

Agenda Including Addeds

Planning and Environment Committee

1st Meeting of the Planning and Environment Committee

December 4, 2023

4:00 PM

Council Chambers - Please check the City website for additional meeting detail information. Meetings can be viewed via live-streaming on YouTube and the City Website.

The City of London is situated on the traditional lands of the Anishinaabek (AUh-nish-in-ah-bek), Haudenosaunee (Ho-den-no-show-nee), Lūnaapéewak (Len-ah-pay-wuk) and Attawandaron (Add-a-won-da-run).

We honour and respect the history, languages and culture of the diverse Indigenous people who call this territory home. The City of London is currently home to many First Nations, Métis and Inuit today.

As representatives of the people of the City of London, we are grateful to have the opportunity to work and live in this territory.

Members

Councillors S. Lehman (Chair), S. Lewis, C.Rahman, S. Franke, S. Hillier

The City of London is committed to making every effort to provide alternate formats and communication supports for meetings upon request. To make a request specific to this meeting, please contact PEC@london.ca or 519-661-2489 ext. 2425.

Pages

1. Call to Order

1.1 Disclosures of Pecuniary Interest

1.2 Election of Vice Chair for the term ending November 30, 2024

2. Consent

2.1 Inclusionary Zoning Review Update

3

2.2 12th Report of the Ecological Community Advisory Committee

10

2.3 Building Division Monthly Report, July 2023

45

2.4 Building Division Monthly Report, August 2023

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2.5 Building Division Monthly Report, September 2023

68

2.6 Building Division Monthly Report, October 2023

79

3. Scheduled Items

3.1 Public Participation Meeting - Not to be Heard before 4:00 PM - 1350 Wharncliffe Road South (Z-9611)

88

a. *(ADDED) Project Fact Sheet*

121

3.2 Public Participation Meeting - Not to be Heard before 4:00 PM - 1680 Richmond Street (Z-9667)

123

3.3 Public Participation Meeting - Not to be Heard before 4:00 PM - 130 Southdale Road West (Z-9663)

141

a.	<i>(ADDED) Revised By-law</i>	159
3.4	Public Participation Meeting - Not to be Heard before 4:00 PM - 625 Mornington Avenue (1299 Oxford Street East) (Z-9589)	162
3.5	Public Participation Meeting - Not to be Heard before 4:30 PM - 488-492 Pond Mills Road (Z-9625)	200
4.	Items for Direction	
5.	Deferred Matters/Additional Business	
5.1	Deferred Matters List	235
6.	Confidential (Enclosed for Members Only)	
6.1	Solicitor-Client Privileged Advice / Litigation/Potential Litigation	
	<p>A matter pertaining to advice that is subject to solicitor-client privilege, including communications necessary for that purpose from the solicitor and officers and employees of the Corporation; the subject matter pertains to litigation or potential litigation with respect to appeals related to the Victoria Park Secondary Plan at the Ontario Land Tribunal (“OLT”), and for the purpose of providing instructions and directions to officers and employees of the Corporation.</p>	
6.2	Solicitor-Client Privileged Advice / Litigation/Potential Litigation	
	<p>A matter pertaining to advice that is subject to solicitor-client privilege, including communications necessary for that purpose from the solicitor and officers and employees of the Corporation; the subject matter pertains to litigation or potential litigation with respect to appeals related to 755, 765, 785, and 815 Wonderland Road at the Ontario Land Tribunal (“OLT”), and for the purpose of providing instructions and directions to officers and employees of the Corporation.</p>	
6.3	Solicitor-Client Privileged Advice / Litigation/Potential Litigation	
	<p>A matter pertaining to advice that is subject to solicitor-client privilege, including communications necessary for that purpose from the solicitor and officers and employees of the Corporation; the subject matter pertains to litigation or potential litigation with respect to appeals related to 3089 Singleton Avenue at the Ontario Land Tribunal (“OLT”), and for the purpose of providing instructions and directions to officers and employees of the Corporation.</p>	
7.	Adjournment	

Report to Planning and Environment Committee

To: Chair and Members
Planning and Environment Committee
From: Scott Mathers, MPA, P.Eng.
Deputy City Manager, Planning and Economic Development
Subject: Inclusionary Zoning Review Update
Date: December 4, 2023

Recommendation

That, on the recommendation of the Director, Planning and Development, the following actions be taken with respect to Inclusionary Zoning review:

- (a) That **NO FURTHER ACTION** be taken with respect to the Inclusionary Zoning review.

IT BEING NOTED THAT Civic Administration will consider the findings of the Inclusionary Zoning feasibility analysis in related policy and program reviews to support the development of new affordable housing units, including but not limited to the review of incentive programs, policy reviews in support of the Housing Accelerator Fund (HAF) initiatives, and updates to the programs included in the Roadmap to 3,000 Affordable Units.

FURTHER, IT BEING NOTED THAT Inclusionary Zoning is one potential tool to encourage the development of new affordable housing units; however, the financial feasibility analysis demonstrates that IZ is not a consistently viable mechanism to achieve this goal for all tenures of housing or for all market areas of the city.

Executive Summary

Inclusionary Zoning (IZ) is a tool by which the City of London could require affordable units to be included in certain new market-rate housing developments. Provincial legislation states that IZ may only be permitted within areas of a city designated as a “Protected Major Transit Station Area” (PMTSA) unless the Minister prescribes an alternative application of the IZ regulations.

Prior to introducing IZ regulations, the City must undertake an Assessment Report consistent with Provincial regulations. The Assessment Report identifies the need for affordable housing units and evaluates the impact of IZ regulations on the housing market, costs, and land. The Provincial regulations are to ensure that financial feasibility is maintained for the private land developer.

Previous reporting to Council in February 2022 identified significant limitations in the Provincial legislation that restrict the potential effectiveness of IZ under the current regulations. Through Council direction in 2022, the Inclusionary Zoning review has been updated to a municipality-wide analysis, including lands outside of the Protected Major Transit Station Areas (PMTSA). The following is an update to the 2022 feasibility analysis. The updated analysis is a city-wide review and also incorporates recent changes to Provincial legislation regarding development charges and the planning policy framework, such as More Homes Built Faster Act, 2022 (Bill 23).

Under the updated policy framework, the financial feasibility of Inclusionary Zoning remains generally consistent with previous reporting. Financial incentives and other “offsets” would be required to maintain financial feasibility of market development in the majority of the city’s market areas if an Inclusionary Zoning regulation is introduced to require affordable housing units.

Linkage to the Corporate Strategic Plan

The Review of Inclusionary Zoning policy is consistent with Council's Strategic Plan 2023-2027, including Expected Result 1.1 of the Housing and Homelessness Strategic Area of Focus, which is: "Increased access to a range of quality, affordable, and supportive housing options that meet the unique needs of Londoners."

Analysis

1.0 Background Information

1.1 Introduction

Access to stable and affordable housing for all individuals and families is an important issue for the London community. Rising housing costs relative to incomes and the ability for the community to find adequate housing is the reason the City of London is looking to various legislation and tools to support the creation of new affordable housing units.

Previously, through Provincial legislation, the City was permitted to enter into "Bonus Zoning" agreements for creation of affordable units in some new market developments. The agreement was that for Zoning amendments to add additional height and density, a certain portion of that additional residential density would be delivered as affordable units. However, Bonus Zoning agreements can no longer be entered into after September 2022, based on changes to Provincial legislation.

Inclusionary Zoning is one of the potential new tools being evaluated to support the creation of new affordable housing units in the city.

1.2 What is Inclusionary Zoning?

Inclusionary Zoning (IZ) is a regulatory tool the City of London may consider as a means of supporting the provision of affordable housing within new development.

Inclusionary Zoning refers to zoning regulations that would require certain types of new residential development to include affordable housing units as part of the proposal.

Inclusionary Zoning is not meant to replace publicly provided housing, nor is it a municipal incentive program with financial support. It may, however, be complementary to those programs.

2.0 Provincial IZ Legislation and Previous Findings

2.1 Provincial Legislation for IZ

As identified in previous reports, the *More Homes, More Choice Act, 2019* included a number of requirements and criteria for municipalities to satisfy in order to introduce Inclusionary Zoning policies and regulations, including identification of: minimum development size, geographic areas of eligibility, affordability levels for non-market units, types/sizes of units to be provided, and the length of time units must be maintained as affordable (i.e. the "affordability period"). Additionally, through regulation, it is identified that IZ can only be approved after an assessment report is completed. The assessment report must include a financial feasibility analysis. The financial analysis must demonstrate the impact of the IZ requirement on the financial feasibility of the overall market development.

The purpose of the feasibility analysis is to demonstrate that market developers would still choose to proceed with projects even if required to include delivery of affordable units. If financial feasibility cannot be achieved with IZ requirements for affordable units, then either a smaller building would be built below the IZ threshold, or developers would choose to not build at all. The result of either scenario would be a reduced supply of

affordable housing units constructed as well as a reduced supply of new housing units overall.

2.2 Previous Findings

The approach to the Inclusionary Zoning Review has been to apply The London Plan's framework of heights and include Inclusionary Zoning requirements where "Bonus Zoning" would previously have been required. As such, new development up to the "Standard Maximum" Height (in storeys) of a Place Type would not require affordable units, whereas developments above the Standard Maximum Height and up to the "Upper Maximum Height" of the Place Type would require a certain portion of the additional units to be set aside as affordable through the IZ regulations. This analysis was previously applied to the "PMTSA" area, which includes the Downtown, Rapid Transit Corridor, and Transit Village Place Types only.

Some key findings from the previous feasibility analysis conducted by N. Barry Lyons Consulting (NBLC) on behalf of the City of London included the following:

- In most cases the analysis indicated the additional density is not sufficient on its own to offset impacts of IZ for a wide range of outcomes (i.e. some projects may be feasible, but others may not). Additional offsets, such as incentives, may be necessary to support a viable IZ policy and encourage transit-oriented development.
- An IZ policy that only applies to the PMTSA will likely limit the development interest in the strategic growth nodes and corridors associated with rapid transit, potentially limiting number of units in this area.
- Additional density over the "Standard Maximum Height" of The London Plan permissions may only provide limited value in certain market areas.
- Additional density increases the demand for automobile parking, which typically must be underground or in parking structures for larger buildings in the PMTSA. The delivery of additional parking for larger buildings, regardless of City parking regulations reductions, means that the cost of the additional parking can erode the value of adding the additional density.
- The downtown presents some viability for IZ because it is supported by a combined Development Charges and Tax Increment Grant incentive program offered through the Downtown Community Improvement Plan.
- London's rapid transit system is not yet fully constructed. Higher land values are not reflected relative to other areas of the city without immediate access to the rapid transit system.

Some factors which impacted these initial findings included:

- Market interest in higher-density development has been demonstrated across the city, not just within the PMTSA area.
- Private automobiles are the predominant mode of travel in London, which in part results in similar land values across the city (inside and outside the PMTSA).
- The cost of underground and structured parking is included in the feasibility analysis because it is assumed that the market will demand parking spaces with most residential units. The cost of parking is included regardless of whether a site is required by City regulation to have parking or whether the parking is because of developer's assumptions about market expectations.
- Inclusionary Zoning is an inflexible regulatory tool. Financial feasibility must be demonstrated for all lands across a broad geographic area where IZ is being applied. Since IZ policies apply to wide areas, the financial test of feasibility must be based on assumptions that can apply to all properties, not a site-by-site assessment of unique characteristics of a specific property or a specific development application.
- Standardized assumptions are made for cost input factors such as parking rates, parking formats, and timing of land sales at current market value.

3.0 Legislation Changes

Since the February 2022 report to Council, a city-wide update to the feasibility analysis was prepared by NBLC in order to add additional market areas to be considered by a new IZ policy. This was prepared for the City to initiate discussions with the Provincial Ministry regarding city-wide eligibility for London's IZ regulation rather than eligibility limited to the PMTSA. However, in fall 2022 a number of legislative changes were introduced by the Province which impact Development Charges and Planning policy frameworks, including the introduction of Bill 23 (the *More Homes Built Faster Act, 2022*).

Since 2022 there have also been changes to the residential construction cost benchmark (cost of materials, construction labour), and market conditions have also changed, including carrying costs of land with higher interest rates, supply chain constraints, and a decrease in average sales price for average market units.

New provincial legislation includes the following:

- Inclusionary Zoning policies can have a maximum "set aside rate" of 5% of a development's units; and
- Purpose-built rental developments must have their Development Charges reduced, based on a unit's number of bedrooms, as follows:
 - o 3 bedrooms: a 25% DC reduction;
 - o 2 bedrooms: a 20% DC reduction; and
 - o 1 bedroom / bachelor units: a 15% DC reduction.

In September 2023 the Federal Government also announced that it would waive the GST portion of the HST on new purpose-built rental developments. The Province has since announced with the same waiver.

The changes in project costs and unit costs associated with changes in government policy and market conditions, have resulted in several updates to key findings of the IZ financial feasibility analysis.

4.0 Updated Feasibility Findings

The financial feasibility review updated the analysis of the various market areas across the city with updated costs. For each market area, condominium ownership and rental tenures were modelled, based on land use permissions and heights of The London Plan Place Types.

The analysis tested whether there was an increase in land value associated with the increase in units for all scenarios. It tested whether a positive residual land value results based on current prices/rents for units, and potential profit expected after all developer costs and inputs into the development.

Table 1, below, summarizes which market areas demonstrate positive land values for the scenarios where 5% of units are delivered as affordable housing units under an Inclusionary Zoning regulation.

The results of all scenarios are included in Appendix A, attached to this report.

Market Area	Scenario		
	Condominium Affordable Ownersip IZ	Condominium Affordable Rental IZ	Rental Affordable Rental IZ
Downtown	No	No	Yes
North RTC Richmond	Yes	Yes	Yes
East RTC King-Dundas	No	No	No
South RTC Wellington	No	No	No
West RTC Oxford W	No	No	No
North TV Masonville	No	No	Yes
East TV Oxford/Highbury	No	No	No
South TV White Oaks	No	No	No
West TV Oxford/Wonderland	No	No	No
Inside PTA, Large Site	No	No	No
Inside PTA, Small Site	No	No	No
Outside PTA, Large Site	No	No	No
Outside PTA, Small Site	No	No	No

Table 1: Summary of land value for scenarios including IZ (Source: NBLC).

In some market areas and scenarios, a positive land value resulted but a larger building did not demonstrate an increase in profitability. If a positive land value resulted but was not considered sufficient to compel a developer to build the larger building with the IZ requirement, the financial incentive to return a project to status quo financial feasibility was also identified.

No increase in residual land value despite increased development density can be a result of various costs, including changes to the automobile parking format. For example, this may result from moving from the relatively inexpensive delivery of surface parking to the relatively expensive cost of building structured/underground parking in a higher density building. Changes in construction materials, such as a move from wood-framed construction to concrete construction is also another possible cause of no increase in value despite a larger building being proposed.

If there is no increase in residual land value, then the expectation is a market developer would not be compelled to build the larger building with the affordable unit requirement through the IZ policy. Instead, a smaller building which does not require the IZ units would be built, or the development proposal would not proceed until a change in market conditions.

The analysis also found that in most scenarios, even where positive land values were attained, developers would not be incentivized to take up the additional density without additional financial incentives to offset the costs associated with the IZ units. The financial incentive offset required to maintain the status quo of financial feasibility relative to development without the IZ requirements ranges from \$30,000 to \$100,000 per affordable unit.

