drainage By-law, the consultant would be required to connect a storm PDC.
- As part of climate change resiliency objectives, the consultant is to use best efforts to maximize the provided site storage. The consultant is encouraged to make use of rooftop storage.
- The Owner agrees to promote the implementation of SWM Best Management Practices (BMP's) within the plan, including Low Impact Development (LID) where possible, to the satisfaction of the City Engineer.
- The owner is required to provide a lot grading plan for stormwater flows and major overland flows on site, ensuring that stormwater flows are self-contained, and that grading can safely convey up to the 250 year storm event, all to be designed by a Professional Engineer for review.
- The Owner shall allow for conveyance of overland flows from external drainage areas that naturally drain by topography through the subject lands.
- Stormwater run-off from the subject lands shall not cause any adverse effects to adjacent or downstream lands.
- An erosion/sediment control plan that will identify all erosion and sediment control measures for the subject site and that will be in accordance with City of London and MECP (formerly MOECC) standards and requirements, all to the specification and satisfaction of the City Engineer. This plan is to include measures to be used during all phases of construction. These measures shall be identified in the Storm/Drainage Servicing Report.

***Transportation:***

- A Transportation Impact Assessment (TIA) will be required, the TIA will evaluate the impact the development will have on the transportation infrastructure in the area and provide recommendations for any mitigation measures. The TIA will need to be scoped with City staff prior to undertaking and be undertaken in general conformance with the City's TIA guidelines.
- Detailed comments regarding access design and location will be made through the site plan process.

## Appendix E – Public Engagement

Community Engagement:

**Notice of Application** - On July 18, 2024, a Revised Notice of Application was sent to property owners and residents in the surrounding area. A Revised Notice of Application was also published in the Public Notices and Bidding Opportunities section of The Londoner on May 23, 2024. A “Planning Application” sign was also placed on the site.

There was one response received during the public consultation period.

### **Public Comment #1 – Pat Brichell**

Concerns include parking, lack of privacy, and intensity

# Appendix F – Relevant Background

## Zoning By-law No. Z-1 – Zoning Excerpt



Zoning as of June 30, 2024



### COUNCIL APPROVED ZONING FOR THE SUBJECT SITE: AC4

#### 1) LEGEND FOR ZONING BY-LAW Z-1

- |   |                                   |
|---|-----------------------------------|
| R1 - SINGLE DETACHED DWELLINGS            | RF - REGIONAL FACILITY            |
| R2 - SINGLE AND TWO UNIT DWELLINGS        | CF - COMMUNITY FACILITY           |
| R3 - SINGLE TO FOUR UNIT DWELLINGS        | NF - NEIGHBOURHOOD FACILITY       |
| R4 - STREET TOWNHOUSE                     | HER - HERITAGE                    |
| R5 - CLUSTER TOWNHOUSE                    | DC - DAY CARE                     |
| R6 - CLUSTER HOUSING ALL FORMS            | OS - OPEN SPACE                   |
| R7 - SENIOR'S HOUSING                     | CR - COMMERCIAL RECREATION        |
| R8 - MEDIUM DENSITY/LOW RISE APTS.        | ER - ENVIRONMENTAL REVIEW         |
| R9 - MEDIUM TO HIGH DENSITY APTS.         | OB - OFFICE BUSINESS PARK         |
| R10 - HIGH DENSITY APARTMENTS             | LI - LIGHT INDUSTRIAL             |
| R11 - LODGING HOUSE                       | GI - GENERAL INDUSTRIAL           |
| DA - DOWNTOWN AREA                        | HI - HEAVY INDUSTRIAL             |
| RSA - REGIONAL SHOPPING AREA              | EX - RESOURCE EXTRACTIVE          |
| CSA - COMMUNITY SHOPPING AREA             | UR - URBAN RESERVE                |
| NSA - NEIGHBOURHOOD SHOPPING AREA         | AG - AGRICULTURAL                 |
| BDC - BUSINESS DISTRICT COMMERCIAL        | AGC - AGRICULTURAL COMMERCIAL     |
| AC - ARTERIAL COMMERCIAL                  | RRC - RURAL SETTLEMENT COMMERCIAL |
| HS - HIGHWAY SERVICE COMMERCIAL           | TGS - TEMPORARY GARDEN SUITE      |
| RSC - RESTRICTED SERVICE COMMERCIAL       | RT - RAIL TRANSPORTATION          |
| CC - CONVENIENCE COMMERCIAL               |                                   |
| SS - AUTOMOBILE SERVICE STATION           | "h" - HOLDING SYMBOL              |
| ASA - ASSOCIATED SHOPPING AREA COMMERCIAL | "D" - DENSITY SYMBOL              |
| OR - OFFICE/RESIDENTIAL                   | "H" - HEIGHT SYMBOL               |
| OC - OFFICE CONVERSION                    | "B" - BONUS SYMBOL                |
| RO - RESTRICTED OFFICE                    | "T" - TEMPORARY USE SYMBOL        |
| OF - OFFICE                               |                                   |

### CITY OF LONDON

PLANNING SERVICES / DEVELOPMENT SERVICES

#### ZONING BY-LAW NO. Z-1 SCHEDULE A



THIS MAP IS AN UNOFFICIAL EXTRACT FROM THE ZONING BY-LAW WITH ADDED NOTATIONS

FILE NO:

Z-9616

AR

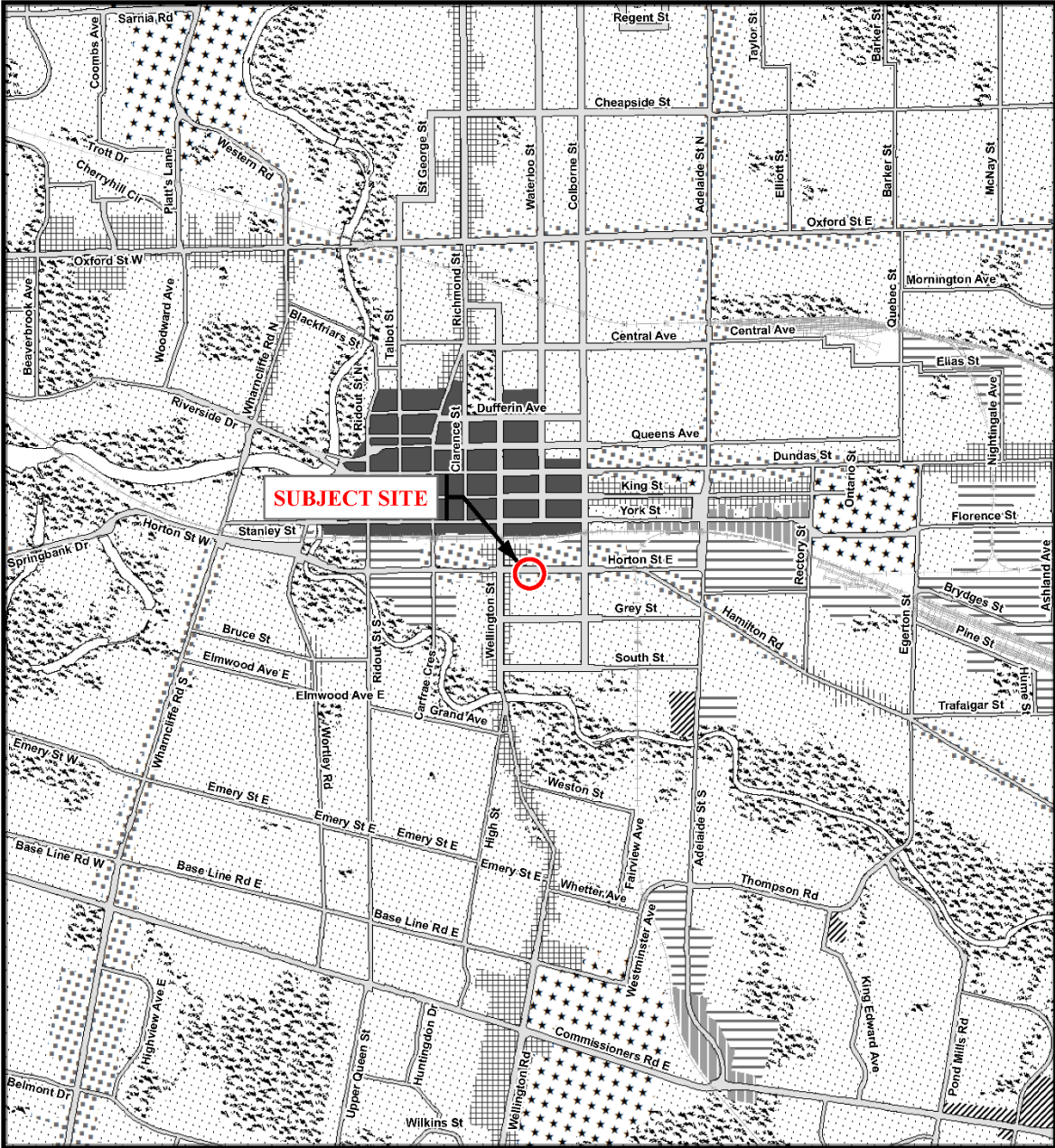
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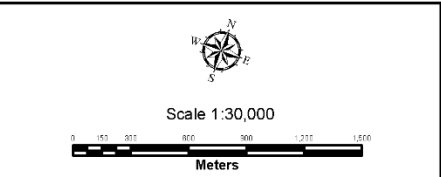


**Legend**

- |                        |                          |   |
|------------------------|--------------------------|---|
| Downtown               | Future Community Growth  | Environmental Review                    |
| Transit Village        | Heavy Industrial         | Farmland                                |
| Shopping Area          | Light Industrial         | Rural Neighbourhood                     |
| Rapid Transit Corridor | Future Industrial Growth | Waste Management Resource Recovery Area |
| Urban Corridor         | Commercial Industrial    | Urban Growth Boundary                   |
| Main Street            | Institutional            |   |
| Neighbourhood          | Green Space              |   |

*This is an excerpt from Planning & Development's working consolidation of Map 1 - Place Types of the London Plan, with added notations.*

**CITY OF LONDON**  
**Official Plan**  
**LONDON PLAN MAP 1**  
**- PLACE TYPES -**  
 PREPARED BY: Planning & Development



**File Number:** Z-9616  
**Planner:** AR  
**Technician:** RC  
**Date:** 2024/7/17

Project Location: E:\Planning\Projects\p\_officialplan\work\consol\00\excerpts\_LondonPlan\mxd\Z-9616-Map1-Excerpt-PlaceTypes.mxd

## Report to Planning and Environment Committee

**To:** Chair and Members  
Planning and Environment Committee  
**From:** Scott Mathers, MPA, P.Eng.  
Deputy City Manager, Planning and Economic Development  
**Subject:** Oxford West Gateway Inc. (c/o Strik Baldinelli Moniz)  
1856-1910 Oxford Street West  
File Number: O-9484/Z-9485, Ward 9  
Public Participation Meeting  
**Date:** October 22, 2024

## Recommendation

That, on the recommendation of the Director, Planning and Development, the following actions be taken with respect to the application of Oxford West Gateway Inc. (c/o Strik Baldinelli Moniz) relating to the property located at 1856-1910 Oxford Street West:

- (a) the proposed by-law attached hereto as Appendix "A" **BE INTRODUCED** at the Municipal Council meeting on November 5, 2024, to amend the Riverbend South Secondary Plan forming part of the Official Plan, The London Plan by **ADDING** a Site-Specific Policy to the Commercial Land Use Designation Section;
- (b) the proposed by-law attached hereto as Appendix "B" **BE INTRODUCED** at the Municipal Council meeting on November 5, 2024, to amend Zoning By-law No. Z.-1, in conformity with the Official Plan, The London Plan, and the Riverbend South Secondary Plan, as amended in part (a) above, to change the zoning of the subject property **FROM** a Community Shopping Area Special Provision (CSA5(6)) Zone **TO** a Holding Residential R9 Special Provision/Community Shopping Area Special Provision (h-149\*R9-7(\_)/CSA5(6)) Zone;
- (c) The Site Plan Approval Authority **BE REQUESTED** to consider the following design issues through the site plan process:
  - i) Increase the amount of transparent glazing on the ground floor elevation to provide increased passive surveillance and activation of the public realm;
  - ii) Provide a pedestrian walkway that connects between the proposed landscape parkette and the rest of the pedestrian network on the site;
  - iii) Provide a paratransit layby;
  - iv) Provide a loading space for the commercial uses within the proposed mixed-use apartment building;
  - v) Provide a direct pedestrian connection to nearby open space to the west to provide for off-site outdoor amenity space;
  - vi) Provide a connection to the SWM block to the south;
- (d) pursuant to Section 34(17) of the *Planning Act*, as determined by the Municipal Council, no further notice **BE GIVEN** in respect of the proposed by-laws as the recommended amendments are reflective of the proposed development circulated in the Notice of Application and Notice of Public Meeting, existing permissions, and the existing development on site.

**IT BEING NOTED**, that the above noted amendments are being recommended for the following reasons:

- i) The recommended amendment is consistent with the Provincial Planning Statement, 2024 (PPS);
- ii) The recommended amendments conform to the Riverbend South Secondary Plan;
- iii) The recommended amendments conform to The London Plan, including but not limited to the Key Directions, City Design and Building policies, the



- Neighbourhoods Place Type policies, and will facilitate a built form that contributes to achieving a compact, mixed-use City;
- iv) The recommended amendments facilitate an appropriate form of development at an intensity that is appropriate for the context of the site and surrounding neighbourhood.

## Executive Summary

### Summary of Request

The applicant has requested an amendment to The London Plan, to add a Specific Policy Area to the Commercial Land Use Designation of the Riverbend Secondary Plan to permit a mixed-use apartment building. The applicant has also requested an amendment to the Zoning By-law Z.-1 to rezone the property from a Commercial Shopping Area Special Provision (CSA5(6)) Zone to a Residential R9 Special Provision/Community Shopping Area Special Provision (R9-7(\_)/CSA5(6)) Zone.

### Purpose and the Effect of Recommended Action

The recommended action will permit a mixed-use apartment building consisting of two towers with heights of 10 and 11 storeys containing **223 residential units** and a maximum of 850m<sup>2</sup> of ground floor commercial space.

Staff are recommending approval of the requested Official Plan and Zoning By-law amendments with special provisions and a holding provision to ensure there is adequate sanitary capacity prior to development.

## Linkage to the Corporate Strategic Plan

This recommendation supports the following Strategic Areas of Focus:

- **Housing and Homelessness**, by ensuring London's growth and development is well-planned and considers use, intensity, and form.
- **Wellbeing and Safety**, by promoting neighbourhood planning and design that creates safe, accessible, diverse, walkable, healthy, and connected communities.
- **Economic Growth, Culture, and Prosperity** by supporting small and growing businesses, entrepreneurs and non-profits to be successful.

## Analysis

### 1.0 Background Information

#### 1.1 Previous Reports Related to this Matter

PEC Report – Lands bounded by Oxford Street West to the north, Westdel Bourne to the west, the Warbler Woods ESA on the east, and the City's Urban Growth Boundary to the south 39T14505/OZ-8426 - March 23, 2015

#### 1.2 Planning History

March 31, 2015, City Council approved a Draft Plan of Subdivision, Official Plan Amendment, Zoning By-law Amendment and the adoption of a Secondary Plan for the area generally bounded by Oxford Street West, Westdel Bourne, Warbler Woods ESA and the City of London Urban Growth Boundary to permit 90 single detached dwelling lots, 1 medium density residential block, 1 medium-high density residential block, 1 commercial block, 5 park blocks, 1 walkway block, 2 open space blocks served by a secondary collector road, 3 local streets, 2 road widening blocks, and the extension of Riverbend Road and Warbler Woods Walk.

January 5, 2017, for 1854 Oxford St W, the Consent Authority granted a consent to sever approximately 3.3 hectares for the purpose of proposed commercial uses, and to retain approximately 29.2 hectares for the purpose of proposed residential uses; and to establish an easement for the purpose of servicing.

May 2, 2017, for 1231 Riverbend Road, the Committee of Adjustment passed two variances to permit 3.0m (9.8') rear yard setbacks whereas 6.0m (19.6') is required

and to permit 3.0m (9.8') interior side yard setbacks whereas 6.0m (19.6') is required.

August 31, 2017, for 1826 Oxford St W, the Consent Authority granted a consent to establish an easement of approximately 1401m<sup>2</sup> over Block 95 on Plan 33M-711, in favour of Block 94 on Plan 33M-711, for the purpose of servicing and access.

April 22, 2019, for 1886 Oxford St W, the Committee of Adjustment passed three variances to permit a total gross floor area for office use as a proportion of gross floor area of 22.0%, whereas 15% is the permitted maximum, to permit a building height of 15.0m (49.2'), whereas 12.0m (39.4') is the permitted maximum, and to permit 388 off-street parking spaces whereas 419 are required.

November 11, 2019, Site Plan Approval was granted for 1868 Oxford St W to permit a commercial development.

April 14, 2021, for 1170 Riverbend Road, the Committee of Adjustment passed a variance to permit a total gross floor area for office uses as a proportion of total gross floor area of 25%, whereas 22% is the maximum permitted.

### **1.3 Property Description and Location**

The subject lands are located on the south side of Oxford Street West, between Westdel Bourne to the west and Riverbend Road to the east, within the River Bend Planning District. The site has a total area of approximately 3.28 hectares, 438 metres of frontage along Oxford St W and 138 metres along Riverbend Road. Currently, the subject lands contain a large commercial lot that is approximately 3.28 hectares (8.1 acres) in area and is offered high visibility, frontage and access onto the two arterials (Westdel Bourne and Oxford Street West) and a collector (Riverbend Road). The site is currently developed with eight commercial/office buildings and associated surface parking within the west and north portions of the site.

Surrounding uses consist of a mix of low to medium density residential, open space, commercial and office uses. Vacant lands to the south are planned for high-density residential development and townhouses which are currently under construction. To the north across Oxford Street West is the West Five community, consisting of Sifton Properties offices/headquarters and ground floor retail uses, with the Kilbourne Cemetery to the west of the office use and vacant lands planned for future commercial development. To the east across Riverbend Road is a cluster townhouse development and single detached dwellings on Sumac Way. Lastly, to the west, directly abutting the site is a stormwater management facility.

#### **Site Statistics:**

- Current Land Use: Commercial
- Frontage: 435 metres along Oxford Street West and 138 metres along Riverbend Road
- Depth: 435 metres
- Area: 3.28 hectares
- Shape: Irregular
- Located within the Built Area Boundary: No
- Located within the Primary Transit Area: No

#### **Surrounding Land Uses:**

- North: Retail, Commercial, Offices, Cemetery
- East: Residential
- South: Residential
- West: Residential

#### **Existing Planning Information:**

- Existing London Plan Place Types: Shopping Area and Neighbourhood
- Existing Special Policies: Riverbend South Secondary Plan
- Existing Zoning: Community Shopping Area Special Provision (CSA5(6)) Zone

Additional site information and context is provided in Appendix “C”.



Figure 1: Aerial Photo of Subject Lands and surrounding lands



Figure 2: View of Subject lands looking east from Westdel Bourne



Figure 3: View of subject lands looking southeast from Oxford St W



Figure 4: View of subject lands looking southwest from Oxford St W and Riverbend Road



Figure 5: View of subject lands and proposed development area, looking west from Riverbend Road

## 2.0 Discussion and Considerations

### 2.1 Development Proposal

The original development concept proposed a mixed-use building on the southeast portion of the site consisting of a base podium with approximately 840 m<sup>2</sup> of ground floor commercial and two 10 and 11 storey towers containing 223 residential units.

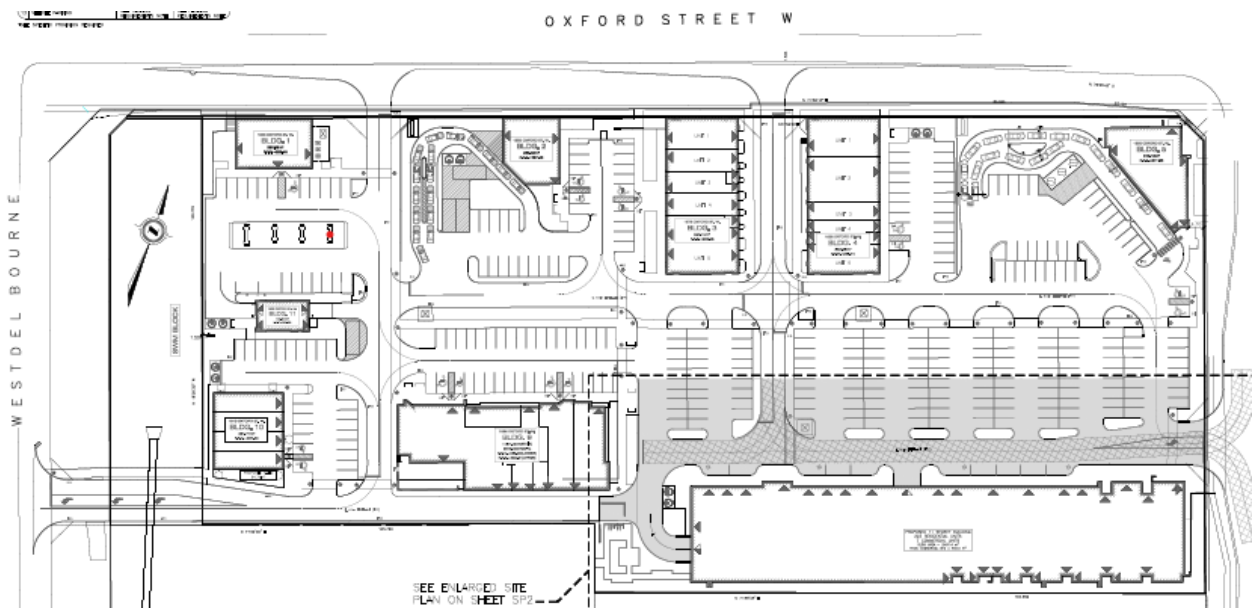


Figure 6: Original Conceptual Site Plan

### 2.2 Revised Development Proposal

Following discussions with City staff, the applicant submitted a revised development proposal which provides an increased setback to the south property line to provide greater separation from the future development to the south. No changes were made to the building heights, proposed number of residential units, or proposed commercial gross floor area. Surface parking for the proposed commercial units within the mixed-use building is proposed to be shared with the existing commercial development on the subject lands. Parking for the residential units is provided internal to the building with

one level of underground parking and one level of parking at grade within the podium, which are accessed on the north and west sides of the building.

Currently, there are four accesses to the subject lands, one on Westdel Bourne, two on Oxford Street West and one on Riverbend Road. Sidewalks are currently located on Westel Bourne, Oxford Street West and Riverbend Road. Proposed walkways would provide connection to the mixed-use building, internally through the site to the public sidewalks. Amenity area is provided in a parkette to the west of the mixed-use building along with on top of some terraces.

The proposed development includes the following features:

- Land use: Mixed-use apartment building
- Form: Two towers connected by a common podium
- Height: 10 and 11 storeys
- Residential units: 223
- Building coverage: 23.5%
- Parking spaces: 259
- Bicycle parking spaces: 264
- Landscape open space: 24.9%

Additional information on the development proposal is provided in Appendix “B”.

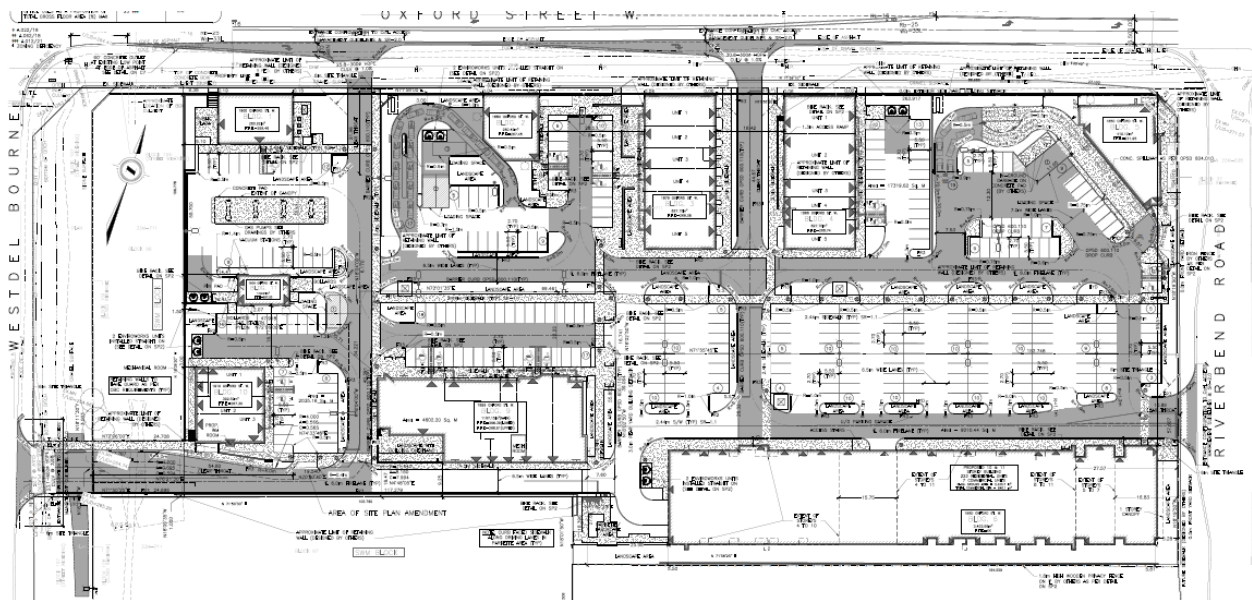


Figure 7: Revised Conceptual Site Plan



Figure 4: Rendering – North elevation – View from Oxford St W



Figure 5: Rendering – South Elevation – View from Riverbend Road

## 2.2 Requested Amendments

The applicant has requested to add a Specific Policy to the Commercial Land Use designation of the Riverbend South Secondary Plan to facilitate the above noted development proposal.

The applicant has requested an amendment to the Zoning Bylaw Z.-1 to rezone a portion of the property from a Community Shopping Area Special Provision (CSA5(6)) Zone to a Residential R9 Special Provision/Community Shopping Area Special Provision (R9-7(\_)/CSA5(6)) Zone.

The following table summarizes the special provisions that have been proposed by the applicant.

<b>Regulation (R9-7)</b>	<b>Required</b>	<b>Proposed</b>	<b>Recommended</b>
Additional Permitted Uses of the CSA5 Zone shall be permitted on the ground floor of the development.			
Additional Permitted Uses – Mixed-Use Apartment Buildings			
The front lot line shall be deemed to be Oxford Street West			
Exterior Side Yard Depth (Minimum)	10.0m	5.2m	5.0m
Rear Yard Depth (Minimum)	16.8m	5.5m	5.0m
Landscaped Open Space (%) (Minimum)	30%	20%	20%
Building Height (Maximum)	As per 13.3 (ZBL)	40m	40m
Commercial GFA (Maximum)	N/A	840sq.m	850sq.m
Encroachments for balconies and canopies	A maximum of 1.5m projection no closer than 3.0m to the lot line for balconies and 0.5m maximum projection for canopies	N/A	To permit unlimited encroachments for balconies and canopies
<b>Regulation (CSA5(6))</b>	<b>Required</b>	<b>Proposed</b>	<b>Recommended</b>
Front and Exterior Yard Depth (Minimum)		0.0m	0.0m

Permitted Uses may be in stand-alone buildings that do not form part of a shopping centre, and/or apartment buildings.	N/A	Permitted Uses may be in stand-alone buildings that do not form part of a shopping centre, and/or apartment buildings.	Permitted Uses may be in stand-alone buildings that do not form part of a shopping centre, and/or apartment buildings.
Total GFA for Office Uses as a proportion of total GFA (%) (max)	N/A	25.1%	30%
Gross floor area maximum shall not include any residential gross floor area			

### 2.3 Internal and Agency Comments

The application and associated materials were circulated for internal comments and public agencies to review. Comments received were considered in the review of this application and are addressed in Section 4.0 of this report.

Detailed internal and agency comments are included in Appendix “E” of this report.

### 2.4 Public Engagement

On March 17, 2022, Notice of Application was sent to property owners and residents in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on March 17, 2022. A “Planning Application” sign was also placed on the site. On December 21, 2023, Notice of Application was sent to property owners and residents in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on December 21, 2023.

There were 4 responses received during the public consultation period. Detailed public comments are included in Appendix “F” of this report. Concerns expressed including the development does not meet the polices of the London Plan and Riverbend South Secondary Plan, traffic, the use and intensity are concerning.

### 2.5 Policy Context

#### ***The Planning Act and the Provincial Planning Statement, 2024***

The Provincial planning policy framework is established through the *Planning Act* (Section 3) and the *Provincial Planning Statement, 2024 (PPS)*. The *Planning Act* requires that all municipal land use decisions affecting planning matters shall be consistent with the *PPS*.

The mechanism for implementing Provincial policies is through the Official Plan, The London Plan. Through the preparation, adoption and subsequent Ontario Land Tribunal (OLT) approval of The London Plan, the City of London has established the local policy framework for the implementation of the Provincial planning policy framework. As such, matters of provincial interest are reviewed and discussed in The London Plan analysis below.

As the application for an Official Plan amendment and a Zoning By-law amendment complies with The London Plan, it is staff’s opinion that the application is consistent with the *Planning Act* and the *PPS*.

The proposed development meets the intent of the PPS policies by introducing a compact, mixed-use, development that would intensify an existing commercial centre in an area designated for commercial and residential uses.

## ***The London Plan, 2016***

The London Plan (TLP) includes evaluation criteria for all planning and development applications with respect to use, intensity and form, as well as with consideration of the following (TLP 1577-1579):

1. Consistency with the Provincial Policy Statement and all applicable legislation.
2. Conformity with the Our City, Our Strategy, City Building, and Environmental policies.
3. Conformity with the Place Type policies.
4. Consideration of applicable guideline documents.
5. The availability of municipal services.
6. Potential impacts on adjacent and nearby properties in the area and the degree to which such impacts can be managed and mitigated.
7. The degree to which the proposal fits within its existing and planned context.

Staff are of the opinion that all the above criteria have been satisfied.

The London Plan includes conditions for evaluating the appropriateness of Specific Area Policies where the applicable place type policies would not accurately reflect the intent of City Council with respect to a specific site or area (TLP 1729-1734).

The following conditions apply when considering a new Specific Area Policy:

1. The proposal meets all other policies of the Plan beyond those that the specific policy identifies.
2. The proposed policy does not have an adverse impact on the integrity of the place type policies or other relevant parts of this Plan.
3. The proposed use is sufficiently unique and distinctive such that it does not establish an argument for a similar exception on other properties in the area.
4. The proposed use cannot be reasonably altered to conform to the policies of the place type.
5. The proposed policy is in the public interest and represents good planning.

Staff are of the opinion that all the above conditions have been met.

## ***Riverbend South Secondary Plan***

The Riverbend South Secondary Plan (RSSP) was adopted by Council in 2015 to redesignate the subject lands for community growth to be read and interpreted in conjunction with the 1989 Official Plan, which refers to specific land use designations. With the RSSP, the subject lands were designated “Community Commercial Node under the 1989 Official Plan and permitted high density apartment buildings. In 2022 the Riverbend South Secondary Plan was amended to change referenced to reflect the policies of The London Plan which currently designates the larger site as Shopping Area and Neighbourhoods. This application was initiated prior to the amendments to the RSSP.

The subject site is located within the Commercial designation within the Riverbend South Secondary Plan. The Secondary Plan has been reviewed in its entirety and it is staff’s opinion that the proposed Official Plan and Zoning By-law amendment is consistent with it.

## **3.0 Financial Impact/Considerations**

None.

## **4.0 Key Issues and Considerations**

### **4.1 Amendment to the Riverbend South Secondary Plan**

An amendment to the Riverbend South Secondary Plan to add a Specific Policy Area to Commercial Designation to permit a mixed-use building with heights of 10 and 11



storeys and a maximum of 850m<sup>2</sup> of ground floor commercial space is recommended. As discussed in Section 2.5 of this Report, The London Plan sets out policies for Specific Areas that may be considered in limited circumstances where the following conditions apply (TLP 1729-1734):

1. *The proposal meets all other policies of the Plan beyond those that the specific policy identifies.*

The recommended site-specific amendment for a mixed-used development and complementary commercial uses is in keeping with The London Plan Key Directions by introducing intensification on an underutilized parcel.

2. *The proposed policy does not have an adverse impact on the integrity of the commercial designation policies in the Riverbend South Secondary Plan or other relevant parts of the London Plan.*

The site-specific amendment to the Commercial Designation in the Riverbend South Secondary Plan does not have an adverse impact on the integrity of the area and other relevant parts of The London Plan. The subject site is situated within an established development node that is designated for residential and commercial purposes. The site is currently underutilized, and based on proximity to arterial roads, public transit and active transportation routes is well suited for intensification. The proposed development expresses a high-quality architectural design, is transit-supportive, includes a podium with structured parking that supports a high-quality pedestrian environment and adds to the choice of dwelling types with varying locations, size, affordability, tenure, design and accessibility (TLP 810\_ & 811\_)

3. *The proposed use is sufficiently unique and distinctive such that it does not establish an argument for a similar exception on other properties in the area.*

The site-specific amendment would permit a unique mixed-use development with two towers to complement the existing commercial uses on site. The site has a relatively large size of 3.8 hectares, is fronting onto one Urban Thoroughfare and one Neighbourhood Connector, and is supported by existing and planned public transit routes. The proposal represents a site-specific intensification opportunity that would not establish a precedent for other properties in the area. The RSSP, prior to amendments in 2022, contemplated the development of high density residential on this site.

4. *The proposed use cannot be reasonably altered to conform to the policies of the place type.*

The proposed development is comprised of 10 and 11 storey towers containing a total of 223 residential units, and 840m<sup>2</sup> of commercial gross floor area on the round floor. The building massing, orientation and articulation are designed to be compatible with the surrounding land uses and promote an appropriate form of infill development within a site within the Commercial Land Use Designation, justifying the increased building height on the undeveloped and underutilized portion of the site.

5. *The proposed policy is in the public interest, and represents good planning.*

The site-specific amendment is in the public interest and represents good planning as it facilitates the intensification of an underutilized site. The proposed mixed-use development is located at a strategic location that utilizes existing services and facilities, ensures a mix of housing types and is compatible with the surrounding neighbourhood.