Overall, many of the key findings from the previous analyses hold true under the updated feasibility analysis which includes the Province of Ontario’s updated development charges and planning policy framework. Findings of NBLC include that:

- Additional density is not anticipated to be a significant driver of land value.
- There are few scenarios where the additional density generated enough value to offset IZ requirements to compel developers to build a larger building that includes the affordable IZ units.
- Reduced parking may be a solution to add value to the land and reduce construction costs; however, despite any changes in City regulations requiring parking, it is market demands that drive the amount of parking a building includes. So, the

market demand for parking must change (i.e. be reduced) before this value could potentially be realized.

- Since September 2022 there has been further increases in construction costs and interest rates, resulting in developers postponing, revising, or cancelling some housing projects. All of which directly affect the cost of labour and construction goods. High interest rates have also reduced market demand and resulted in a corresponding reduction in sales prices of units.

Inclusionary Zoning is one potential tool to encourage the development of new affordable housing units; however, the financial feasibility analysis demonstrates that IZ is not a consistently viable mechanism to achieve this goal for all tenures of housing or for all market areas of the city.

Conclusions and Next Steps

A financial feasibility analysis must demonstrate viability in order for the City to enter into negotiations with the Province and seek Provincial approval to expand the scope of the tool beyond the existing area where IZ is currently permitted. The current area where IZ is permitted is limited to the Protected Major Transit Station Areas (PMTSA), which are the Downtown, Rapid Transit Corridor, and Transit Village Place Types of The London Plan.

An unintended consequence may be that Inclusionary Zoning might also result in reduced housing supply through smaller buildings constructed which are under the minimum threshold size that requires the affordable units. The IZ program and its impact on land value may also contribute to delays and/or cancellation of development projects in the PMTSA which is a primary strategic growth area of the City. There may also be pressures to develop on areas not planned for higher intensity development.

It is recommended that no further action be taken at this time with regards to the Inclusionary Zoning Review under the existing Provincial IZ regulations.

As the financial feasibility tests under the current provincial IZ regulations are not met by current housing market conditions, other municipal tools and actions are planned and ongoing to support the delivery of new affordable housing units. Civic Administration will consider the findings of the IZ feasibility analysis in the review and update of incentive programs for affordable housing units.

Civic Administration will additionally consider the findings of the feasibility analysis in the planning policy reviews associated with the Federal Housing Accelerator Fund (HAF) recently awarded to the City. The HAF is to include a forthcoming review of development permissions, heights, and densities permitted on sites in proximity to Rapid Transit stations.

Prepared by: Travis Macbeth, MCIP, RPP
Manager, Planning Policy (Growth Management)

Reviewed by: Justin Adema, MCIP, RPP
Manager, Long Range Planning

Recommended by: Heather McNeely, MCIP, RPP
Director, Planning and Development

Submitted by: Scott Mathers, MPA, P.Eng.
Deputy City Manager, Planning and Economic Development

TM/tm

Appendix A: Financial Analysis Scenario Findings

Appendix A: Financial Analysis Scenario Findings

Summary of Financial Analysis Results of Feasibility Tests - Highlighting Land Value Uplift							
Scenario Matrix	1	2	3	4	5	6	7
Principle Building Tenure	Condominium	Condominium	Condominium	Condominium	Rental	Rental	Rental
Tenure of IZ Units	Condo Ownership	Condo Ownership	Condo Ownership	Rented Condo	Rental	Rental	Rental
Entitlements	Standard Maximum Height	Upper Maximum Height	Upper Maximum Height	Upper Maximum Height	Standard Maximum Height	Upper Maximum Height	Upper Maximum Height
IZ Application (% of Bonus Density)	No IZ	No IZ	5% Ownership	5% Rented Condo	No IZ	No IZ	5% Rental
Summary of Feasibility Tests							
	Positive RLV?	Land Value Uplift?	Offset IZ Req?	Offset IZ Req?	Positive RLV?	Land Value Uplift?	Offset IZ Req?
Downtown	No	No	No	No	No	Yes	Yes
North RTC Richmond	Yes	Yes	Yes	Yes	Yes	Yes	Yes
East RTC King-Dundas	No	No	No	No	No	No	No
South RTC Wellington	No	No	No	No	No	No	No
West RTC Oxford W	Yes	No	No	No	Yes	No	No
North TV Masonville	Yes	No	No	No	Yes	Yes	Yes
East TV Oxford / Highbury	No	No	No	No	No	No	No
South TV White Oaks	No	No	No	No	No	No	No
West TV Oxford / Wonderland	No	No	No	No	Yes	No	No
Inside PTA, Large Site	Yes	No	No	No	Yes	No	No
Inside PTA, Small Site	Yes	No	No	No	Yes	No	No
Outside PTA, Large Site	Yes	No	No	No	Yes	No	No
Outside PTA, Small Site	Yes	No	No	No	Yes	No	No

Ecological Community Advisory Committee

Report

The 12th Meeting of the Ecological Community Advisory Committee
November 16, 2023

Attendance S. Levin (Chair), E. Dusenge, S. Evans, T. Hain, S. Hall, B. Krichker, R. McGarry, K. Moser, S. Sivakumar and V. Tai and H. Lysynski (Committee Clerk)

ABSENT: K. Lee, M. Lima and G. Sankar

ALSO PRESENT: K. Edwards, P. Masse, M. Shepley, M. Szarka and E. Williamson

The meeting was called to order at 4:31 PM; it being noted that E. Dusenge, S. Evans, T. Hain, B. Krichker, K. Moser, S. Sivakumar and V. Tai were in remote attendance.

1. Call to Order

1.1 Disclosures of Pecuniary Interest

That it BE NOTED that no pecuniary interests were disclosed.

2. Scheduled Items

2.1 Dingman Creek Stage 2 EA – Floodplain Update Mitigation Strategy and Official Plan Amendment process

That it BE NOTED that the Ecological Community Advisory Committee received the attached presentation and heard verbal presentations from A. Sones, Environmental Services Engineer, with respect to the Dingman Creek Stage 2 Environmental Assessment - Floodplain Update Mitigation Strategy and Official Plan Amendment process.

3. Consent

3.1 11th Report of the Ecological Community Advisory Committee

That it BE NOTED that the 11th Report of the Ecological Community Advisory Committee, from its meeting held on October 19, 2023, was received.

3.2 Municipal Council Resolution – 10th Report of the Ecological Community Advisory Committee

That, it BE NOTED that the Municipal Council resolution adopted at its meeting held on October 17, 2023, with respect to the 10th Report of the Ecological Community Advisory Committee, from its meeting held on September 21, 2023, was received.

3.3 Sarnia Road/Philip Aziz Environmental Assessment

That it BE NOTED that the Minutes of the EIS Scoping Meeting Consultation for the Western/Sarnia/Philip Aziz EA Detailed Design, from its meeting held on September 18, 2023, was received.

3.4 Revised Notice of Planning Application – 1982 Commissioners Road East
That it BE NOTED that the Revised Notice of Planning Application for Zoning By-law Amendments and the Notice of Public Meeting dated November 6, 2023, from M. Hynes, relating to the property located at 1982 Commissioners Road East, was received for information.

3.5 (ADDED) Notice of Planning Application - 2598-2624 Woodhull Road
That it BE NOTED that the Revised Notice of Planning Application for Zoning By-law Amendments and the Notice of Public Meeting dated November 6, 2023, from M. Hynes, relating to the property located at 2598-2624 Woodhull Road, was received for information.

4. Sub-Committees and Working Groups

None.

5. Items for Discussion

5.1 Lambeth Centennial Park Boardwalk Lifecycle Renewal

That it BE NOTED that the presentation from S. Levin, Chair, Ecological Community Advisory Committee, on how to review Environmental Impact Statements and received the Lambeth Centennial Park Boardwalk Lifecycle Renewal presentation as appended to the Agenda, was received.

5.2 (ADDED) December Meeting Date

That it BE NOTED that the December Ecological Community Advisory Committee meeting date will be changed to December 14, 2023.

5.3 (ADDED) Attendance

That the appointment of K. Lee BE RESCINDED from the Ecological Community Advisory Committee due to lack of attendance.

6. Adjournment

The meeting adjourned at 6:09 PM.



WELCOME

Dingman Creek Subwatershed Stage 2 Schedule C Municipal Class Environmental Assessment and Proposed Official Plan Amendment for Regulatory Floodplain Mapping

Open House — 6:00 to 7:30 p.m.
Thursday, October 19, 2023

Presented by:





The City of London is situated on the traditional lands of the Anishinaabek, Haudenosaunee, Lūnaapéewak and Attawandaron. We honour and respect the history, languages and culture of the diverse Indigenous people who call this territory home. The City of London is currently home to many First Nations, Métis and Inuit today. As representatives of the people of the City of London, we are grateful to have the opportunity to work and live in this territory.

Dingman Creek Subwatershed Stage 2 Schedule C Municipal Class Environmental Assessment and Proposed Official Plan Amendment for Regulatory Floodplain Mapping



WHAT IS THE PURPOSE OF THE PROJECT?

The objective of the Dingman Creek Subwatershed Stage 2 Environmental Assessment (EA) is to:

- Establish stormwater servicing strategy by accommodating future growth
- Assess potential flood mitigation and stormwater servicing alternatives to address imminent and future flooding and erosion risks
- Propose an approach that aligns with the City's vision of a complete corridor that integrates natural heritage, stormwater management and recreational uses

In parallel with the EA, an update of the Dingman Creek Regulatory Floodplain is under review and will conclude through an Official Plan Amendment process.



Jeffery the Salamander is the mascot for the Dingman Creek EA study! He is based on the Jefferson Salamander, which is an endangered species in Ontario.

Dingman Creek Subwatershed Stage 2 Schedule C Municipal Class Environmental Assessment and Proposed Official Plan Amendment for Regulatory Floodplain Mapping



WHAT IS DINGMAN CREEK EA STAGE 2?



Dingman Creek Master Plan

Stage 1 Lands: Schedule B EA

Tributary 12

Thornicroft Drain

Pincombe Drain

White Oaks Drain

Stage 1: lands less impacted by floodplain expansion to:

- Recommend Stormwater Servicing solutions for developable lands within 5-7 year Growth Period
- Generally outside of Dingman Creek zone of influence
- For more information on the Stage 1 EA visit: <https://getinvolved.london.ca/DingmanCreek>

Stage 2 Lands: Schedule C EA

Complete Corridor

Flood Mitigation

Stage 2: lands directly impacted by the proposed floodplain (by 2024)

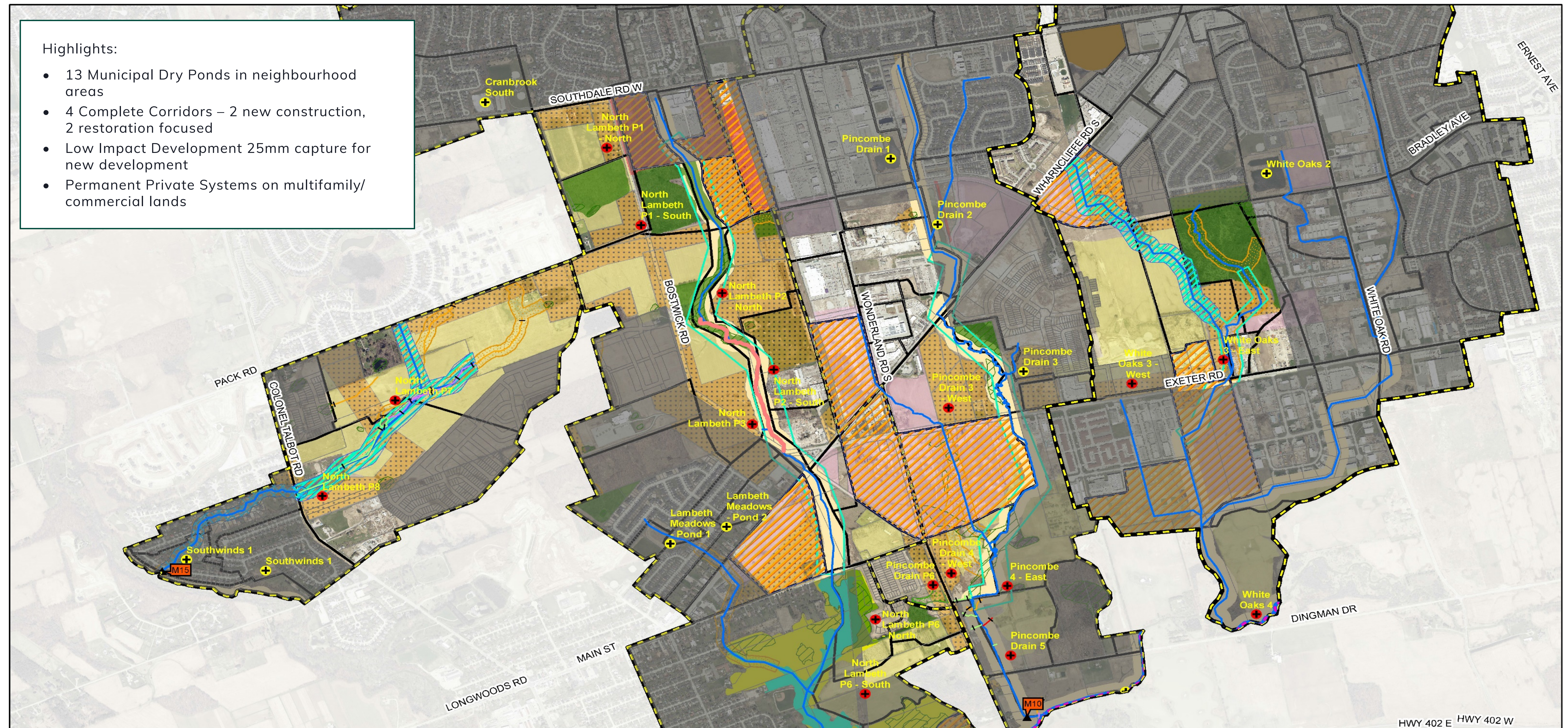
- Update floodplain and assess mitigation options

Stormwater Strategy – Stage 1 EA Preferred Alternative (completed 2020)



Highlights:

- 13 Municipal Dry Ponds in neighbourhood areas
- 4 Complete Corridors – 2 new construction, 2 restoration focused
- Low Impact Development 25mm capture for new development
- Permanent Private Systems on multifamily/commercial lands



Date: November 2019
Source: City of London, 2016

Dingman Creek Subwatershed Study

Implementation Plan - Overview

Municipal Stormwater Facilities:

- ▲ Control Point
- Stream
- Dingman Creek
- Existing
- Future
- Slope Stability Hazard
- Meander Belt
- Private Permanent Systems (PPS)
- Property Parcels

HDF Management Recommendation:

- Protection
- Conservation
- Mitigation
- No Management Required

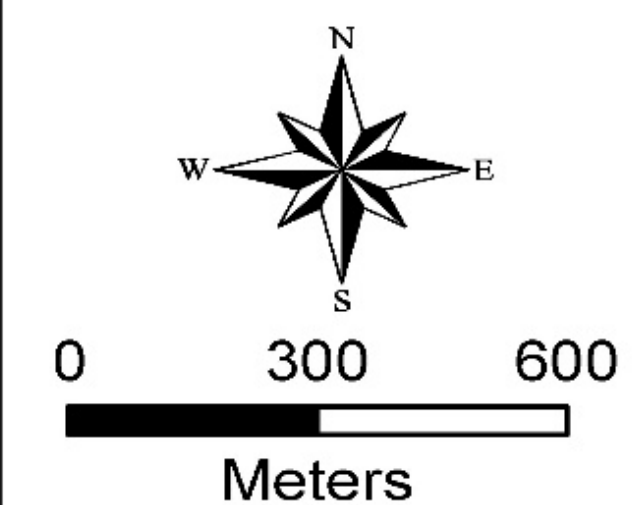
Developments - Nov 2019:

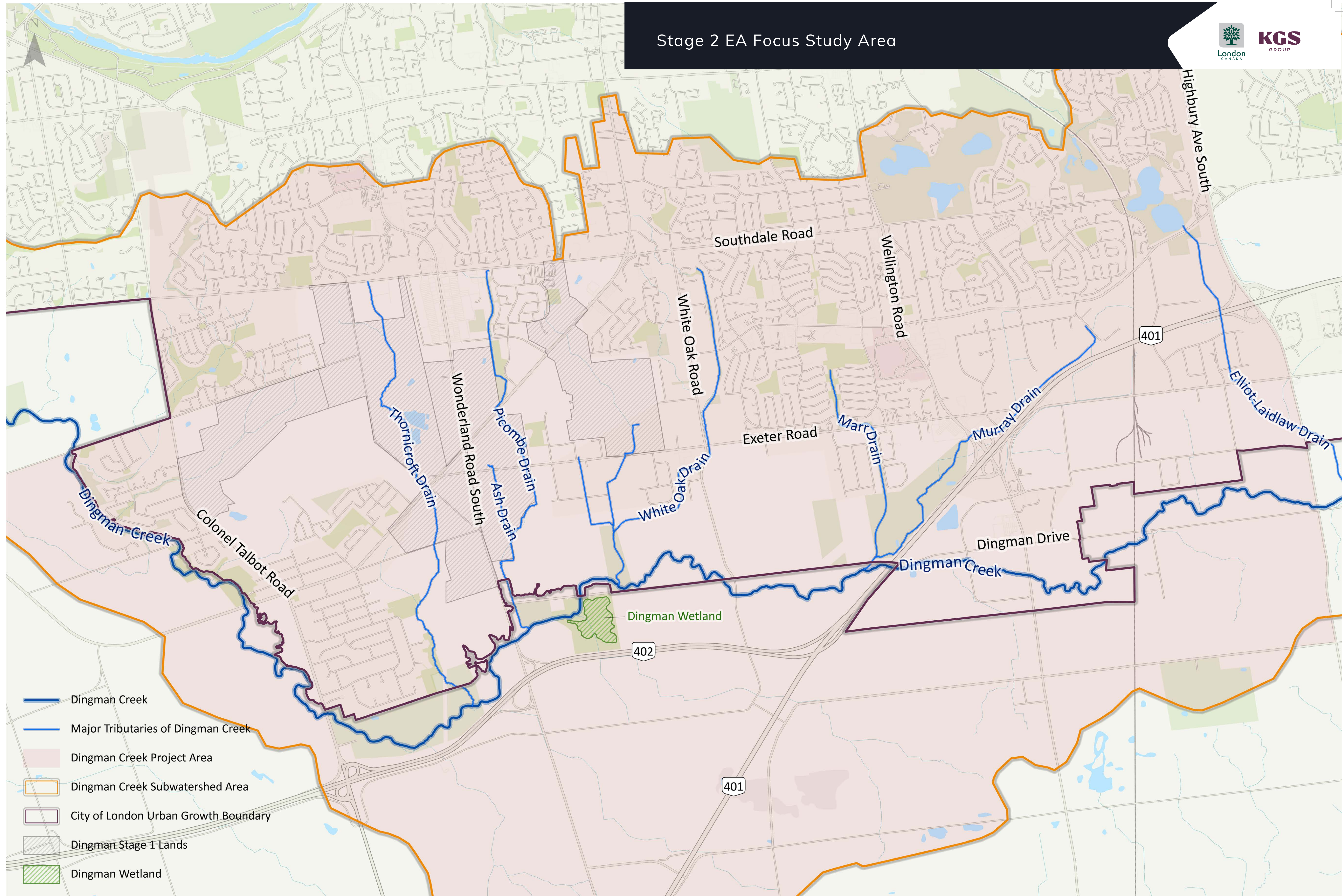
- EMPLOYMENT
- LOW DENSITY RESIDENTIAL
- MEDIUM DENSITY RESIDENTIAL
- HIGH DENSITY RESIDENTIAL
- RESIDENTIAL GROWTH

- Channel Reconstruct
- Complete Corridor Width to be confirmed at functional design stage
- Provincially Significant Wetlands
- Unevaluated Wetland
- Locally Significant Wetlands
- Significant Woodlands
- Woodlands
- Environmentally Significant Areas
- Significant Valleylands
- Potential ESAs
- Valleylands

- Dingman Creek EA Stage 1
- Dingman Creek EA Stage 2
- Subcatchments
- Tributaries

Notes:
Stream System Studies for White Oaks Fluvial Geomorphic Assessment - to be updated
HDF Assessments - to be completed
Erosion Hazards - meander belt assessment provided, stable slope hazard to be confirmed
Maps are representative and do not include all features





Dingman Creek Subwatershed Stage 2 Schedule C Municipal Class Environmental Assessment and Proposed Official Plan Amendment for Regulatory Floodplain Mapping



CLASS EA PROCESS AND PROJECT TIMELINE

The Dingman Creek Stage 2 Lands is following the Class EA Process for Ontario Environmental Assessment Act and will cover all necessary phases of the Schedule 'C' EA Process.

- Step 1 – Define Problem / Opportunity (completed)
- **Step 2 – Public Consultation: Draft Floodplain and Mitigation Strategies | Fall 2023 (this event)**
- Step 3 – Publish updated Preliminary Draft Floodplain Mapping online | October 2023
- Step 4 – Targeted Consultation with Impacted Landowners and Neighbourhoods | Fall/Winter 2023
- Step 5 – Develop Alternative Solutions and Select Preferred Alternative/s | Winter 2023/24
- Step 6 – Develop Design Concepts for the Preferred Alternative/s | Winter 2023/24
- Step 7 – Official Plan Amendment for Regulatory Floodplain | Summer 2024
- Step 8 – Complete the Environmental Study Report | Summer 2024
- Step 9 – EA Approval | Summer/Fall 2024

Dingman Creek Subwatershed Stage 2 Schedule C Municipal Class Environmental Assessment and Proposed Official Plan Amendment for Regulatory Floodplain Mapping



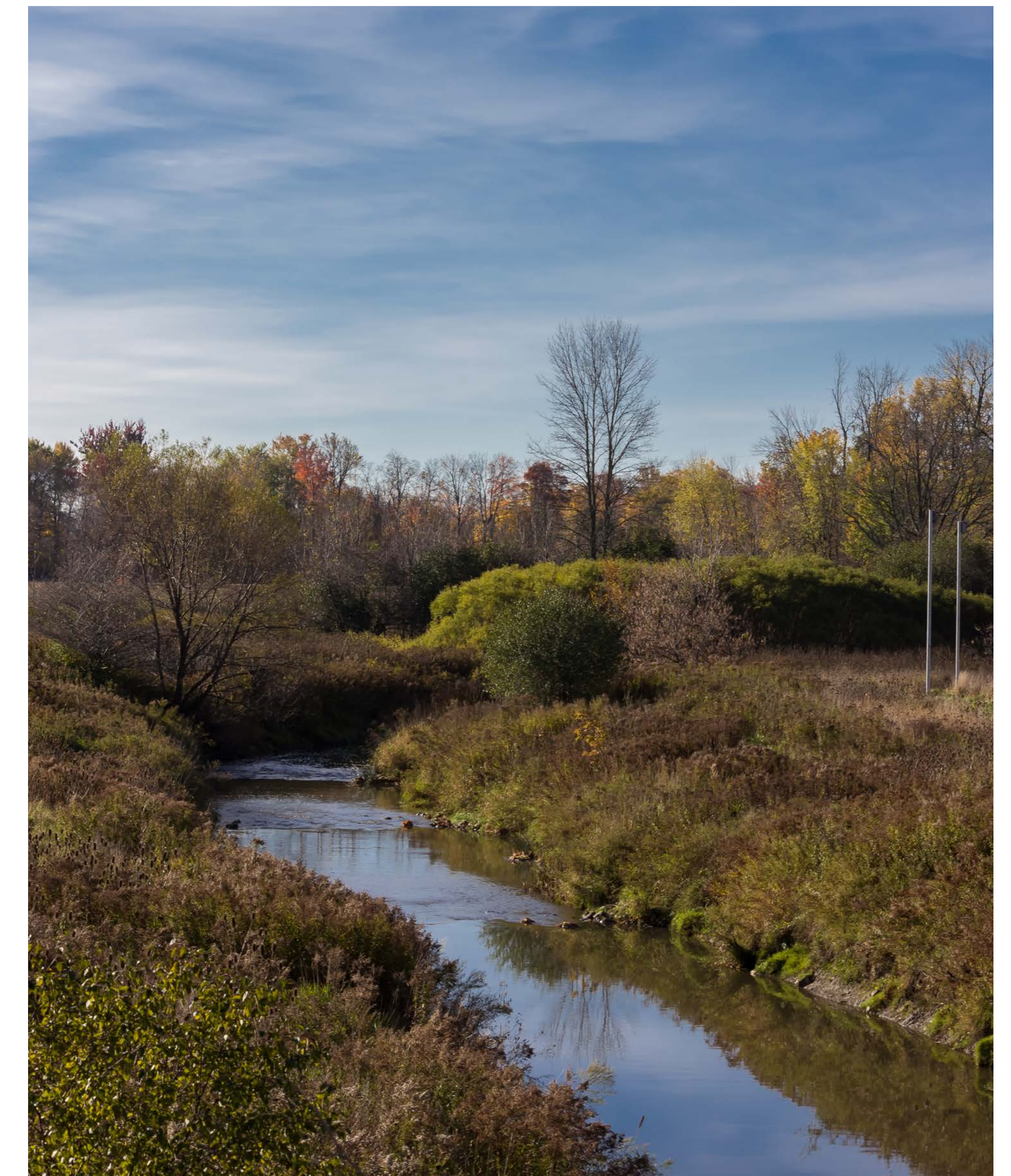
STAGE 2 - EA OPPORTUNITY / PROBLEM STATEMENT

The Dingman Creek Subwatershed (DCS) suffers from poor water quality, lack of wildlife habitat, loss of trees and vegetation as well as flooding and erosion issues.

Sustainable growth within the Urban Growth Boundary of the DCS is a City of London priority. To maintain, enhance, and restore the DCS, the City needs a comprehensive plan to support both environmental and development goals.

This plan must:

- Build on the 1995 and 2005 Dingman Creek Subwatershed Studies and be consistent with the goals and objectives of the Official Plan and Southwest Area Secondary Plan
- Meet the targets established in the Environmental Compliance Approval
- Create a complete corridor that provides a continuous natural area for the movement of water, wildlife and people.



Dingman Creek Subwatershed Stage 2 Schedule C Municipal Class Environmental Assessment and Proposed Official Plan Amendment for Regulatory Floodplain Mapping



WHY IS THE FLOODPLAIN CHANGING?

The floodplain map was developed over 30 years ago. The updated floodplain considers the following:

Climate Change

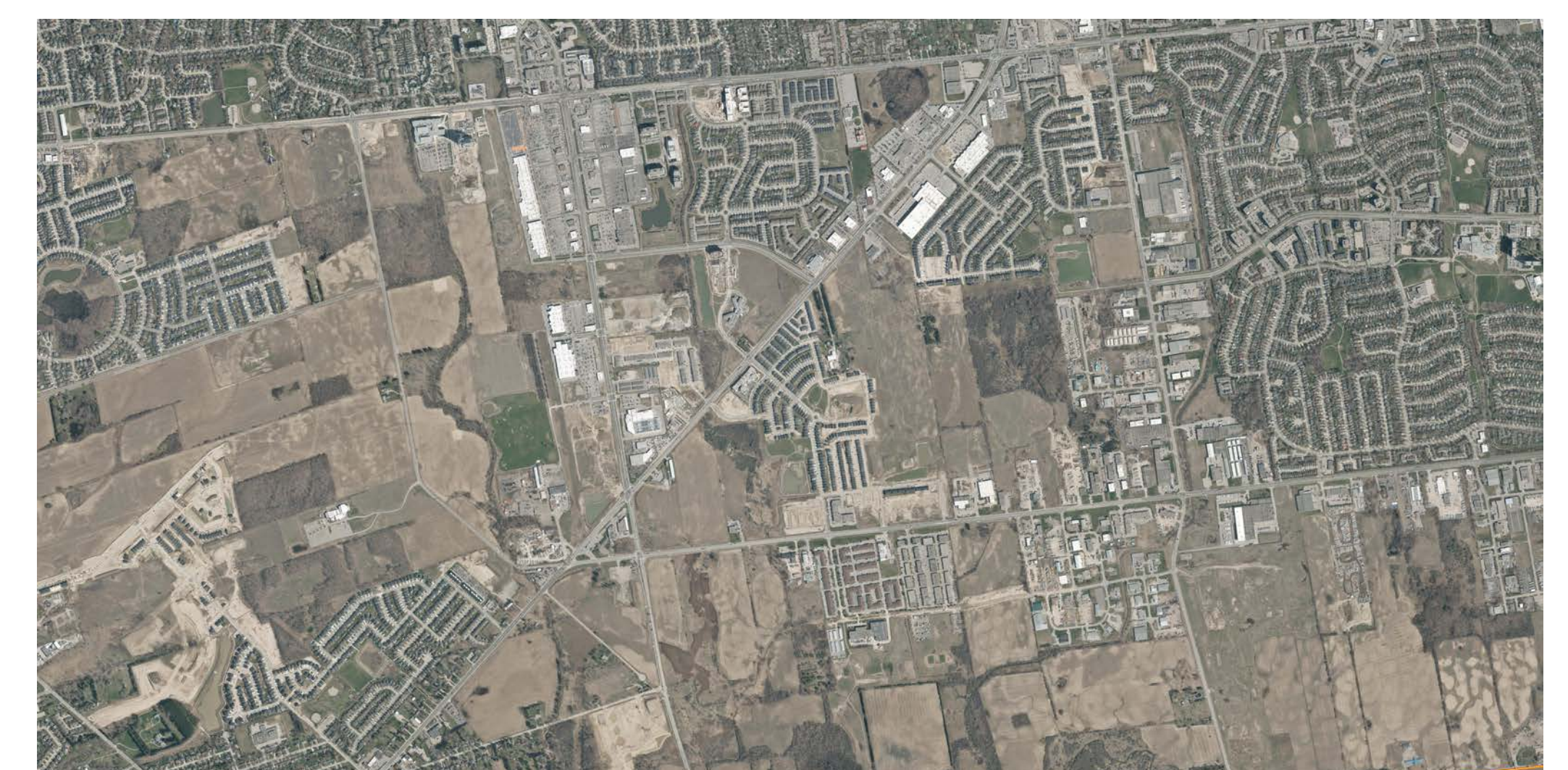
- The climate is changing
- The new model incorporates potential effects of climate change
- Large storm events continue to happen in urban areas. The City needs to prepare for flooding in existing and new development areas

Better Technology and Data

- More advanced and accurate analysis and mapping tools have become available
- New and improved data includes meteorological / hydrological records and topographic base maps

Development within the Urban Growth Boundary

- Existing and future changes to the landscape in the Dingman Subwatershed have been included in the update
- The updated floodplain targets the City's 20-year growth boundary



WHAT IS A REGULATORY FLOODPLAIN?

What is a floodplain?

During normal conditions, the flow in the creek is contained within the main channel (i.e. within the creek banks).