It should be noted that prior to 2022 and at the time of acceptance of this application, the Commercial designation of the Riverbend South Secondary Plan referred to the parent policies of the 1989 Official Plan, which designated this site as Community

Commercial Node on Schedule A – Land Use. In accordance with policy 4.3.7.3 of the Community Commercial Node designation in the 1989 Official Plan, multi-family, high density residential uses may be permitted through a zoning by-law amendment, site plan application, and consideration of design features which provide for the proper integration of the two uses. As such, the purpose of this Official Plan Amendment is to implement the former policy framework which was in force and effect at the time this application was submitted and deemed complete.

On this basis, staff are of the opinion that all the above criteria have been met and are supportive of the site-specific policy to the Riverbend South Secondary Plan.

## **4.2 Land Use**

The proposed mixed-use apartment use is supported by the policies of the PPS 2024 and the Commercial designation of the Riverbend South Secondary Plan. The subject site is located within the Shopping Area and Neighbourhood Place Types. Place Type boundaries are not meant to be interpreted as rigid unless they coincide with a physical feature. The Shopping Area Place Type permits a range of commercial and residential uses and promotes mixed-use development. The intent of the Neighbourhoods Place Type is to permit a range of residential land uses and intensities that are compatible in form and offer a variety of housing choice, particularly with higher intensity/density along higher-order streets (TLP 919\_).

### *Riverbend South Secondary Plan*

The Commercial designation will provide a range of retail and service opportunities to meet the regular shopping needs of local and surrounding residents. Permitted uses within this area will be consistent with those permitted in the Shopping Area Place Type of The London Plan. In addition, residential units above commercial and office uses will be permitted to foster live-work opportunities. The Shopping Area Place Type contemplates a broad range of retail, service, office, entertainment, recreational, educational, institutional, and residential uses (TLP 877\_1). Mixed-use buildings are encouraged (TLP 877\_2). The recommended amendments seek to permit the commercial and residential uses of a mixed-use development which is in keeping with the Commercial designation. In accordance with policies 1558 and 958\_6 of The London Plan, the more specific policies of the Riverbend South Secondary Plan prevail over those of The London Plan. As such, staff are satisfied the recommended amendment is in conformity with the Riverbend South Secondary Plan and is therefore in conformity with The London Plan.

## **4.3 Intensity**

The proposed residential intensity is consistent with the policies of the PPS that encourage all types of residential intensification, including redevelopment which results in a net increase in residential uses (2.2.1.2), compact form (2.4.1.3.c), and an appropriate mix of housing options and densities (2.3.1.3). Within the Neighbourhood Place Type, the maximum building height for sites fronting on a Neighbourhood Connector is 3 storeys in accordance with Table 11. The 11-storey tower is proposed to be oriented towards Riverbend Road and the 10-storey tower sited at the southwest corner of the site. Both towers are designed to provide a transition in height towards the existing and future surrounding residential uses (TLP 953\_2). The residential use is accommodated on an undeveloped portion of the site that is of sufficient size to support the proposed use and can provide sufficient setbacks to buffer to existing and future abutting residential developments. The redevelopment of the parcel will facilitate the efficient use of land and is appropriate within the existing and planned context (TLP 953\_2 and 3).

The subject site is served by a broad range of commercial and service uses on site and nearby. Public transit is also available in the broader vicinity. The subject site is in an appropriate location for a mixed-use development based on the planned uses, location criteria, and the form, design and mitigation measures identified in this report.

## *Riverbend South Secondary Plan (RSSP)*

The Riverbend South Secondary Plan does not establish minimum or maximum height or intensities in the Commercial designation. As the Shopping Area Place Type permits a standard maximum height of four storeys and an upper maximum height of six storeys, an Official Plan Amendment is required to add a specific policy to the Commercial designation of the Riverbend South Secondary plan to permit a mixed-use apartment building with heights of 10 and 11 storeys, 223 residential units, and 850 square metres of commercial gross floor area. This is consistent with the policies of the 1989 Official Plan that was in effect at the time the application was submitted.

Staff are satisfied that appropriate mitigation measures have been incorporated into the building design, and that the positioning of the building justifies the additional building height. Given the surrounding context and potential future development with this range of height and intensity, the proposed mixed-use development is considered appropriate and compatible within the neighbourhood context. As such, staff are satisfied the proposed intensity is in conformity with the criteria for the Evaluation Criteria for Planning and Development Applications.

The proposed residential intensity will facilitate an appropriate scale of development that makes efficient use of lands and services and is compatible and complementary to the existing and planned residential development in the area in conformity with The London Plan and Riverbend South Secondary Plan.

### **4.4 Form**

The proposed form is consistent with the policies of the PPS 2024 and in conformity with The London Plan. The London Plan encourages compact forms of development for planning new growth, including “inward and upward” compact forms of development (TLP 7\_, 59\_2, 66\_, 79\_). Various forms of infill and intensification are accommodated to realize the compact, inward and upward patterns of planned growth (TLP 59\_4). Within the Neighbourhoods Place Type, and according to the urban design considerations for residential intensification, compatibility and fit will be evaluated from a form-based perspective through consideration of site layout, access points, driveways, landscaping, amenity areas, building location and parking, building and main entrance orientation, building line and setback from the street, height transitions with adjacent development, and massing (953\_2, a. to f.).

All planning and development applications will conform with the City Design policies of The London Plan (TLP 194\_). These policies direct all planning and development to foster a well-designed built form, and ensure development is designed to be a good fit and compatible within its context (TLP 193\_1 and 193\_2). The site layout of new development should be designed to respond to its context, the existing and planned character of the surrounding area, and promote connectivity and safe movements for pedestrians, cyclists, and motorists between and within sites (TLP 252\_ and 255\_). In terms of built form, buildings should be sited so that they maintain and reinforce the prevailing street wall or street line of existing buildings and minimize the visual exposure of parking areas to the street (TLP 256\_ & 269\_).

In accordance with policy 289\_, high and mid-rise buildings should be designed to express three defined components: a base, middle, and top (TLP, 289\_). Alternative design solutions that address the following intentions may be permitted:

1. The base should establish a human-scale façade with active frontages including, where appropriate, windows with transparent glass, forecourts, patios, awnings, lighting, and the use of materials that reinforce a human-scale.
2. The middle should be visually cohesive with, but distinct from, the base and top.
3. The top should provide a finishing treatment, such as roof or a cornice treatment, to hide and integrate mechanical penthouses into the overall building design.

The mixed-use building is proposed to be positioned forward with a reduced front yard setback to Riverbend Road to help reinforce the urban street wall. To further address Riverbend Road, there are setbacks to create a more human scale street edge.

Additionally, this street edge includes a principal entrance for the 11-storey building connected by a wide pedestrian walkway to the public sidewalk along Riverbend Road. The two towers are connected by a podium that varies in height. The proposed development integrates three defined built-form components:

### *Base*

The base of the building was designed with positive design features. These include a podium with a variety of heights, delineated by using building articulation to emphasize the interface on Riverbend Road and between residential commercial units. Brick masonry veneer is used to highlight the residential and commercial entrances/spaces as well as anchoring the base of the building. The proposed podium floor plates would contain commercial units (combined commercial GFA of 840m<sup>2</sup>) on the ground floor. These commercial units would include street entrances from Riverbend Road and Oxford Street West. The façade treatment along the commercial store fronts includes a higher proportion of glazing to clearly delineate commercial unit entrances which accents the ground floor commercial space at a pedestrian scale. Also, variations in colour, building articulation and canopies for each residential entrance to highlight and direct pedestrian flow have been emphasized. As indicated one principal entrance faces Riverbend Road, and two face Oxford Street West to reinforce the public realm with strong and vibrant street vibrance, establish an active frontage and provide for convenient pedestrian access (TLP 291\_). The podium elements are proposed to contain amenity space, lobby, vehicular parking, bike storage and entrances. Further, podium elements will integrate design features to screen structured parking.



Figure 10: - Rendering – East Elevation – View from Riverbend Road

### *Middle*

The middle portion is visually cohesive with, but distinct from the base and top. It contains two residential towers on top of the podium adequately separated to provide privacy between buildings and reduce sun shadow impacts. This element includes the apartment units, step backs and individual balconies and large wrap around balconies, extensive glazing and vertical and horizontal material patterns which provide articulation for the towers elements to break up the continuous massing. This contributes to distinctive tower elements intended to help frame and enhance the adjacent streetscapes.



Figure 11: Rendering – North Elevation – View from Riverbend Road



Figure 11: Rendering – North Elevation – View from site interior

*Top*

The rooftop elements are proposed to be setback from the core tower features and would contain penthouse units, rooftop terraces and mechanical equipment. The top portion of the towers have been differentiated from the middle portion through the use of stepbacks and varying building materials.

The proposed building is designed with two step backs to the tower from Riverbend Road using a 45 degree angular plane to provide a transition from the townhouses to the east. This will assist with a better transition in building height, as well as increased privacy, amenity space, and a human scale along Riverbend Road.

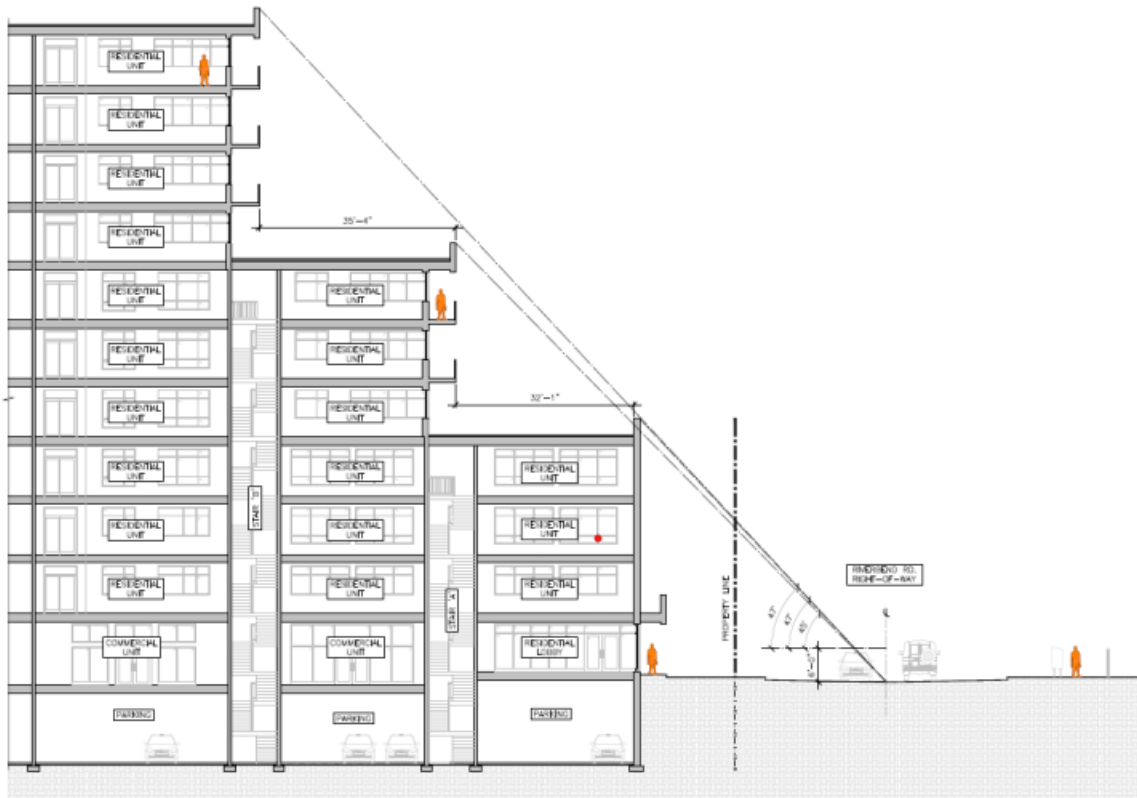


Figure 12: - Angular Plane measure from the Centreline of Riverbend Road

As part of the application, the applicant submitted a shadow study showing the shadows that will fall in March/September, June and December at 8:00AM, 10:00AM, 12:00PM, 2:00PM, 4:00PM and 6:00PM. The shadow study indicates that shadows move relatively quickly, with minimal shadows for all seasons at 6:00PM on some existing residential to the east. The shadow study is identified as Appendix D.

The proposed built form is consistent with the Shopping Area and Neighbourhoods Place Type policies and the City Design policies of The London Plan by facilitating a compatible form of development that will help support the growing demands of London residents (TLP, 937\_). Specifically, facilitating a development that supports aging in place, affordability, and the effective use of land in neighbourhoods (TLP, 193\_7).

#### *Riverbend South Secondary Plan*

Finally, the Riverbend Secondary Plan promotes the creation of attractive and cohesive neighbourhoods that are compatible and sensitive to existing surroundings, while recognizing that there is a need for change and higher intensity to assist in achieving compact form and transit supportive development over time (RSSP 5.0) The proposed development conforms to the policies and urban design objectives of the RSSP.

The proposed built form is in conformity with the City Design policies of The London Plan, and the Riverbend South Secondary Plan by facilitating a compatible form of development that will help support the growing demands of London residents including an opportunity to enhance an important part of the Riverbend Rd streetscape and public realm.

#### **4.5 Holding Provision**

Due to sanitary capacity constraints within the Riverbend Road sewer system, an h-149 holding provision is being recommended. A detailed assessment of the actual buildout of the area is required to demonstrate that the current area has underdeveloped in comparison to the design populations and identify if any potential sewer upgrades are necessary.

## Conclusion

The applicant has requested an amendment to the Official Plan and Zoning By-law Z.-1 to add a specific policy to the Riverbend South Secondary Plan and to rezone the subject lands from a Community Shopping Area Special Provision (CSA5(6)) Zone to a Residential R9 Special Provision/Community Shopping Area Special Provision (R9-7(\_)/CSA5(6)) Zone. Staff are recommending approval of the requested Official Plan and Zoning By-law amendment with special provisions and a holding provision to ensure there is adequate sanitary capacity prior to development.

The recommended action is consistent with the Provincial Planning Statement, 2024, and conforms to The London Plan and the Riverbend South Secondary Plan. The recommended amendments will facilitate the development of a mixed-use apartment building consisting of two towers with heights of 10 and 11 storeys containing 223 residential units and a maximum of 850m<sup>2</sup> of ground floor commercial space.

**Prepared by:** Alanna Riley, MCIP, RPP  
Senior Planner, Planning Implementation

**Reviewed by:** Catherine Maton, MCIP, RPP  
Manager, Planning Implementation

**Recommended by:** Heather McNeely, MCIP, RPP  
Director, Planning and Development

**Submitted by:** Scott Mathers, MPA, P.Eng.  
Deputy City Manager, Planning and Economic  
Development

Copy:  
Britt O'Hagan, Manager, Current Development  
Michael Pease, Manager, Site Plans  
Brent Lambert, Manager, Development Engineering

## Appendix A – Official Plan Amendment

Bill No. (number to be inserted by Clerk's Office)  
2024

By-law No. C.P.-XXXX-

A by-law to amend the Official Plan, The Riverbend South Secondary Plan which forms part of The London Plan for the City of London, 2016 relating to 1856-1910 Oxford Street West

The Municipal Council of The Corporation of the City of London enacts as follows:

1. Amendment No. (to be inserted by Clerk's Office) to the Riverbend South Secondary Plan forming part of Official Plan, The London Plan for the City of London Planning Area – 2016, as contained in the text attached hereto and forming part of this by-law, is adopted.
2. This Amendment shall come into effect in accordance with subsection 17(27) of the *Planning Act, R.S.O. 1990, c.P.13*.

PASSED in Open Council on November 5, 2024 subject to the provisions of PART VI.1 of the *Municipal Act, 2001*.

Josh Morgan  
Mayor

Michael Schulthess  
City Clerk

First Reading – November 5, 2024  
Second Reading – November 5, 2024  
Third Reading – November 5, 2024



**AMENDMENT NO.**  
**to the**  
**OFFICIAL PLAN, THE LONDON PLAN, FOR THE CITY OF LONDON**

**A. PURPOSE OF THIS AMENDMENT**

The purpose of this Amendment is to add a Specific Area Policy to the Commercial Land Use Designation Policies for the Riverbend South Secondary Plan - of the City of London to permit a mixed-use apartment building consisting of two towers with heights of 10 and 11 storeys, containing 223 residential units and a maximum of 850m<sup>2</sup> of ground floor commercial space to be located on the southeast portion of the site, subject to the policies for Specific Area Policies contained in the Our Tools part of this Plan.

**B. LOCATION OF THIS AMENDMENT**

This Amendment applies to lands located at 1856-1910 Oxford Street West in the City of London.

**C. BASIS OF THE AMENDMENT**

The site-specific amendment would allow for a mixed-use apartment building consisting of two towers with heights of 10 and 11 storeys, with 223 residential units and a maximum of 850m<sup>2</sup> of ground floor commercial space to be located on the southeast portion of the site. The recommended amendment is consistent with the *Provincial Planning Statement, 2024 (PPS)*, which encourages all types of residential intensification, including redevelopment which results in a net increase in residential uses, compact form, and an appropriate mix of housing options and densities. The recommended amendment conforms to The London Plan, including but not limited to Key Directions, City Design and Building policies, and will facilitate a built form that contributes to achieving a compact, mixed-use City with an appropriate form of infill development at an intensity that is appropriate for the site and surrounding neighbourhood.

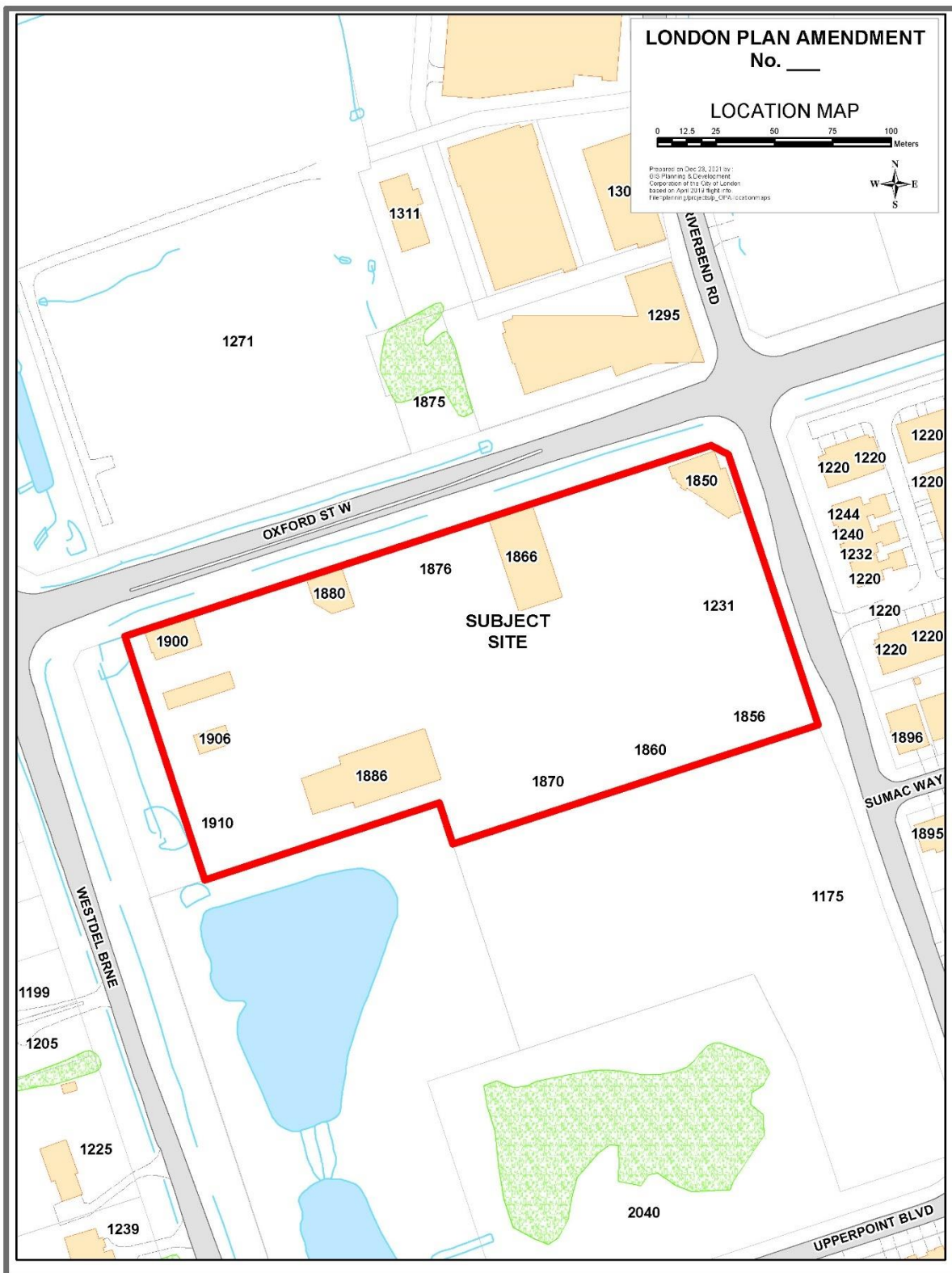
**D. THE AMENDMENT**

The Riverbend South Secondary Plan, Commercial Designation for the City of London is hereby amended by adding the following policy:

( ) 1856-1910 Oxford Street W

In the Commercial Land Use Designation located at 1856-1910 Oxford St W, a mixed-use apartment building consisting of two towers with heights of 10 and 11 storeys, on the southeast corner of the property is permitted.

“Schedule 1”



## Appendix B – Zoning Bylaw Amendment

Bill No. (number to be inserted by Clerk's Office)  
2024

By-law No. Z.-1-

A by-law to amend By-law No. Z.-1 to  
rezone an area of land located at 1856-  
1910 Oxford Street West

WHEREAS upon approval of Official Plan Amendment Number (number to be inserted by Clerk's Office) this rezoning will conform to the Official Plan;

NOW THEREFORE the Municipal Council of The Corporation of the City of London enacts as follows:

1. Schedule "A" to By-law No. Z.-1 is amended by changing the zoning applicable to lands located at 1856-1910 Oxford Street West, as shown on the attached map **FROM** a Community Shopping Area Special Provision (CSA5(6)) Zone **TO** a Holding Residential R9 Special Provision/Community Shopping Area Special Provision (h-149\*R9-7(\_)/CSA5(6)) Zone.
2. Section Number 13.4.g) of the Residential R9 (R9-7) Zone is amended by adding the following Special Provisions:

R9-7(\_) 1856-1910 Oxford Street West

a. Additional Permitted Uses

1. Mixed-use apartment buildings
2. All permitted uses within the CSA5 Zone variation on the ground floor of an apartment building and regulated by the Residential R9 Zone

b. Regulations

1. The front lot line shall be deemed to be Oxford Street West
2. Exterior Side Yard Depth (Minimum): 5 metres
3. Rear Yard Depth (Minimum): 5 metres
4. Landscaped Open Space (Minimum): 20%
5. Building Height (Maximum): 40 metres
6. Commercial Gross Floor Area within mixed use buildings (Maximum): 850 square metres
7. Encroachments for Balconies and Canopies: Unlimited

3. Section Number 22.4.e) of the Commercial Shopping Area (CSA5) Zone is amended by deleting and replacing the following Special Provisions:

CSA5(6) 1856-1910 Oxford Street West

a. Regulations

1. Front and Exterior Side Yard Depth (Minimum): 0.0 metres
2. Permitted uses may be in stand-alone buildings that do not form part of a shopping centre, and/or apartment buildings
3. Total Gross Floor Area of Office Uses as a Proportion of Total Gross Floor Area (Maximum): 30%
4. Gross floor area maximum shall not include any residential gross floor area

4. This Amendment shall come into effect in accordance with Section 34 of the *Planning Act, R.S.O. 1990, c. P13*, either upon the date of the passage of this by-law or as otherwise provided by the said section.

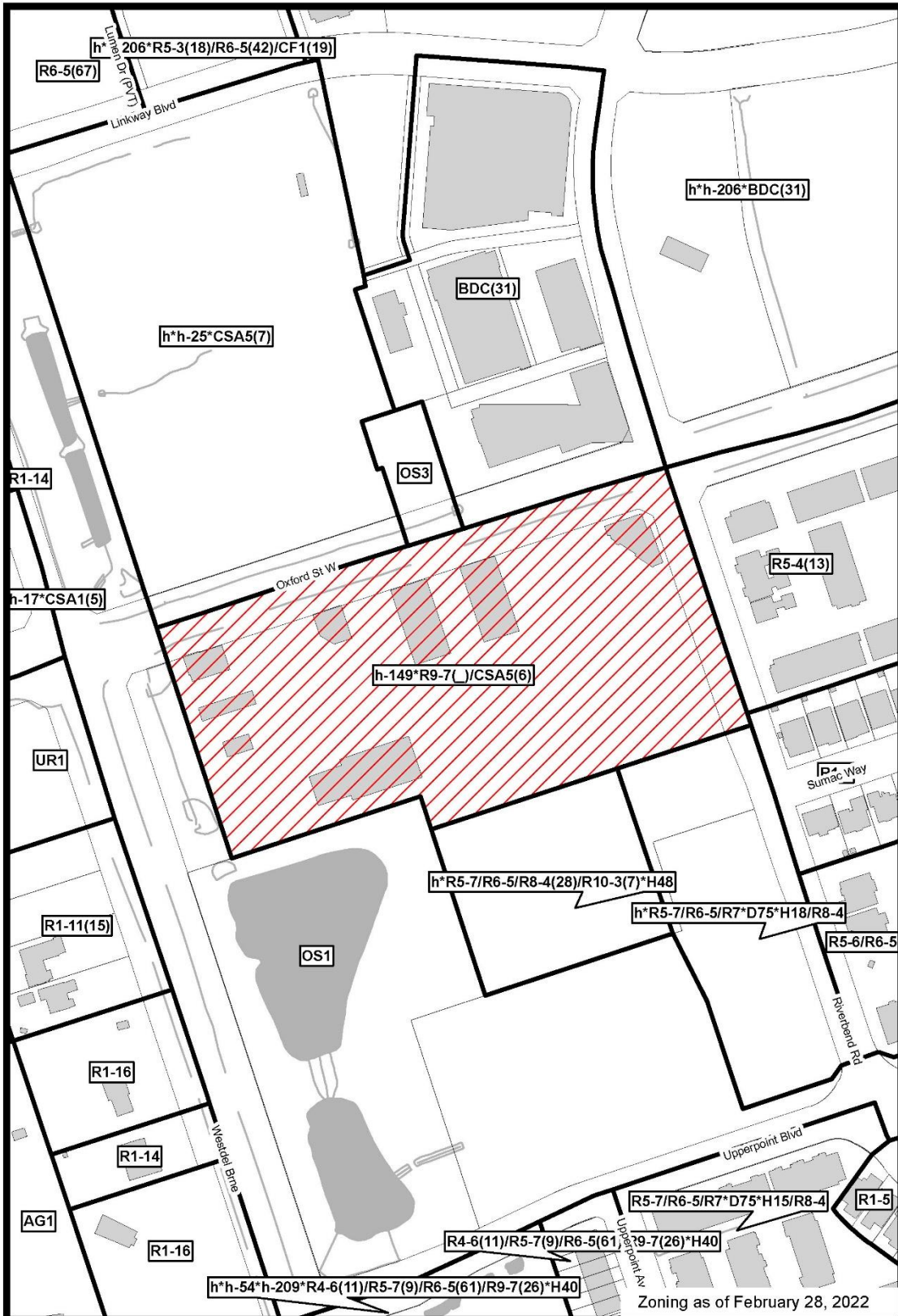
PASSED in Open Council on November 5, 2024 subject to the provisions of PART VI.1 of the *Municipal Act*, 2001.

Josh Morgan  
Mayor



Michael Schulthess  
City Clerk

First Reading – November 5, 2024  
Second Reading – November 5, 2024  
Third Reading – November 5, 2024

AMENDMENT TO SCHEDULE "A" (BY-LAW NO. Z.-1)



Zoning as of February 28, 2022

<p>File Number: O-9484/Z-9485                  Planner: AR                  Date Prepared: 2024/10/10                  Technician: rc                  By-Law No: Z.-1-</p>	<p>SUBJECT SITE </p> <p>1:2,500</p> <p>0 12.525 50 75 100 Meters</p> 
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Geodatabase

## Appendix C - Site and Development Summary

### A. Site Information and Context

#### Site Statistics

Current Land Use	Commercial & Surface Parking Lot
Frontage	438 along Oxford Street W & 138 along Riverbend Road
Depth	N/A
Area	Total area: 3.28 hec
Shape	irregular
Within Built Area Boundary	No
Within Primary Transit Area	No

#### Surrounding Land Uses

North	Retail, Commercial, Offices, Cemetery
East	Residential
South	Residential
West	Residential

#### Proximity to Nearest Amenities

Major Intersection	Oxford S W & Westdel Bourne
Dedicated cycling infrastructure	Westdel Bourne, 40 m
London Transit stop	Oxford Street West, 572 m
Public open space	Tributary C SWM, 0 m & Mow Sifton Ridge, 90m
Commercial area/use	On site
Food store	Aisle 24 Oxford & Riverbend, 350 metres
Community/recreation amenity	Byron Optimist Community Centre, 3,180 metres

### B. Planning Information and Request

#### Current Planning Information

Current Place Type	Shopping Area and Neighbourhood
Current Special Policies	Riverbend South Secondary Plan
Current Zoning	Community Shopping Area Special Provision (CSA5(6)) Zone

#### Requested Designation and Zone

Requested Place Type	N/A
Requested Special Policies	Site-specific Policy Area
Requested Zoning	Residential R9 Special Provision/Community Shopping Area (R9-7(_)/CSA5(6))

#### Requested Special Provisions

Regulation (R9-7)	Required	Proposed	Recommended
Additional Permitted Uses of the CSA5 Zone shall be permitted on the ground floor of the development.			
Additional Permitted Uses – Mixed-Use Apartment Buildings			
The front lot line shall be deemed to be Oxford Street West			
Exterior Side Yard Depth (Minimum)	10.0m	5.2m	5.0m
Rear Yard Depth (Minimum)	16.8m	5.5m	5.0m

Landscaped Open Space (%) (Minimum)	30%	20%	20%
Building Height (Maximum)	As per 13.3 (ZBL)	40m	40m
Commercial GFA (Maximum)	N/A	840sq.m	850sq.m
Encroachments for balconies and canopies	A maximum of 1.5m projection no closer than 3.0m to the lot line for balconies and 0.5m maximum projection for canopies	N/A	To permit unlimited encroachments for balconies and canopies
<b>Regulation (CSA5(6))</b>	<b>Required</b>	<b>Proposed</b>	<b>Recommended</b>
Front and Exterior Yard Depth (Minimum)		0.0m	0.0m
Permitted Uses may be in stand-alone buildings that do not form part of a shopping centre, and/or apartment buildings.	N/A	Permitted Uses may be in stand-alone buildings that do not form part of a shopping centre, and/or apartment buildings.	Permitted Uses may be in stand-alone buildings that do not form part of a shopping centre, and/or apartment buildings.
Total GFA for Office Uses as a proportion of total GFA (%) (max)	N/A	25.1%	30%
Gross floor area maximum shall not include any residential gross floor area			

## C. Development Proposal Summary

### Development Overview

A mixed-use apartment building consisting of two residential towers with heights of 10 and 11 storeys, 223 residential units, and 840m<sup>2</sup> of ground floor commercial space.

### Proposal Statistics

Land use	Mixed-use
Form	Podium with two high rise towers
Height	10-storeys and 11-storeys
Residential units	223
Density	93 units per hectare
Gross floor area	14,564 m <sup>2</sup> (existing + proposed commercial)
Building coverage	23.5%
Landscape open space	24.9%
Functional amenity space	Rooftops, balconies, Parkette
New use being added to the local community	No

### Mobility

Parking spaces	501 structured/underground and surface
Vehicle parking ratio	0.5 per unit (residential)
New electric vehicles charging stations	Unknown
Secured bike parking spaces	201 long-term, 71 short-term

Secured bike parking ratio	0.9 space per unit for long-term and 0.1 spaces per unit for short-term
Completes gaps in the public sidewalk	N/A
Connection from the site to a public sidewalk	Yes
Connection from the site to a multi-use path	Yes

### Environmental Impact

Tree removals	No
Tree plantings	Yes
Tree Protection Area	No
Loss of natural heritage features	No
Species at Risk Habitat loss	No
Minimum Environmental Management Guideline buffer met	N/A
Existing structures repurposed or reused	Yes
Green building features	Unknown



# Appendix D – Additional Plans and Drawings

Shadow-Study– March/September



March 21 – 8am



March 21 – 10am



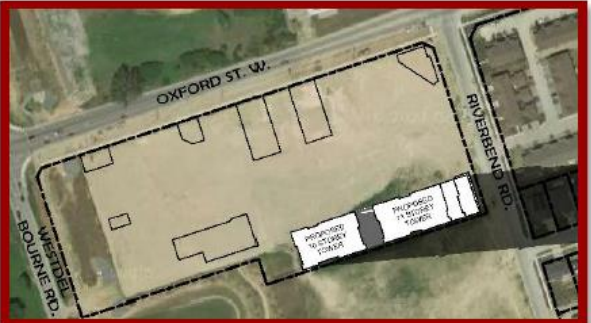
March 21 – 12pm



March 21 – 2pm



March 21 - 4pm

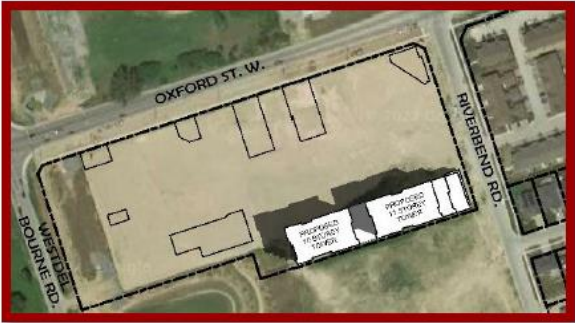


March 21 - 6pm

Shadow-Study- June



June 21 - 8am



June 21 - 10am



June 21 - 12pm



June 21 - 2pm



June 21 - 4pm

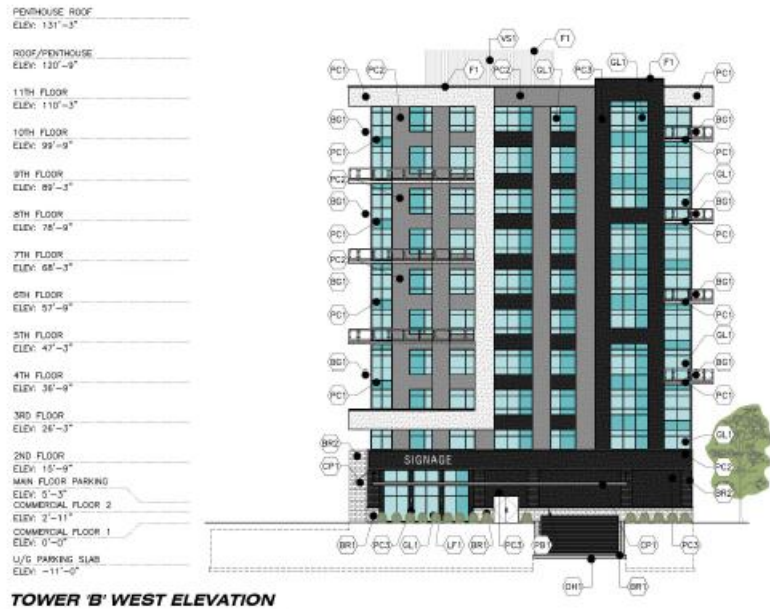


June 21 - 6pm





North Elevation

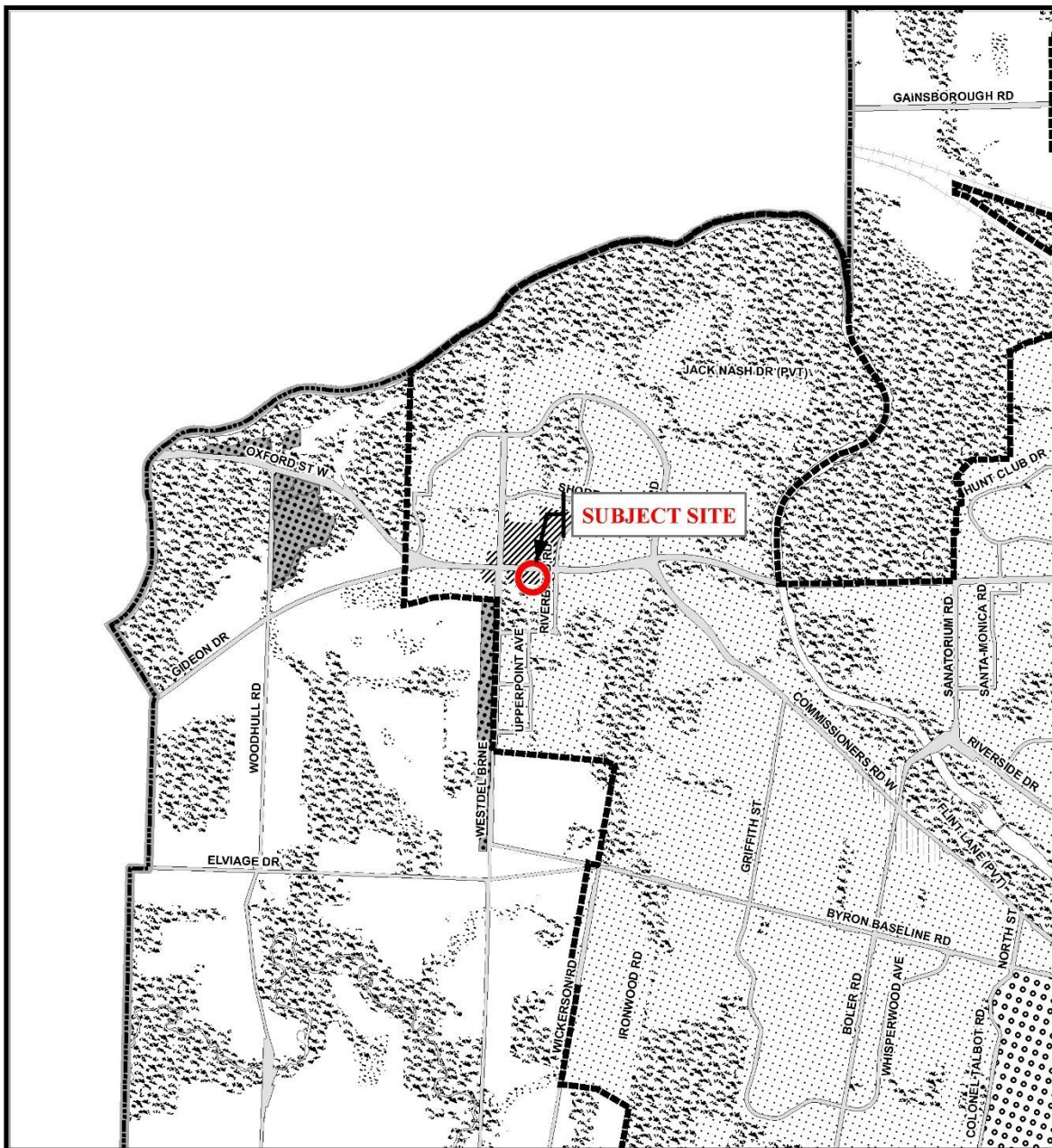


West Elevation



East Elevation

London Plan Map 1 – Place Types



**Legend**

- |                        |                          |   |
|------------------------|--------------------------|---|
| Downtown               | Future Community Growth  | Environmental Review                    |
| Transit Village        | Heavy Industrial         | Farmland                                |
| Shopping Area          | Light Industrial         | Rural Neighbourhood                     |
| Rapid Transit Corridor | Future Industrial Growth | Waste Management Resource Recovery Area |
| Urban Corridor         | Commercial Industrial    | Urban Growth Boundary                   |
| Main Street            | Institutional            |   |
| Neighbourhood          | Green Space              |   |

*This is an excerpt from the Planning Division's working consolidation of Map 1 - Place Types of the London Plan, with added notations.*

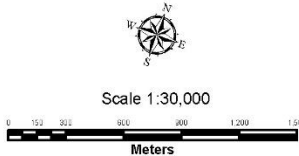
*At the time of the printing of this map, the Rapid Transit EA is in progress. This map shows the Rapid Transit Corridors and Urban Corridors to recognize potential alignments. These Place Types will be modified to align with the results of the EA process for the final version of The London Plan.*

**CITY OF LONDON**

Planning Services /  
Development Services

**LONDON PLAN MAP 1  
- PLACE TYPES -**

PREPARED BY: Planning Services



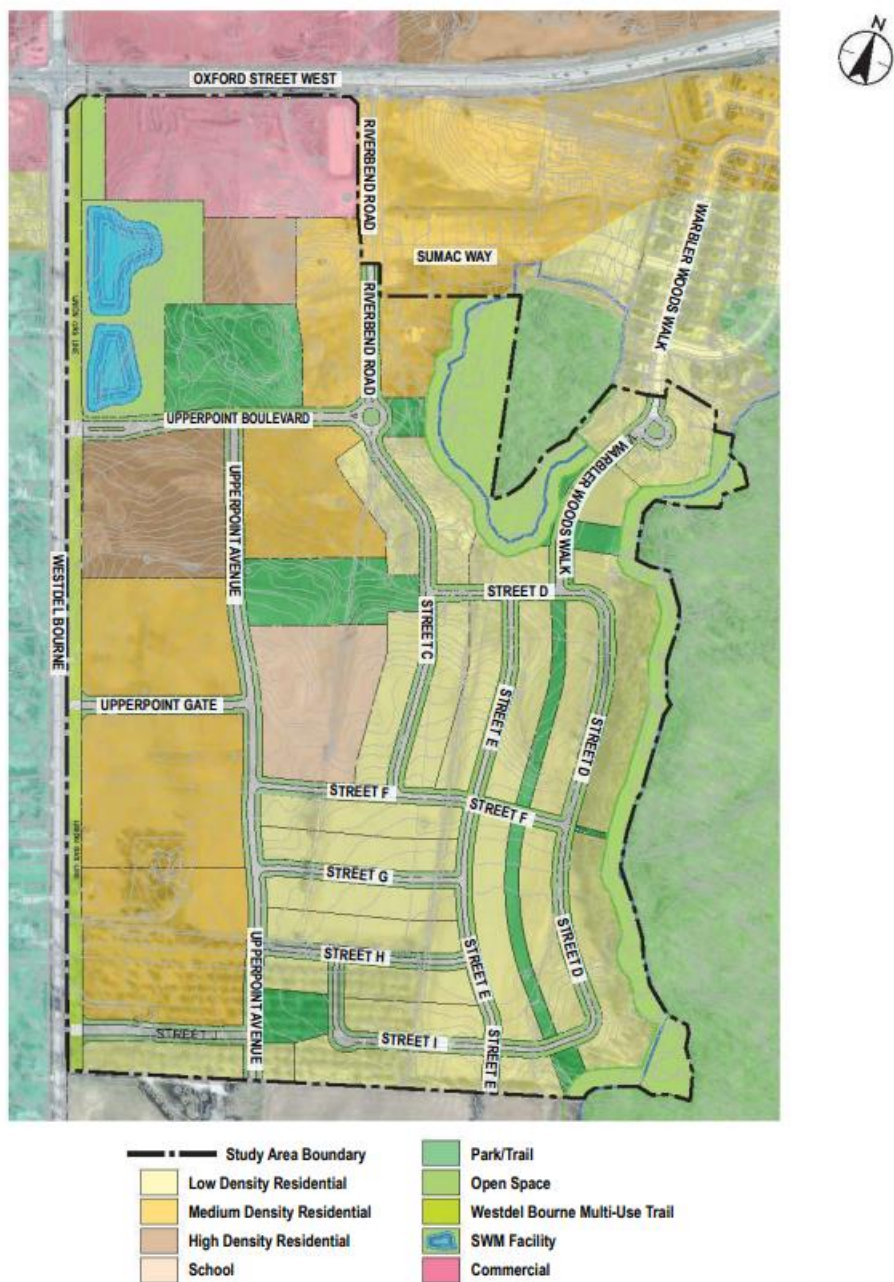
**File Number:** O-9484/Z-9485

**Planner:** AR

**Technician:** RC

**Date:** March 15, 2022

Riverbend South Secondary Plan – Land Use



Zoning By-law No. Z.-1 Schedule A



**COUNCIL APPROVED ZONING FOR THE SUBJECT SITE:**

1) **LEGEND FOR ZONING BY-LAW Z-1**

- |   |                                   |
|---|-----------------------------------|
| R1 - SINGLE DETACHED DWELLINGS            | RF - REGIONAL FACILITY            |
| R2 - SINGLE AND TWO UNIT DWELLINGS        | CF - COMMUNITY FACILITY           |
| R3 - SINGLE TO FOUR UNIT DWELLINGS        | NF - NEIGHBOURHOOD FACILITY       |
| R4 - STREET TOWNHOUSE                     | HER - HERITAGE                    |
| R5 - CLUSTER TOWNHOUSE                    | DC - DAY CARE                     |
| R6 - CLUSTER HOUSING ALL FORMS            |                                   |
| R7 - SENIOR'S HOUSING                     | OS - OPEN SPACE                   |
| R8 - MEDIUM DENSITY/LOW RISE APTS.        | CR - COMMERCIAL RECREATION        |
| R9 - MEDIUM TO HIGH DENSITY APTS.         | ER - ENVIRONMENTAL REVIEW         |
| R10 - HIGH DENSITY APARTMENTS             |                                   |
| R11 - LODGING HOUSE                       | OB - OFFICE BUSINESS PARK         |
| DA - DOWNTOWN AREA                        | LI - LIGHT INDUSTRIAL             |
| RSA - REGIONAL SHOPPING AREA              | GI - GENERAL INDUSTRIAL           |
| CSA - COMMUNITY SHOPPING AREA             | HI - HEAVY INDUSTRIAL             |
| NSA - NEIGHBOURHOOD SHOPPING AREA         | EX - RESOURCE EXTRACTIVE          |
| BDC - BUSINESS DISTRICT COMMERCIAL        | UR - URBAN RESERVE                |
| AC - ARTERIAL COMMERCIAL                  |                                   |
| HS - HIGHWAY SERVICE COMMERCIAL           | AG - AGRICULTURAL                 |
| RSC - RESTRICTED SERVICE COMMERCIAL       | AGC - AGRICULTURAL COMMERCIAL     |
| CC - CONVENIENCE COMMERCIAL               | RRC - RURAL SETTLEMENT COMMERCIAL |
| SS - AUTOMOBILE SERVICE STATION           | TGS - TEMPORARY GARDEN SUITE      |
| ASA - ASSOCIATED SHOPPING AREA COMMERCIAL | RT - RAIL TRANSPORTATION          |
| OR - OFFICE/RESIDENTIAL                   | "h" - HOLDING SYMBOL              |
| OC - OFFICE CONVERSION                    | "D" - DENSITY SYMBOL              |
| RO - RESTRICTED OFFICE                    | "H" - HEIGHT SYMBOL               |
| OF - OFFICE                               | "B" - BONUS SYMBOL                |
|   | "T" - TEMPORARY USE SYMBOL        |

**CITY OF LONDON**

PLANNING SERVICES / DEVELOPMENT SERVICES

**ZONING  
BY-LAW NO. Z-1  
SCHEDULE A**



THIS MAP IS AN UNOFFICIAL EXTRACT FROM THE ZONING BY-LAW WITH ADDED NOTATIONS

FILE NO:

O-9484/Z-9485 AR

MAP PREPARED:

2022/03/15 rc

1:2,000

0 10 20 40 60 80 Meters

## Appendix E – Internal and Agency Comments

### UTRCA – Revised January 3, 2024

The UTRCA has no objections to the application and we have no Section 28 approval requirements.

### Site Plan – Revised January 23, 2024

#### 1. Major Issues

- *Potential nuisances for residential uses located the same site as a gar bar*
- *The CSA zone preamble identifies that a R8 or R9 zone should be compounded to permit residential uses and not a special provision to the existing CSA zone*

#### 2. Matters for OPA/ZBA

- *The CSA zone is a commercial zone that does not have regulations that would commonly be found for a residential zone or a higher density development outcome. There are concerns with the requested zoning approach including:*
  - *CSA zone does not require greater building setbacks that are commensurate to greater building heights like the R8 or R9*
  - *CSA has no density regulation*
  - *There is a low landscaped open space of 10% which does not provide any functional outdoor amenity area for residents (R8 & R9 = 30%)*
  - *Distributes the GFA permitted (30,000sqm) to the zone boundary. The proposed residential GFA would count towards the total and results in 35,624sqm, which exceeds the maximum for the site and also impacts the CSA5 zone to the north which is currently a vacant property.*

#### 3. Matters for Site Plan

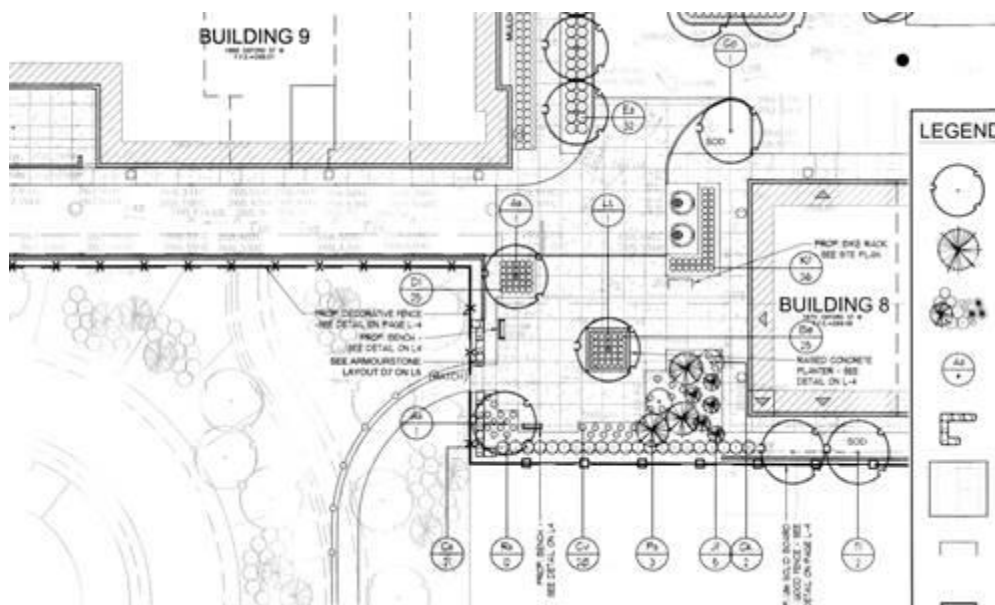
- *A paratransit layby is required with dimensions of 3.6 x 12*
- *A direct pedestrian connection to nearby open space to the west should be provided for off-site outdoor amenity space in proximity to the site.*

### Parks Planning

#### Matters for Site Plan – Revised January 16, 2024

The proposed apartment and site plan shows a reduced parkette but it would still allow for the connection to the pathway in the swm block. Parks comments remain the same as was provided to site plan in 2022 that design of the on-site amenity space should be more in keeping with the previously approved design.

Thank you.



### Landscape Architect – Revised January 7, 2024



The Site Plan drawing does not indicate the footprint of the underground parking structure it may encroach into the setback along the south property. Regardless, the reduced setback along the south property line may cause conflicts between the building façade and Site Plan required tree planting. Sufficient volume of soil must be provided to support tree growth, as required in Site Plan Control Bylaw and to meet canopy goals of the London Plan and the Urban Forest Strategy. The side yards must accommodate fencing, retaining walls, drainage features [above and below ground] and tree planting. Reduced setbacks will cause conflicts.

### **Heritage – Revised January 11, 2024**

This is to confirm that there are no cultural heritage or archaeological concerns associated with this application.

### **Urban Design – Revised January 17, 2024**

#### Major Issues:

- This site is located within the Neighbourhoods Place Type in The London Plan [TLP] and is within the Commercial Land Use Designation in the Riverbend South Secondary Plan [RSSP] area. Urban Design staff are generally supportive of the proposed development.
- The applicant is commended for providing a mixed-use form that wraps the above-grade parking in active uses, for stepping the building heights down toward the lower intensity uses along Riverbend Road, for providing an entrance to the residential lobby on Riverbend Road, for including architectural details which differentiate the commercial and residential entrances and for incorporating a high degree of transparent glazing and articulation in the design of the base and tower portions of the building.

#### Matters for OPA/ZBA:

- Urban Design recommends the following Special Provisions be incorporated into the proposed CSA5( ) Zone to foster a safe, comfortable and accessible public realm, and to reduce potential impacts on neighbouring properties:
  - Maximum height;
  - Minimum step-backs above the 4<sup>th</sup> and 7<sup>th</sup> storeys along Riverbend Road that reflect the angular plane provided;
  - Street orientation (principal entrance) toward Riverbend Road;
  - Minimum rear yard setback
  - Maximum exterior side yard setback along Riverbend Road to ensure the building is located close to the public street [TLP Policy 259, RSSP 5.2(ii, iv)].
- The property to the south (1195 Riverbend Road) is within the High Density Residential Overlay area in The London Plan, and is therefore expected to develop with a high-rise form in the future. Urban Design recommends shifting the proposed building northward to allow for a minimum separation distance between the proposed building and the property to the south [TLP Policy 253].

#### Matters for Site Plan:

- Provide architectural details which differentiate the commercial unit entrances from the residential entrances to assist with wayfinding [TLP Policy 301];
- Incorporate a high degree of transparent glazing and articulation in the design of the base and tower portions of the building [TLP Policy 285, 289];
- Reduce the amount of spandrel glazing on the ground floor elevation facing toward Riverbend Road in favour of transparent glazing to provide increased passive surveillance and activation of the public realm [TLP Policy 291];
- Provide a pedestrian walkway that connects between the proposed 'landscape parkette' and the rest of the pedestrian network on site [TLP Policy 255, RSSP 5.3(vi)];
- Ensure pedestrians can safely traverse the site by extending the concrete walkway across the drive aisles within the parking area to delineate these spaces as a pedestrian crossing [TLP Policy 255, 268];

- Explore opportunities for increased outdoor common amenity space and landscaped area on site [TLP Policy 259];
- Screen any surface parking areas exposed to the public street with enhanced all-season landscaping [TLP Policy 278];
- Ensure garbage pick-up areas, loading areas and associated facilities are located away from the public street frontage and do not detract from pedestrian connections [TLP Policy 266];
- Ensure rooftop mechanical and utility equipment is screened and/or incorporated into the overall building design [TLP Policy 296, RSSP 5.2(xv)].

## **Engineering – Revised February 6, 2024**

The City of London’s Environmental and Engineering Services Department offers the following comments with respect to the aforementioned pre-application:

### Re-Zoning Application Comments

#### *Planning & Development*

- Due to sanitary capacity constraints within the Riverbend Rd sewer system, an h-149 holding provision will be requirement of zoning approval for this application. As per the attached commentary Sewer Engineering, a detailed assessment of the actual buildout of the area (to will need demonstrate that the current area has underdeveloped in comparison to the design populations and identify if any potential sewer upgrades are necessary.
- Due to the number of proposed units, a traffic study will need to be submitted and reviewed concurrently with a future site plan application. A h-55 holding provision will be requirement of zoning approval for this application.
- Engineering has no further comments on this application other than the above noted holding provisions.

The following items are to be considered during a future site plan application stage:

#### *Wastewater*

- The subject lands are contemplated as commercial density to the Westdel Bourne outlet, albeit there is no additional surplus capacity for the proposed development.
- Consistent with the Warbler Woods West Phase 2 Subdivision Drainage Area Plan and Design Sheet (25250 & 25254), there is no surplus capacity within the Riverbend Road system for intensification beyond the allocated populations from the Warbler Woods Phase 2 Subdivision. It should be noted that there are four runs of local sanitary sewer on Riverbend Road that were constructed at flatter slopes than captured on the design sheet. This results in sections of local sewer operating above 95% conveyance capacity under the accepted design populations.
- Holding provisions will be required due to conveyance constraints. That being said, SED is amicable to reviewing a detailed assessment of the actual buildout of the area (to completed by the applicants engineer) which must demonstrate that the current area has underdeveloped in comparison to the design populations. If it is determined that there is additional surplus capacity remaining within the system and the proposed population increase will not cause an exceedance of 95% conveyance capacity of the sewer system, SED is amicable to the intensification but will require updated area plans and design sheets to track this un contemplated development.

#### *Water*

- Water is available to the subject site via the existing private 200mm watermain internal to the site. A connection to the municipal watermain on Riverbend Road will not be permitted.

- The owner shall insure that the existing water service has capacity to service the updated use within this existing development. An updated water servicing report and drawings will be required to ensure the existing infrastructure has the capacity to handle the increase in use.
- Water looping requirement shall be addressed.
- Please ensure a regulated drinking water system will not be created with the updated use within the existing development.

*Stormwater:*

- SWED staff have no comments to this pre-application for a zoning by-law amendment and red-line revisions to draft plan of subdivision. All necessary SWM servicing and drainage requirements/controls for this site have been (or will be) provided as part of SPA18-010 and registered development agreement. The Applicant should ensure any impacted/revise as-constructed and drainage area plan drawings are superseded, as necessary.

*Transportation*

- Right-of-way dedication of 18.0 m from the centre line be required along Oxford St. W (Urban Thoroughfare), 22.5m from centre line will be pursued along Oxford St. W (Urban Thoroughfare) within 150m of the intersection.
- A Transportation Impact Assessment (TIA) will be required, the TIA will evaluate the impact the development will have on the transportation infrastructure in the area and provide recommendations for any mitigation measures. The TIA will need to be scoped with City staff prior to undertaking and be undertaken in general conformance with the City's TIA guidelines;
- Detailed comments regarding access design and location will be made through the site plan process.

## Appendix F – Public Engagement

Community Engagement:

On March 17, 2022, Notice of Application was sent to property owners and residents in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on March 17, 2022. A “Planning Application” sign was also placed on the site. On December 21, 2023, Notice of Application was sent to property owners and residents in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on December 21, 2023.

There were 4 responses received during the public consultation period.

### **Public Comment #1 – Jared Alger**

**April 7, 2022**

I received a notice in the email regarding a plan submission for a new development at 1856-1910 Oxford St W by Oxford West Gateway Inc (owned by York Developments I presume?).

As a resident living across the street from this proposed development, I wanted to share some of my concerns.

This apartment complex is set to be built in a plaza parking lot where the majority of people travelling from the East (heading West) will have to make the left turn onto Riverbend Road to access this building – is there a traffic management plan in place for this? This could bring considerable traffic to a two lane road that is used by other residents in the subdivision, people accessing the office/retail in the plaza and, with a school to be built up the road, the possibility of school buses and parents dropping off children. These buildings would also be built in very close proximity to two apartment buildings built by Sifton (I presume that development has been approved) which would also add to the congestion of the area. West5 is being developed just across the road from these buildings and once complete, will also add significant traffic and population to the area, in conjunction with the current traffic volumes from Komoka/Kilworth.

This development would bring significant vehicle traffic that could result in increased noise levels. Has the city considered green space in the area? There isn't much, so where might kids from this building play? I have noticed when out walking my dog there is garbage on the ground from the local restaurants, including food waste and dog waste left behind by irresponsible dog owners. I currently don't see city staff or York Development staff cleaning this up – with an increased population, I would be inclined to believe that this waste will also increase. Does the city or developer have a plan for handling this?

How might the lights from these two buildings, in combination with Sifton's two building, add light pollution to the neighbourhood? Will there be constant bright lights shining in people's homes?

The rendering fails to accurately represent the area in which these apartments are set to be built – it shows mature trees in the background where a building will exist, a large open parking lot which won't exist, and the west rendering appears to show a driveway to somewhere, that, in reality, will take you into the side of a Sifton building. I am confused how they can get away with submitting a rendering that bends the truth a little bit. Having said that, do city staff or the Planning Committee visit the physical site in order to envision what a development like this will look like in the neighbourhood?

As a resident, this development has me very concerned that we are entering into an era of ultra-high density building as there will be 4 apartment building within a very small space. London appears to have lots of undeveloped land, many of which having signs on them for future developments (perhaps just not by this developer). I know the city wants “inward and upward” but I don’t understand how that works with proposals for neighbourhoods as far out as Wonderland Road S by the 402. I understand the builder is looking to maximize their profits by changing from medical/office/retail development to apartments, but to do it at the expense of a nice neighbourhood that already is holding its share of medium and high density residential, seems egregious.

Lastly, an article in the London Free Press indicated that the city is not seeing increased building of single family detached residential homes – if this information is accurate, the approval of the multitude of high density residential in this city is tied, in part, to the failure of our city to approve single family detached homes resulting in the apparent need for these large apartment buildings because the individuals are unable to purchase other accommodation (the same could be said for all the medium density condominium building taking place in the city). Are apartments where people want to live or are they just living there because they don’t have other options?

I hope the city and Planning Committee will decline this proposal. York has significant building taking place in London, so this development won’t ruin their business, but it could ruin a nice neighbourhood which I call home.

Thanks for taking the time to read this (long winded) email.

Regards,

Jared Alger, B. Eng (Aerospace), CPA, CGA

**January 21, 2024**

I hope the past year has treated you well.

I have received an updated notice for the development at Riverbend Road and Oxford St. If I am understanding it correctly, is the developer now looking to build 3 apartment buildings? It says increased height of 40 metres, which sounds like a 10/11 storey building.

The concerns I brought forward in the email below would still exist, and perhaps be greater if there is now a third building to be constructed. Likely more light pollution, more garbage, more traffic congestion, more noise.

So, I am hoping you can help me understand the process involved in approving developments. Is a scoring system used to rank the pros and cons? Do city staff/planners review developments as a team? Is there a listing of items the city looks for in new developments and depending on how many items are achieved, determines the outcome for the developer? Does neighbourhood density play a factor – if so, is it based on the number of units or the number of physical people living in a neighbourhood? There are no grocery stores, “big box” stores (think Shoppers/Rexall) and mostly fast food establishments, meaning people drive more – does that factor into the decision process? This property was originally supposed to fill that gap, by creating space for retail and office, and now that is off the table. There is no transit hub out here (I believe residents out here specifically said they didn’t want BRT), does that impact the decision? It appears that the site plan for West 5 has changed over the years to reduce green space but increase residential, do these types of surrounding developments impact the decision? Or does the traffic from Komoka/Kilworth impact the decision? Sifton was approved to build two building next to this development, that land has now been sold, so if Oxford West Gateway (I presume this is a subsidiary of York) gets approval for 3 buildings, will that new owner not also expect to get the same density, adding more congestion? Is there a neighbourhood in the city that will be as densely populated as this area would be? Does the city consider

how the developer gives back to the population – the developer is aiming to profit as much as possible of the population, do we consider their charitable work in the community?

The application requests reduced setbacks – is this because zoning does not allow for such small setbacks? If so, how often do we allow developers/builders to break these rules?

During the construction phase, where will construction traffic enter this site? How long would construction take? Where will the workers be parking their vehicles - the workers building the retirement home on Southdale near Tillman, park their cars on Southdale creating a traffic hazard, will this be occurring here?

I have put forth a lot of questions because it really does concern mean the amount of density that will exist in this area and how we are building this city. I understand we need housing. We failed to build enough over the past decade but is approving all this mass development the right solution or are we just going to create another problem because we are now rushing to close the gap. It makes me think of how companies grow too big, too fast and then suffer losses and failures as a result. I hope that doesn't become London.

Regards,

Jared Alger, CPA, CGA

### **Public Comment #2 – Timothy Beal**

I am writing to share my **disapproval and concerns** about the this planning application change in my neighbourhood.

I moved into the Warbler Woods neighborhood in 2014, just before the mass development began around me. I have been following the zoning and development closely since. This Sifton project has included a number of different styles of housing that are sandwiched together, leading to increased foot and car traffic throughout my once fairly quite neighborhood. My main concern has increasingly been the traffic into/out of the neighborhood. We already lack a right hand turn lane out of the neighborhood (eastbound onto Oxford) so traffic gets bottlenecked with a single user at that intersection heading north. Further, when there is a break in oncoming eastbound traffic on Oxford, vehicles pull out from Tim Horton's and the Gas station exits filling in those gaps for those looking to turn right at the signal. We also lack a left hand turning signal at the entrance into the neighborhood (southbound on Riverbend), which leads to long waits, especially during rush hours before and after work/school. School buses also take this route and all traffic comes to a standstill when they load/unload. I take this route back and forth almost everyday to get my daughter to school and myself to work and to head towards the city for shopping.

This Westrock property was permitted for commercial use. I was excited in reviewing their early stage website with plans for a "bistro-pub" (since removed from their website) and potentially other neighborhood friendly operations (grocery, deli, bakery, etc.) but mostly what has gone in are take out food joints that take advantage of (and add to) the throughfare traffic on Oxford. The other business are largely specialty providers (law firm, insurance, pharmacy, dentist, elite fitness club, tutoring, etc.) and not places that neighbors would frequent or "window shop". It has been a failure, in my opinion, if the goal was "**to balance neighbourhood stability and predictability with the goals of ... vibrant, interesting communities**" as stated in the justification document. The occupancy has been historically low and slow. My assumption is that the commercial plan has not earned Westrock the money they were expecting, so they are trying to slide in a bunch of residential units to make more profit on a flop of an intended "neighbourhood" shopping centre.

The amount of traffic that would add to the existing main entrance into and out of the neighborhood would be nightmarish. The parking lots too would likely be overflowing with tenants and visitors to these apartments and create an incentive for thieves to strike unattended vehicles overnight. This has already been a problem in the existing single family home areas when cars are left out in the driveway overnight. Further, the land adjacent to Westrock is already zoned for high density and medium density housing, so do we really need any more???? The city is already in the process of plans to expand Oxford Street to accommodate the existing and forecast traffic.

I can see absolutely no benefit of these proposed changes to the neighboring community, only more inconvenience, problems, and traffic. This proposed changes would not be consistent with the stated overarching objectives of: **1. Development that is designed to be a good fit and compatible within its context, 6. High-quality public spaces that are safe, accessible, attractive and vibrant 9. Healthy, diverse and vibrant neighbourhoods that promote a sense of place and character ...** among others. It would also not provide **live-work opportunities**, as stated, in any meaningful manner. I completely disagree with most of the conclusions drawn from the author of this document, who was paid to write this to sell the idea.

Thoughtfully from a concerned resident,  
Timothy Beal

**Public Comment #3 – Carrie O'Brien – Drewlo Holdings**



**“Family Built, Owned and Managed For Over 60 Years”**  
680 Waterloo St., London, ON N6A 0B3  
Tel.: 519-472-8200

January 22, 2024

**Attn: Alanna Riley**  
Senior Planner  
Planning and Development, City of London  
300 Dufferin Avenue  
London, ON N6A 4L9

**Re: O-9484 / Z-9485: 1856-1910 Oxford Street W**

---

Dear Ms. Riley,

On behalf of Drewlo Holdings Inc., please consider this letter our formal comments in relation to Official Plan Amendment application O-9484 and Zoning By-law Amendment application Z-9485 regarding the property municipally known as 1856-1910 Oxford Street West.

In late December 2023, Drewlo Holdings Inc. acquired the lands municipally known as 1195 Riverbend Road which are located directly south of the subject property.

After reviewing the revised Planning & Design Report (November 2023) we are concerned that the analysis and justification does not adequately consider the impacts of the proposed development on 1195 Riverbend Road.

**Requested Amendment to The London Plan:**

As indicated in the notice, it is understood that the Amendment seeks to add a Specific Area Policy to permit a mixed-use development with 840m<sup>2</sup> of ground floor commercial along with two 10 & 11 storey residential towers within the Neighbourhood Place Type.

Policies 1729\_ to 1734\_ of The London Plan outline the requirements associated with Specific Area Policy requests. Policy 1730\_ indicates that Specific Policy requests may be considered in limited circumstances subject to five (5) conditions. These conditions are not addressed in the Planning & Design Report. Drewlo Holdings Inc. does not believe that the proposed development presents a distinguishing or unique feature that would require a specific policy area without setting precedence for other applications fronting a Neighbourhood Connector within the Neighbourhood Place Type.

Within the Neighbourhoods Place Type, the permitted uses fronting a Neighbourhood Connector outside Central London are limited to low density forms of residential (single detached, semi-detached, converted dwellings, townhouses, ARUS's, triplexes), as well discreet institutional or commercial uses (home occupations, group homes, and small-scale community facilities). The height of these uses is limited to a maximum of 3 storeys.

The proposed 10 & 11 storey apartment development is a significant deviation of form and height.

**Requested Zoning By-law Amendment:**

It is understood that the Zoning Amendment seeks to amend the CSA5 Zone as outlined in the notice. Drewlo Holdings Inc. has concerns with the proposal to drastically reduce the interior side yard setback and increase the permitted height.

The required interior yard setback of the standard CSA5 zone requires separation of at least 15.0m from abutting residential zones. The requested amendment seeks to reduce that to 2.0m.

The maximum height of the standard CSA5 zone permits buildings up to 12.0m in height. The proposal seeks to increase that to 40.0m.