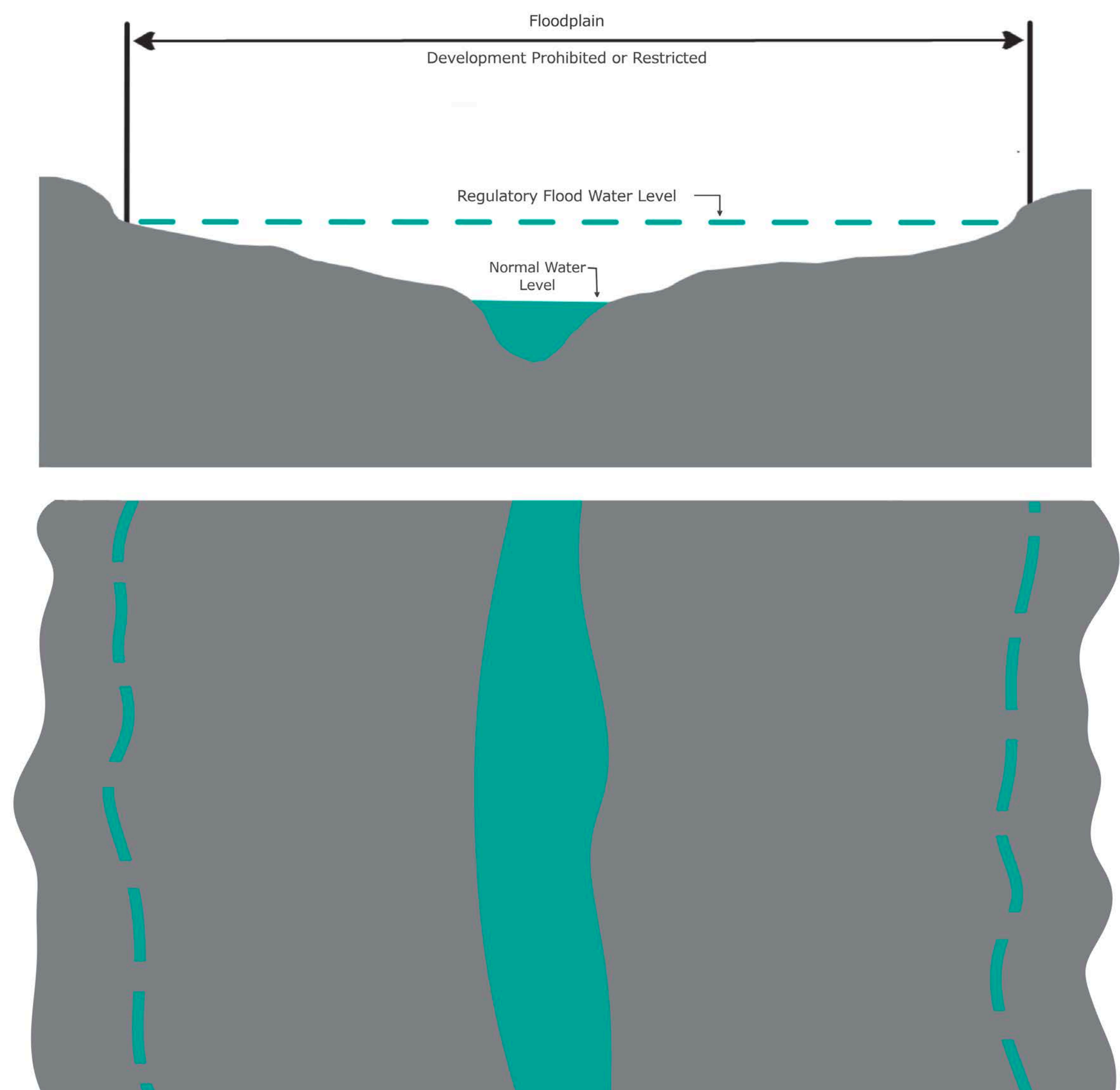
The floodplain is the area next to the main channel that is occupied by water during a flood, when the creek banks are overtopped.

What is a Regulatory Floodplain?

The floodplain that corresponds to the Regulatory Flood event.

In London, the Regulatory Flood is based on the flood of 1937. This event has an annual chance of 0.4% (250-year return period).

Within the Regulatory Floodplain, development is restricted to protect people and properties.

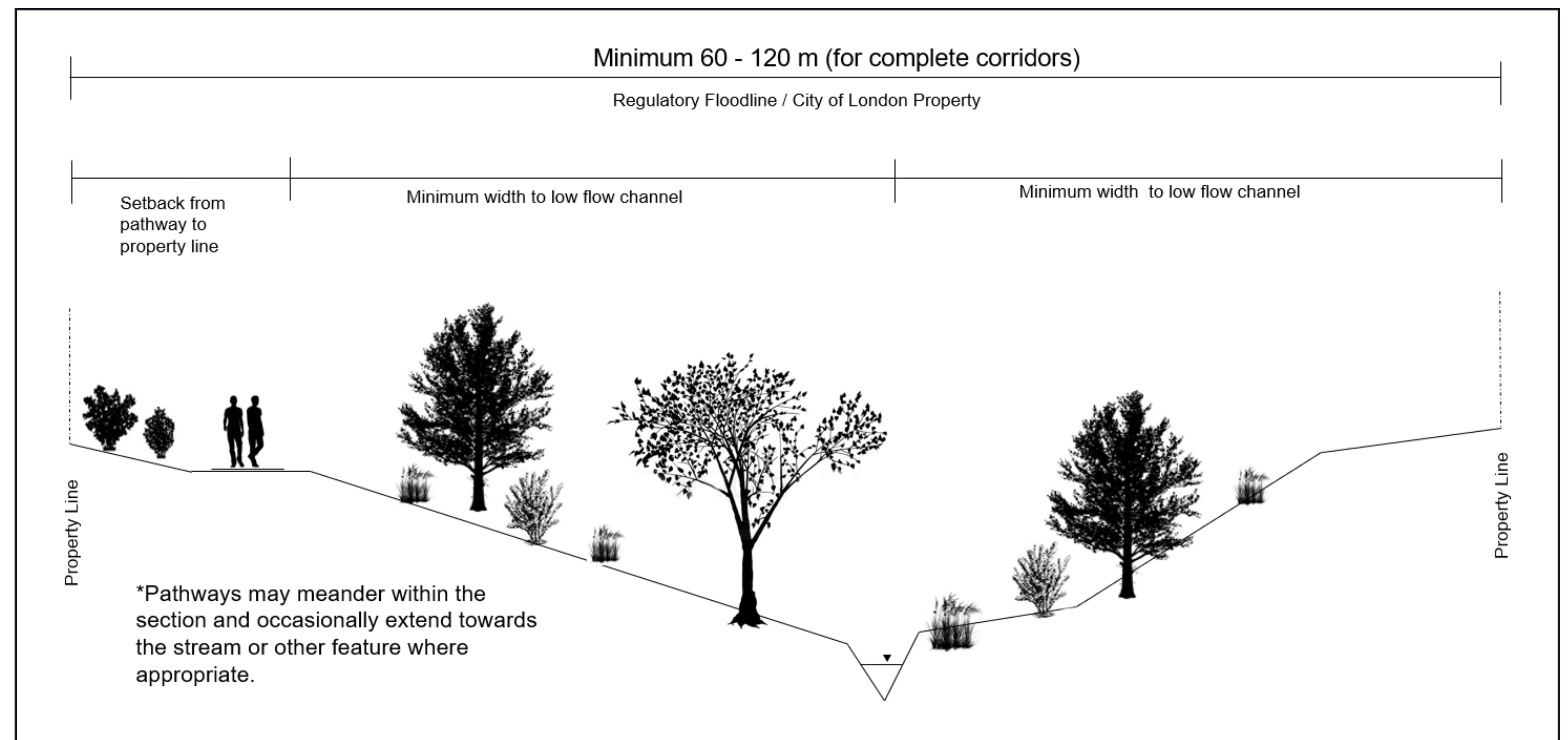


WHAT IS A COMPLETE CORRIDOR?

An overarching concept of the project is to create a naturalized corridor within South London, that promotes movement of water, wildlife and people.

The 3 components of the complete corridor:

- Natural Heritage – to connect significant natural features
- Floodplain Corridor – to convey water, provide habitat for aquatic life and expand flood storage
- Multiuse Pathway – to encourage physical activity, such as walking, running, and cycling

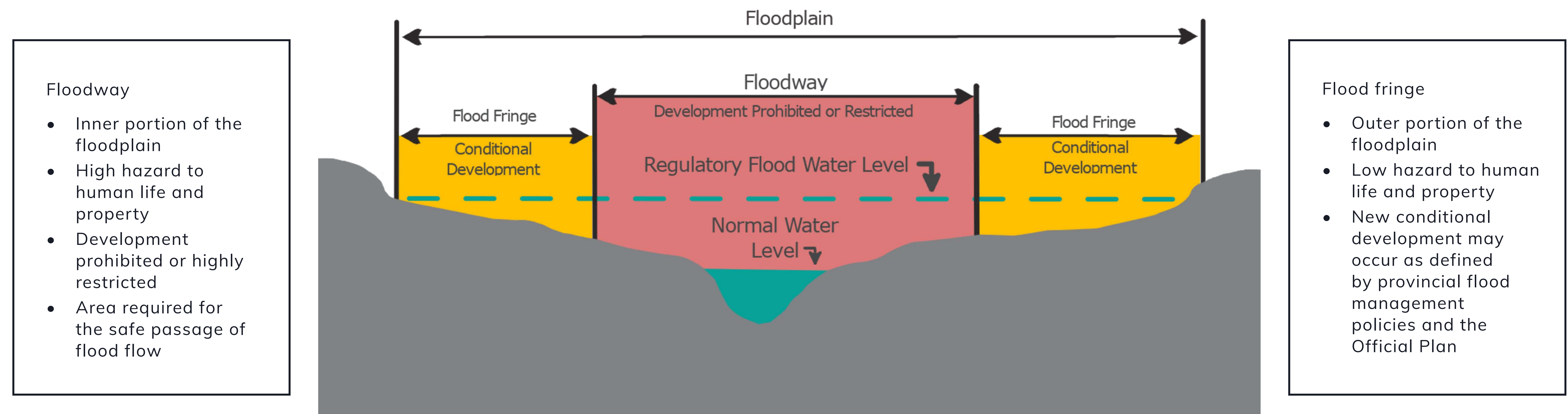


WHAT IS A TWO ZONE FLOODPLAIN?

The previous Dingman Creek floodplain employed a one-zone that limited development anywhere in the floodplain.

A new approach is proposed to differentiate the floodplain into two zones, the floodway and flood fringe.

The use of the two-zone concept may allow for some new development within the flood fringe subject to policies and criteria to be developed through the Official Plan Amendment process.



Dingman Creek Subwatershed Stage 2 Schedule C Municipal Class Environmental Assessment and Proposed Official Plan Amendment for Regulatory Floodplain Mapping



WHAT IS THE LONDON PLAN?

The London Plan is the official plan for the City of London, adopted in 2016. An official plan describes policies on how land should be used.

An official plan deals mainly with issues such as:

- Where new housing, industry, offices and shops will be located
- What services like roads, watermains, sewers, parks and schools will be needed
- When, and in what order, parts of your community will grow
- Where natural heritage or hazard lands (such as floodplain) are located and how to maintain/protect them

The London Plan includes:

- Policies that apply city-wide, such as urban design, or servicing
- Policies that apply to areas specified on the map (designations or “place types”)
- The guiding principles for growth and development over a set planning horizon
- Mapping to implement the above policies and place types

All by-laws and public works must conform with the official plan. Amendments can be made to the Official Plan/London Plan at any time, subject to a mandatory public meeting, and Council approval.

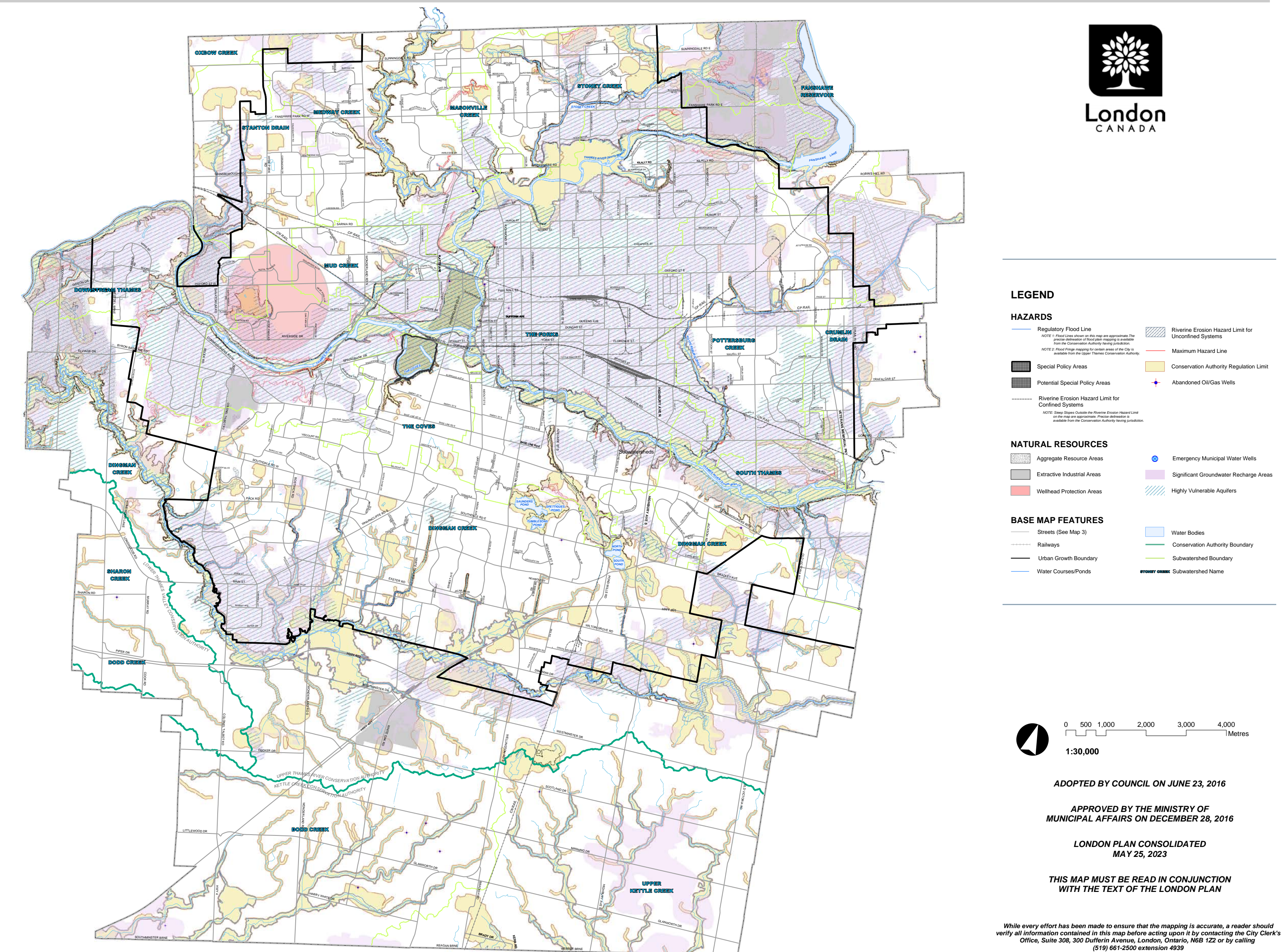
Dingman Creek Subwatershed Stage 2 Schedule C Municipal Class Environmental Assessment and Proposed Official Plan Amendment for Regulatory Floodplain Mapping