1195 Riverbend Rd ("Drewlo lands") has a split residential zone. The "front portion" abutting Riverbend Road is zoned h\*R5-7\*R6-5\*R7\*D75\*H18\*R8-4 permitting various forms of medium density residential development up to 18.0m (+/- 4 storeys) in height. The "back portion" is zoned to support high density residential development, permitting various forms of residential including apartment buildings up to 48m (+/- 12 storeys in height).

The zoning of the "front portion" would permit the development of cluster and stacked townhouse dwelling units +/- 4 storeys in height, located 3-6m\* from the north property line (\*depending on building height and presence of windows to habitable rooms).

Maintaining the current setbacks of the CSA5(6) zone would have ensured that there was at least 18m between structures on the two properties. The proposed ZBA would reduce that to 5m, at the same time it increases the height differential.

The height of the current CSA5(6) zone would have allowed a building 12.0m (+/- 3 storeys) in height. This would be consistent and compatible with the height of permitted development on 1195 Riverbend Road (max 18.0m).

We would request that the existing interior yard setback be maintained to ensure adequate separation between future medium density development on 1195 Riverbend Road and proposed development at 1856-1910 Oxford Street W.

Please do not hesitate to contact the undersigned if there are any questions or concerns. We look forward to being provided notice of future revisions and/or notice of a scheduled public meeting to discuss this application.

Sincerely,  
DREWLO HOLDINGS INC.



Carrie O'Brien  
[cobrien@drewloholdings.com](mailto:cobrien@drewloholdings.com)  
Office: 519-472-8200 x.1077  
Cell: 226-926-5709

## **Public Comment #4– Sifton Properties**



Experience. The Difference.™

February 5, 2024

Alanna Riley  
Senior Planner  
Planning & Development, City of London  
300 Dufferin Avenue  
London, ON N6A 4L9

**Re: O-9484/Z-9485: 1856-1910 Oxford Street West**

Dear Ms. Riley,

On behalf of Sifton Properties Limited (Sifton), please consider the following as our formal written comments with respect to Official Plan Amendment Application (O-9484) and Zoning By-law Amendment Application (Z-9485) applicable to the property municipally known as 1856-1910 Oxford Street West.

### **Requested Amendment**

Per the revised notice of planning application, it is our understanding that an amendment to the official plan and zoning by-law is proposed to add a site-specific policy to permit development of two 10 & 11 storey residential towers with 840 square meters of ground floor commercial and to change the zoning from CSA5(6) to a site specific CSA5( ) zone, respectively.

Per review of the revised Planning & Design Report (November 2023), we are concerned that the reporting has not adequately addressed applicable policies of the London Plan. Further, it is in our opinion that the proposal does not align with the vision of the London Plan and Neighbourhoods Place Type with respect to development with frontage onto a Neighbourhood Connector. Consideration for the market, specifically available/ future supply within the applicable planning district should also be given.

### **The London Plan**

The Our Tools section of the London Plan provides for Specific Area Policies. Site Specific Area Policies 1729-1734 are to be applied where the applicable place type policies would not accurately reflect the intent of City Council with respect to the site or area. These policies were not referenced in the revised Planning & Design Report.



**Sifton Properties Limited | Sifton.com**  
1295 Riverbend Road, Suite 300, London, ON N6K 0G2  
519.434.1000 Fax 519.434.1009



Policy 1730\_ specifies five conditions to be addressed, in consideration of adoption of a site-specific area policy.

*Condition 2 – The proposed policy does not present an adverse impact to the integrity of the place type policies of this Plan.*

Per Policy 919\_2, the range of uses and intensity of development that may be allowed will be related to the classification of the street onto which the property has frontage. The proposed development is located within a block which has frontage onto Oxford Street West (Urban Thoroughfare) and Riverbend Road (Neighbourhood Connector). Per Policy 920\_6, where development is being considered on a lot (block) that has frontage on two or more streets of different classifications but is not located at an intersection, the lower order street will generally be used to establish the permitted uses and intensity of development.

Per Tables 10 and 11, low-rise apartments are identified as a secondary use along a neighborhood connector when intersecting with an urban thoroughfare to a standard maximum height of 4 stories and upper maximum height of 6 stories.

Per Policy 935\_ zoning is to be applied to ensure an intensity of development that is appropriate to the neighbourhood context. The surrounding land uses consist primarily of 1-3 storey commercial to the north and west, existing 1-2 storey multi-family and single detached dwellings to the east and future medium and high-density development to the south. It is to be noted that lands to the south, municipally known as 1195 Riverbend Road, designated under Map 1 as Neighbourhoods, subject to the High Density Overlay (Map 2) and zoned for high density development to a maximum height of 48 m (+/-12 stories) and density of 250 UPH, was established prior to policies of the London Plan coming into full force and effect (May 2022). Previous approvals for development of the lands at 1195 Riverbend Road does not set a precedent for other projects of this nature within the context of the in force and effect framework of the London Plan.

Understanding the above, we are concerned that the proposed specific area policy would lead to adverse impacts to the integrity of policies guiding development along a neighbourhood connector within the neighbourhood place type.

*Condition 3 – The proposed use is sufficiently unique and distinctive such that it does not establish an argument for a similar exception on other properties in the area.*

It is in our opinion that the proposal does not provide unique or distinctive features requiring a site-specific area policy, without establishing a precedent for similar exceptions on other properties.

The London Plan also provides for policies applicable to Zoning to the Upper Maximum Height. Policies 1638-1641 are to be applied when zoning to the upper maximum height and should be considered for proposals which contemplate zoning beyond the upper maximum height as identified in Table 11 of the London Plan.

Policy 1640\_ stipulates that in order to provide certainty and ensure that features required to mitigate impacts of additional height and densities are provided, a site specific ZBA will be required.... Through the amendment process the community, City Council and other stakeholders can be assured that measures will be implemented to mitigate any impacts of additional height or density.

It is in our opinion that while massing/scale at the interface with the neighbourhood connector, amenity space and shadowing impacts have been taken into consideration, the proposed setback specifically to the interior side yard to the south is insufficient to provide appropriate separation between the proposal and future medium density development, to effectively offset the requested increase in height.

#### **Riverbend South Secondary Plan**

The proposal generally conforms with the principles, guidelines and policies of the Riverbend South Secondary Plan. Policy 20.6.3.2 i) stipulates that the Community Commercial Node designation will provide a range of retail and service opportunities to meet the regular shopping needs of local and surrounding residents. In addition, residential units above commercial and office uses will also be permitted to foster live-work opportunities.

Policy 20.6.1.4 speaks to principles of the Secondary Plan. Section 1 x) notes that an objective of the plan is to respect existing low-density uses by providing transition in height and form to promote compatibility.

Policy 20.6.5 speaks to Urban Design principles and guidelines for development of Riverbend South. Section 1 ii) notes that consideration and integration of the existing surrounding context will be undertaken through attention to site and building design...

We are concerned that the proposed specific area policy provides for a level of intensity of development that is too great when considering existing and future residential uses to the east and south.

#### Riverbend Planning District

The proposal is located within the Riverbend Planning District. This district is made up of various existing and expanding communities inclusive of Riverbend Golf Community, Riverbend, West 5 and Warbler Woods. It is to be noted that within these communities, there are approximately 14.51 ha of Sifton owned lands zoned for medium/high density residential uses amounting to approximately 1,679 residential units.

The above demonstrates an abundance of supply with respect to future medium/high density residential units within the communities of Riverbend Golf, West 5 and Warbler Woods. While we are not opposed to permissions for new development to further increase our supply of residential units, we do believe that the level of intensity of new development shall appropriately reflect the vision of the respective place type and street classification.

We respectfully request that the proposal be revised:

- To more appropriately reflect the use and intensity envisioned along a neighbourhood connector per applicable policies of the London Plan;
- To consider existing and future adjacent residential uses to ensure compatibility and to provide an appropriate transition in use, intensity and form; and
- To consider the available/future supply of high density residential within the Riverbend Planning District to ensure that mix and range of housing options are provided.

Sincerely,



Phil Masschelein  
Senior Vice President, Neighbourhood Developments  
Sifton Properties Limited

## Report to Planning and Environment Committee

**To:** Chair and Members  
Planning and Environment Committee  
**From:** Scott Mathers, MPA, P.Eng.  
Deputy City Manager, Planning and Economic Development  
**Subject:** Palumbo Properties Ltd. (c/o Zelinka Priamo Ltd.)  
279 Sarnia Road  
File Number: Z-9774, Ward 6  
Public Participation Meeting  
**Date:** October 22, 2024

## Recommendation

That, on the recommendation of the Director, Planning and Development, the following actions be taken with respect to the application of Palumbo Properties Ltd. relating to the property located at 279 Sarnia Road:

- (a) The proposed by-law attached hereto as Appendix "A" **BE INTRODUCED** at the Municipal Council meeting on November 5, 2024, to amend Zoning By-law No. Z.-1, in conformity with the Official Plan, The London Plan, to change the zoning of the subject property **FROM** a Residential R1 (R1-10) Zone, **TO** a Holding Residential R8 Special Provision (h-213\*R8-4(\_)) Zone;
- (b) The Site Plan Approval Authority **BE REQUESTED** to consider the following design issues through the site plan process:
  - i) Consent to remove any boundary trees is required prior to final Site Plan Approval;
  - ii) Provide a low-height all-season landscape buffer for the below-grade units fronting onto Sarnia Road and/or the parking area to provide privacy and avoid headlights shining into the spaces;
  - iii) Consider providing additional windows on the side elevation of the corner units, and minimizing any portion of blank wall facing the public realm, to create an active streetscape and offer passive surveillance;
  - iv) Relocation of the existing Hydro Pole and London Transit Commission shelter pad at the expense of the applicant.

**IT BEING NOTED**, that the above noted amendment is being recommended for the following reasons:

- i) The recommended amendment is consistent with the *Provincial Planning Statement, 2024 (PPS)*;
- ii) The recommended amendment conforms to The London Plan, including but not limited to the Key Directions, City Design and Building policies, and the Neighbourhoods Place Type policies.
- iii) The recommended amendment would permit an appropriate form of development at an intensity that is appropriate for the site and surrounding neighbourhood.

## Executive Summary

### Summary of Request

The applicant has requested an amendment to the Zoning By-law Z.-1 to rezone the property from a Residential R1 (R1-10) Zone to a Residential R8 Special Provision (R8-4(\_)) Zone. Special provisions are requested to permit: a minimum front yard setback of 3.2 metres; a minimum westerly interior side yard setback of 3.0m; and a maximum density of 91 units per hectare.

### Purpose and the Effect of Recommended Action

The recommended action will permit a 3-storey, **20-unit** back-to-back stacked

townhouse development.

Staff are recommending approval of the requested Zoning By-law amendment and special provisions, with additional special provisions to ensure there are no adverse impacts on adjacent properties. A holding provision is also recommended to ensure that development will not occur until a sanitary servicing capacity report has been prepared and confirmation that a municipal sanitary sewer outlet is available to service the site to the satisfaction of the City Engineer (h-213).

## Linkage to the Corporate Strategic Plan

This recommendation will contribute to the advancement of Municipal Council's 2023-2027 Strategic Plan in the following ways:

- **Wellbeing and Safety**, by promoting neighbourhood planning and design that creates safe, accessible, diverse, walkable, healthy, and connected communities.
- **Housing and Homelessness**, by ensuring London's growth and development is well-planned and considers use, intensity, and form.
- **Housing and Homelessness**, by supporting faster/streamlined approvals and increasing the supply of housing with a focus on achieving intensification targets.

## Analysis

### 1.0 Background Information

#### 1.1 Property Description and Location

The subject lands are located on the north side of Sarnia Road between Kininvie Drive and Brescia Lane, in the Medway Planning District. The site has a total area of approximately 0.22 hectares, with frontage of 32.6 metres along Sarnia Road and a depth of approximately 71.8 metres. The lands currently contain a single-storey single detached dwelling and an accessory structure, with the remainder of the subject site consisting of grass and vegetated areas. Vehicular access is currently provided by a driveway connecting to Sarnia Road on the eastern portion of the subject site.

The subject lands are within an urban neighbourhood consisting primarily of low-density residential uses in the form of single detached dwellings. The neighbouring properties to the west and north contain a single detached dwelling. Directly to the east is an institutional building (Mary Manor) associated with Brescia University College. Broader surrounding land uses include a mix of low-rise residential, multi-unit residential dwellings for students, Western University facilities and open space.

Sarnia Road is classified as a Civic Boulevard on Map 3 – Street Classifications of The London Plan. The road segment between Brescia Lane and Kininvie Drive has a traffic volume of approximately 24,000 vehicles per day. Sarnia Road has public sidewalks on both sides of the street, and a signalized pedestrian crossover is located approximately 75 meters to the east at the intersection of Brescia Lane and Sarnia Road. The subject lands are within walking distance of Brescia University College, Western University, and commercial uses along Western Road. Finally, approximately 10 metres to the east of the subject lands is an LTC stop serviced by LTC transit routes 9, 10, 27 and 31.

#### Site Statistics:

- Current Land Use: Residential
- Frontage: 32.6 metres (107 feet) onto Sarnia Road
- Depth: 62.7 metres (221 feet)
- Area: 0.22 hectares (0.54 acres)
- Shape: Regular (rectangle)
- Located within the Built Area Boundary: Yes
- Located within the Primary Transit Area: Yes

#### Surrounding Land Uses:

- North: Residential

- East: Institutional
- South: Residential
- West: Residential

**Existing Planning Information:**

- Existing The London Plan Place Type: Neighbourhoods
- Existing Special Policies: Near-Campus Neighbourhood
- Existing Zoning: Residential R1 (R1-10) Zone

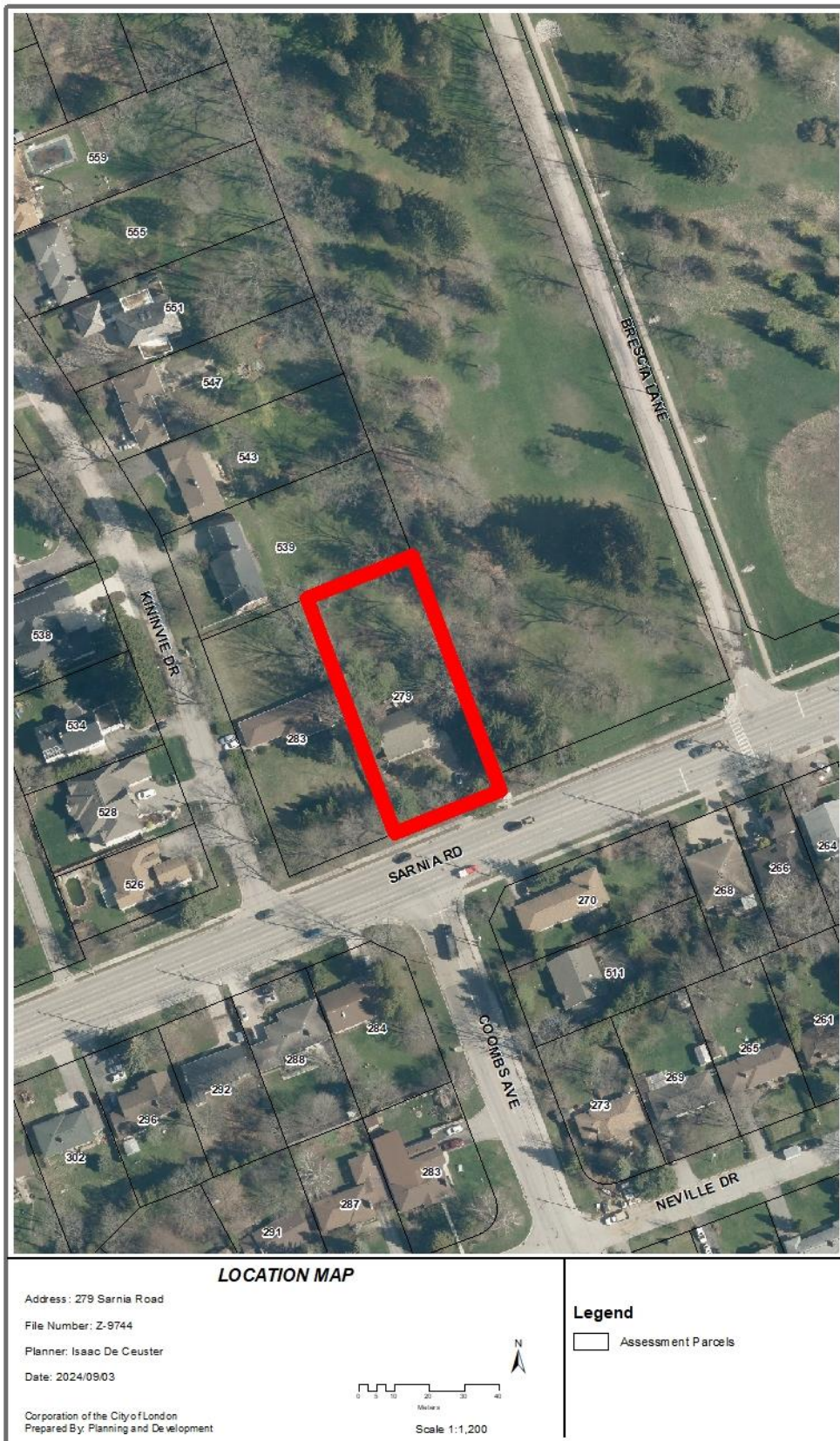


Figure 1- Aerial Photo of 279 Sarnia Road and surrounding lands.





Figure 2 - Streetview of 279 Sarnia (view looking north)

## 2.0 Discussion and Considerations

### 2.1 Development Proposal

In August 2024, the City accepted a complete Zoning By-law Amendment to permit two blocks of 3-storey, 20-unit, back-to-back stacked townhouse developments with a maximum density of 91 units per hectare. Block A is proposed to front on Sarnia Road, and Block B is proposed in the rear yard. Each residential unit is proposed to have a total of three bedrooms, with an average unit-size of 106 m<sup>2</sup> (1150 square feet).

A common outdoor amenity space is proposed on the northeasterly portion of the subject lands, connected to the internal sidewalk for convenient access for future residents. A new driveway is proposed to provide access from Sarnia Road to the parking area with a total of 14 parking spaces including one (1) Type A accessible parking space and two (2) visitor parking spaces (0.5 spaces per unit). Pedestrian connections are provided with a 1.5 metre internal sidewalk leading to the individual entrances, amenity area and public sidewalk.

The proposed development includes the following features:

- Land use: residential
- Form: stacked townhouse development
- Height: 3 storeys (11.3 m)
- Residential units: 20 units
- Density: 91 units per hectare
- Building coverage: 28.2%
- Parking spaces: 14 surface parking spaces
- Landscape open space: 43.9%
- Functional amenity space: outdoor common amenity space

The conceptual site plans are shown below as Figure 3. Building renderings and elevations are shown in Figures 4 and 5 below.

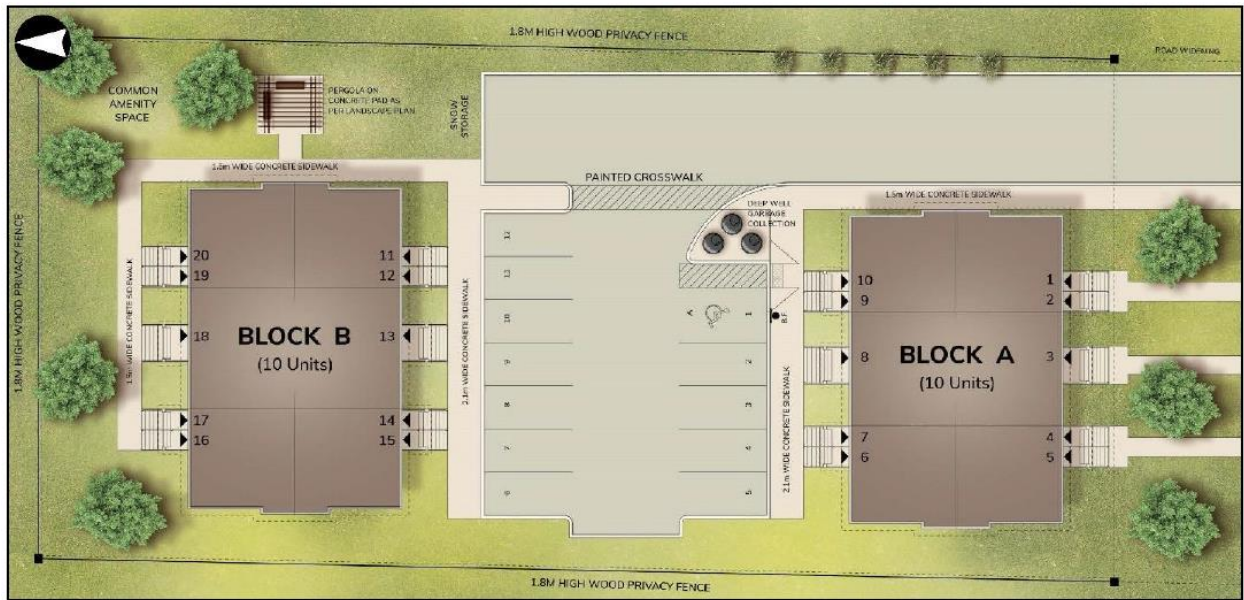


Figure 3 - Conceptual Site Plan (Received August 2024)



Figure 4 – Front Elevation Block A - 279 Sarnia Road.

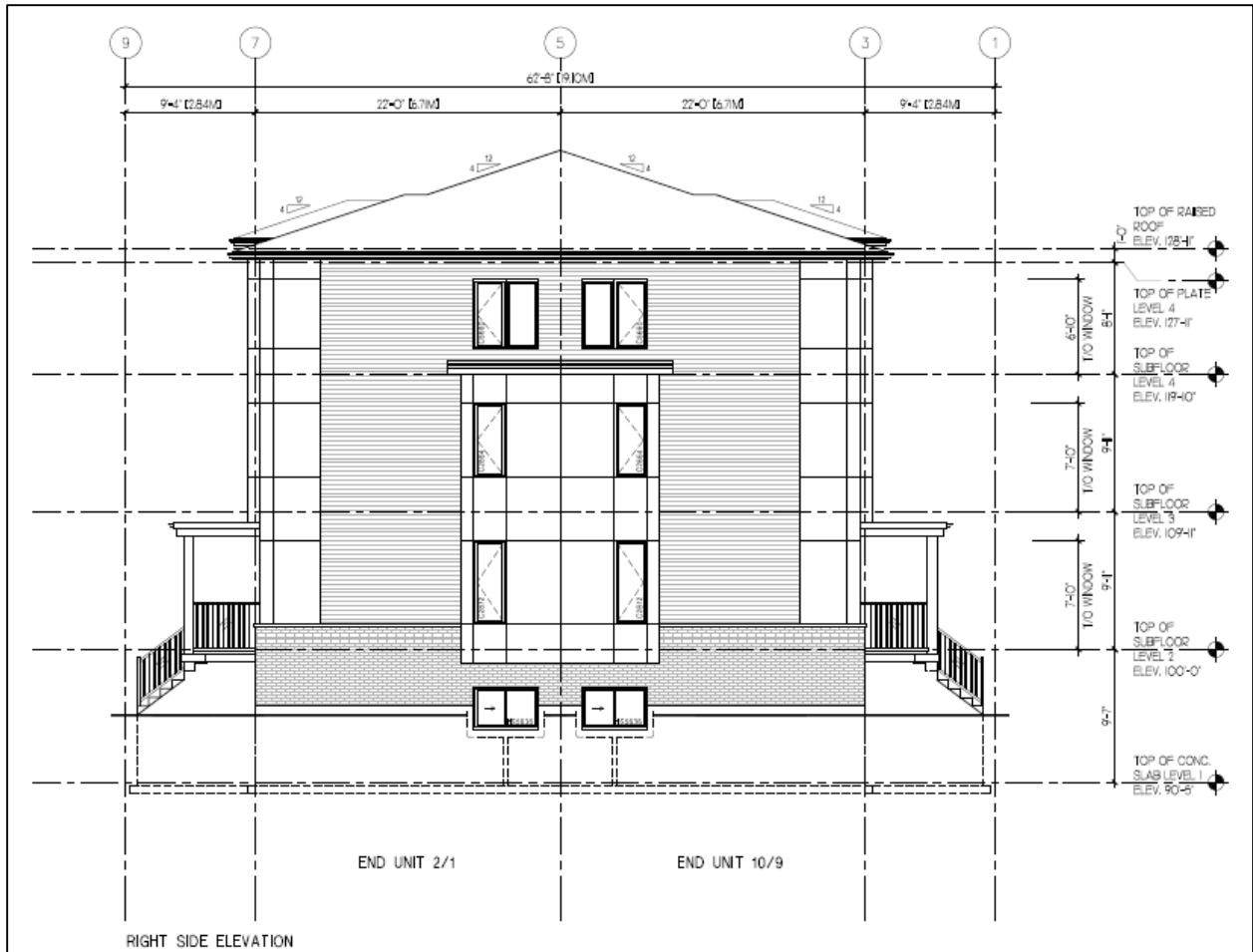


Figure 5 – Right Side Elevation Block A - 279 Samia Road.

Additional information on the development proposal is provided in Appendix “B”.

## 2.2 Requested Amendment

The applicant has requested an amendment to the Zoning By-law Z.-1 to rezone the property from a Residential R1-10 Zone to a Residential R8 Special Provision (R8-4(\_)) Zone

The following table summarizes the special provisions that have been proposed by the applicant and those that are being recommended by staff.

Regulation (R8-4 Zone)	Required	Proposed
Minimum Front Yard Setback (metres)	7.0	3.2
Minimum Interior Side Yard – West (metres)	4.8	3.0
Maximum Density (units per hectare)	75	91

The following table summarizes the special provisions that have been recommended by staff:

Regulation (R8-4 Zone)	Required	Proposed
Maximum Front Yard Setback (metres)	7.0	4.5
Minimum Rear Yard Setback (metres)	8.0m where principal entrances and windows face the rear yard	9.2

## 2.3 Internal and Agency Comments

The application and [associated materials](#) were circulated for internal comments and public agencies to review. Comments received were considered in the review of this application and are addressed in Section 4.0 of this report.

Key issues identified by staff and agencies included:

- A h-213 holding provision which addresses sanitary capacity concerns will be required as part of zoning approval.
- The proposed access is in conflict with the existing Hydro Pole and LTC shelter pad. Costs for relocation of these and any other utilities will be the onus of the applicant.

Detailed internal and agency comments are included in Appendix “D” of this report.

## **2.4 Public Engagement**

On August 23, 2024, Notice of Application was sent to 39 property owners and residents in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on September 5, 2024. A “Planning Application” sign was also placed on the site.

There were two responses received during the public consultation period. Comments received were considered in the review of this application and are addressed in Section 4.0 of this report.

Concerns expressed by the public relate to:

- Lack of parking

Detailed public comments are included in Appendix “E” of this report.

## **2.5 Policy Context**

### ***The Planning Act and the Provincial Planning Statement, 2024***

The Provincial planning policy framework is established through the *Planning Act* (Section 3) and the *Provincial Planning Statement, 2024 (PPS)*. The *Planning Act* requires that all municipal land use decisions affecting planning matters shall be consistent with the *PPS*.

The mechanism for implementing Provincial policies is through the Official Plan, The London Plan. Through the preparation, adoption and subsequent Ontario Land Tribunal (OLT) approval of The London Plan, the City of London has established the local policy framework for the implementation of the Provincial planning policy framework. As such, matters of provincial interest are reviewed and discussed in The London Plan analysis below.

As the application for a Zoning By-law amendment complies with The London Plan, it is staff’s opinion that the application is consistent with the *Planning Act* and the *PPS*.

### ***The London Plan, 2016***

The London Plan (TLP) includes evaluation criteria for all planning and development applications with respect to use, intensity and form, as well as with consideration of the following (TLP 1577-1579):

1. Consistency with the Provincial Policy Statement and all applicable legislation.
2. Conformity with the Our City, Our Strategy, City Building, and Environmental policies.
3. Conformity with the Place Type policies.
4. Consideration of applicable guideline documents.
5. The availability of municipal services.
6. Potential impacts on adjacent and nearby properties in the area and the degree to which such impacts can be managed and mitigated.
7. The degree to which the proposal fits within its existing and planned context.

Staff are of the opinion that all the above criteria have been satisfied.

### **3.0 Financial Impact/Considerations**

#### **3.1 Financial Impact**

There are no direct municipal financial expenditures associated with this application.

### **4.0 Key Issues and Considerations**

#### **4.1 Land Use**

The proposed residential use is supported by the policies of the PPS 2024 and aligns with the vision of the Neighbourhoods Place Type by contributing to a diversity and mix of different housing types, intensities, and forms (TLP 918\_2), and will respect the existing neighbourhood character (TLP 918\_13). The proposed residential use aligns with Key Direction #5 – Building a mixed-use compact city, by planning for infill and intensification of various types and forms to take advantage of existing services and facilities and to reduce our need to grow outward (TLP 59\_4), and to ensure a mix of housing types within our neighbourhoods so that they are complete and support aging in place (TLP 59\_5).

The Neighbourhoods Place Type contemplates a range of residential uses, dependent upon the street classification on which the property has frontage. As set out in Table 10, the range of permitted uses along a Civic Boulevard include stacked townhouses (TLP Table 10 – Range of Permitted Uses in Neighbourhoods Place Type). As such, the proposed stacked townhouse dwellings are a permitted use in conformity with the Neighbourhoods Place Type policies of The London Plan.

#### **4.2 Intensity**

The proposed residential intensity is consistent with the policies of the PPS 2024 that encourage all types of residential intensification, including redevelopment which results in a net increase in residential uses (2.2.1.2), compact form (2.4.1.3.c), and an appropriate mix of housing options and densities (2.3.1.3). Table 11 of The London Plan provides a range of permitted heights in the Neighbourhoods Place Type based on street classification (TLP Table 11 – Range of Permitted Heights in Neighbourhoods Place Type). As the subject site has frontage on a Civic Boulevard, the minimum permitted height is 2 storeys and the standard maximum height is 4 storeys.

The proposed residential intensity will facilitate an appropriate scale of development, that is compatible with the existing neighbourhood character and can be accommodated on a parcel that is of sufficient size to support the use. The redevelopment of the parcel will facilitate the efficient use of land and is in conformity with the residential intensification policies of The London Plan that encourage infill development on vacant or underutilized lots (TLP 939\_5).

Based on the above, the proposed 3 storey stacked townhouse development with 20 residential units is in conformity with the intensity policies of the Neighbourhoods Place Type in The London Plan.

#### **4.3 Form**

The proposed built form is supported by the policies of the PPS 2024 and is consistent with the Neighbourhoods Place Type policies and the City Design policies of The London Plan by facilitating an appropriate form and scale of residential intensification that is compatible with the existing neighbourhood character (TLP 953\_2). Specifically, the proposed built form supports a positive pedestrian environment on Sarnia Road, a mix of housing types within the neighbourhood to support ageing in place, and is designed to be a good fit and compatible with the surrounding context (TLP 193\_).

The proposed built form consists of two 3-storey back-to-back stacked townhouse blocks, with Block A proposed fronting Sarnia Road and Block B in the rear yard. The

proposed built form and massing of the stacked back-to-back townhouse blocks have consideration for the surrounding land uses and is appropriate in scale compared to the surrounding neighbourhood character (TLP 953\_2).

Access to the subject lands will be provided through a new driveway off Sarnia Road, promoting connectivity and safe movement for pedestrians, cyclists, and motorists (TLP 255\_). Parking for the proposed stacked back-to-back townhouse development will be located internally, visually screening the surface parking from the street, encouraging a pedestrian oriented streetscape (TLP 936\_4).

The proposed stacked back-to-back townhouse development with special provisions for front yard setbacks, west interior side yard setbacks and density are supported by the policies of the PPS 2024, contemplated in the Neighbourhoods Place Type, and in keeping with the City Design policies of The London Plan.

#### **4.4 Near-Campus Neighbourhood**

The subject lands are located within a Near-Campus Neighbourhood on Map 7 – Specific Policy Areas of The London Plan. The Near-Campus Neighbourhoods are located within proximity to Western University and Fanshawe College and are identified as extremely valuable city neighbourhoods that will be planned to enhance their livability, diversity, vibrancy, culture, sense of place, and quality of housing options for all (TLP 962\_ to 964\_). The subject site is identified as being in proximity to Western University. The policies are meant to augment the applicable place type policies and the Our Tools within The London Plan (TLP 962\_).

A number of planning goals have been established to serve as an additional framework for all planning applications (TLP 965\_), including:

- Plan for residential intensification in a proactive, coordinated, and comprehensive fashion;
- Identifying strategic locations where residential intensification is appropriate within Near-Campus Neighbourhoods and which use strong transit connections to link these opportunities to campuses;
- Avoiding incremental changes in use, density, and intensity that cumulatively lead to undesirable changes in the character and amenity of streetscapes and neighbourhoods;
- Encourage appropriate forms of intensification that support the vision for Near-Campus Neighbourhoods and encouraging residential intensification in mid-rise and high-rise forms of development;
- Recognize areas that have already absorbed significant amounts of residential intensification and residential intensity and direct proposals for additional intensification away from such areas;
- Encouraging a balanced mix of residential structure types at appropriate locations while preserving stable residential areas;
- Encourage residential intensification in mid-rise or high-rise forms of development and discourage a concentration of residential intensification and residential intensity in low-rise forms of housing;
- Directing residential intensification to transportation nodes and corridors and away from interior of neighbourhoods;
- Utilizing zoning and other planning tools to allow for residential intensification, which is appropriate in form, size, scale, mass, density, and intensity.
- Ensuring that residential projects incorporate urban design qualities that enhance streetscapes and contribute to the character of the neighbourhood while respecting the residential amenity of nearby properties;
- Conserve heritage resources in ways that contribute to the identity of streetscapes and neighbourhoods, in compliance with the Cultural Heritage chapter of The London Plan;
- Encourage affordable housing opportunities; and,
- Ensure intensification is located and designed to respect the residential amenity of nearby properties.

The Near-Campus Neighbourhoods policies in The London Plan set out that intensification may occur in some locations within the Neighbourhoods Place Type where it is permitted in Tables 10 to 12 and meets the Near-Campus Neighbourhoods policies of The London Plan (TLP 967\_). In this instance, the proposed redevelopment will provide intensification on an underutilized site on a Civic Boulevard with strong transit connections. The proposed zoning will ensure that the development will be appropriately accommodated on site, and within the surrounding neighbourhood context. Further, the proposed development provides for a built form that is considered compatible and respectful of the residential amenity of nearby properties. The design of the site will enhance the streetscape and contribute to the overall character of the neighbourhood by providing a mix of housing types in proximity to Western University.

The Near-Campus Neighbourhood policies in The London Plan have been reviewed in their entirety and it is staff's opinion that the proposed Zoning By-law amendment is in conformity.

#### **4.5 Zoning**

The applicant has requested an amendment to Zoning By-law Z.-1 to rezone the subject site from a Residential R1 (R1-10) Zone to a Residential R8 Special Provision (R8-4(\_)) Zone. The following summarizes the special provisions that have been proposed by the applicant and those that are being recommended by staff.

**Front Yard Depth** – The applicant is requesting a special provision to permit a front yard depth of 3.2 metres, whereas a minimum of 7.0 metres is required. A 7.94 metre road widening is provided in the front yard to facilitate an ultimate width of 18.0 metres from centerline for Sarnia Road. The reduced front yard setback is supported by the policies of The London Plan, which state that buildings should be sited close to the street to maintain and reinforce the prevailing street wall and create an inviting and comfortable pedestrian environment (TLP 259\_). Staff is supportive of the reduced front yard setback. Staff are recommending an additional special provision to permit a maximum front yard setback of 4.5 metres to discourage parking in the front yard.

**Interior Side and Rear Yard Depth** – The applicant is requesting a special provision to permit a westerly interior side yard depth of 3.0 metres, whereas a minimum of 4.5 metres is required. Based on the development concept and existing neighbourhood context, staff have no concerns with the reduced west interior side yard. Staff are recommending an additional special provision to permit a minimum rear yard setback of 8.0 metres where principal entrances and windows face the rear yard to allow for privacy and not hinder adjacent properties. This recommended special provision conforms to the City Building Policies, providing that site layout should be designed to minimize and mitigate impacts on adjacent properties (TLP 253). Additional mitigation measures include a 1.8 metre wood privacy fence along the lot lines to reduce privacy concerns on abutting parcels, and landscape buffers. These measures will be addressed through the Site Plan Approval process.

**Density** – The applicant is requesting a special provision to permit a density of 91 units per hectare, whereas 75 units per hectare is the maximum permitted in the requested R8-4 Zone variation. The proposed maximum density provision will allow for the implementation of the proposed development, facilitating an appropriate scale of development that is compatible with the existing neighbourhood character (TLP 918\_13). Staff are supportive of the increased density to facilitate intensification on a Civic Boulevard within the Urban Growth Boundary and in proximity to public services and public transit.

#### **4.6 Noise Study**

MTE Consultants was retained by the applicant to complete an Environmental Noise Assessment for the proposed residential development. The purpose of this study is to evaluate the traffic noise impacts from forecasted traffic volumes of Sarnia Road on the subject property, and to recommend noise control measures to meet the Ministry of the

Environment, Conservation and Parks (MECP) guidelines and planning requirements of the City. The analysis found that a noise attenuating barrier is not required for the subject site.

For Block A fronting Sarnia Road, architectural details such as walls, windows and doors, are to be verified by a qualified Acoustical Professional to ensure the required sound transmission loss rating will be acceptable to meet indoor sound levels. Furthermore, all units shall be fitted with central air conditions and a Noise Warning Clause shall be registered on title. Implementation of the recommendations of the Environmental Noise Study will be required through a future Site Plan application.

#### **4.7 Trees**

MTE Consultants was retained by the applicant to complete a detailed tree inventory for the proposed development. A total of 38 trees were inventoried for this Tree Preservation Plan. Twenty-five (25) trees are intended to be removed to accommodate the proposed development, eight (8) of these trees are boundary trees and will require neighbour's consent, and three (3) trees are located on the City of London's boulevard and require City permits for removal. Thirteen (13) trees will be preserved.

Detailed review and approval of the Tree Preservation Plan will occur through a future Site Plan application. In addition, consent to remove the above noted boundary trees will be required prior to final Site Plan Approval.

## **Conclusion**

The applicant has requested an amendment to the Zoning By-law Z.-1 to rezone the property from a Residential R1 (R1-10) Zone to a Holding Residential R8 Special Provision (h-213\*R8-4(\_)) Zone. Staff are recommending approval of the requested Zoning By-law amendment and special provisions with a holding provision.

The recommended action is consistent with the PPS 2024, conforms to The London Plan and will permit a 3-storey, 20-unit, back-to-back stacked townhouse development.

**Prepared by:** Isaac de Ceuster  
Planner, Planning Implementation

**Reviewed by:** Catherine Maton, MCIP, RPP  
Manager, Planning Implementation

**Recommended by:** Heather McNeely, MCIP, RPP  
Director, Planning and Development

**Submitted by:** Scott Mathers, MPA, P.Eng.  
Deputy City Manager, Planning and Economic  
Development

Copy: Britt O'Hagan, Manager, Current Development  
Mike Corby, Manager, Site Plans  
Brent Lambert, Manager, Development Engineering



## Appendix A – Zoning Bylaw Amendment

Bill No. (number to be inserted by Clerk's Office)  
2024

By-law No. Z.-1-

A by-law to amend By-law No. Z.-1 to rezone an area of land located at 279 Sarnia Road.

WHEREAS this amendment to the Zoning By-law Z.-1 conforms to the Official Plan;

NOW THEREFORE the Municipal Council of The Corporation of the City of London enacts as follows:

1. Schedule "A" to By-law No. Z.-1 is amended by changing the zoning applicable to lands located at 279 Sarnia Road, as shown on the attached map **FROM** a Residential R1 (R1-10) Zone **TO** a Holding Residential R8 Special Provision (h-213\*R8-4(\_)) Zone.
2. Section Number 12.4 of the Residential R8 (R8-4) Zone is amended by adding the following Special Provisions:  
  
R8-4(\_) 279 Sarnia Road
  - a. Regulations
    - i) Front Yard Setback (minimum) – 3.0m
    - ii) Front Yard Setback (maximum) – 4.5m
    - iii) Interior (West) Side Yard Setback (minimum) – 3.0m
    - iv) Rear Yard Setback (minimum) - 8.0m where principal entrances and windows to habitable rooms face the rear yard
    - v) Density (maximum) – 91 units per hectare
3. This Amendment shall come into effect in accordance with Section 34 of the *Planning Act, R.S.O. 1990, c. P13*, either upon the date of the passage of this by-law or as otherwise provided by the said section.

PASSED in Open Council on November 5, 2024, subject to the provisions of PART VI.1 of the *Municipal Act, 2001*.


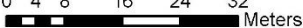

Josh Morgan  
Mayor

Michael Schulthess  
City Clerk

First Reading – November 5, 2024  
Second Reading – November 5, 2024  
Third Reading – November 5, 2024

AMENDMENT TO SCHEDULE "A" (BY-LAW NO. Z.-1)



<p>File Number: Z-9744  Planner: IDC  Date Prepared: 2024/09/03  Technician: JI  By-Law No: Z.-1-</p>	<p>SUBJECT SITE </p> <p>1:800</p> <p>0 4 8 16 24 32 Meters </p> <p></p>
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Geodatabase

## Appendix B - Site and Development Summary

### A. Site Information and Context

#### Site Statistics

Current Land Use	Residential
Frontage	32.6 metres (107 feet)
Depth	62.7 metres (221 feet)
Area	0,22 hectares (0.54 acres)
Shape	Regular
Within Built Area Boundary	Yes
Within Primary Transit Area	Yes

#### Surrounding Land Uses

North	Residential
East	Institutional
South	Residential
West	Residential

#### Proximity to Nearest Amenities

Major Intersection	Sarnia Road and Western Road, 500 metres
Dedicated cycling infrastructure	Brescia Lane, 75 metres
London Transit stop	Sarnia Road at Brescia Lane, 5 metres
Public open space	Chesham Heights Park, 700 metres
Commercial area/use	Sarnia Road & Western Road, 650 metres
Food store	Food Basics, 1,500 metres
Community/recreation amenity	Medway Community Centre, 1,400 metres

### B. Planning Information and Request

#### Current Planning Information

Current Place Type	Neighbourhoods, Civic Boulevard
Current Special Policies	Near-Campus Neighbourhood
Current Zoning	Residential R1 (R1-10) Zone

#### Requested Designation and Zone

Requested Place Type	N/A
Requested Special Policies	N/A
Requested Zoning	Residential R8 (R8-4(_)) Zone

#### Requested Special Provisions

Regulation (R8-4 Zone)	Required	Proposed
Front Yard Setback (minimum)	7.0m	3.2m
Interior Side Yard Setback (minimum)	4.8m	3.0m
Density (maximum)	75 uph	91 uph

### C. Development Proposal Summary

#### Development Overview

The purpose and effect of the recommended action is to permit the development of a 3-storey, 20-unit, back-to-back stacked townhouse development with 14 surface parking spaces and a maximum density of 91 units per hectare.

### Proposal Statistics

Land use	Residential
Form	Stacked back-to-back townhouse development
Height	3-storeys (11.3 metres)
Residential units	20
Density	91 units per hectare
Gross floor area	2089.55 m <sup>2</sup>
Building coverage	28.2%
Landscape open space	43.9%
New use being added to the local community	Yes

### Mobility

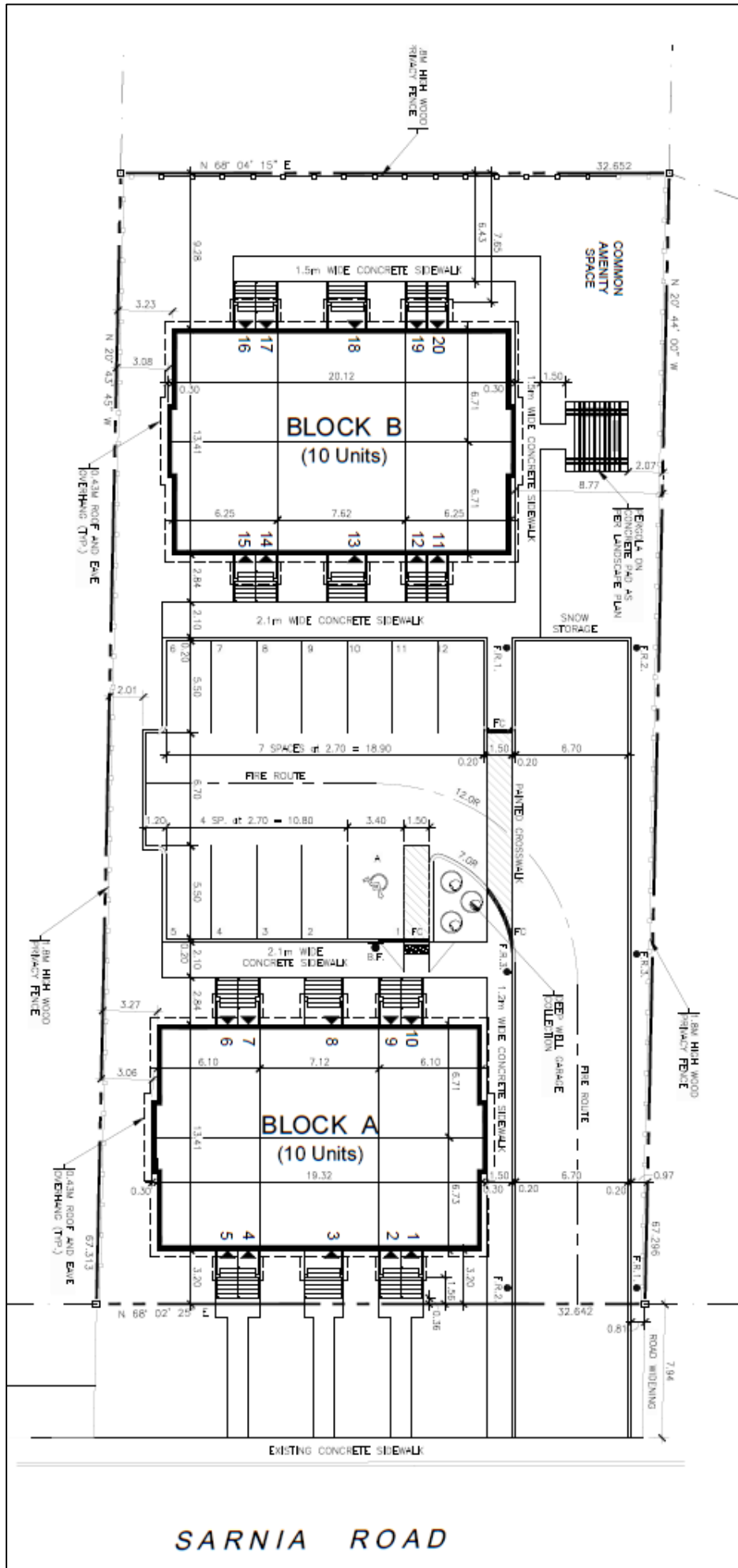
Parking spaces	14 surface parking spaces
Vehicle parking ratio	0.5 spaces per unit
New electric vehicles charging stations	TBD
Secured bike parking spaces	Determined through Site Plan Approval
Secured bike parking ratio	Determined through Site Plan Approval
Completes gaps in the public sidewalk	N/A
Connection from the site to a public sidewalk	Yes
Connection from the site to a multi-use path	N/A

### Environment

Tree removals	Yes
Tree plantings	TBD
Tree Protection Area	No
Loss of natural heritage features	No
Species at Risk Habitat loss	No
Minimum Environmental Management Guideline buffer met	N/A
Existing structures repurposed or reused	No
Green building features	Unknown

# Appendix C – Additional Plans and Drawings

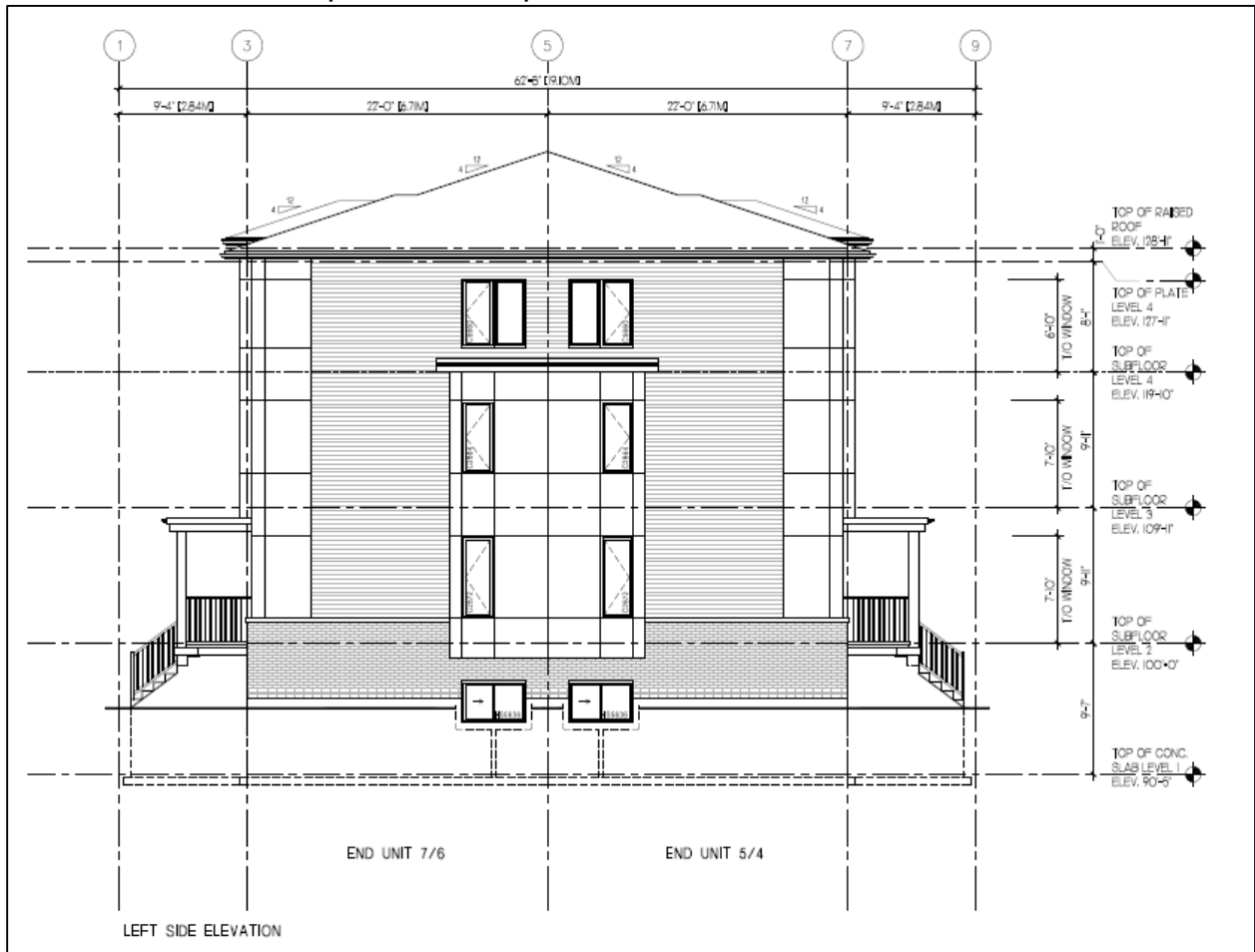
## Conceptual Site Plan 279 Sarnia Road



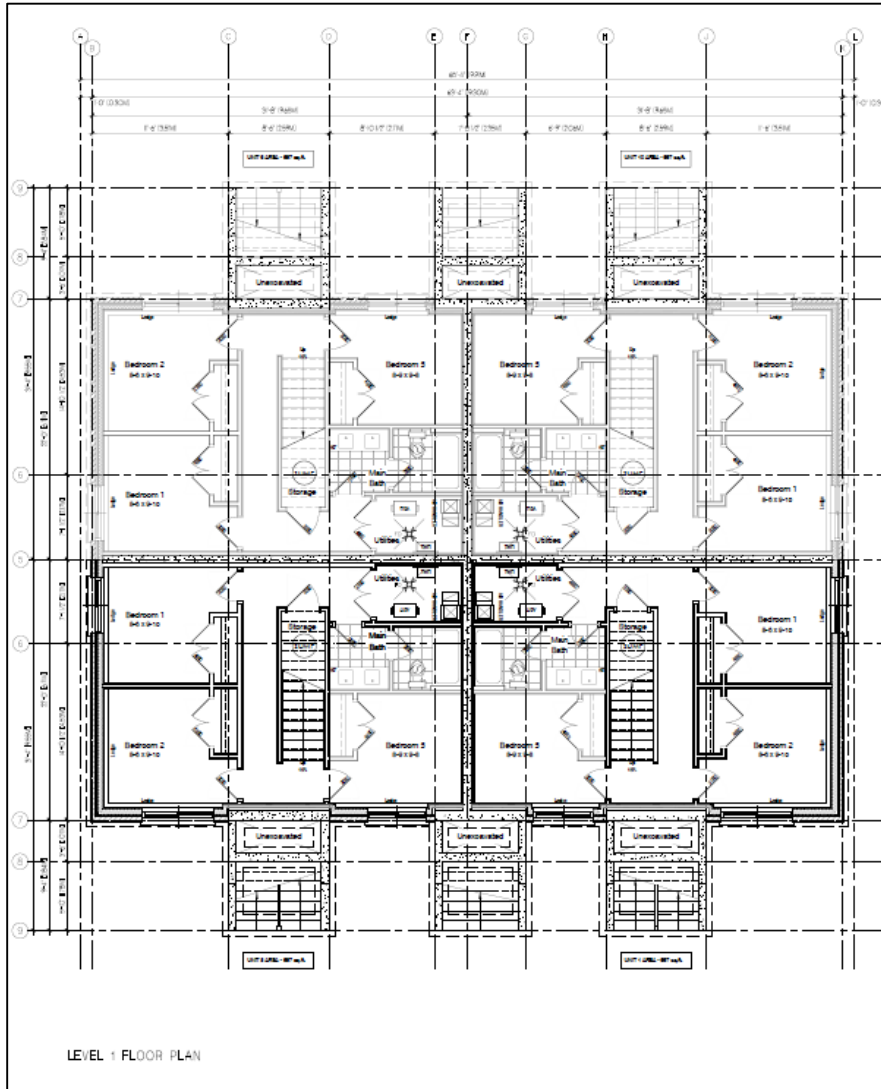
Rear Elevation Proposed Development 279 Sarnia Road



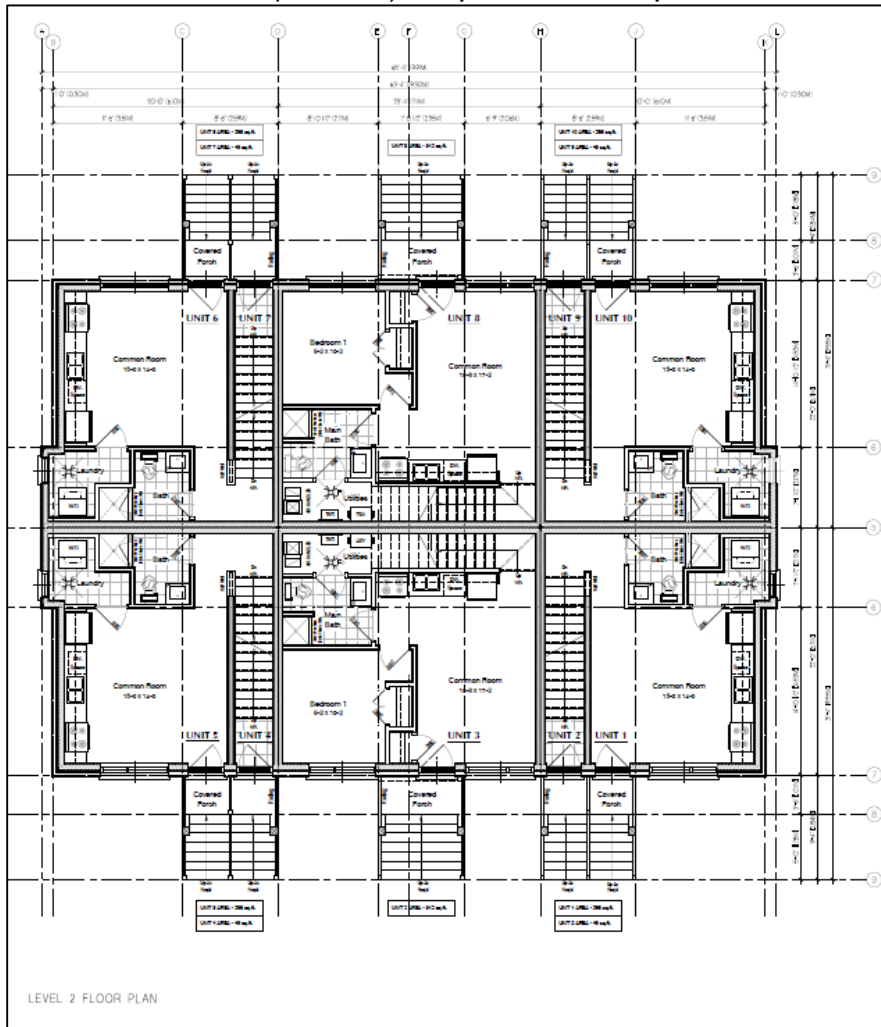
Left Side Elevation Proposed Development 279 Sarnia Road



Level 1 Floor Plan (Block A) Proposed Development

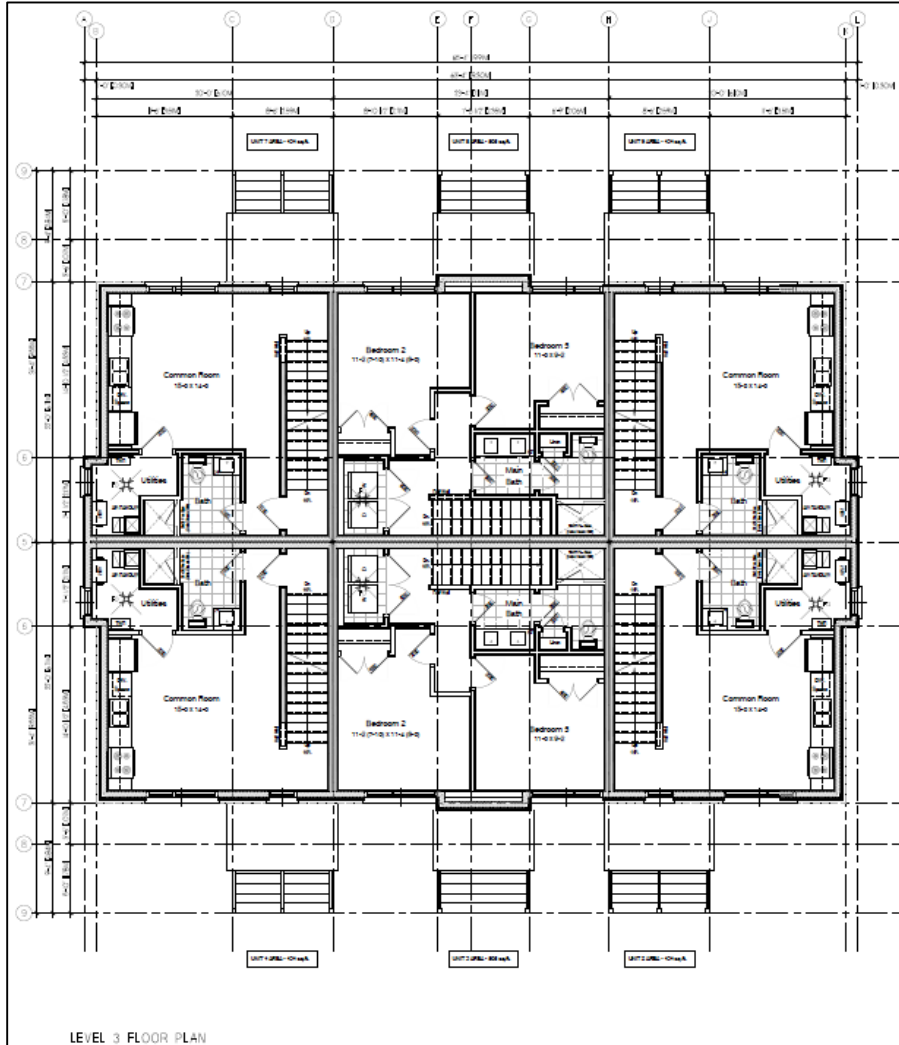


Level 2 Floor Plan (Block A) Proposed Development

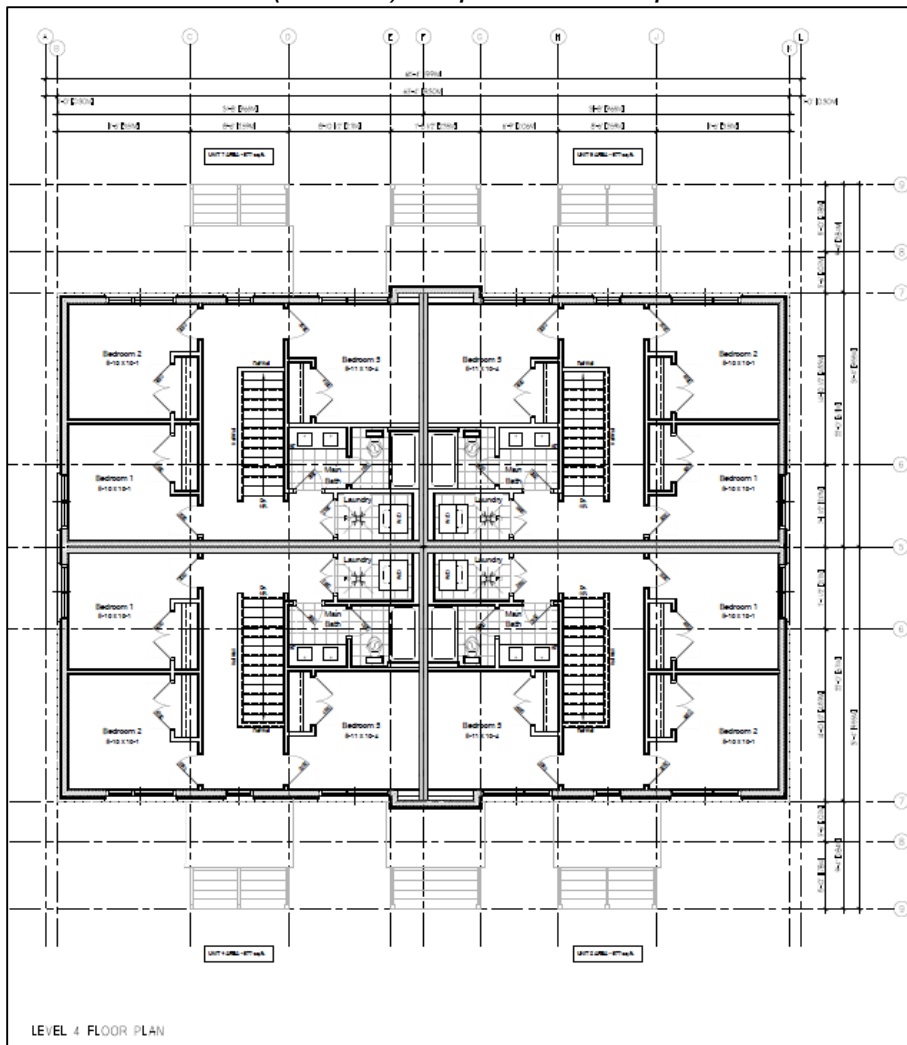




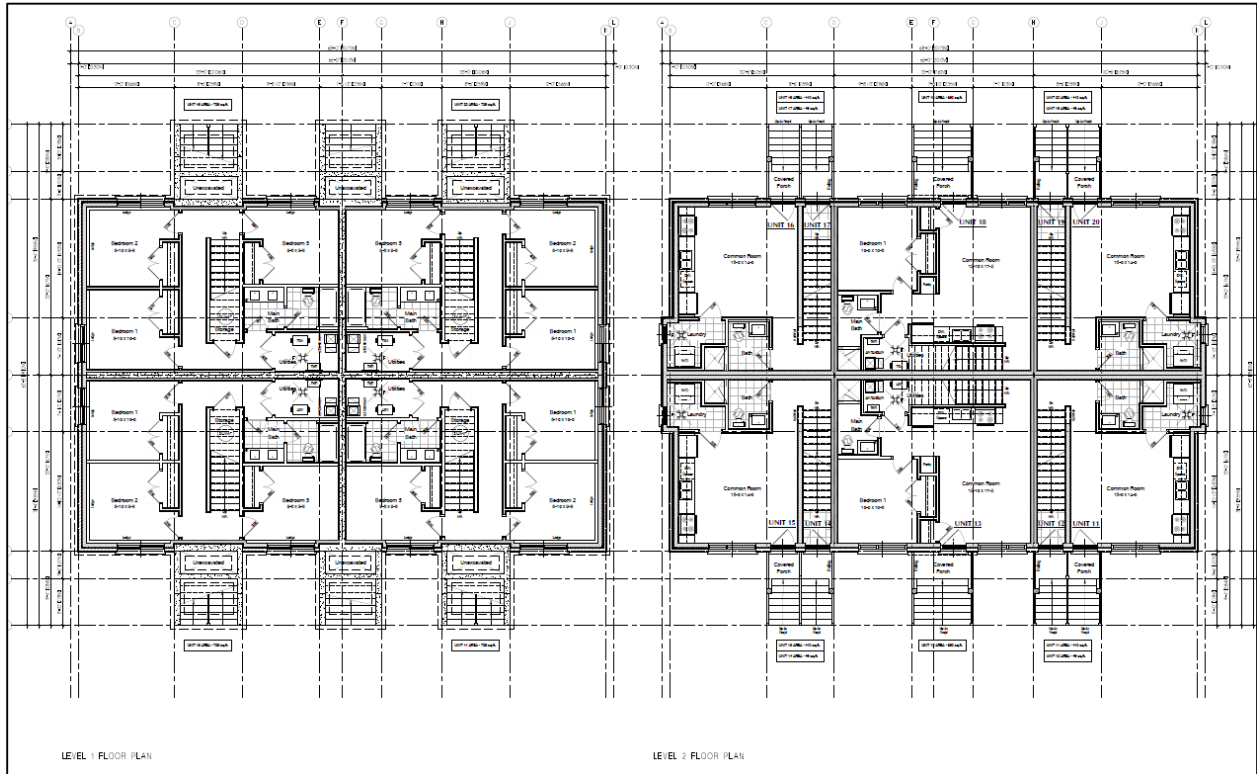
*Level 3 Floor Plan (Block A) Proposed Development*



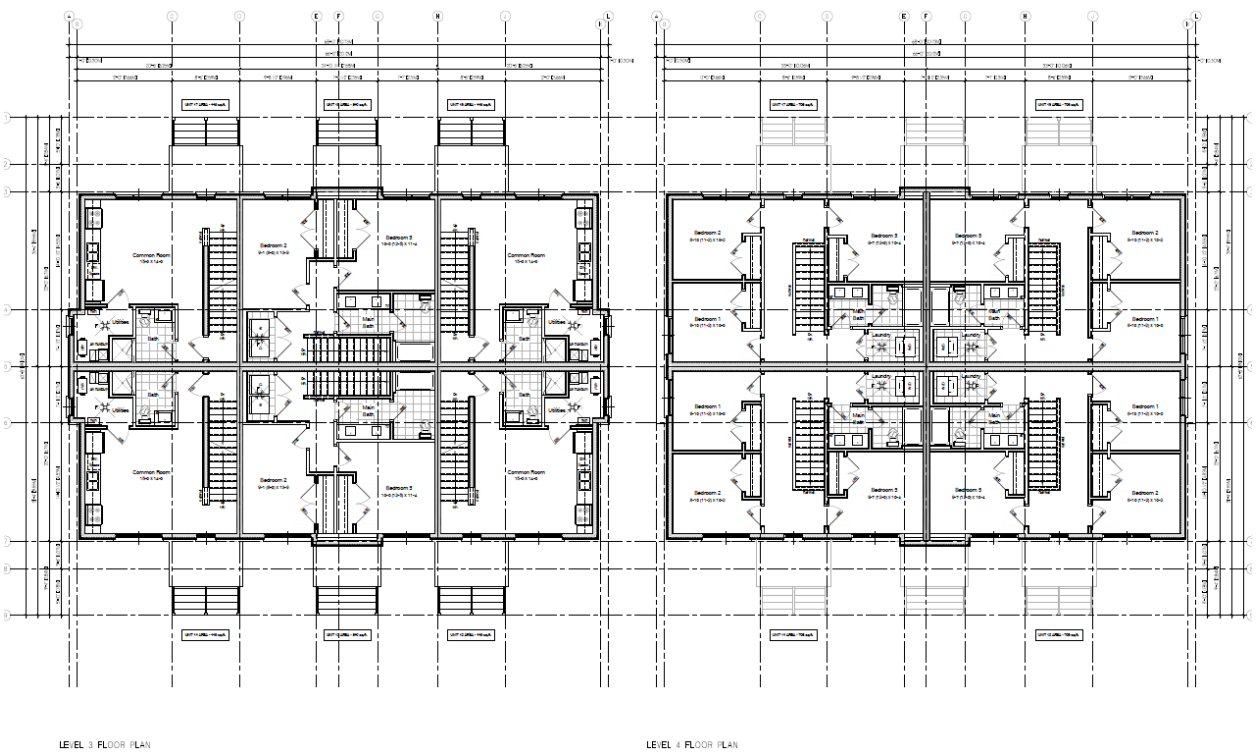
*Level 4 Floor Plan (Block A) Proposed Development*



*Level 1 (left) and Level 2-Block B Proposed Development*



*Level 3 (left) and Level 4 right -Block B Proposed Development*



## Appendix D – Internal and Agency Comments

### Urban Design

Urban Design is **generally supportive of the proposed development** and has the following comments:

#### Matters for Zoning:

1. **Provide a minimum front yard setback of 3m** to encourage street-orientation while avoiding encroachment of footings and canopies and to accommodate a landscape buffer to avoid potential negative impacts on the below-grade spaces. TLP 259, 286, 288
2. **Provide a maximum front yard setback of 4.5m** to discourage parking in the front yard. TLP 272
3. **Provide a minimum interior side-yard setback of 3m** to allow privacy and not hinder redevelopment of the adjacent property. TLP 253
4. **Provide a minimum rear yard setback of 8m where principal entrances and windows to habitable rooms face the rear yard** to allow for circulation and an adequate landscape buffer in the rear yard. TLP 253

#### Matters for Site Plan:

1. The following site layout and building design features are supported and should be carried forward through site plan approval process:
  - o Orienting the townhouses to Sarnia Road with principal entrances, windows and balconies facing the Civic Boulevard and providing direct walkway access from both the townhouse blocks to the public sidewalks. TLP, 285, 286, 288, 291
2. Consider providing additional windows on the side elevation of the corner units that are facing the driveway to offer passive surveillance. TLP 290
3. Minimize any portion of blank wall facing the public realm to create an active streetscape and offer passive surveillance. TLP 285, 228
  - o Provide windows on the blank central portion of the building, above the unit 3 entrance facing Sarnia Road.
4. The board on board fence provided along the property line is noted. To further minimize any potential negative impacts on the adjacent properties, provide an all-season landscape buffer along the property line, particularly to the west and north. TLP 253
5. Provide adequate lighting, particularly for the principal entrances of the rear units (16, 17, 18, 19 & 20) and the common amenity space, for safe circulation and alleviate any CPTED concerns. TLP 253, 228
  - o Avoid any light encroachment or spillover onto to the adjacent properties
6. Provide a low-height all-season landscape buffer for the below-grade units fronting onto Sarnia Road and/or the parking area to provide privacy and avoid headlights shining into the spaces. TLP 235.
7. Ensure the elevations are labelled as per the cardinal directions.