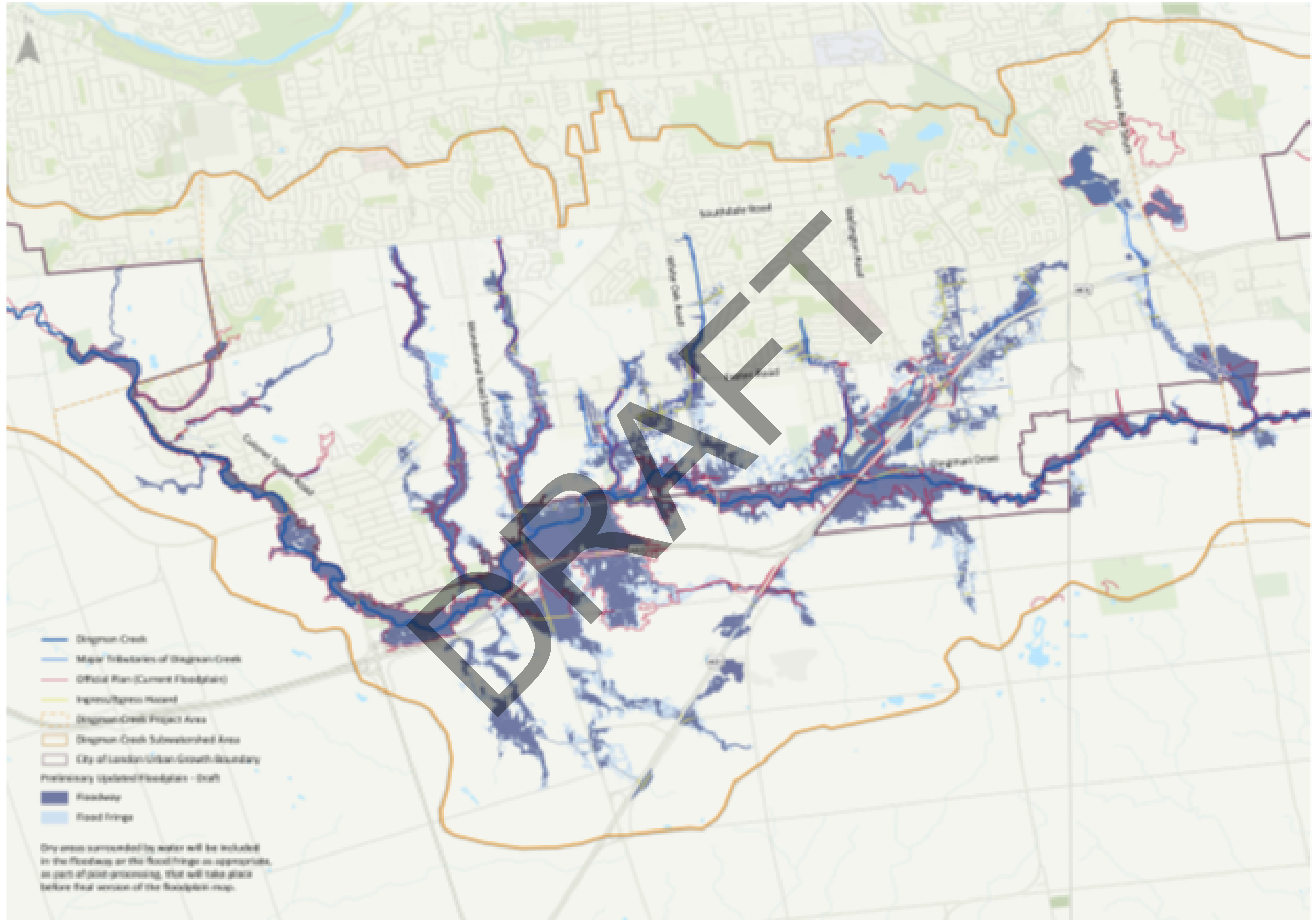


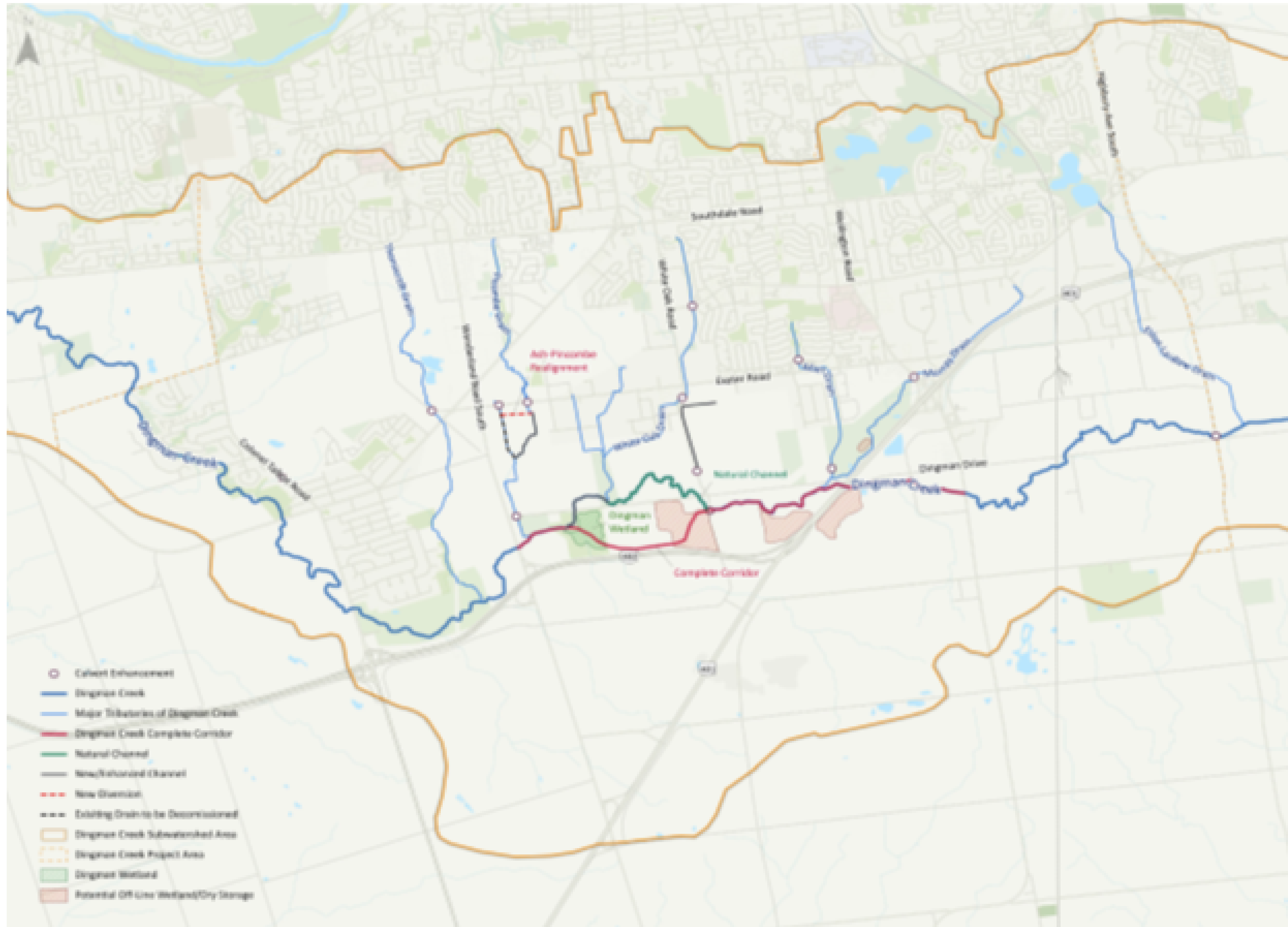
OFFICIAL PLAN AMENDMENT PROCESS

- An Official Plan Amendment (OPA) will update Map 6 (Hazards and Natural Resources) based on updated existing conditions modeling i.e. new floodplain limits
- The OPA may update potential Place Types (land use designations) within the Dingman Creek floodplain
- The OPA may add criteria to permit certain types of development within the “flood fringe” of the Urban Growth Boundary
- As City-led mitigation measures are constructed, OPA’s will be initiated to reflect revised floodplain mapping
- A future public meeting will present a draft Official Plan Amendment including policies on the 2-zone floodplain
- Any policy change will require Council approval and a statutory public meeting

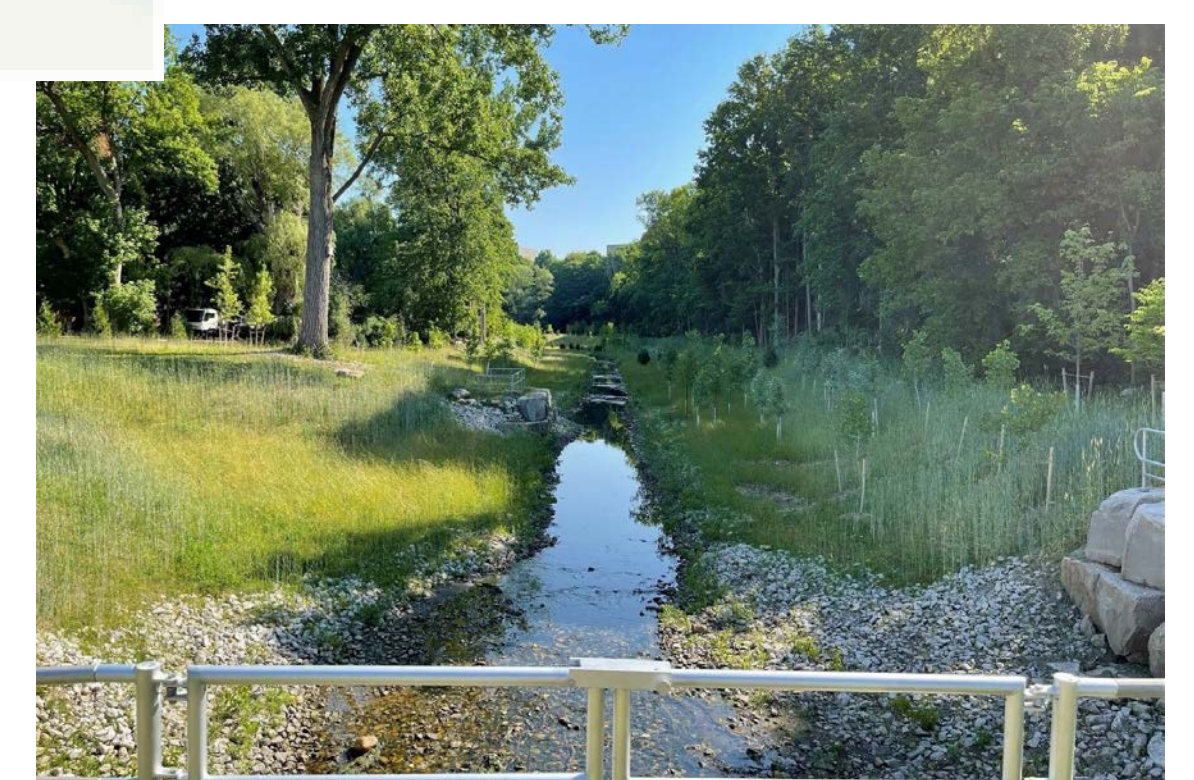
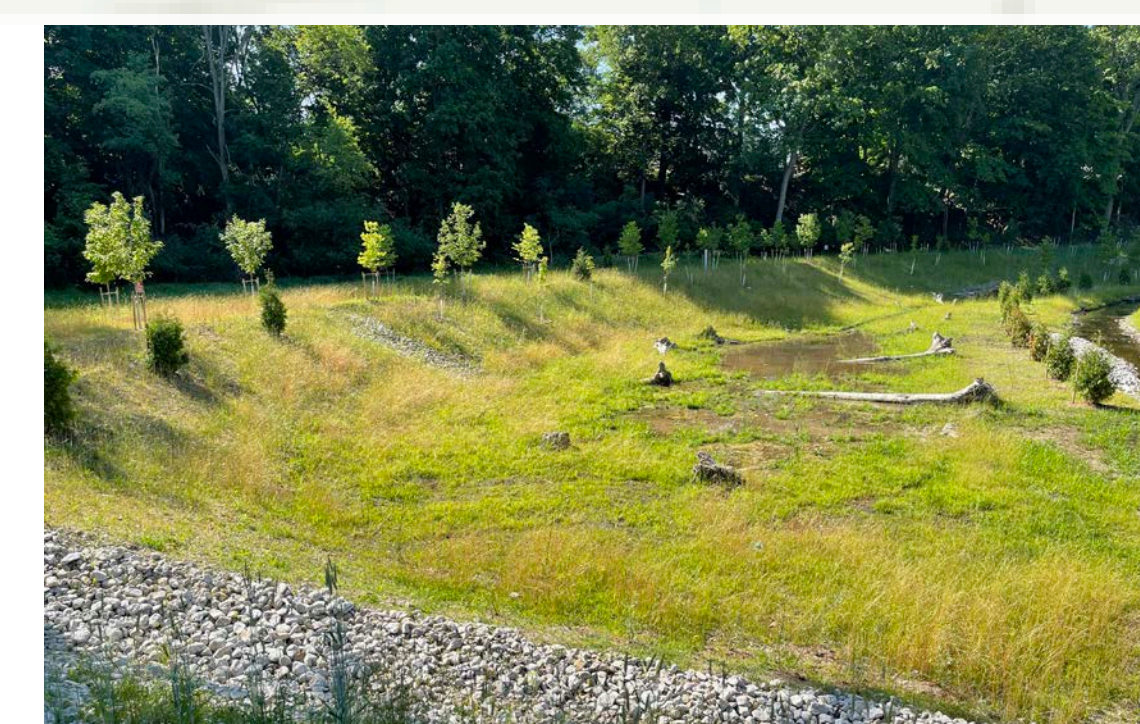
MAP 6 - HAZARDS AND NATURAL RESOURCES







New / Enhanced Channel



Dingman Creek Subwatershed Stage 2 Schedule C Municipal Class Environmental Assessment and Proposed Official Plan Amendment for Regulatory Floodplain Mapping



YOUR FEEDBACK IS IMPORTANT TO US!

<https://getinvolved.london.ca/dingmancreek>

To provide comments, request additional information, or receive future correspondence related to the project, please contact a member of the project team below:

Adrienne Sones, P.Eng.
Environmental Services Engineer
City of London
(519) 661-2489 ext.5593
asones@london.ca

Fuad Curi, P.Eng.
Project Manager
KGS Group
(905) 848-7884 ext.516
fcuri@ksgsgroup.com



Under the Freedom of Information and Protection of Privacy Act and the Environmental Assessment Act, unless otherwise stated in the submission, any personal information such as name, address, telephone number and property location included in a submission will become part of the public record files for this matter and may be released, if requested, to any person.