### Parks Planning & Design

Parks Planning and Design staff have reviewed the submitted notice of application and offer the following comments:

1. Major Issues
  - a. None.
2. Matters for OPA/ZBA
  - a. None.
3. Matters for Site Plan
  - a. Parkland dedication has not been taken for this site. It is to be noted that the applicant, as a condition of site plan approval, will be required to

provide parkland dedication in the form of cash-in-lieu pursuant to By-law CP-25.

## **Heritage**

There are no cultural heritage or archaeological concerns with this application.

## **Site Plan**

Please note the following for 279 Sarnia Road:

1. Major Issues
  - The proposed access is in conflict with existing Hydro Pole and LTC shelter pad. Costs for relocation of these and any other utilities will be the onus of the applicant.
2. Matters for OPA/ZBA
  - Based on the provided drawings, special provisions are required for the following:
    - o Front Yard Setback,
    - o Interior Side Yard Setback (west),
    - o Density.
  - See the attached Zoning Referral Record for more information.
3. Matters for Site Plan
  - Ensure that all sidewalks are a minimum of 1.5 metres wide, including those leading from the public sidewalk to the street facing units.
  - Provide enhanced landscape screening along the west and rear property lines to protect the neighbouring low density residential properties.
  - Update the Fire Route details to match Site Plan Control By-law standards.
  - Update the Accessibility details to match Site Plan Control By-law standards.
  - Explore options to provide 1.5 metres of clearance around the proposed accessible curb ramps.
  - Label and provide signage for the visitor parking spaces, noting that 1 visitor parking space is required for every 10 residential units.
  - Ensure that all required fire route signs are provided in accordance with the Site Plan Control By-law requirements.
4. Complete Application Requirements
  - Noise Study

## **Landscape Architect**

1. Major Issues
  - No potential grounds for refusal, or issues that could require significant changes to the proposal.
2. Matters for OPA/ZBA
  - No matters that will influence the OP/ZBL mapping, designation/zone, regulations, special provisions, holding provisions, etc.
3. Matters for Site Plan
  - Boundary trees were identified for removal in the tree preservation plan [#6, #7, #16, #22, #29, #30, #33, #38], consent to remove boundary trees is a requirement of Site Plan approval. A recommendation for approval will be forwarded for Site Plan Review. If consent cannot be obtained from co-owner, then a non-disturbance setback will need to be established at each tree's critical root zone limits as determined by dbh.

Tree #	Co-owner	Critical Root Zone Diameter [m]
6	Western University	6.8
7	Western University	1.2
16	Western University	8.1
22	283 Sarnia Rd	3.3
29	283 Sarnia Rd	4.2
30	283 Sarnia Rd	3.2
33	283 Sarnia Rd	1.7
34	283 Sarnia Rd	10.2
35	283 Sarnia Rd	5.0
38 *not on plan	283 Sarnia Rd	2.3

- Boundary tree 34, is proposed for preservation, the development will remove 34% of its critical root zone of the tree. The critical root zone of a tree is the portion of the root system that is the minimum necessary to maintain tree vitality and stability. Consent to injure boundary trees is a requirement of Site Plan approval. A recommendation for approval will be forwarded for Site Plan Review. If consent cannot be obtained from co-owner, then a non-disturbance setback will need to be established at each tree's critical root zone limits as determined by dbh.
- Boundary tree 35 is proposed for preservation, the development will remove 27% of its critical root zone of the tree. The critical root zone of a tree is the portion of the root system that is the minimum necessary to maintain tree vitality and stability. Consent to injure boundary trees is a requirement of Site Plan approval. A recommendation for approval will be forwarded for Site Plan Review. . If consent cannot be obtained from co-owner, then a non-disturbance setback will need to be established at each tree's critical root zone limits as determined by dbh.
- Replacement trees to be recommendation to Site Plan Review based on total dbh removed. 495 cm dbh is proposed for removal, in accordance with LP Policy 399, 49 replacement trees are required. However, the city is in the process of developing a bylaw to implement Policy 399, so the follow ratio will be applied: 1 replacement tree for a trunk diameter of 50.99 cm, 2 replacement trees for a trunk diameter between 51cm and 60cm, and 1 additional replacement tree for every 10cm of trunk diameter thereafter to a maximum of 11 replacement trees. To this end 11 replacement trees would be required. Tree planting required as part of the planning and development approvals process may be counted as replacement trees as required by these policies.

## Ecology

This e-mail is to confirm that there are currently no ecological planning issues related to this property and/or associated study requirements.

### Major Issues Identified

- None

### Matters for OPA/ZBA

- None

### Matters for Site Plan

- Consider use of bird-friendly glass treatments where trees are in proximity to glazing, particularly on the east side of the development.

### Ecology – Complete Application Requirements

- None

## Notes

- Avoid tree removal within the active bat roosting period (April 30 – September 1) to reduce potential interactions with Endangered bat species, to avoid contravention of the Endangered Species Act.
- Avoid vegetation removal within the active breeding bird period (April 1 – August 30) to avoid disturbing nesting birds and contravening the Migratory Bird Convention Act.

## **UTRCA**

- The subject lands **are not** affected by any regulations (Ontario Regulation 41/24) made pursuant to Section 28 of the *Conservation Authorities Act*.
- The UTRCA has **no objections** to the application and we have no Section 28 approval requirements.

## **Engineering**

The City of London's Environmental and Engineering Services Department offers the following comments with respect to the aforementioned zoning-application:

### Zoning Application Comments

#### Wastewater

- SED does not currently support any intensification that is ultimately tributary to the sanitary sewer on Coombs Ave as there is a section of sanitary sewer that is operating over conveyance capacity. This pipe is from maintenance hole PC113 to PC 114. The city has no planned projects at this time that will provide additional capacity.

#### Planning & Development

- Engineering has no further comments on the above noted application. An h-213 holding provision which addresses sanitary capacity concerns will be required as part of zoning approval.
- The following comments shall be addressed by the applicant at the site plan application stage.

### Matters for Site Plan

#### Planning & Development

- Servicing for the units fronting Sarnia Rd must be accommodated internal to the site. Individual PDC and water service connections to Sarnia Rd will not be permitted

#### Wastewater

- Applicant to provide a sanitary servicing brief that detail maximum population and peak flows for the subject site.

#### Stormwater

- The City cannot confirm a storm pdc exists to service the property. In accordance with the Drainage By-law, the consultant would be required to provide for a storm pdc, with connection to Sarnia Road (existing drainage).
- The proposed land use of medium/high density residential will trigger the application of design requirements of Permanent Private Storm System (PPS) as approved by Council resolution on January 18, 2010. A standalone Operation and

Maintenance manual document for the proposed SWM system is to be included as part of the system design and submitted to the City for review.

- As per the City of London's Design Requirements for Permanent Private Systems, the proposed application falls within the Central Subwatershed (case 4), therefore the following design criteria should be implemented:
- the flow from the site must be discharged at a rate equal to or less than the existing condition flow;
  - the discharge flow from the site must not exceed the capacity of the stormwater conveyance system;
  - the design must account the sites unique discharge conditions (velocities and fluvial geomorphological requirements);
  - "normal" level water quality is required (70% TSS removal) as per the MECP guidelines; and
  - shall comply with riparian right (common) law.
- The Developer shall be required to provide a Storm/drainage Servicing Report demonstrating that the proper SWM practices will be applied to ensure the maximum permissible storm run-off discharge from the subject site will not exceed the peak discharge of storm run-off under pre-development conditions up to and including 100-year storm events. On-site SWM controls design should include, but not be limited to required storage volume calculations, flow restrictor sizing, bioswales, etc.
- As part of climate change resiliency objectives the consultant is to use best efforts to maximize the provided site storage. The consultant is encouraged to make use of rooftop storage.
- The Owner agrees to promote the implementation of SWM Best Management Practices (BMP's) within the plan, including Low Impact Development (LID) where possible, to the satisfaction of the City Engineer.
- Any proposed LID solutions should be supported by a Geotechnical Report and/or a Hydrogeological Assessment report prepared with a focus on the type(s) of soil present at the Site, measured infiltration rate, hydraulic conductivity (under field saturated conditions), and seasonal high groundwater elevation. The report(s) should include geotechnical and hydrogeological recommendations of any preferred/suitable LID solution. All LID proposals are to be in accordance with Section 6 Stormwater Management of the Design Specifications & Requirements manual.
- The owner is required to provide a lot grading plan for stormwater flows and major overland flows on site, ensuring that stormwater flows are self-contained and that grading can safely convey up to the 250 year storm event, all to be designed by a Professional Engineer for review.
- The existing topography directs overland flows from the subject property through 427 Eaton Park Drive. The consultant is to reduce the overland flow through the private property, and ensure that Stormwater run-off from the subject lands shall not cause any adverse effects to adjacent or downstream lands. The proposed overland flow route for the development should convey flows to Topping Lane ROW as best practical.
- An erosion/sediment control plan that will identify all erosion and sediment control measures for the subject site and that will be in accordance with City of London and MECP standards and requirements, all to the specification and satisfaction of the City Engineer. This plan is to include measures to be used during all phases of construction. These measures shall be identified in the Storm/Drainage Servicing Report.

#### Water

- Water is available to the site via municipal 300mm watermain on Sarnia Road.
- The Site is in the City's low Level service area, which has a hydraulic grade line of 301.8 m.
- Existing water service shall be abandoned to City Standard (Cut and Cap from main)

- A water servicing report will be required addressing domestic demands, fire flows, water quality.
- Water servicing for stacked townhouses shall be in accordance City Standards 7.9.4.5
- Water servicing shall be configured in a way to avoid the creation of a regulated drinking water system.

#### Transportation

- This property is subject to a 7.942 metre widening to achieve the required 18.0m from centreline.
- Detailed comments regarding access design and location will be made through the site plan process.



## Appendix E – Public Engagement

On August 23, 2024, Notice of Application was sent to 39 property owners and residents in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on September 5, 2024. A “Planning Application” sign was also placed on the site.

There were two responses received during the public consultation period.

**Nature of Liaison:** The purpose and effect of this Zoning amendment is to permit two blocks of 3-storey back-to-back stacked townhouse development with a total of 20 residential units, and a total maximum density of 91 units per hectare. Possible change to Zoning By-law Z.-1 **FROM** a Residential R1 (R1-10) zone **TO** a Residential R8 Special Provision (R8-4(\_)) Zone. Requested special provisions for front yard setback, westerly interior side yard setback and maximum density. The City may also contemplate alternative zoning such as a different base zone, additional permitted uses, additional special provisions and/or the use of holding provisions. File Z-9774. Planner: Isaac de Ceuster

### Public Comment #1 – Sandy Levin

Please find attached a letter regarding this file from the Executive of the Orchard Park/Sherwood Forest Neighbourhood Association. If you have any questions or new information, please do not hesitate to contact us.

Sandy Levin, president

September 4, 2024

Isaac de Ceuster  
Planner, City of London  
Re: **Z-9774 , 279 Sarnia Road**

We attended the non statutory meeting the proponent and his agent conducted and have reviewed the Planning Justification Report. We circulated the notice and information to our e-mail list of over 500 residents of our neighbourhood. It is the opinion of the Executive that we have no objections to the as proposed three story buildings with units having 3 bedrooms and managed by Mr. Palumbo. If there is any change to these, we reserve the right to a different opinion.

Sincerely,

By e-mail  
Sandy Levin, President  
On behalf of Members of the Executive Committee  
Orchard Park/Sherwood Forest Neighborhood Association  
London, ON,

cc: Councillor Trosow (by e mail)  
H. Froussios, Zelinka Priamo (by e mail)

### Public Comment #2 – Liddy Johnston

Good morning,  
I realize that this is a little late, but I have been away.  
I have one concern with the proposed development at 279 Sarnia Rd. There are to be 20 units, each with 3 bedrooms. However, there are only 14 parking spaces. This is not even one per unit. Where does the builder propose that tenants park? I live on

Bloomfield Drive. In most cases, rental properties have multiple vehicles parked in the driveways. I have seen up to 6 vehicles in some driveways in the Orchard Park area. Thank you for your consideration of this matter.

Libby Johnston

## Report to Planning and Environment Committee

**To:** Chair and Members  
Planning and Environment Committee

**From:** Scott Mathers, MPA, P.Eng.  
Deputy City Manager, Planning and Economic Development

**Subject:** G.R. Investments Ltd. & Sub-Subsational Inc. c/o Dillon Consulting Ltd.  
1210-1240 Wharncliffe Road South  
File Number: Z-9778, Ward 12

**Date:** Public Participation Meeting on: October 22, 2024

## Recommendation

That, on the recommendation of the Director, Planning and Development, the following actions be taken with respect to the application of G.R. Investments Ltd. & Sub-Subsational Inc. (c/o Dillon Consulting Ltd.) relating to the property located at 1210-1240 Wharncliffe Road South:

- (a) the proposed by-law attached hereto as Appendix "A" **BE INTRODUCED** at the Municipal Council meeting November 5, 2024, to amend Zoning By-law No. Z.-1, in conformity with the Official Plan, The London Plan, to change the zoning of a portion of the subject property **FROM** a Restricted Service Commercial (RSC1/RSC3/RSC4/RSC5) Zone and Urban Reserve (UR4) Zone, **TO** a Residential R5 Special Provision (R5-7(\_)) Zone and an Open Space (OS5) Zone;
- (b) The Site Plan Approval Authority **BE REQUESTED** to consider the following design issues through the site plan process:
  - i) Further refinement to the site access location and design.
  - ii) Explore opportunities to screen any parking areas exposed to the public street with enhanced all-season landscaping and incorporate significant mature trees.

**IT BEING NOTED**, that the above noted amendments are being recommended for the following reasons:

- i) The recommended amendments are consistent with the *Provincial Planning Statement, 2024 (PPS)*;
- ii) The recommended amendments conform to The London Plan, including but not limited to Key Directions, City Design and Building policies, and the Neighbourhoods Place Type policies.
- iii) The recommended amendments conform to the Southwest Area Secondary plan, including but not limited to the North Longwoods Residential Neighbourhood policies, and
- iv) The recommended amendment would permit an appropriate form of development at an intensity that is appropriate for the site and surrounding neighbourhood.

## Executive Summary

### Summary of Request

The applicant has requested an amendment to the Zoning By-law Z.-1 to rezone a portion of the property from a Restricted Service Commercial (RSC1/RSC3/RSC4/RSC5) Zone and Urban Reserve (UR4) Zone to a Residential R5 Special Provision (R5-7(\_)) Zone and an Open Space (OS5) Zone.

### Purpose and the Effect of Recommended Action

Staff are recommending approval of the requested Zoning By-law amendment with special provisions to permit a 3.5-storey stacked townhouse development with a

maximum density of 54 units per hectare.

Special provisions requested by the applicant and recommended by staff include: a minimum lot frontage of 13.0 metres, whereas a minimum of 30.0 metres is required; a minimum parking ratio of 0.7 vehicle parking spaces per unit, whereas a minimum parking ratio of 1.0 vehicle parking spaces per unit is required.

The recommended action will permit a 3.5 storey, **108-unit** stacked townhouse development with a maximum height of 12.0 metres.

## Linkage to the Corporate Strategic Plan

This recommendation will contribute to the advancement of Municipal Council's 2023-2027 Strategic Plan in the following ways:

- **Wellbeing and Safety**, by promoting neighbourhood planning and design that creates safe, accessible, diverse, walkable, healthy, and connected communities.
- **Housing and Homelessness**, by ensuring London's growth and development is well-planned and considers use, intensity, and form.
- **Housing and Homelessness**, by supporting faster/streamlined approvals and increasing the supply of housing with a focus on achieving intensification targets.

## Analysis

### 1.0 Background Information

#### 1.1 Previous Reports Related to this Matter

*1210 Wharncliffe Road South:*

**December 9, 2002** – Information Report to Planning Committee on the North Longwoods Area Plan Review.

**May 12, 2003** – Report and Public Participation Meeting to Planning Committee on the introduction of the North Longwoods Area Plan and corresponding Official Plan Amendments (O-6424)

**June 30, 2003** – Report to Planning Committee on the introduction of the North Longwoods Area and Plan and corresponding Official Plan Amendments after referral back to Staff (O-6424)

**July 6, 2007** – Report to the London Consent Authority regarding a Consent to create an easement (B.037-07)

**September 24, 2007** – Report to the London Consent Authority regarding a Consent to create an easement (B.046-07)

*1240 Wharncliffe Road South:*

**December 9, 2002** – Information Report to Planning Committee on the North Longwoods Area Plan Review.

**May 12, 2003** – Report and Public Participation Meeting to Planning Committee on the introduction of the North Longwoods Area Plan and corresponding Official Plan Amendments (O-6424)

**June 30, 2003** – Report to Planning Committee on the introduction of the North Longwoods Area and Plan and corresponding Official Plan Amendments after referral back to Staff (O-6424)

## 1.2 Planning History

In 2003, the North Longwoods Area Plan (NLAP) was prepared for 106 hectares of land bounded by Wharncliffe Road South, Southdale Road East, White Oaks Road, and the future Bradley Avenue extension. The NLAP was created to respond to development demands in the area and re-designated the lands “Urban Reserve – Community Growth”.

The lands are also subject to the *Southwest Area Secondary Plan (SWAP)*, and more specifically the policies for the North Longwoods Residential Neighborhood. *SWAP* was approved by the Ontario Municipal Board on April 29, 2014, and is intended to provide a comprehensive land use plan, servicing requirements and servicing strategy for the lands south of Southdale Road, east of the Dingman Creek and north of the Highway 401/402 corridor.

In 2007, two Consent applications were made to establish an easement for 1210 Wharncliffe Road over the lands at 1150 and 1200 1210 Wharncliffe Road South. The Consent request B.037-07 was presented to the Committee of Adjustment on July 6, 2007, and granted on August 27, 2007.

## 1.2 Property Description and Location

The subject lands are located on the south side of Wharncliffe Road South between Bradley Avenue and Legendary Drive, in the Longwoods Planning District. The subject lands consist of two lots, 1210 Wharncliffe Road South and 1240 Wharncliffe Road South which have a combined area of approximately 5.12 hectares, frontage of 120.2 metres along Wharncliffe Road South, and depth of approximately 235.1 metres. 1210 Wharncliffe Road South contains an automobile sales and service establishment at the front of the site. 1240 Wharncliffe Road South contains a commercial building and associated surface parking, which are proposed to remain. Vehicular access is currently provided by three driveways connecting to Wharncliffe Road South.

The lands subject to this Zoning By-law Amendment form the rear-portion of the larger parcel and has a total area of approximately 1.98 hectares (4.9 acres). Following a future severance of the commercial portion of the larger subject site fronting Wharncliffe Road, access to the proposed development will be provided from Lismer Avenue, with a frontage of 13.0 metres along Lismer Avenue. The subject lands are delineated with red lines in Figure 1 below, while the larger parcel municipally addresses as 1210 & 1240 Wharncliffe Road South is delineated with black lines.

The subject lands are part of an established commercial corridor containing a mix of large-scale retail, automotive sales and service, and restaurant uses. The neighbouring property to the north contains a large furniture-retail store. Directly to the east are low-density residential uses in the form of single detached dwellings and townhouse dwellings. To the south and west is the future Richardson subdivision and future Bradley Road extension.

Wharncliffe Road South is classified as a Civic Boulevard on Map 3 – Street Classifications of The London Plan. The road segment between Bradley Avenue West and Legendary Drive has a traffic volume of approximately 13,000 vehicles per day. Wharncliffe Road South has public sidewalks on both sides of the street. A LTC bus-stop is located approximately 200 metres to the south and is serviced by transit route 12.

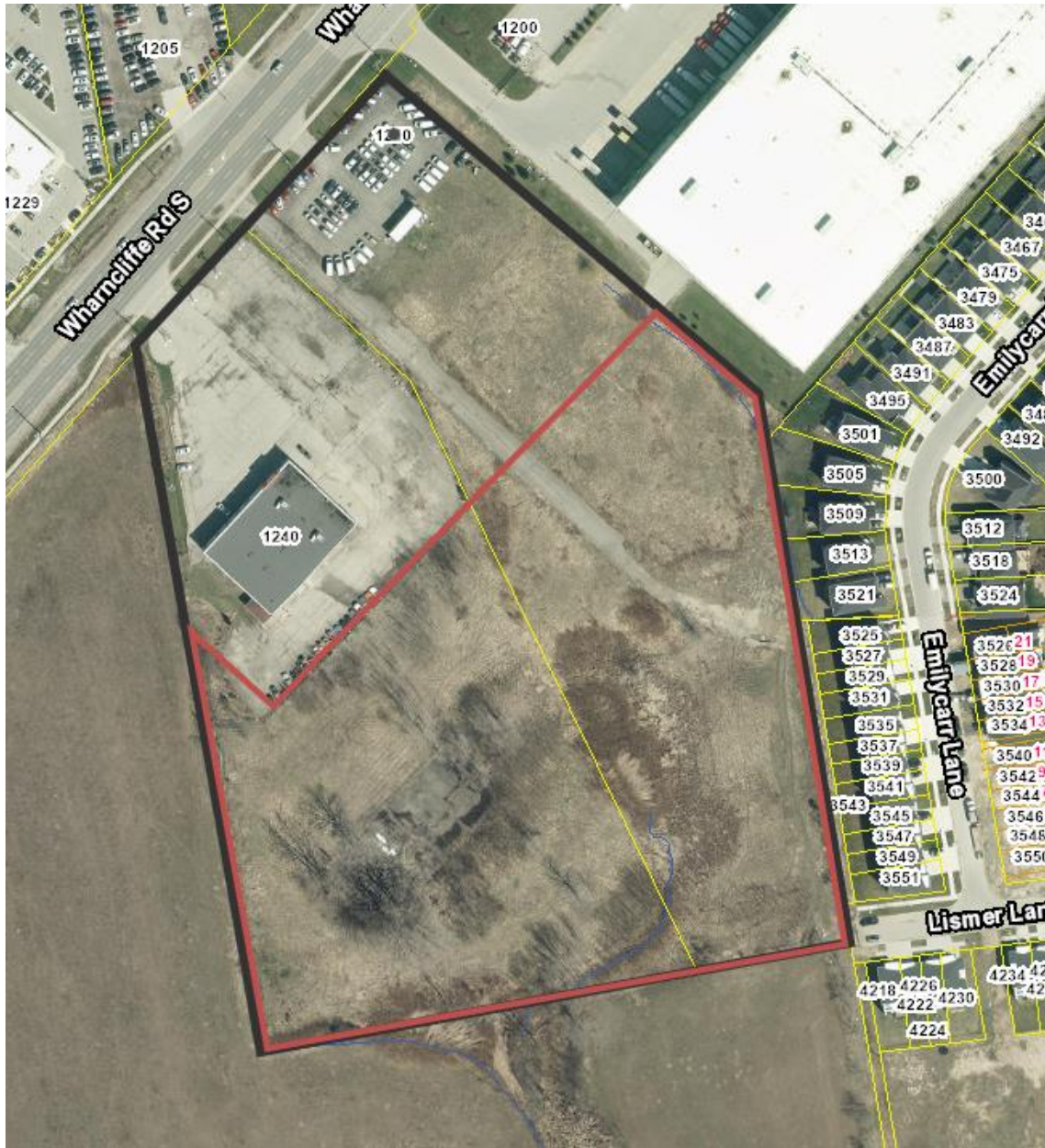


Figure 1- Aerial Image of 1210-1240 Wharncliffe Road S with subject parcel marked in red and larger parcel in black.

**Site Statistics:**

- Current Land Use: Commercial & Vacant
- Frontage: 120.2 metres
- Depth: 235.1 metres
- Area: 5.12 hectares
- Shape: Irregular
- Located within the Built Area Boundary: No
- Located within the Primary Transit Area: No

**Surrounding Land Uses:**

- North: Commercial
- East: Residential
- South: Future Residential
- West: Commercial

**Existing Planning Information:**

- The London Plan Place Type: Commercial Industrial & Neighbourhoods
- Existing Special Policies: Southwest Area Secondary Plan: Commercial & Medium Density Residential
  - Existing Zoning: Restricted Service Commercial (RSC1/RSC3/RSC4/RSC5) Zone and Urban Reserve (UR4) Zone

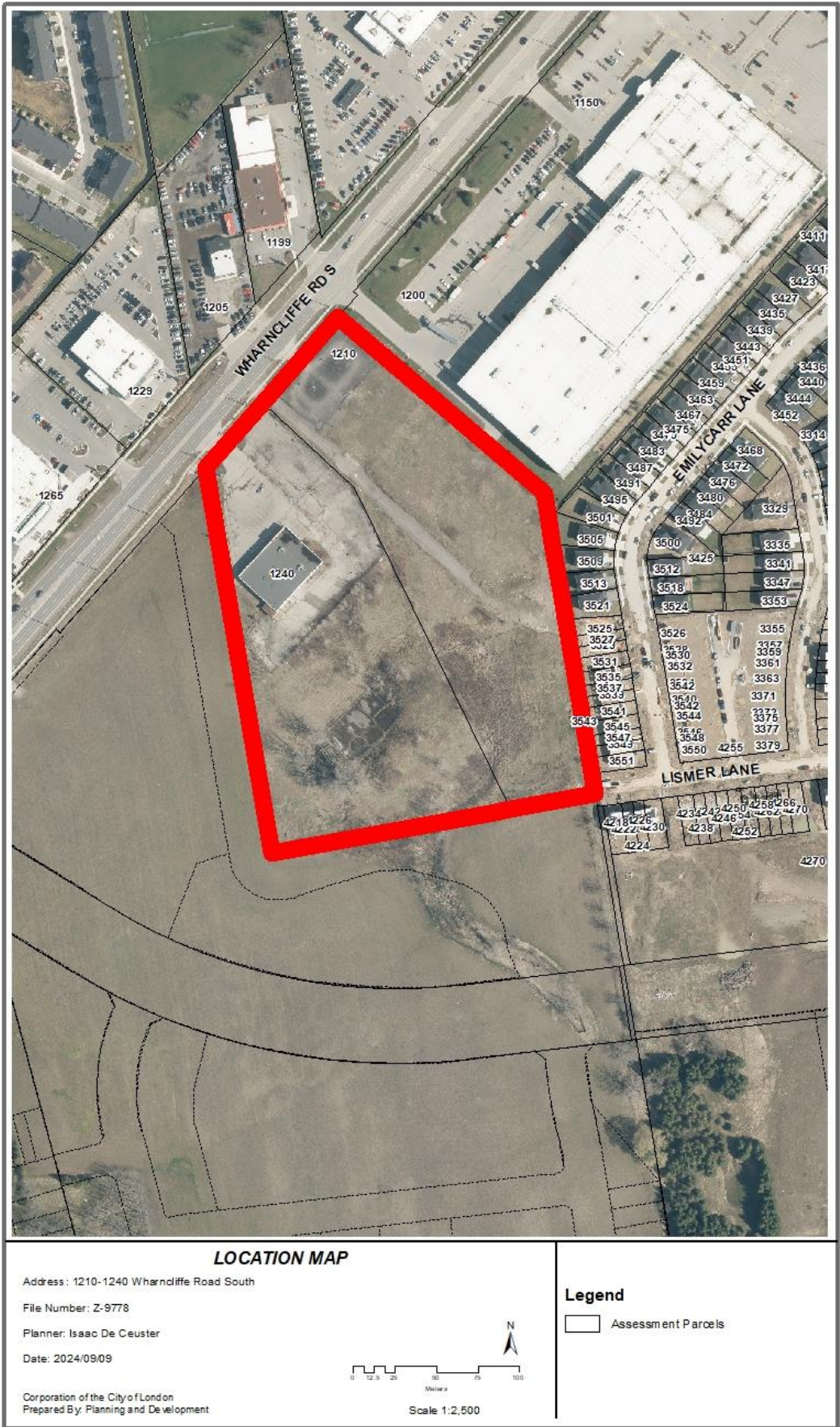


Figure 2- Aerial Photo of 1210 & 1240 Wharnclyffe Road South and surrounding lands.



Figure 3 - Streetview of 1210 Wharncliffe Road South (view looking southeast)



Figure 4 - Streetview of 1240 Wharncliffe Road South (view looking southeast)

**2.0 Discussion and Considerations**

**2.1 Development Proposal**

In August 2024, the City accepted a complete Zoning By-law amendment application to permit eleven (11) blocks of 3.5-storey stacked townhouses with a total of 108 residential units and a maximum density of 54 units per hectare. Each block contains eight (8) to twelve (12) residential units. A common outdoor amenity area is proposed on the southwest portion of the subject lands, connected to the internal sidewalk for convenient access for future residents.

The proposed development will be accessed by an extension of the cul-de-sac Lismer Lane to the east of the subject site, with a private right-of-way providing connectivity through the site. A storm water management (SWM) pond is proposed in the eastern portion of the subject lands.

The proposed development includes the following features:

- Land use: Residential
- Form: Cluster stacked townhouse
- Height: 3.5 storeys (12.0m)
- Residential units: 108 units
- Density: 54 units per hectare
- Building coverage: 18.5%
- Parking spaces: 76 surface parking spaces
- Landscape open space: 44%



The conceptual site plan is shown below as Figure 4. Building renderings and elevations are shown in Figures 5-6 below.



Figure 5 - Conceptual Site Plan (Received August 2024)



Figure 6 – Rendering Stacked Townhouse development 1210-1240 Wharncliffe Road South (Received August 2024)



Figure 7 – Front Elevation Stacked Townhouse development 1210-1240 Wharncliffe Road South (Received August 2024)

Additional plans and drawings of the development proposal are provided in Appendix “B”.

## 2.2 Revised Development Proposal (October 2024)

Based on comments provided by Staff, the applicant submitted a revised conceptual site plan, shown in Figure 8 below. The revised development proposal continues to comprise of eleven (11) blocks of 3.5-storey stacked townhouses with a total of 108 residential units and a maximum density of 54 units per hectare. Revisions to the proposal include:

- Removal of the turn circle at the end of Lismer Lane.
- A standard access connection to Lismer Lane (with a standard width of 6.7m) without encroachment into the OS5 lands to the south.
- Extension of the sidewalk into the site.
- Addition of a hammerhead turnaround.



Figure 8 – Revised Conceptual Site Plan (Received October 2024)

## 2.3 Requested Amendment

The applicant has requested an amendment to the Zoning Bylaw Z.-1 to rezone the property from a Restricted Service Commercial (RSC1/RSC3/RSC4/RSC5) Zone and Urban Reserve (UR4) Zone to a Residential R5 Special Provision (R5-7(\_)) Zone.

The following table summarizes the special provisions that have been proposed by the applicant and those that are being recommended by staff.

Regulation (R5-7)	Required	Proposed
Minimum Lot Frontage	30.0	13.0

## 2.4 Internal and Agency Comments

The application and [associated materials](#) were circulated for internal comments and public agencies to review. Comments received were considered in the review of this application and are addressed in Section 4.0 of this report.

Key issues identified by staff and agencies included:

- As part of this Zoning By-law application, the wetland compensation area to be dedicated (orange area concept plan), shall be zoned Open Space (OS5) Zone.
- Remove right of way extension and turn circle (City does not require a turn circle since the dead end at Lismer is less than 45.0m).
- Provide a standard access connection to Lismer Lane (standard width of 6.7m) and ensure no encroachment into the OS5 lands to the south.
- Extend north sidewalk into site.
- Add a hammerhead turnaround within the site near the entrance to allow vehicles to turnaround without needing to drive through the site.
- If the applicant can contain the access on their property to the satisfaction of the City, then no holding provision would be required. However, if the applicant cannot satisfy this, then a holding provision regarding access will be added onto the property.

Detailed internal and agency comments are included in Appendix “D” of this report.

## 2.5 Public Engagement

On August 30, 2024, Notice of Application was sent to 65 property owners and residents in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on September 19, 2024. A “Planning Application” sign was also placed on the site.

There were no responses received during the public consultation period.

## 2.6 Policy Context

### ***The Planning Act and the Provincial Planning Statement, 2024***

The Provincial planning policy framework is established through the *Planning Act* (Section 3) and the *Provincial Planning Statement, 2024 (PPS)*. The *Planning Act* requires that all municipal land use decisions affecting planning matters shall be consistent with the *PPS*.

The mechanism for implementing Provincial policies is through the Official Plan, The London Plan. Through the preparation, adoption and subsequent Ontario Land Tribunal (OLT) approval of The London Plan, the City of London has established the local policy framework for the implementation of the Provincial planning policy framework. As such, matters of provincial interest are reviewed and discussed in The London Plan analysis below.

As the application for a Zoning By-law amendment complies with The London Plan, it is staff's opinion that the application is consistent with the *Planning Act* and the *PPS*.

### ***The London Plan, 2016***

The London Plan (TLP) includes evaluation criteria for all planning and development applications with respect to use, intensity and form, as well as with consideration of the following (TLP 1577-1579):

1. Consistency with the Provincial Policy Statement and all applicable legislation.
2. Conformity with the Our City, Our Strategy, City Building, and Environmental policies.
3. Conformity with the Place Type policies.
4. Consideration of applicable guideline documents.
5. The availability of municipal services.
6. Potential impacts on adjacent and nearby properties in the area and the degree to which such impacts can be managed and mitigated.
7. The degree to which the proposal fits within its existing and planned context.

Staff are of the opinion that all the above criteria have been satisfied.

### ***Southwest Area Secondary Plan***

The *Southwest Area Secondary Plan (SWAP)* has been reviewed in its entirety and it is staff's opinion that the proposed Zoning By-law amendment is consistent with it. The subject lands are designated Commercial pursuant to Schedule 13 (North Longwoods Residential Neighbourhood Land Use Designations) of the *SWAP*.

## **3.0 Financial Impact/Considerations**

### **3.1 Financial Impact**

There are no direct municipal financial expenditures associated with this application.

## **4.0 Key Issues and Considerations**

### **4.1 Land Use**

The proposed residential use on the subject site is supported by the policies of the PPS 2024 and aligns with the vision of the Neighbourhoods Place Type by contributing to a diversity and mix of housing types, intensities, and forms (TLP 918\_2). Further, the proposed residential use aligns with Key Direction #5 – Building a mixed-use compact city, by planning for infill and intensification of various types and forms to take advantage of existing services and facilities and to reduce our need to grow outward (TLP 59\_4), and to ensure a mix of housing types within our neighbourhoods so that they are complete and support aging in place (TLP 59\_5).

The Neighbourhoods Place Type contemplates a range of residential uses, dependent upon the street classification on which the property has frontage. The portion of the subject site proposed for development fronts on a Neighbourhood Street (Lismer Lane). As set out in Table 10, the range of permitted uses along a Neighbourhood Street do not contemplate stacked townhouse developments (TLP Table 10 – Range of Permitted Uses in Neighbourhoods Place Type).

### ***Southwest Area Secondary Plan***

The subject site is located in the North Longwoods Residential Neighbourhood of SWAP, with the front portion of the subject site designated Commercial and the rear portion designated Medium Density Residential (*SWAP* Schedule 13 – North Longwoods Residential Neighbourhood Land Use Designations). Permitted uses in the Medium Density Residential designation include multiple-attached dwellings, low-rise apartment buildings, rooming and boarding houses, emergency care facilities, converted dwellings, small-scale nursing homes, rest homes, and homes for the aged.

The Medium Density Residential designation permits the proposed stacked townhouse development on the rear-portion of the subject lands (SWAP, 11.1.ii) b).

As set out in The London Plan, where there is inconsistency between The London Plan and policies of the Secondary Plan, the Secondary Plan policies will prevail (TLP, 1558\_). Staff are satisfied the proposed stacked townhouse development is supported by the policies of the PPS 2024 and are a permitted use in the Medium Density Residential Designation of SWAP (11.1.ii). As the SWAP policies supersede The London Plan, staff is satisfied the recommended amendment is in conformity with the Official Plan.

## **4.2 Intensity**

The proposed residential intensity is consistent with the policies of the PPS 2024 that encourage all types of residential intensification, including redevelopment which results in a net increase in residential uses (2.2.1.2), compact form (2.4.1.3.c), and an appropriate mix of housing options and densities (2.3.1.3). Table 11 of The London Plan provides a range of permitted heights in the Neighbourhoods Place Type based on street classification (TLP, Table 11 – Range of Permitted Heights in Neighbourhoods Place Type). As the portion of the subject site proposed for development fronts on a Neighbourhood Street (Lismer Lane), the minimum permitted height is 1 storey and the standard maximum height is 3 storeys.

The proposed residential intensity will facilitate an appropriate scale of development, that is compatible with the existing neighbourhood character and can be accommodated on a parcel that is of sufficient size to support their use. The redevelopment of the parcel will facilitate the efficient use of land, and is consistent with the residential intensification policies of The London Plan that encourage infill development on vacant or underutilized lots (TLP 939\_5).

### ***Southwest Area Secondary Plan***

SWAP provides that the Medium Density Residential designation in the North Longwoods Neighbourhood shall have a maximum density of 75 units per hectare and building heights shall not exceed four storeys (11.1.iii) a). As set out in The London Plan, where there is inconsistency between The London Plan and policies of the Secondary Plan, the Secondary Plan policies will prevail (TLP, 1558\_). Based on the above, the proposed 3.5-storey stacked townhouse development is supported by the policies of the PPS 2024, contemplated in the Neighbourhoods Place Type in the London Plan, and in keeping with the intensity policies for the Medium Density Residential designation of SWAP (11.1.iii).

## **4.3 Form**

The proposed built form on the subject site is supported by the policies of the PPS 2024 and is consistent with the Neighbourhoods Place Type policies and the City Design policies of The London Plan by facilitating an appropriate form and scale of residential intensification that is compatible with the existing neighbourhood character (TLP 953\_2). Specifically, the proposed built form supports infill development of a vacant and underutilized lot, a mix of housing types to support ageing in place and affordability and is designed to be a good fit and compatible within the surrounding context (TLP 193).

The proposed built form consists of eleven (11) blocks of 3.5-storey stacked townhouse dwellings. Block C and Blocks H to K are oriented towards the internal right-of-way, with Blocks A, B, and D to G proposed perpendicular to this driveway. As proposed, the built form directs the greatest heights either internal to the site or towards the commercial portion of the subject lands, with a transition in height towards the existing low-density residential uses to the northeast. The proposed built form and massing of the stacked townhouse blocks have consideration for the surrounding land uses and is appropriate in scale compared to the surrounding neighbourhood character (TLP 953\_2).

Access to the subject lands is proposed through an extension of the cul-de-sac Lismer Lane to the east of the subject lands, with a private right-of-way providing connectivity and safe movement for pedestrians, cyclists and motorists (TLP 255\_). Additionally, an easement is proposed along the eastern property line to provide a pedestrian pathway connection from Lismer Lane to Wharncliffe Road South. Parking for the proposed stacked townhouse development will be located internally, visually screening the surface parking from the street, encouraging a pedestrian oriented streetscape.

The proposed stacked townhouse development is supported by the policies of the PPS 2024, contemplated in the Neighbourhoods Place Type, and in keeping with the City Design policies of The London Plan.

### **Southwest Area Secondary Plan**

As set out in *SWAP*, the Urban Design policies in Section 3.9 provide that all development shall be designed in a form that is compact, pedestrian-oriented and transit friendly (3.9.1a)). Further, building densities and land uses located along transit routes are to be designed to support transit and the users of those services, which is the case for the proposed stacked townhouse development located in proximity to a public transit stop. Based on the above, the proposed stacked townhouse development is in keeping with the Urban Design policies of *SWAP* (3.9) and the built form policies of the North Longwoods Neighbourhood (11.1.iii)a)).

### **4.4 Zoning**

The applicant has requested an amendment to the Zoning By-law Z.-1 to rezone the property from a Restricted Service Commercial (RSC1/RSC3/RSC4/RSC5) Zone and Urban Reserve (UR4) Zone to a Residential R5 Special Provision (R5-7(\_)) Zone. The following summarizes the special provision that has been proposed by the applicant and those that is being recommended by staff.

**Lot Frontage** – The applicant is requesting a special provision to permit a lot frontage of 13.0 metres, whereas a minimum of 30.0 metres is required for the R5-7 Zone variation. This requested reduction in lot frontage is a result of the proposed extension of Lismer Lane to the east of the subject lands. Following a future severance of the rear-portion of the subject lands, the site will have a new street frontage on Lismer Lane and no longer front onto Wharncliffe Road South. Due to the lot configuration, staff are supportive of the requested special provision for reduced lot frontage to accommodate access to the proposed stacked townhouse development.

### **4.5 Wetland Dedication**

Matrix Solutions Inc. was retained by the applicant to complete an Environment Impact Study (EIS) for the proposed residential development. The EIS identified small watercourses and a small low-functioning wetland on the subject lands, based on the London Plan development cannot occur in any wetland unless it can be compensated adequately. As a result of the proposed development, improvements to the natural environment and features can be obtained through a natural heritage compensation process. Firstly, the applicant will install a stormwater management facility on the eastern section of the subject lands to ensure that existing groundwater and baseflow conditions are preserved. Secondly, the applicant will dedicate a portion of the subject lands to the City as part of the White Oaks Complete Corridor. The White Oaks Complete Corridor will redesign a tributary channel of the White Oaks Drain to a complete corridor by integrating stormwater management, protection and enhancement of natural features and functions and establish amenity space within the future neighbourhood. As such, the existing wetland is proposed to be removed and will be compensated at a greater than 1:1 ratio with improvements to wetland habitat and surface water sequestration. The wetland compensation area will be designed to function as a natural wetland, including wildlife features (such as turtle basking logs, bat boxes, turtle nesting areas, brush piles and potential snake hibernaculum). The proposed wetland is designed to provide habitat for a wide variety of species, and the addition of adjacent meadow and forest habitat will provide multi-season opportunities

for bird and bats.

To achieve this compensation, a portion of the subject lands shall be rezoned to an Open Space (OS5) Zone as part of this Zoning By-law application. Specific details are being developed as part of the White Oaks Complete Corridor Design Project based on the City of London EMGs and with UTRCA guidance. As the current natural habitat on the subject lands is disturbed, the wetland compensation is anticipated to have an overall net benefit through enhancement of the habitat, planting of native seeds, shrubs and trees and implementation of wildlife features.

The subject lands are also within the UTRCA's mapped regulation limits due to the presence of a flooding hazard associated with watercourses known as UT-DC283 and UT-DC282, and a wetland and surrounding area. UTRCA recommends that the lands which are to be dedicated to the City for the compensation of the wetland and the watercourse be zoned with the appropriate Open Space Zone. While the technical supporting information required by UTRCA has not been provided, UTRCA is of the opinion that this issue can be addressed through the Site Plan Approval process and through the Section 28 Permit process. As such UTRCA has no objections to this application, detailed UTRCA comments are included in Appendix "D" of this report.

## Conclusion

The applicant has requested an amendment to the Zoning By-law Z.-1 to rezone the property from a Restricted Service Commercial (RSC1/RSC3/RSC4/RSC5) Zone and Urban Reserve (UR4) Zone to a Residential R5 Special Provision (R5-7(\_)) Zone and Open Space (OS5) Zone. Staff are recommending approval of the requested Zoning Bylaw amendment with special provisions.

The recommended action is consistent with the PPS 2024, conforms to The London Plan and SWAP and will permit a 3.5-storey, 108-unit stacked townhouse development.

**Prepared by:** Isaac de Ceuster  
Planner, Planning Implementation

**Reviewed by:** Catherine Maton, MCIP, RPP  
Manager, Planning Implementation

**Recommended by:** Heather McNeely, MCIP, RPP  
Director, Planning and Development

**Submitted by:** Scott Mathers, MPA, P.Eng.  
Deputy City Manager, Planning and Economic  
Development

Copy:  
Britt O'Hagan, Manager, Current Development  
Mike Corby, Manager, Site Plans  
Brent Lambert, Manager, Development Engineering

## Appendix A – Zoning Bylaw Amendment

Bill No. (number to be inserted by Clerk's Office)  
2024

By-law No. Z.-1-

A by-law to amend By-law No. Z.-1 to rezone an area of land located at 1210 & 1240 Wharncliffe Road South.

WHEREAS this amendment to the Zoning By-law Z.-1 conforms to the Official Plan;

THEREFORE the Municipal Council of The Corporation of the City of London enacts as follows:

1. Schedule "A" to By-law No. Z.-1 is amended by changing the zoning applicable to a portion of the lands located at 1210 & 1240 Wharncliffe Road South, as shown on the attached map **FROM** a Restricted Service Commercial (RSC1/RSC3/RSC4/RSC5) Zone and Urban Reserve (UR4) Zone **TO** a Residential R5 Special Provision (R5-7(\_)) and an Open Space (OS5) Zone.
2. Section Number 9.4 of the R5 (R5-7(\_)) Zone is amended by adding the following Special Provisions:  
  
R5-7 (\_\_) 1210 & 1240 Wharncliffe Road South
  - a. Regulations
    - i) Lot Frontage (minimum) – 13.0m
3. This Amendment shall come into effect in accordance with Section 34 of the *Planning Act, R.S.O. 1990, c. P13*, either upon the date of the passage of this by-law or as otherwise provided by the said section.

PASSED in Open Council on November 5, 2024, subject to the provisions of PART VI.1 of the *Municipal Act, 2001*.

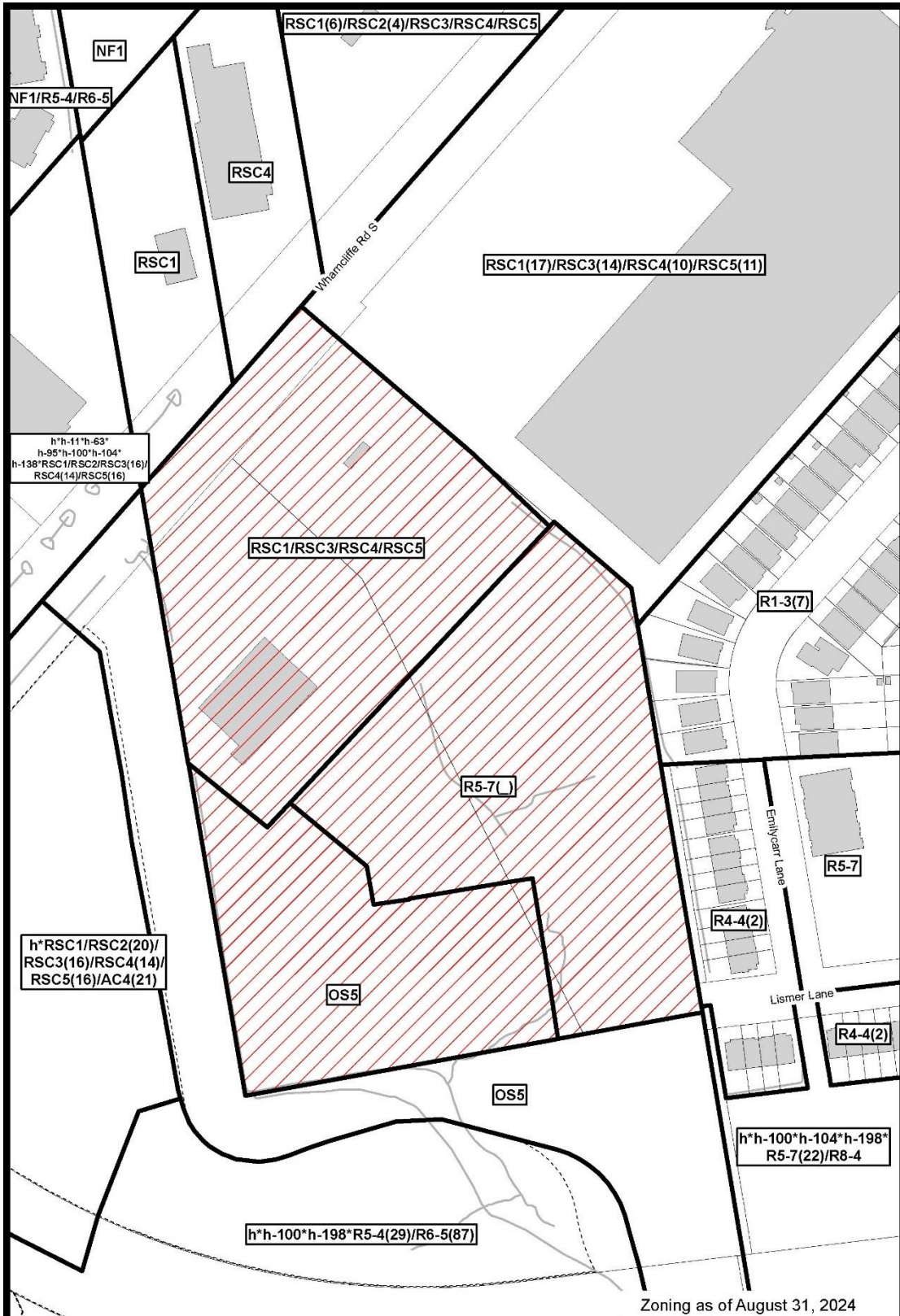
Josh Morgan  
Mayor


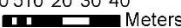

Michael Schulthess  
City Clerk



First Reading – November 5, 2024  
Second Reading – November 5, 2024  
Third Reading – November 5, 2024

AMENDMENT TO SCHEDULE "A" (BY-LAW NO. Z.-1)



<p>File Number: Z-9778                  Planner: IDC                  Date Prepared: 2024/09/13                  Technician: JI                  By-Law No: Z.-1-</p>	<p>SUBJECT SITE </p> <p>1:2,000</p> <p>0 10 20 30 40 Meters </p> 
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Geodatabase

## Appendix B - Site and Development Summary

### A. Site Information and Context

#### Site Statistics

Current Land Use	Commercial & Vacant
Frontage	120.2 metres
Depth	235.1 metres
Area	5.12 hectares
Shape	Irregular
Within Built Area Boundary	No
Within Primary Transit Area	No

#### Surrounding Land Uses

North	Commercial
East	Residential
South	Future Residential
West	Commercial

#### Proximity to Nearest Amenities

Major Intersection	Wharncliffe Road South & Legendary Drive, 350 metres
Dedicated cycling infrastructure	Bradley Avenue West, 300 metres
London Transit stop	Wharncliffe Road at Castle Rock, 250 metres
Public open space	Paul Haggis Park, 650 metres
Commercial area/use	Wharncliffe Road South, 50 metres
Food store	Loblaws Wonderland Road, 1,600 metres
Community/recreation amenity	Bostwick Community Centre, 2,700 metres

### B. Planning Information and Request

#### Current Planning Information

Current Place Type	Neighbourhoods & Commercial Industrial, Civic Boulevard
Current Special Policies	SWAP - North Longwoods Neighbourhood
Current Zoning	Restricted Service Commercial (RSC1/RSC3/RSC4/RSC5) Zone and Urban Reserve (UR4) Zone

#### Requested Designation and Zone

Requested Place Type	N/A
Requested Special Policies	N/A
Requested Zoning	Residential R5 Special Provision (R5-7(_)) Zone and Open Space (OS5) Zone

#### Requested Special Provisions

Regulation (R5-7))	Required	Proposed
Lot Frontage (minimum)	30.0	13.0

## C. Development Proposal Summary

### Development Overview

The applicant is proposing the development of eleven (11) blocks of 3.5-storey stacked townhouse developments with a total of 108 residential units, 76 parking spaces and a maximum density of 54 units per hectare.

### Proposal Statistics

Land use	Residential
Form	Stacked townhouse
Height	3.5 storeys (11.5 metres)
Residential units	108
Density	54.4 units per hectare
Building coverage	18.5%
Landscape open space	44%
New use being added to the local community	Yes

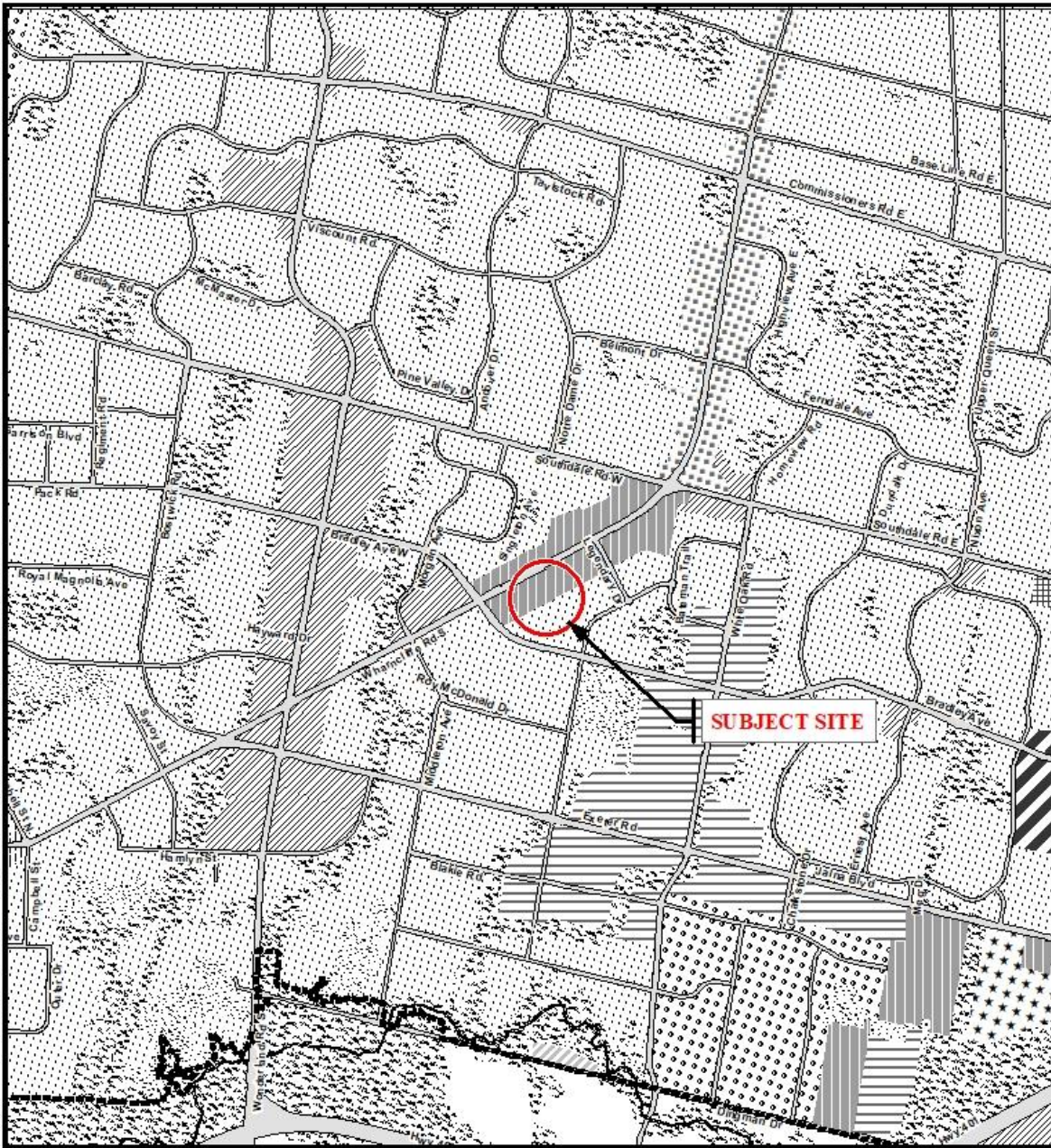
### Mobility

Parking spaces	76 surface parking spaces
Vehicle parking ratio	0.7 spaces per unit
New electric vehicles charging stations	TBD
Secured bike parking spaces	Determined through Site Plan Approval
Secured bike parking ratio	Determined through Site Plan Approval
Completes gaps in the public sidewalk	N/A
Connection from the site to a public sidewalk	Yes
Connection from the site to a multi-use path	Yes

### Environment

Tree removals	Yes
Tree plantings	TBD
Tree Protection Area	No
Loss of natural heritage features	No, habitat will be compensated on a 1:1 ratio
Species at Risk Habitat loss	No, habitat will be compensated on a 1:1 ratio
Minimum Environmental Management Guideline buffer met	Yes
Existing structures repurposed or reused	TBD
Green building features	Unknown

# Appendix C – Additional Plans and Drawings



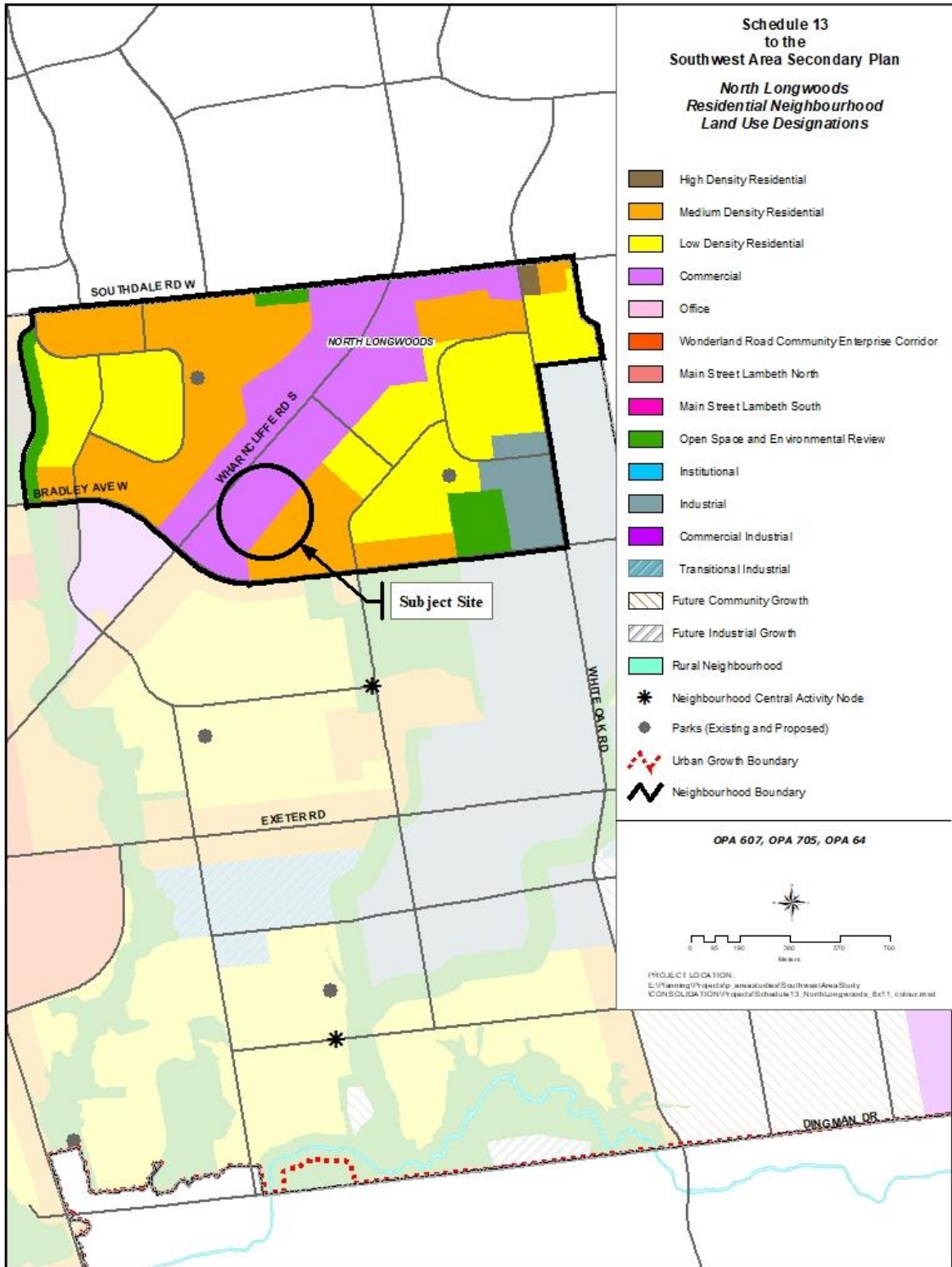
### Legend

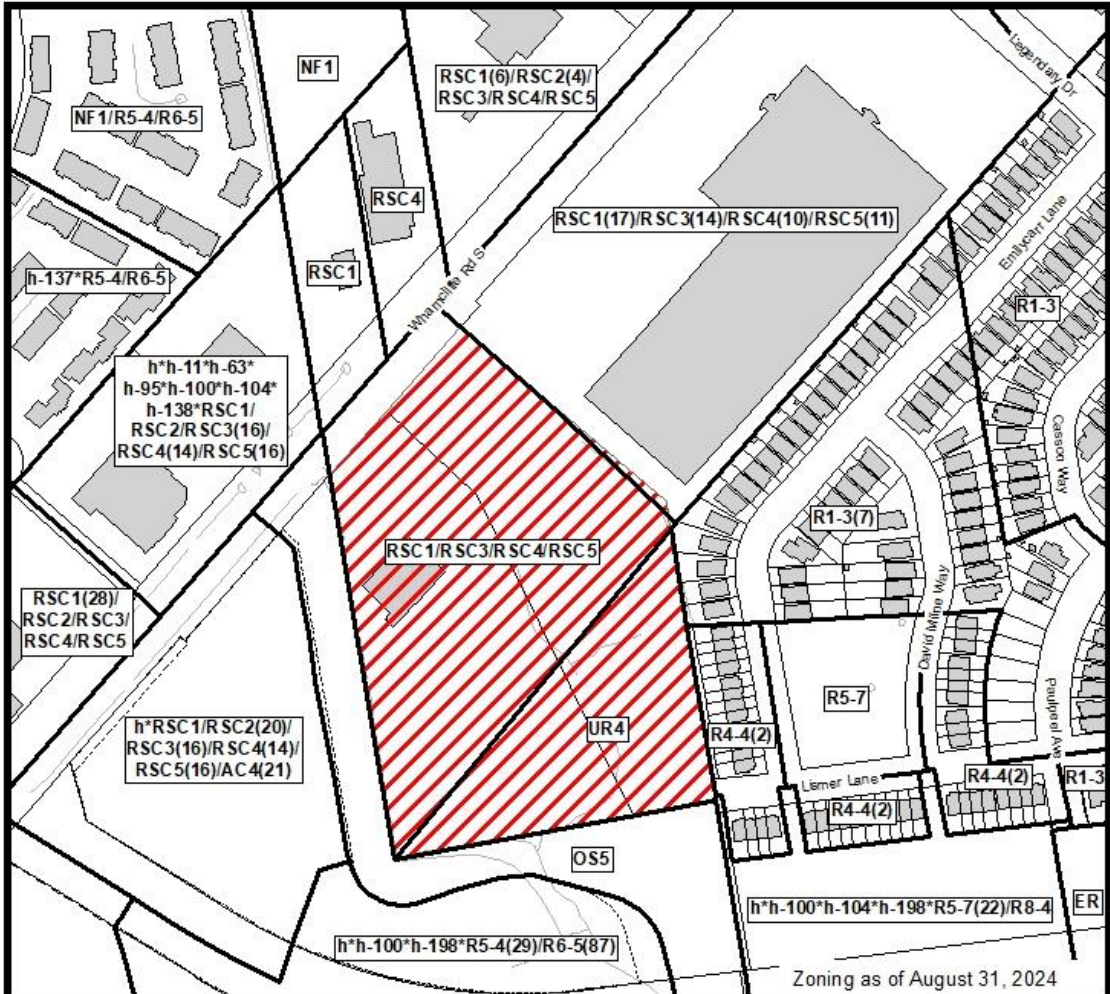
Downtown	Future Community Growth	Environmental Review
Transit Village	Heavy Industrial	Farmland
Shopping Area	Light Industrial	Rural Neighbourhood
Rapid Transit Corridor	Future Industrial Growth	Waste Management Resource Recovery Area
Urban Corridor	Commercial Industrial	Urban Growth Boundary
Main Street	Institutional	
Neighbourhood	Green Space	

*This is an excerpt from Planning & Development's working consolidation of Map 1 - Place Types of the London Plan, with added notations.*

<p><b>CITY OF LONDON</b> Official Plan</p> <p><b>LONDON PLAN MAP 1</b> - PLACE TYPES -</p> <p>PREPARED BY: Planning &amp; Development</p>	<p>Scale 1:30,000</p> <p>Meters</p>	<p><b>File Number:</b> Z-9778</p> <p><b>Planner:</b> IDC</p> <p><b>Technician:</b> JI</p> <p><b>Date:</b> 2024/09/09</p>
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Project Location: E:\Planning\Projects\lp\_officialplan\work\consolid\00\excerpts\_LondonPlan\mxd\Z-9778\_Map1\_PlaceTypes.mxd





**COUNCIL APPROVED ZONING FOR THE SUBJECT SITE:**

1) **LEGEND FOR ZONING BY-LAW Z-1**

- |  |   |
|--|---|
| <ul style="list-style-type: none"> <li>R1 - SINGLE DETACHED DWELLINGS</li> <li>R2 - SINGLE AND TWO UNIT DWELLINGS</li> <li>R3 - SINGLE TO FOUR UNIT DWELLINGS</li> <li>R4 - STREET TOWNHOUSE</li> <li>R5 - CLUSTER TOWNHOUSE</li> <li>R6 - CLUSTER HOUSING ALL FORMS</li> <li>R7 - SENIOR'S HOUSING</li> <li>R8 - MEDIUM DENSITY/LOW RISE APTS.</li> <li>R9 - MEDIUM TO HIGH DENSITY APTS.</li> <li>R10 - HIGH DENSITY APARTMENTS</li> <li>R11 - LODGING HOUSE</li> <br/> <li>DA - DOWNTOWN AREA</li> <li>RSA - REGIONAL SHOPPING AREA</li> <li>CSA - COMMUNITY SHOPPING AREA</li> <li>NSA - NEIGHBOURHOOD SHOPPING AREA</li> <li>BDC - BUSINESS DISTRICT COMMERCIAL</li> <li>AC - ARTERIAL COMMERCIAL</li> <li>HS - HIGHWAY SERVICE COMMERCIAL</li> <li>RSC - RESTRICTED SERVICE COMMERCIAL</li> <li>CC - CONVENIENCE COMMERCIAL</li> <li>SS - AUTOMOBILE SERVICE STATION</li> <li>ASA - ASSOCIATED SHOPPING AREA COMMERCIAL</li> <br/> <li>OR - OFFICE/RESIDENTIAL</li> <li>OC - OFFICE CONVERSION</li> <li>RO - RESTRICTED OFFICE</li> <li>OF - OFFICE</li> </ul> | <ul style="list-style-type: none"> <li>RF - REGIONAL FACILITY</li> <li>CF - COMMUNITY FACILITY</li> <li>NF - NEIGHBOURHOOD FACILITY</li> <li>HER - HERITAGE</li> <li>DC - DAY CARE</li> <br/> <li>OS - OPEN SPACE</li> <li>CR - COMMERCIAL RECREATION</li> <li>ER - ENVIRONMENTAL REVIEW</li> <br/> <li>OB - OFFICE BUSINESS PARK</li> <li>LI - LIGHT INDUSTRIAL</li> <li>GI - GENERAL INDUSTRIAL</li> <li>HI - HEAVY INDUSTRIAL</li> <li>EX - RESOURCE EXTRACTIVE</li> <li>UR - URBAN RESERVE</li> <br/> <li>AG - AGRICULTURAL</li> <li>AGC - AGRICULTURAL COMMERCIAL</li> <li>RRC - RURAL SETTLEMENT COMMERCIAL</li> <li>TGS - TEMPORARY GARDEN SUITE</li> <li>RT - RAIL TRANSPORTATION</li> <br/> <li>"h" - HOLDING SYMBOL</li> <li>"D" - DENSITY SYMBOL</li> <li>"H" - HEIGHT SYMBOL</li> <li>"B" - BONUS SYMBOL</li> <li>"T" - TEMPORARY USE SYMBOL</li> </ul> |
|--|---|

**CITY OF LONDON**  
 PLANNING SERVICES / DEVELOPMENT SERVICES  
  
**ZONING BY-LAW NO. Z.-1 SCHEDULE A**



THIS MAP IS AN UNOFFICIAL EXTRACT FROM THE ZONING BY-LAW WITH ADDED NOTATIONS

FILE NO:  
 ZZ-ZZZZ CP

MAP PREPARED:  
 YYYY/MM/DD CK

1:3,500  
 0 15 30 60 90 120 Meters

## Appendix D – Internal and Agency Comments

### Urban Design

#### Major Issues:

- This site is located within the Commercial Industrial and Neighbourhoods Place Types in The London Plan [TLP] and is designated Commercial and Medium Density Residential in the Southwest Area Secondary Plan [SWASP]. Urban Design is generally supportive of the proposed townhouse development and acknowledges the applicant for providing common outdoor amenity spaces throughout the site and for minimizing the amount of surface parking area provided.