Natural Channel Design Solutions and Municipal Infrastructure: They Can Co-exist

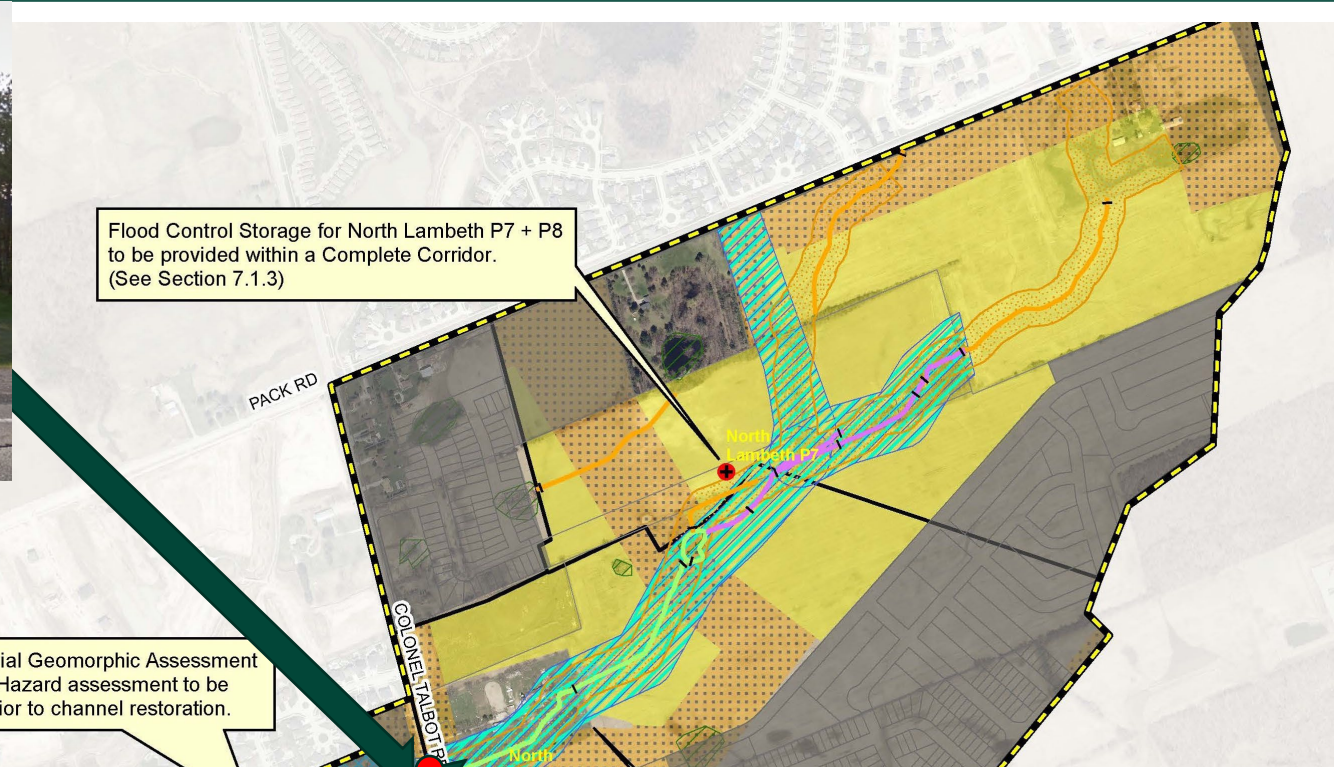


City of London
Environment and Infrastructure
Adrienne Sones, P.Eng.
Environmental Services Engineer
Stormwater Engineering Division



<https://youtu.be/Z3gEz8uU7-8>

Tributary 12/Southwinds Channel



Reach 1 Fluvial Geomorphic Assessment and Erosion Hazard assessment to be completed prior to channel restoration.

Stream Restoration Including Channel Reconstruction and Riparian Revegetation (See Figure 7.6)



Existing and Proposed Flood Conditions



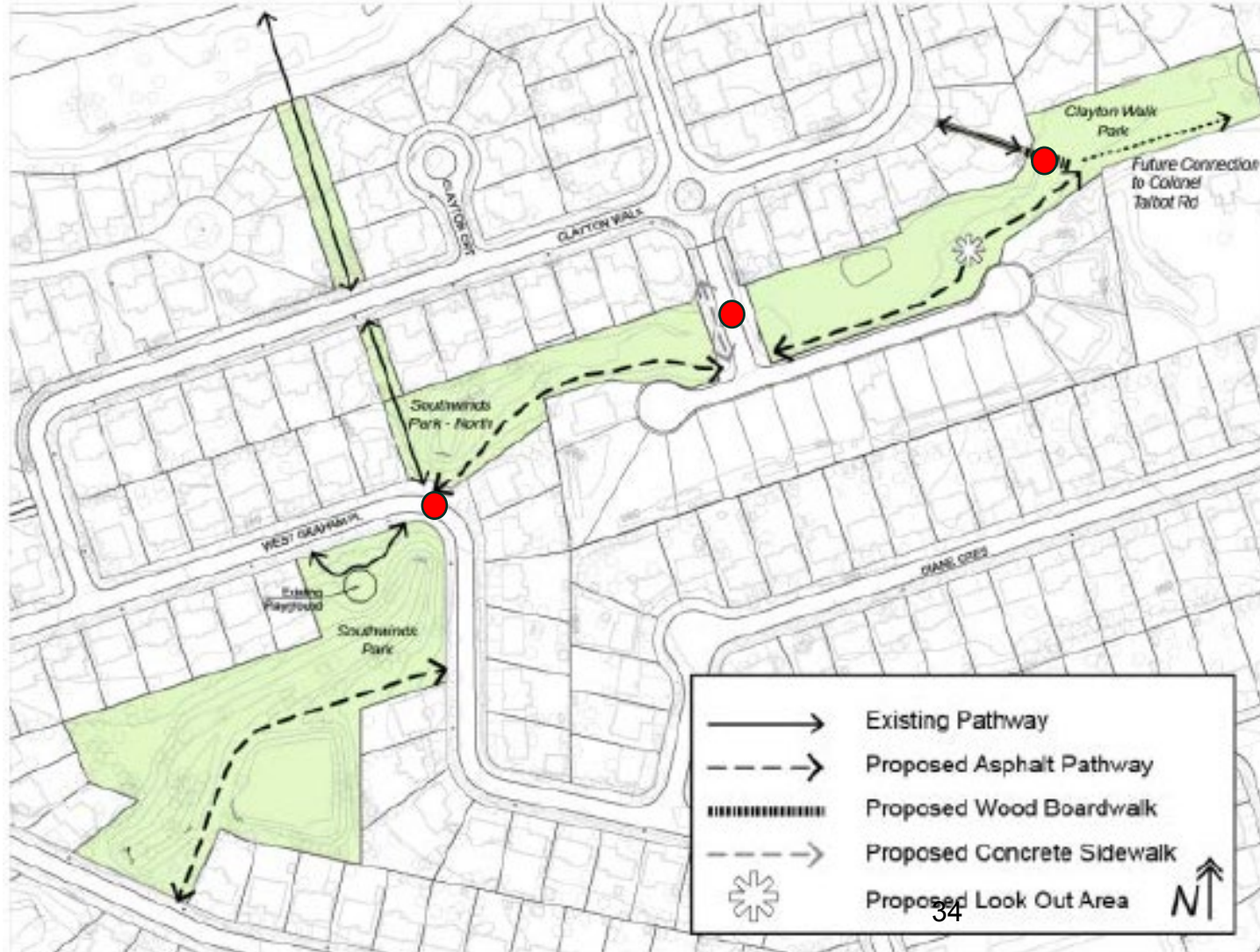


New Culverts

Crossing	Existing Culvert	Proposed Culvert	Utilities	Drop in Invert
Colonel Talbot Road	1.75 m x 1.5 m box	Twin 2.4 m x 1.8 m box (future work by others)	150 mm sanitary pipe 600 mm watermain	0.7 m
Pedestrian Crossing	-	2.1 m x 1.8 m box		-
Isaac Drive	1.8 x 0.9 m box 1.8 x 1.25 m box	2.1 m circular	200 mm sanitary pipe 200 mm watermain	3.7 m
Malpass Road	2.6 x 1.9 m arch	2.7 m x 3.0 m box	200 mm watermain	1.0 m
West Graham Place	2.6 x 1.9 m arch	-	200 mm sanitary pipe.	-



Multiuse Pathway Integration



City of London
Cycling Master Plan
Final Executive Summary | September 2016





Engineered Design Elements

- Side slopes
 - Armourstone wall
 - Rocky slopes
- Plunge pool
- Watermain support
- Pedestrian
 - Crossing
 - Trail



Permits

- Permits:

- UTRCA

- O. Reg 162/06
- Section 28

- MECP

- Environmental Activity Sector
Registration (EASR)

- DFO

- Request for Review
- Letter of Advice

- Construction:

- In-water work: July 1 – March 31
- Migratory birds: Sept. 30 – March 31
- Began: October 2021
- Completed: September 2022



Natural Channel Design Elements

- Instream features

- Riffle logs
- Rootwads
- Pools
- Riffles
- Bifurcated channel

- Corridor features

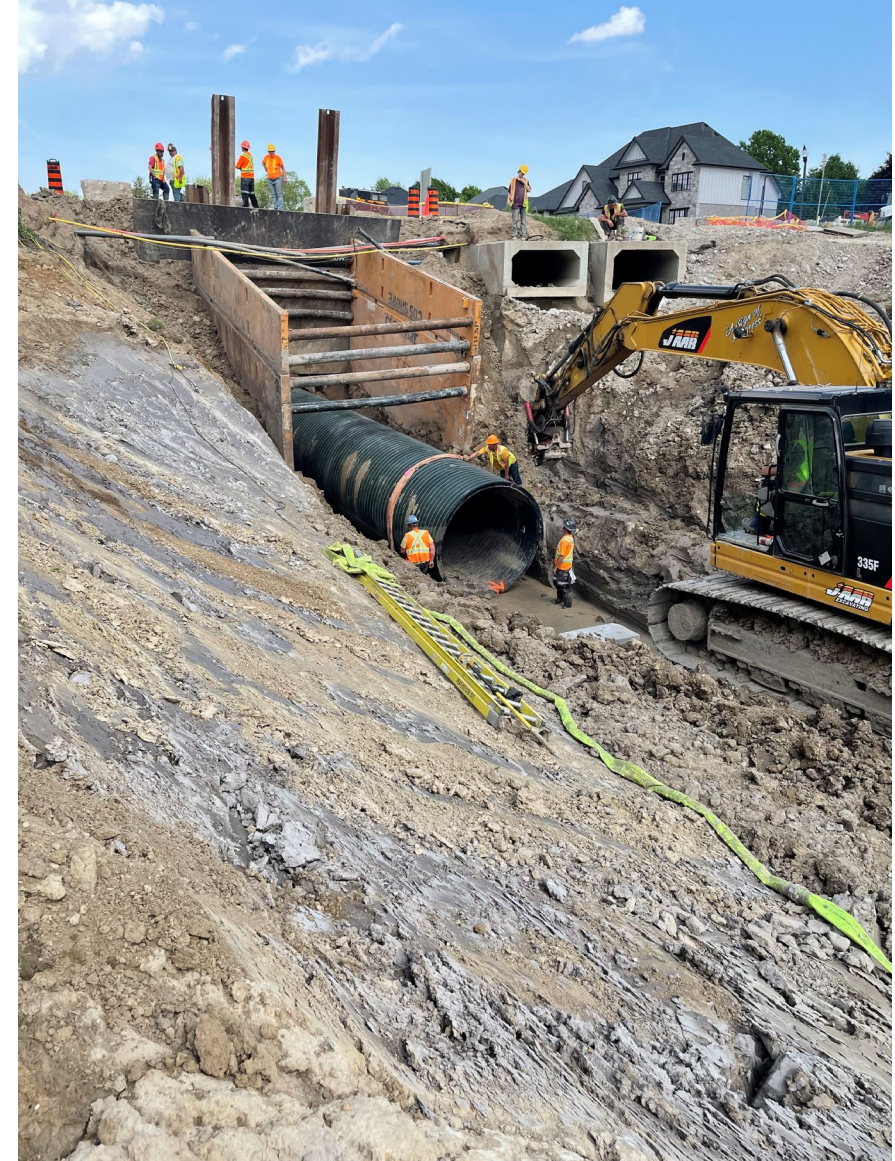
- Pocket wetlands
- Snake
hibernaculum
- Vegetation
- Brush layers
and rootwads





Future Considerations

- Impetus was uncontrolled regulatory flow
 - Leads to oversized channel
 - Consider risk-based approach
 - Consider level of service
 - Cost-benefit
- Environmental impact
 - Climate change resilience
 - Groundwater seepage
 - Groundwater infiltration
 - Narrower creek corridor
 - Downstream impacts





Then and Now

Downstream of
Colonel Talbot Road

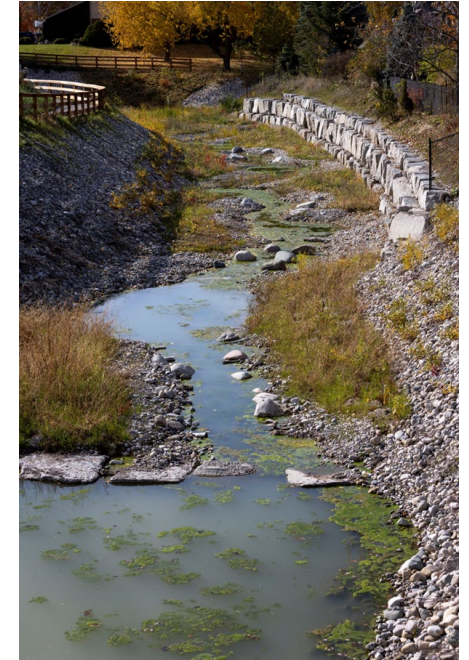


Upstream of
Isaac Drive/
Ped bridge



Then and Now (Cont.)

Upstream of
Malpass Road

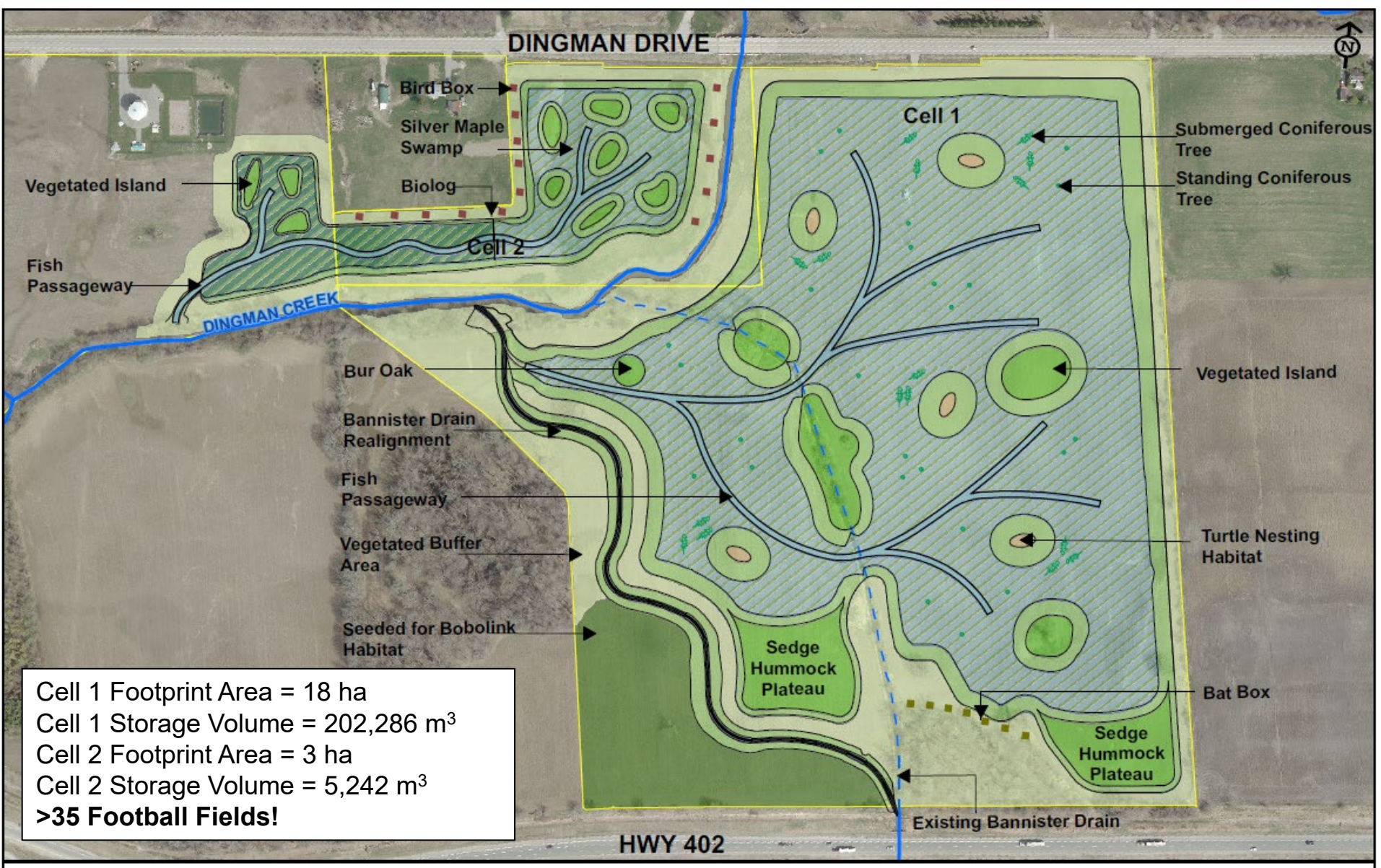


Downstream of
Malpass Road



Thank You





Dingman ECW - Habitat Enhancements



London
CANADA

London's "En Route"



Increasing Biodiversity

- **New nesting site: Blue-Wing Teal Duck** discovered by Environment Canada Biologist summer 2019
- Media release Feb 20, 2020: Four media outlets picked up the story...Blackburn, Global, CTV and CBC
- <https://www.cbc.ca/news/canada/london/dingman-creek-wetland-1.5475134>



“Breeding [of the Blue-Wing Teal Duck] in Ontario is on the decline, and the fact that a successful brood was raised at the Dingman site, speaks to the quality of the habitat in there.”
- *Debby Sadler, Environment Canada Wildlife Biologist*

Report to Planning & Environment Committee

To: Chair and Members
Planning & Environment Committee

From: Peter Kokkoros, P.Eng., B.A. (Econ)
Director Building & Chief Building Official

Subject: Building Division Monthly Report
July 2023

Date: December 4, 2023

Recommendation

That the report dated July 2023 entitled “Building Division Monthly Report July 2023”, **BE RECEIVED** for information.

Executive Summary

The Building Division is responsible for the administration and enforcement of the *Ontario Building Code Act* and the *Ontario Building Code*. Related activities undertaken by the Building Division include the processing of building permit applications and inspections of associated construction work. The Building Division also issues sign and pool fence permits. The purpose of this report is to provide Municipal Council with information related to permit issuance and inspection activities for the month of July 2023.

Linkage to the Corporate Strategic Plan

Growing our Economy

- London is a leader in Ontario for attracting new jobs and investments.

Leading in Public Service

- The City of London is trusted, open, and accountable in service of our community.
- Improve public accountability and transparency in decision making.

Analysis

1.0 Background Information

This report provides information on permit and associated inspection activities for the month of July 2023. Attached as Appendix “A” to this report is a “Summary Listing of Building Construction Activity for the Month of July 2023”, as well as respective “Principle Permits Reports”.

2.0 Discussion and Considerations

2.1 Building permit data and associated inspection activities – **July 2023**

Permits Issued to the end of the month

As of July 2023, a total of 2,139 permits were issued, with a construction value of \$560.9 million, representing 756 new dwelling units. Compared to the same period in 2022, this represents a 15.6% decrease in the number of building permits, with a 34.3% decrease in construction value and an 45.9% decrease in the number of dwelling units constructed.

Total permits to construct New Single and Semi-Dwelling Units

As of the end of July 2023, the number of building permits issued for the construction of single and semi-detached dwellings was 136, representing a 67.8% decrease over the same period in 2022.

Number of Applications in Process

As of the end of July 2023, 790 applications are in process, representing approximately \$840.6 million in construction value and an additional 1,070 dwelling units compared with 1,260 applications, with a construction value of \$1.3 billion and an additional 3,148 dwelling units in the same period in 2022.

Rate of Application Submission

Applications received in July 2023 averaged to 15.8 applications per business day, for a total of 317 applications. Of the applications submitted 26 were for the construction of single detached dwellings and 19 townhouse units.

Permits issued for the month

In July 2023, 317 permits were issued for 66 new dwelling units, totaling a construction value of \$117.2 million.

Inspections – Building

A total of 1,510 inspection requests were received with 1,433 inspections being conducted.

In addition, 33 inspections were completed related to complaints, business licenses, orders and miscellaneous inspections.

Of the 3,384 inspections requested, 98% were conducted within the provincially mandated 48 hour period.

Inspections - Code Compliance

A total of 792 inspection requests were received, with 1,038 inspections being conducted.

An additional 140 inspections were completed relating to complaints, business licences, orders and miscellaneous inspections.

Of the 792 inspections requested, 98% were conducted within the provincially mandated 48 hour period.

Inspections - Plumbing

A total of 750 inspection requests were received with 956 inspections being conducted related to building permit activity.

An additional 7 inspections were completed related to complaints, business licenses, orders and miscellaneous inspections.

Of the 750 inspections requested, 100% were conducted within the provincially mandated 48 hour period.

2018 - 2020 Permit Data

Additional permit data has been provided in Appendix “A” to reflect 2018 – 2020 permit data.

Conclusion

The purpose of this report is to provide Municipal Council with information regarding the building permit issuance and building & plumbing inspection activities for the month of July 2023. Attached as Appendix “A” to this report is a “Summary Listing of Building Construction Activity” for the month of July 2023 as well as “Principle Permits Reports”.

Prepared by: Peter Kokkoros, P.Eng.
Director, Building and Chief Building Official
Planning and Economic Development

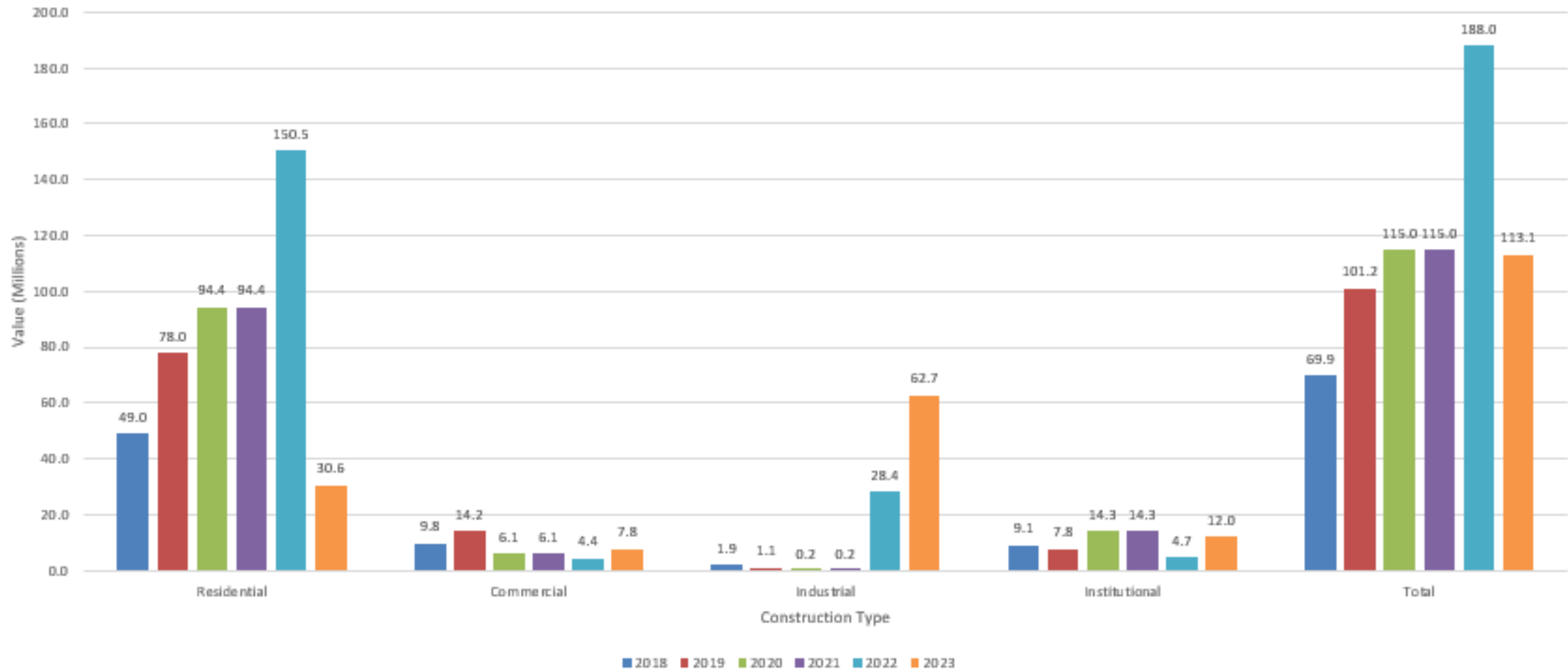
Submitted by: Scott Mathers, MPA, P.Eng.
Deputy City Manager
Planning and Economic Development

Recommended by: Scott Mathers, MPA, P.Eng.
Deputy City Manager
Planning and Economic Development

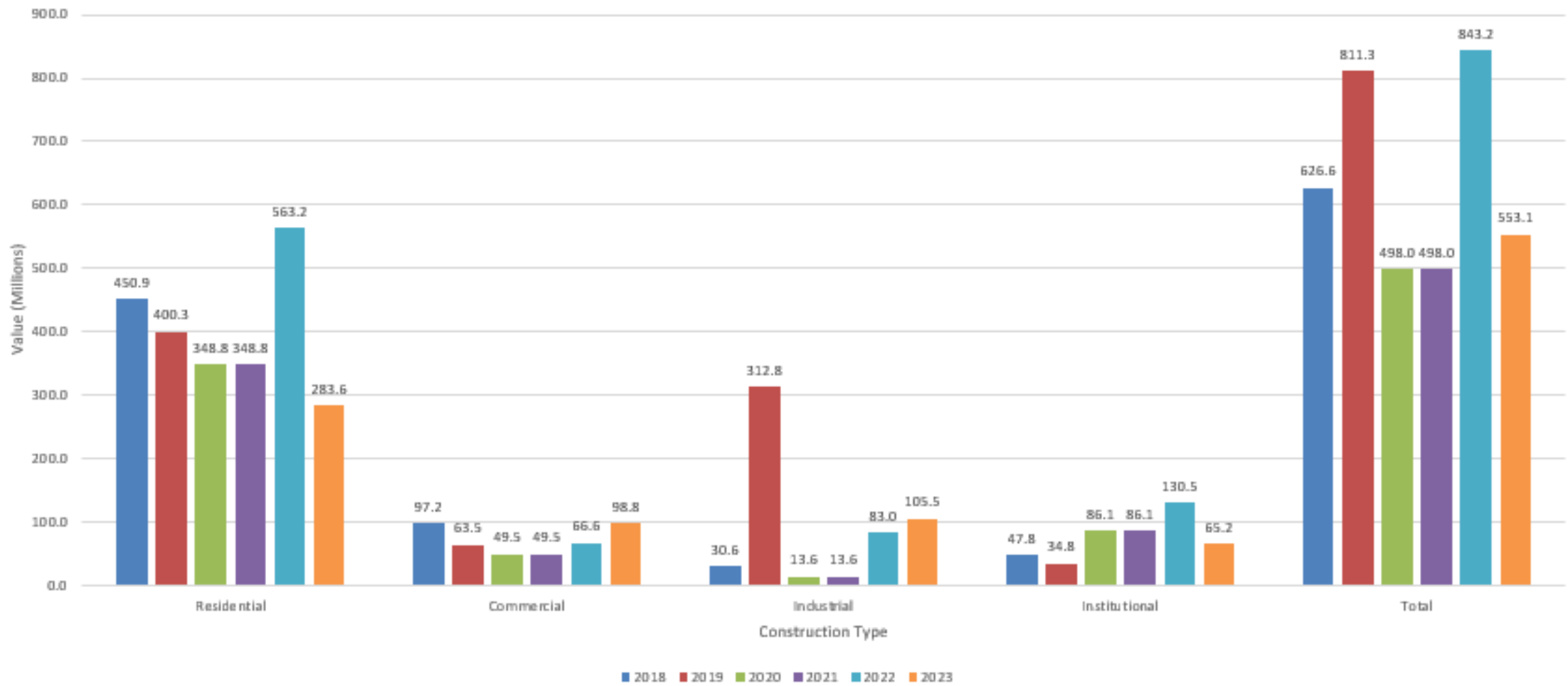
APPENDIX "A"