#### Matters for ZBA:

- Urban Design has no concerns regarding the requested Zoning By-law Amendment.

#### Matters to be Addressed at Site Plan:

- Orient and design any end units flanking Lismer Lane to include a similar level of architectural detail as is provided on the front elevation, including wrap around porches, front entrances, size and number of windows, materials, massing and any other relevant architectural detailing [TLP Policy 291].
- Provide increased transparent glazing on side elevations of units flanking amenity spaces and drive aisles to allow for passive surveillance into common areas to reduce potential Crime Prevention Through Environmental Design (CPTED) concerns.
- Consider incorporating a mix of complementary architectural styles, materials and colours in the design of individual townhouse units to assist with wayfinding, break-up the massing and to add interest to the overall building design [TLP Policy 301].
- Provide direct and safe access to the public sidewalk on Lismer Lane by extending walkways to address any missing connections or gaps in the pedestrian network and by delineating pathways that cross drive aisles (e.g. inset concrete, painted lines) [TLP Policy 255, 268].
- Ensure buffering and screening is provided for the townhouses adjacent to the rear yards of the dwellings to the west, along Emily Carr Lane (e.g. landscaping, fencing) [TLP Policy 253].
- Ensure garbage pick-up areas, loading areas and associated facilities are located away from the public street frontage and do not detract from pedestrian connections [TLP Policy 266, SWASP Policy 20.5.3.9 iii, g].
- Provide buffering and privacy for the proposed below-grade units in the form of enhanced all-season landscaping between the parking areas and buildings [TLP Policy 278].
- Include facilities for temporary bicycle storage (bike racks) to promote active transportation [TLP Policy 280].
- Screen any parking areas exposed to the public street with enhanced all-season landscaping [TLP Policy 278, SWASP Policy 20.5.3.9 iii, g].
- The design and layout of the site should have regard for and incorporate significant mature trees [TLP Policy 210, 258].
- Consider providing at least one pedestrian connection to Wharncliffe Road South [TLP Policy 255].
- Submit a full set of dimensioned elevations for all sides of the proposed buildings as well as a fully dimensioned and labelled site plan. Further Urban Design comments may follow upon receipt of the updated drawings.

### Heritage

The City had received the Ministry's review of this report in 2022

I have reviewed and can confirm that the archaeological requirements for Z-9778 have been satisfied.



## Site Plan

Please note the following for 1210-1240 Wharncliffe Road S:

1. Major Issues
  - None.
2. Matters for OPA/ZBA
  - Special Provisions Required for lot frontage (13.0 metres whereas 30.0 metres is the minimum required).
  - The applicant noted that they need a special provision for parking reduction. In this instance, as the requested use is Stacked Townhouses, the applicant is required to provide parking at a rate of 0.5 spaces per unit. The provided 76 parking spaces (0.7 spaces per unit) meets the minimum required parking rate outlined in the Zoning By-law.
3. Matters for Site Plan
  - Provide Visitor Parking at a rate of 1 space per 10 residential units. Based on the provided unit count, 11 visitor parking spaces will be required.
  - Provide Bicycle Parking at a rate of 0.1 spaces per residential unit. Based on the provided unit count, 11 bicycle parking spaces will be required.
  - Outline the access routes for emergency vehicles including the location, width, turning radius, vertical alignment, and location of the fire route. Show the location of the fire route sign(s) on the site plan. Consider how your fire servicing route will function (Site Plan Control By-law, Section 6.7.7).
  - Clarify how waste collection and waste vehicle turnaround will function on the proposed development. Specify the proposed waste collection method, screening, storage location and collection point (Site Plan Control By-law, Section 10.3.b).
  - Identify locations for snow storage on-site (Site Plan Control By-law, Section 1.5.).
  - Show the location of the accessible parking sign(s) on the site plan.
4. Complete Application Requirements
  - Site Plan Application is required.

## Ecology

All issues related to the ZBA have been resolved through the subdivision draft plan process.

The compensation area (in orange on the proposal) is to be re-zoned as an appropriate OS following City acquisition.

Ecology has no further comments or concerns at this time.

## Landscape Architect

No comments to provide on the ZBA for 1210-1240 Wharncliffe Rd S.

## Parks Planning and Design

### Matters for Site Plan

- Parkland dedication has not been taken for this site. It is to be noted that the applicant, as a condition of site plan approval, will be required to provide parkland dedication in the form of cash-in-lieu pursuant to By-law CP-25.

## Engineering, 2<sup>nd</sup> Revised Comments

We are not acceptable to the design as shown, specifically the curb transition from the existing curb on Lismer Lane. The radius starts at an existing driveway and does not appear to be smooth. The turnaround is acceptable.

The property has approx. 12.4m frontage on Lismer Lane, which is enough space to provide an adequate design and there may be opportunity to accommodate an encroachment in the future once the SWM Complete Corridor design progresses. A holding provision will not be required, but instead provide direction to SPA regarding the design of the access.

## Engineering, Revised Comments

1. Remove right of way extension and turn circle (City does not require a turn circle since the dead end at Lismer is less than 45.0m).
2. Provide a standard access connection to Lismer Lane (standard width of 6.7m) and ensure no encroachment into the OS5 lands to the south.
3. Extend north sidewalk into site.
4. Add a hammerhead turnaround within the site near the entrance to allow vehicles to turnaround without needing to drive through the site.

## Engineering

### *Planning & Development*

- Engineering has no further comments on the above noted application and recommends approval.
- The following comments shall be addressed by the applicant at the site plan application stage.

## Matters for Site Plan

### Planning & Development

- Prior to site plan approval, the applicant shall initiate a consent application to sever and covey lands to create the proposed block.
  - It is noted that the undeveloped portion of the site is still tributary to the sanitary and storm sewers on Lismer Lane, therefore appropriate easements will need to be accommodated through the proposed development to service these lands.
  - The proposed cul-de-sac is shown on lands not owned by the applicant. This will require the applicant to either acquire a portion of these lands or obtain an access easement with the property owner.

### Wastewater

- The municipal sanitary sewer available is to the 250mm diameter sewer on Lismer Lane. City plan no. T16508-09 shows information related to the municipal sanitary sewer and stub.
- The applicant's engineer is to demonstrate how the entirety of the lands including the existing and future commercial lands (1210 & 1240 Wharncliffe Rd S) are proposed to be serviced as per the accepted sanitary area plan.

### Water

- Water for the subject lands is available via the 200mm municipal PVC watermain located on Lismer Lane which is part of the city's low-level distribution system, which has a hydraulic grade line of 301.8m.

- Water servicing shall be configured in such a way as to avoid the creation of a regulated drinking water system.
- A water servicing report and associated modeling will be required, addressing all domestic demands, fire flows, water quality and the various pressure scenarios as outlined in section 7.3.1 of the city's Design Specifications and Requirements Manual (DSRM).
- Water servicing to be in accordance with the City's stacked townhouse standards
- Further comments will be provided during the site plan application.

## Stormwater

- This site falls within the Dingman Subwatershed. As per section 6.3.2 of the Design Specifications & Requirements manual, a water balance assessment is required to examine the site's water balance conditions and propose opportunities to mitigate water balance deficits. As an objective of the Dingman EA, this new development is to achieve the water balance conditions identified from the predevelopment study. The approach for stormwater control hierarchy, and LID design, is included in the Section 6 Stormwater Management of the Design Specifications & Requirements manual. The water balance analysis may be completed as part of a Hydrogeological Assessment, a stormwater management report, or as a standalone document.
- The consultant is expected to reserve space within the site plan for an LID strategy(ies) in efforts to achieve the water balance requirement.
- The Consultant may note that implementation of infiltration or filtration measures for a volume that meets or exceeds the 25mm event as part of the water balance target would be accepted to meet Total Suspended Solids (TSS) reduction target.
- As per the Dingman EA, this site is have a Permanent Private Storm System (PPS) stormwater management strategy for quality and quantity controls. If the open channel east of the site is the proposed outlet, the applicant is to demonstrate the channel has adequate capacity to safely convey post development flows to the proposed future Bradley Ave culvert with no impacts to adjacent properties. Additionally a geomorphic assessment and erosion threshold analysis for the channel, as well as any findings or recommendations of any natural heritage study are to be incorporated in the channel review where appropriate.
- The applicant is required to complete a land dedication of the channel to the City.
- The land on the south limits of the development are designated Natural Heritage Features in the London Plan. Permits and applicable studies (i.e., EIS, hydrogeological, geotechnical, water balance, etc.) may be required to support the developments overall stormwater strategy. The applicant is encouraged to coordinate with neighbouring properties to develop a cohesive stormwater strategy within these limits.
- The proposed land use of a commercial and medium density residential will trigger the application of design requirements of Permanent Private Storm System (PPS) as approved by Council resolution on January 18, 2010. A standalone Operation and Maintenance manual document for the proposed SWM system is to be included as part of the system design and submitted to the City for review.
- The site is located within the UTRCA regulated area and therefore UTRCA approval/permits may be required, including confirmation as to required setbacks.
- This site falls within the Dingman Subwatershed. As per section 6.3.2 of the Design Specifications & Requirements manual, a water balance assessment is required to examine the site's water balance conditions and propose opportunities to mitigate water balance deficits. As an objective of the Dingman EA, this new development is to achieve the water balance conditions identified from the predevelopment study. The approach for stormwater control hierarchy, and LID design, is included in the Section 6 Stormwater Management of the Design Specifications & Requirements manual. The water balance analysis may be completed as part of a Hydrogeological Assessment, a stormwater management report, or as a standalone document.

- The consultant is expected to reserve space within the site plan for an LID strategy(ies) in efforts to achieve the water balance requirement.
- The Consultant may note that implementation of infiltration or filtration measures for a volume that meets or exceeds the 25mm event as part of the water balance target would be accepted to meet Total Suspended Solids (TSS) reduction target.
- Any proposed LID solutions should be supported by a Geotechnical Report and/or a Hydrogeological Assessment report prepared with a focus on the type(s) of soil present at the Site, measured infiltration rate, hydraulic conductivity (under field saturated conditions), and seasonal high groundwater elevation. Please note that the installation of monitoring wells and data loggers may be required to properly evaluate seasonal groundwater fluctuations. The report(s) should include geotechnical and hydrogeological recommendations of any preferred/suitable LID solution. All LID proposals are to be in accordance with Section 6 Stormwater Management of the Design Specifications & Requirements manual.
- The subject lands are located in the Dingman Subwatershed. The Owner shall provide a Storm/Drainage Servicing Report demonstrating compliance with the SWM criteria and environmental targets identified in the Dingman Subwatershed Study that may include but not be limited to, quantity/quality control (80% TSS), erosion, stream morphology, etc
- The Owner agrees to promote the implementation of SWM Best Management Practices (BMP's) within the plan, including Low Impact Development (LID) where possible, to the satisfaction of the City Engineer.
- The owner is required to provide a lot grading plan for stormwater flows and major overland flows on site and ensure that stormwater flows are self-contained on site, up to the 100 year event and safely conveys up to the 250 year storm event, all to be designed by a Professional Engineer for review.
- The Owner shall allow for conveyance of overland flows from external drainage areas that naturally drain by topography through the subject lands.
- Stormwater run-off from the subject lands shall not cause any adverse effects to adjacent or downstream lands.
- An erosion/sediment control plan that will identify all erosion and sediment control measures for the subject site shall be prepared to the specification and satisfaction of the City Engineer and shall be in accordance with City of London and MECP (formerly MOECC) standards and requirements. This plan is to
- Include measures to be used during all phases of construction. These measures shall be identified in the Storm/Drainage Servicing Report.
- Additional SWM related comments will be provided upon future review of this site

#### Transportation

- Provide Engineering Plans showing existing infrastructure, include utility poles/boxes, fire hydrants, light standards, etc.
- Ensure proposed accesses meets minimum clearance requirement of 1.5m from any infrastructure and 2.0m from communication boxes.
- Provide fully dimension access as per Access Management Guidelines, radii 6.0m, width 6.7m, clear throat 6.0m min. from property line.
- Provide land dedication required for the cul-de-sac at the west end of Lismer Lane as identified in the drawing.
- The cul-de-sac will be constructed as per the Standard Contract Document drawing SR-5.0.
- Please contact Irwin Gary (airwin@london.ca) or Adam Carbone (acarbonate@london.ca) at Geomatics for more information.

#### UTRCA

- Conservation Authorities Act: As shown on enclosed mapping, the subject lands are regulated by the UTRCA in accordance with Ontario Regulation 41/24, made pursuant to Section 28 of the Conservation Authorities Act. In cases where a discrepancy in the mapping occurs, the text of the regulation prevails and a

feature determined to be present on the landscape may be regulated by the UTRCA. Although not shown on the Regulated Areas mapping, the applicant has confirmed that there is a wetland located on the subject lands (Map 1 Existing Conditions 1210 & 1240 Wharncliffe Road South prepared by NRSI dated April 22, 2022 – enclosed). Accordingly, the regulation limit is comprised of:

- A riverine flooding hazard associated with watercourses known as UT-DC283 and UT-DC282; and
- A wetland and the surrounding area of interference.
- The UTRCA has jurisdiction over lands within the regulated area and requires that landowners obtain written approval from the Authority prior to undertaking any site alteration or development within this area including filling, grading, construction, alteration to a watercourse and/or interference with a wetland.
- Recommendation: City planning staff has advised that they are supportive of this application for Zoning By-Law Amendment. While the technical supporting information that was required by the UTRCA as part of the complete application has not been provided, we are of the opinion that our interests can likely be addressed through the Site Plan Approval process and through our Section 28 Permit Process. As indicated, we recommend that the lands which are to be dedicated to the City for the compensation of the wetland and the watercourse be zoned with the appropriate Open Space (OS4 or OS5) Zone. **While the UTRCA has no objections to this application, we will require the following information through the various approval processes –**
- Site Plan Approval Process: As part of the site plan consultation/application process, the UTRCA will require the following technical information/studies –
  - Site/Concept Plan delineating the existing watercourse and wetland features on the site.
  - Environmental Impact Study and/or Wetland and Watercourse Compensation Plan – which addresses both the wetland and the watercourse (linear length will not be maintained) removal/relocation/realignment/compensation.
  - Channel Geomorphology Study to support the removal/relocation/realignment of the watercourse which is a headwater drainage feature. The analysis should address (but shall not be limited to) the impacts of the proposed works on downstream properties and also the channel conveyance capacity.
  - Stormwater Management (SWM) Report and Water Balance Analysis. The proposed SWM facility shall be designed for the 250 year regulatory storm, shall be located outside of the 250 year flood plain and the analysis and the design of the SWM facility should consider any external upstream areas which drain to the property.
  - Revised Hydrogeological Assessment and a response table indicating how the UTRCA's comments have been addressed.
- Section 28 Permit Process: The subject lands are regulated by the UTRCA and in accordance with Ontario Regulation 41/24 made pursuant to Section 28 of the Conservation Authorities Act and the proponent is required to obtain the necessary permits and approvals from the Conservation Authority prior to undertaking any site alteration or development within the regulated area. The technical submissions required through the site plan process will also inform the Section 28 approval process. A Section 28 Permit shall be obtained for:
  - The proposed wetland and watercourse removal/relocation/realignment/compensation. As noted, these matters will need to be considered by the UTRCA's Hearings Committee and the proponent is encouraged to contact our Land Use Regulations Staff regarding the hearing process and submission requirements.

- The White Oaks Complete Corridor. No development shall occur within the lands regulated by the Conservation Authority prior to the completion of the Complete Corridor, and the as-built drawings have been accepted by the Conservation Authority.
- The proposed residential development. The issuance of this approval will be contingent upon the completion of the Complete Corridor and the UTRCA's acceptance of the as-built drawings. The proposed townhouse development will not be able to proceed until all of the necessary approvals have been secured and the regulated features have been compensated for.

## Appendix E – Public Engagement

On August 30, 2024, Notice of Application was sent to 65 property owners and residents in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on September 19, 2024. A “Planning Application” sign was also placed on the site

There were no responses received during the public consultation period.

**Nature of Liaison:** The purpose and effect of this Zoning amendment is to permit eleven blocks of 3.5-storey stacked townhouse developments with a total of 108 residential units, and a total maximum density of 54 units per hectare. Possible change to Zoning By-law Z.-1 **FROM** a Restricted Service Commercial (RSC1/RSC3/RSC4/RSC5) and Urban Reserve (UR4) zone **TO** a Residential R5 Special Provision (R5-7(\_)) Zone. Requested special provisions for lot frontage and reduced parking. The City may also contemplate alternative zoning such as a different base zone, additional permitted uses, additional special provisions and/or the use of holding provisions. File Z-9778. Planner: Isaac de Ceuster



October 8, 2024

City of London  
300 Dufferin Avenue  
London ON N6A 4L9

Attn: PEC: Planning & Environment Committee  
(Meeting to be held on October 22<sup>nd</sup>, 2024)  
RE: **Hyde Park BIA Boundary Expansion Request**

To Whom it May Concern,

The Hyde Park Business Association was founded in 1979, incorporated as a non-profit incorporation in 2008 and achieved its BIA status in 2017 representing over 400 businesses. Over the past years, the HPBIA has been proud to work with businesses and the community for the betterment and support of our economic and socio-economic development in this pocket of Northwest London.

***We respectfully request support to initiate the BIA Expansion Process to expand our BIA Boundaries through approval from the Planning & Environment Committee and Council.***

This will allow us to work with City staff over the next many months as we further engage with property owners and tenants within existing and proposed boundaries to share benefits and address concerns. This leading to a vote led by the City Clerk where businesses and property owners will have their final say with our anticipation of a favourable outcome.

On inception of the HPBIA, and with the vision of eventually serving Northwest London as a whole, the Hyde Park BIA Board of Management formally trademarked (with Industry Canada) the words 'Uptown London'.

With the future intention of extending the BIA Boundaries across Northwest London (as depicted in the map below) while renaming the existing Hyde Park BIA to the *Uptown London Business Improvement Association* (Uptown London BIA), the commercial nodes identified will be promoted as Hyde Park in Uptown London, Sherwood Forest in Uptown London and so forth.

In addition to the current node of Hyde Park, the four main proposed nodes are:

1. Hyde Park Rd & Oxford St. W
2. Oxford St W & Wonderland Road N, excluding the South-East corner
3. Sherwood Forest Area
4. At Wonderland North and Fanshawe Park Roads.

Supporting businesses spread over a wide geographic area within Northwest London can benefit both individual businesses and the broader community by creating smaller, interconnected pockets of business networks within the larger BIA.

The future Uptown London BIA will foster an environment where businesses (both large and small) within each of the nodes support each other, nurture collaboration and collectively grow stronger, even within the same industry while still attracting a steady customer base.



## Hyde Park Business Improvement Association

*"Businesses Working Together to Foster a Vibrant Community"*



Over the summer, the HPBIA had Canada Summer Jobs youth engaged with about half the businesses in the proposed boundary expansion area boundaries to share our vision and to facilitate meaningful conversations.

Overall, the feedback showed that a majority of businesses are open to the idea of a BIA in their area. These businesses face similar issues when it comes to growth, infrastructure challenges and opportunities.

This has been encouraging as we seek to expand our work in creating a unified business community across Northwest London while being better positioned to deliver the advocacy, streetscape, marketing, beautification, events and engagement outcomes our community in Northwest (Uptown) London deems to be important.

After much deliberation, the HPBIA's Board of Management voted unanimously to move forward with the process of the expanded proposed boundaries to generally include:

1. Hyde Park Road, north to City limits, and south to both sides of Oxford Street West;
2. Oxford Street West from Royal York Road to Wonderland Road;
3. Wonderland Road, north of Oxford Street through the Sherwood Forest Area, and continuing north to city limits;
4. Fanshawe Park Road, west to the city limits and Gainsborough Road, west to the city limits

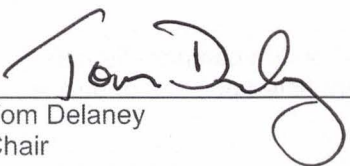
The above is the preferred boundary expansion of the Hyde Park BIA. We will work closely with staff to determine the appropriate nodes in the final presentation to council, understanding it may be slightly different than described above.

Given today's economic challenges faced by local businesses, along with rapid growth in the NW London area, we believe this is the right time for the Hyde Park BIA to expand our service boundaries while rebranding as the Uptown London BIA.

The Hyde Park BIA has achieved significant accomplishments, and we are excited about the possibility of expanding our scope and role to further support the business community whilst benefiting neighbouring communities.

On behalf of the Hyde Park BIA Board of Management, we would like to thank you for your kind consideration.

Yours Truly,

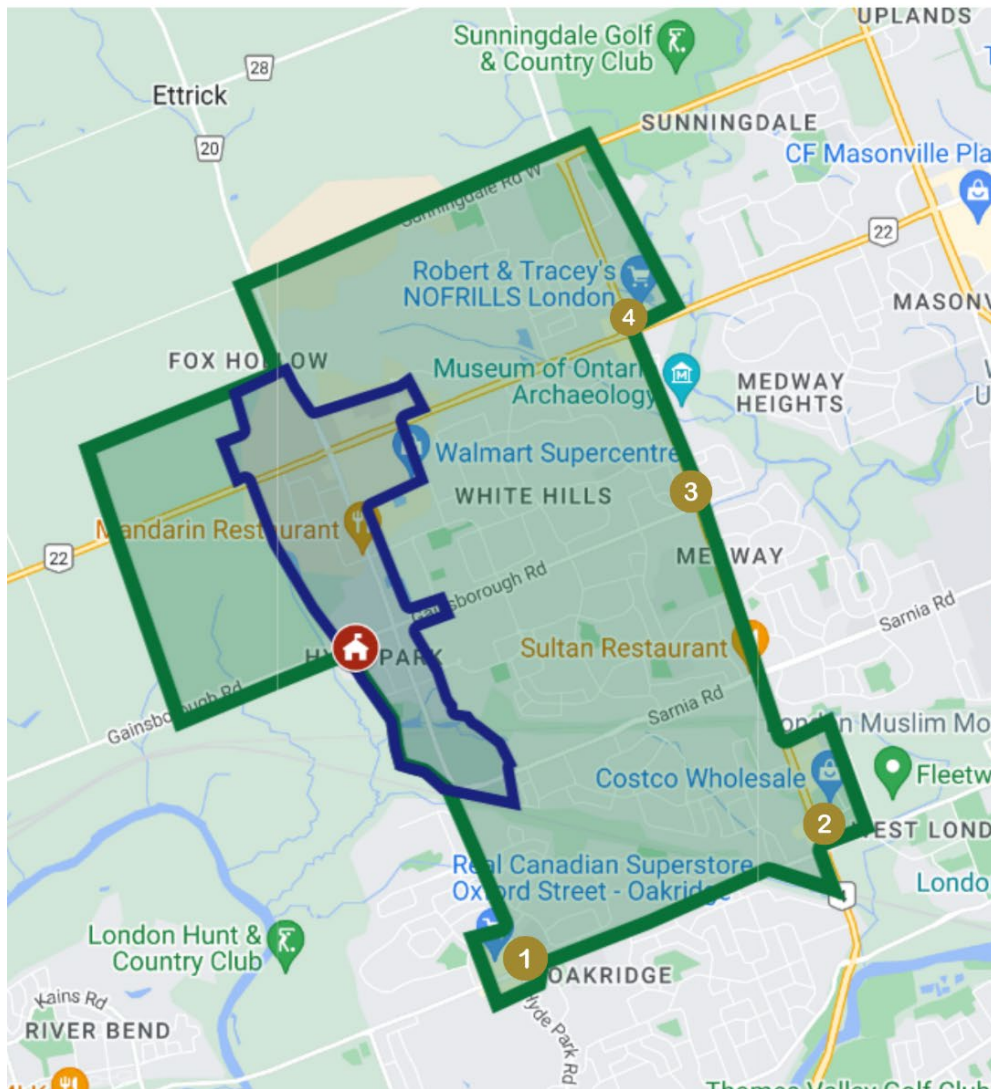
  
\_\_\_\_\_  
Tom Delaney  
Chair

  
\_\_\_\_\_  
Donna Szpakowski  
General Manager & CEO

*Other notes re the BIA Expansion:*

- Expansion adding about 300 new businesses, putting HPBIA at about 700 members
- The geographical area is large; however, the number of businesses is consistent with the average BIA across Ontario which is between 300 and 1000 members
- Sherwood Forest and Fanshawe/Wonderland areas already think of themselves as HPBIA members with frequent inquiries about membership and associate membership
- Land conversions from commercial/industrial to residential along with Supplemental Tax is impacting HPBIA
- City limits are gateways into London

## Approximate Depiction of the Hyde Park BIA Proposed Boundary Expansion





# PEGGY SATTLER

MPP London West  
Députée provinciale de London-Ouest

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Planning and Environment Committee  
City of London Council  
300 Dufferin Avenue, London, ON N6A 4L9

September 20, 2024

Dear Members of the Planning and Environment Committee,

As the Member of Provincial Parliament for London West, I am writing to express my support for the Hyde Park Business Improvement Association's (HPBIA) proposal to expand their BIA boundaries and rebrand as the Uptown London Business Improvement Association (Uptown London BIA).

The HPBIA has played a vital role in strengthening the economic foundation of Northwest London since its inception in 1979, and following its official designation as a BIA in 2017. Their proactive approach to fostering collaboration between local businesses and the community has led to significant improvements in streetscapes, business advocacy, marketing initiatives, and community engagement activities.

The proposed boundary expansion, which includes key commercial nodes such as Hyde Park, Sherwood Forest, and Wonderland Road North, represents a forward-thinking strategy to bring support to a larger section of Northwest London's business community. By extending their reach to more businesses facing similar challenges—such as infrastructure needs and growth management—this expansion will enhance the overall economic vitality of the area.

I believe the timing of this proposal is crucial, as we continue to witness rapid growth and economic changes in London and across Ontario. By enabling the HPBIA to expand and rebrand as Uptown London BIA, the City of London will ensure that more businesses can benefit from the expertise, resources, advocacy, and promotional efforts that the BIA is known for.

I strongly encourage the Planning and Environment Committee and Council to move this request forward to the next step in the process. The HPBIA has demonstrated its effectiveness as a key partner in Northwest London's economic development, and this proposal is an excellent opportunity to build on that success for the betterment of the entire community.

Sincerely,

Peggy Sattler, MPP  
London West

cc. Hyde Park BIA

## COMMUNITY OFFICE

101-240 Commissioners Road West  
London, ON N6J 1Y1  
☎ 519-657-3120 • ✉ PSattler-CO@ndp.on.ca

## QUEEN'S PARK

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Toronto, ON M7A 1A5  
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**DEFERRED MATTERS  
PLANNING AND ENVIRONMENT COMMITTEE  
(AS OF OCTOBER 11, 2024)**

<b>File No.</b>	<b>Subject</b>	<b>Request Date</b>	<b>Requested/ Expected Reply Date</b>	<b>Person Responsible</b>	<b>Status</b>
1	Draft City-Wide Urban Design Guidelines – Civic Admin to report back at a future PPM of the PEC	Oct 29/19 (2.1/18/PEC)	Q4 2024	H. McNeely/ K. Edwards	Staff are working to incorporate the contents of the draft Urban Design Guidelines into the Site Plan Control By-law update (expected Q3 2024) as well as the new Zoning By-law (expected Q4 2024). The need for additional independent UDG will be assessed after those projects are complete.
2	Homeowner Education Package – 3 <sup>rd</sup> Report of EEPAC - part c) the Civic Administration BE REQUESTED to report back at a future Planning and Environment Committee meeting with respect to the feasibility of continuing with the homeowner education package as part of Special Provisions or to replace it with a requirement to post descriptive signage describing the adjacent natural feature; it being noted that the Environmental and Ecological Planning Advisory Committee (EEPAC) was asked to undertake research on best practices of other municipalities to assist in determining the best method(s) of advising new residents as to the importance of and the need to protect, the adjacent feature; and,	May 4/21 (3.1/7/PEC)	Q3 2024	H. McNeely/ M. Davenport/ K. Edwards	Staff have undertaken a detailed review of the recommendations made in the EIS Monitoring Report and are reviewing overall best practices.



File No.	Subject	Request Date	Requested/ Expected Reply Date	Person Responsible	Status
	through section 97.1 of the Municipal Act to implement sustainable building construction features, including but not limited to, energy efficiency, water conservation and green roofs, and report back to Council with options and recommendations, including identifying any required Official Plan, Zoning Bylaw and Site Plan Control Bylaw amendments; and, d) the Civic Administration BE DIRECTED to report back to Council within Q3 2024 with a short update regarding the scope and timeline of the Green Development Guidelines and Green Parking Lot Guidelines.				Part d) work targeted for Q3 2024
8	Materials Guidelines (4th Report of the CACP) - the Civic Administration BE DIRECTED to develop a more permissive set of guidelines for the use of synthetic materials (including composites) in buildings designated under Heritage Conservation Districts to allow greater flexibility in material choices, while also aligning with London's existing HCD policies to preserve the aesthetic of heritage buildings and report back to a future PEC meeting for public input and Council approval;	May 14/24 (2.2/7/PEC)	Q2/2025	H. McNeely/ K. Gonyou	Staff are reviewing synthetic materials (including composites) in relation to their potential use for properties designated as part of a Heritage Conservation District.
9	Civic Administration BE DIRECTED to provide information and associated recommended actions on school block	June 25/24 (4.1./10/PEC)			

File No.	Subject	Request Date	Requested/ Expected Reply Date	Person Responsible	Status
	<p>acquisitions and report back to the Planning and Environment Committee in Q4 of 2024, including, but not limited to:</p> <p>a) background on the acquisition of blocks for the purposes of a school in the context of Planning Act applications;</p> <p>b) a status update on all unacquired Blocks identified in approved Planning Act applications identified for possible School Blocks; and,</p> <p>c) provide options for Council's consideration to provide the School Board(s) with additional flexibility in acquiring School blocks in future Planning Act applications;</p>				
10	<p>The Deputy City Manager, Environment and Infrastructure, BE REQUESTED to report back with an analysis of the Sanitary Servicing Agreement request made by the Municipality of Middlesex Centre, including planning, technical, and financial considerations</p>	<p>July 16, 2024 (4.2/11/PEC)</p>	<p>Q1 2025</p>	<p>K. Scherr/A. Rammeloo</p>	
11	<p>the Civic Administration BE DIRECTED to provide information and associated recommended actions on the following matters in Q4 of 2024:</p> <p>Urban Forestry Strategy Monitoring Report:</p>	<p>July 16, 2024 (4.3/11/PEC)</p>		<p>K. Scherr/P. Yeoman</p>	

File No.	Subject	Request Date	Requested/ Expected Reply Date	Person Responsible	Status
	<p>a) status update of initiatives identified in the Urban Forestry Strategy Implementation Plan;</p> <p>b) the requirements to effect The London Plan policies for tree replanting and/or compensation with development applications;</p> <p>c) opportunities to establish large designed planting sites (e.g., an arboretum or new woodland habitat); and,</p> <p>d) opportunities to require better soil amendments in new developments</p> <p>e) the Civic Administration engage where applicable in stakeholder consultation on these items, including engaging the general public, local agencies, relevant advisory committees and the development industry</p>				
12	<p>the Civic Administration BE DIRECTED to provide information and associated recommended actions on the following matters in Q4 of 2024:</p> <p>Tree Planting Strategy:</p> <p>a) updated policies and numerical standards related to resident input for boulevard tree planting, soil suitability for boulevard tree planting, and selection of tree species to be planted;</p> <p>b) specific planting targets for geographic</p>	July 16, 2024 (4.3/11/PEC)	Q4 2024	K. Sheer/P. Yeoman	



File No.	Subject	Request Date	Requested/ Expected Reply Date	Person Responsible	Status
	<p>areas of the City; and,</p> <p>c) additional planting opportunities on City-owned lands and partnerships with consenting institutional land owners (e.g., schools, hospitals, universities, colleges, etc.);</p> <p>d) the Civic Administration engage where applicable in stakeholder consultation on these items, including engaging the general public, local agencies, relevant advisory committees and the development industry</p>				