CITY OF LONDON																		
SUMMARY LISTING OF BUILDING CONSTRUCTION ACTIVITY FOR THE MONTH OF July 2023																		
CLASSIFICATION	July 2023			to the end of July 2023			July 2022			to the end of July 2022			July 2021			to the end of July 2021		
	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS
SINGLE DETACHED DWELLINGS	26	13,693,744	26	134	73,279,407	134	27	14,419,498	27	422	213,222,342	422	105	44,354,200	105	481	203,460,504	481
SEMI DETACHED DWELLINGS	0	0	0	2	1,854,757	3	0	0	0	0	0	0	1	354,000	2	1	354,000	2
TOWNHOUSES	4	6,464,618	19	39	67,661,167	201	11	29,040,506	109	77	104,315,923	366	19	21,875,300	70	73	65,143,485	242
DUPLEX, TRIPLEX, QUAD, APT BLDG	1	1,667,000	6	11	78,915,730	287	2	100,082,082	236	8	185,277,620	500	2	23,000,000	97	6	49,878,800	224
RES-ALTER & ADDITIONS	171	8,824,039	15	1,076	61,917,012	131	164	6,944,623	16	1,125	60,351,826	108	184	4,835,093	3	741	29,993,516	23
COMMERCIAL -ERECT	1	1,300,000	0	6	38,580,370	0	2	1,841,809	0	9	37,908,279	0	1	500,000	0	8	4,660,300	0
COMMERCIAL - ADDITION	2	248,230	0	7	3,011,730	0	2	700,000	0	6	3,641,900	0	0	0	0	2	791,800	0
COMMERCIAL - OTHER	31	6,242,593	0	190	57,166,150	0	12	1,865,466	0	163	25,070,614	0	23	5,625,250	0	206	44,006,135	0
INDUSTRIAL - ERECT	1	45,311,506	0	4	46,321,468	0	1	28,380,578	0	1	29,380,578	0	0	0	0	2	3,436,700	0
INDUSTRIAL - ADDITION	2	16,962,794	0	7	45,228,428	0	0	0	0	7	52,657,871	0	0	0	0	4	7,918,800	0
INDUSTRIAL - OTHER	4	420,000	0	29	13,915,051	0	1	500	0	15	918,700	0	2	165,000	0	23	2,272,607	0
INSTITUTIONAL - ERECT	0	0	0	2	19,710,400	0	0	0	0	3	99,646,230	0	1	250,000	0	3	32,825,000	0
INSTITUTIONAL - ADDITION	3	6,741,993	0	12	10,204,743	0	1	2,190,000	0	2	2,379,000	0	2	8,618,000	0	8	15,178,000	0
INSTITUTIONAL - OTHER	16	5,254,692	0	129	35,292,216	0	25	2,500,431	0	69	28,427,036	0	39	5,383,800	0	98	38,048,001	0
AGRICULTURE	2	3,200,000	0	4	3,455,000	0	0	0	0	3	1,610,000	0	0	0	0	1	100,000	0
SWIMMING POOL FENCES	17	894,716	0	121	4,040,368	0	35	1,385,363	0	234	8,670,891	0	58	1,368,663	0	220	5,087,499	0
ADMINISTRATIVE	8	7,000	0	92	393,000	0	24	42,000	0	93	807,000	0	3	7,000	0	26	95,000	0
DEMOLITION	6	0	3	60	0	37	22	0	7	68	0	44	8	0	5	40	0	28
SIGNS/CANOPY - CITY PROPERTY	1	0	0	4	0	0	1	0	0	11	0	0	0	0	0	2	0	0
SIGNS/CANOPY - PRIVATE PROPERTY	21	0	0	210	0	0	59	0	0	219	0	0	36	0	0	173	0	0
TOTALS	317	117,232,926	66	2,139	560,946,996	756	389	189,392,856	388	2,535	854,285,810	1,396	484	116,336,306	277	2,118	503,250,147	972
CLASSIFICATION	July 2020			to the end of July 2020			July 2019			to the end of July 2019			July 2018			to the end of July 2018		
	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS
SINGLE DETACHED DWELLINGS	105	44,354,200	105	481	203,460,504	481	79	32,294,885	79	385	162,129,076	385	53	21,744,124	53	430	177,743,184	430
SEMI DETACHED DWELLINGS	1	354,000	2	1	354,000	2	0	0	0	0	0	0	0	0	0	0	0	0
TOWNHOUSES	19	21,875,300	70	73	65,143,485	242	5	4,164,030	17	91	77,333,870	364	19	20,163,879	81	145	108,260,389	429
DUPLEX, TRIPLEX, QUAD, APT BLDG	2	23,000,000	97	6	49,878,800	224	2	36,482,700	146	15	127,089,852	588	1	660,800	4	6	125,303,520	538
RES-ALTER & ADDITIONS	184	4,835,093	3	741	29,993,516	23	205	5,101,260	4	1,116	33,725,362	47	191	6,426,080	5	1,092	39,590,479	50
COMMERCIAL -ERECT	1	500,000	0	8	4,660,300	0	4	7,958,070	0	10	15,297,750	0	2	1,939,240	0	21	51,700,603	0
COMMERCIAL - ADDITION	0	0	0	2	791,800	0	4	3,269,500	0	11	8,619,000	0	3	2,760,000	0	12	9,650,718	0
COMMERCIAL - OTHER	23	5,625,250	0	206	44,006,135	0	32	2,961,910	0	318	39,560,080	0	36	5,095,850	0	263	35,896,737	0
INDUSTRIAL - ERECT	0	0	0	2	3,436,700	0	1	800,000	0	6	302,380,000	0	1	950,000	0	3	9,450,000	0
INDUSTRIAL - ADDITION	0	0	0	4	7,918,800	0	0	0	0	5	5,249,000	0	1	480,000	0	5	7,278,000	0
INDUSTRIAL - OTHER	2	165,000	0	23	2,272,607	0	6	347,020	0	48	5,139,320	0	1	491,100	0	37	13,896,068	0
INSTITUTIONAL - ERECT	1	250,000	0	3	32,825,000	0	0	0	0	1	9,816,800	0	0	0	0	0	0	0
INSTITUTIONAL - ADDITION	2	8,618,000	0	8	15,178,000	0	2	3,448,600	0	4	4,901,800	0	1	7,299,600	0	5	14,049,600	0
INSTITUTIONAL - OTHER	39	5,383,800	0	98	38,048,001	0	53	4,334,050	0	124	20,102,960	0	38	1,850,100	0	159	33,748,410	0
AGRICULTURE	0	0	0	1	100,000	0	1	30,000	0	5	15,640,000	0	0	0	0	1	50,000	0
SWIMMING POOL FENCES	58	1,368,663	0	220	5,087,499	0	35	711,753	0	147	3,172,617	0	29	627,441	0	144	3,093,930	0
ADMINISTRATIVE	3	7,000	0	26	95,000	0	23	61,000	0	93	255,000	0	27	119,450	0	104	370,350	0
DEMOLITION	8	0	5	40	0	28	8	0	5	52	0	28	11	0	4	58	0	25
SIGNS/CANOPY - CITY PROPERTY	0	0	0	2	0	0	6	0	0	24	0	0	2	0	0	10	0	0
SIGNS/CANOPY - PRIVATE PROPERTY	36	0	0	173	0	0	36	0	0	323	0	0	26	0	0	266	0	0
TOTALS	484	116,336,306	277	2,118	503,250,147	972	502	101,964,778	246	2,778	830,412,487	1,384	442	70,607,664	143	2,761	630,081,987	1,447

Construction Value of Building Permits (July)



Construction Value of Building Permits (Jan - July)





City of London - Building Division
Principal Permits Issued from July 1, 2023 to July 31, 2023

Owner	Project Location	Proposed Work	No. of Units	Construction Value
FANSHAWE COLLEGE OF APPLIED ARTS & TECHNOLOGY FANSHAWE COLLEGE OF APPLIED ARTS & TECHNOLOGY	1001 Fanshawe College Blvd	(statcan) Erect - Power Plants Group F, Division 3 Low Hazard Industrial CHILLER CENTER and distribution piping. SHELL PERMIT Only – Provide sealed shop drawings for the stair and guards (stair 105); roof metal stairs and guards; access ladder; mezzanine guards to the Building Division for review prior to work in these areas.	0	45,311,506
1423080 Ontario Ltd	105 Falcon St	(statcan) Add (Non-Residential) - Plant for Manufacturing ADDITION TO MANUFACTURING PLANT Shell permit only. To go to full: provide miscellaneous metals shop drawings including guards/handrails design.	0	4,927,236
WESTERN UNIVERSITY WESTERN UNIVERSITY	1151 Richmond St	(statcan) Alter - University Alterations to existing academic space (313 & 315) on Third to create a new Nursing Skills Lab (315) at the Labatt Health Sciences Building.	0	500,000
THAMES VALLEY DISTRICT SCHOOL BOARD THAMES VALLEY DISTRICT SCHOOL BOARD	121 Ashley Cres	(statcan) Alter - Schools Elementary, Kindergarten Elevator modernization	0	130,000
Housing Development Corporation London	122 Base Line Rd W	(statcan) Alter - Apartment Building Altering of existing space	0	750,000
THAMES VALLEY DISTRICT SCHOOL BOARD THAMES VALLEY DISTRICT SCHOOL BOARD	125 Sherwood Forest Sq	(statcan) Alter - Schools Secondary, High, Jr. High Alterations to 178.47sm of the special education area including finishes, lighting, flooring and adjustments to enlarge a special education classroom.	0	400,000
NOBEL REIT GP INC	1285 Hubrey Rd	(statcan) Add (Non-Residential) - Warehousing Shell building addition ***SHELL PERMIT*** Provide shop drawings for misc meta, ladder and skylight.	0	12,035,558
London District Catholic School Board	1300 Oxford St E	(statcan) Alter - Schools Secondary, High, Jr. High Interior Alteration to provide new Cosmetology Classroom. SHELL - Provide sprinkler shop drawing and GRCC.	0	200,000
1319772 ONTARIO INC C/O RE/MAX ADVANTAGE REALTY 1319772 ONTARIO INC C/O RE/MAX ADVANTAGE REALTY	151 Pine Valley Blvd	(statcan) Alter - Dental Offices Alteration to (2) existing suites within the plaza at 151 Pine Valley Blvd.	0	300,000
MIDKING PROPERTIES LIMITED	155 Clarke Rd	(statcan) Alter - Dental Offices INTERIOR ALTERATIONS TO DENTAL OFFICE	0	200,000
MIDKING PROPERTIES LIMITED	155 Clarke Rd	(statcan) Alter - Retail Store Interior & Exterior renovation for an existing Metro store	0	2,200,000



City of London - Building Division
Principal Permits Issued from July 1, 2023 to July 31, 2023

Owner	Project Location	Proposed Work	No. of Units	Construction Value
5005515 ONTARIO LIMITED 5005515 ONTARIO LIMITED	1570 Allen Pl	(statcan) Erect - Apartment Building ERECT NEW 6 UNIT APARTMENT	6	1,667,000
RICHMOND HYLAND CENTRE INC RICHMOND HYLAND CENTRE INC	1701 Richmond St	(statcan) Alter - Offices SHELL PERMIT ONLY - ALTER TO ADD DEMISING WALL ON 2ND FLOOR	0	122,500
HYDE PARK VENTURES INC	1875 Dalmagarry Rd	Install - Townhouse - Cluster SDD INSTALL SITE SERVICES		2,474,028
LUX HOMES INC. LUX HOMES DESIGN & BUILD INC.	1965 Upperpoint Gate T	(statcan) Erect - Townhouse - Condo ERECT NEW TOWNHOUSE BLOCK - BLDG T, 4 UNITS, DPN 107,109,111,113, 2 STOREY, 1 CAR GARAGE, 3 BEDROOMS, UNFINISHED BASEMENT, A/C, UNCOVERED DECK, SB-12 A3, HOT2000, HRV & DWHR REQUIRED *SOILS RETAINER REPORT REQUIRED*	4	1,274,240
THAMES VALLEY DISTRICT SCHOOL BOARD THAMES VALLEY DISTRICT SCHOOL BOARD	215 Wharncliffe Rd N	(statcan) Alter - Schools Elementary, Kindergarten HVAC Upgrades	0	485,000
552702 ONTARIO INC C/O FAYEZ BEAUTY SPA 552702 ONTARIO INC C/O FAYEZ BEAUTY SPA	2224 Wharncliffe Rd S	(statcan) Add (Non-Residential) - Beauty Parlours TO ADD A LOUNGE ROOM ONTO THE SIDE OF THE EXISTING BUILDING Shell Permit Only – Provide sealed shop drawings for the stair and guards/handrails	0	148,230
THAMES VALLEY DISTRICT SCHOOL BOARD THAMES VALLEY DISTRICT SCHOOL BOARD	284 Oxford St W	(statcan) Add (Non-Residential) - Schools Elementary, Kindergarten New Two Storey Classroom Addition - 1867m2 Interior Alterations in Existing Building - Library Learning Commons and Administration Area - 707m2 Shell permit only: 1. Provide stairs, guards and handrails shop drawings for review. 2. Provide sealed sprinkler shop drawings and hydraulic calculations for review. Provide GRCC for Fire Protection. 3. Integrated Testing Coordinator to complete the attached form. (OBC 3.2.10.1.(1)).	0	6,681,993
HL General Partner Inc	300 King St	(statcan) Alter - Hotel Interior alteration to add kitchenette areas to existing guest rooms in hotel on upper 5 floors for extended stay.	0	2,000,000
THAMES VALLEY DISTRICT SCHOOL BOARD THAMES VALLEY DISTRICT SCHOOL BOARD	335 Belfield St	(statcan) Alter - Schools Elementary, Kindergarten INTERIOR ALT. MECHANICAL UPGRADES	0	768,000
LONDON HEALTH SCIENCES CENTRE LONDON HEALTH SCIENCES CENTRE	339 Windermere Rd	(statcan) Alter - University CM - INTERIOR ALTERATIONS TO ROOM C2-113 ***SHELL PERMIT*** Provide sealed sprinkler drawings for review.	0	656,500



City of London - Building Division
Principal Permits Issued from July 1, 2023 to July 31, 2023

Owner	Project Location	Proposed Work	No. of Units	Construction Value
1279059 ONTARIO INC.	3405 Wonderland Rd S	(statcan) Erect - Automobile Repair Garage Complete Building Permit; Footings, Foundation, Slab on Grade, Exterior walls, Interior walls, Finishes, Fixtures, Equipment, Plumbing, Storm, Sprinklers, Electrical, HVAC, Gas.	0	1,300,000
Al-Omrran Mohammed	3429 Silverleaf Chase	Install Inground Pool Fence - Pool Fence Permit - Inground		102,000
CWH LONDON HOLDINGS INC	3446 White Oak Rd	Install - Sanitary or Storm sewer - Warehousing Install sanitary decommission septic and add to storm sewer.		200,000
NABIL TOUMA	346 Ashland Ave	(statcan) Alter - Triplex Change of use from retail to residential two units on the main floor. One residential unit on the top floor existing. ***SHELL PERMIT***shop drawings for the fire shutters required.	2	120,000
Byron Developers International Inc	349 Southdale Rd E C	(statcan) Erect - Townhouse - Condo ERECT NEW 4 UNIT TOWNHOUSE BLOCK, BLDG C, 3 STOREY, DPN 24, 26, 28, 30.	4	1,322,522
Byron Developers International Inc	349 Southdale Rd E D	(statcan) Erect - Townhouse - Condo ERECT NEW 5 UNIT TOWNHOUSE BLOCK, BLDG D, 3 STOREY, DPN 21, 23, 25, 27, 29.	5	1,649,882
VICTORIA PHILLIPS MSCC No. 692	353 Commissioners Rd W	(statcan) Alter - Duplex Repairs to concrete columns in parking structure. Waterproofing expansion joints	0	120,000
LONDON DAIRY FARMS LTD. LONDON DAIRY FARMS LTD.	3700 Old Victoria Rd	(statcan) Erect - Livestock Barn ID - ERECT 80' x 160' BARN ***FOUNDATION PERMIT ONLY*** Provide FRR assembly of North wall, Provide layout and sealed beams/floor joist designed for the storage mezzanine loads (Live Load + 100 psf) reviewed by DesignLogix Engineering Inc to the Building Division for review prior to work in these areas, please see below.sealed stair, stairs guards and mezzanine guards shop drawings to the Building Division for review prior to work in these areas.		1,600,000
CORPORATION OF OF LONDON	531 Ridout St N	Install - Non-Residential Accessory Building Install a structure at RBC Rock the Park on July 10th. The structure will be made up of two 8'W x 20'L x 8'H cladded steel frame structures that join together to create one large 16'W x 20'L x 8'H structure. All elements will be prefabricated at our production facility, shipped to site on a 53' flatbed truck, and placed on site with a crane and crew.		118,000



City of London - Building Division
Principal Permits Issued from July 1, 2023 to July 31, 2023

Owner	Project Location	Proposed Work	No. of Units	Construction Value
Medway Housing Co-Operative Inc	555 Lawson Rd 30	(statcan) Alter - Townhouse - Rental RESTORATION DUE TO FIRE DAMAGE. REPLACE ROOF TRUSSES, REMOVE AND REPLACE REAR EXTERIOR 2ND FLR WALL, REMOVE & REPLACE INSULATION AND VAPOUR BARRER	0	128,000
Medway Housing Co-Operative Inc	555 Lawson Rd 31	(statcan) Alter - Townhouse - Rental RESTORATION DUE TO FIRE DAMAGE. REPLACE ROOF TRUSSES, REMOVE AND REPLACE REAR EXTERIOR 2ND FLR WALL, REMOVE & REPLACE MAIN FLR LOAD BEARING WALL, REPLACE INSULATION AND VAPOUR BARRER	0	128,000
1569543 ONTARIO LIMITED 1569543 ONTARIO LIMITED	655 Wellington Rd	(statcan) Alter - Restaurant <= 30 People CHANGE OF USE FROM A GROUP D TO A GROUP E RESTAURANT	0	230,000
THAMES VALLEY DISTRICT SCHOOL BOARD THAMES VALLEY DISTRICT SCHOOL BOARD	656 Tennent Ave	(statcan) Alter - Schools Secondary, High, Jr. High Replacement of two interior stair assemblies	0	1,000,000
W-3 LAMBETH FARMS INC.	6711 Royal Magnolia Ave C	(statcan) Erect - Street Townhouse - Condo ERECT NEW 6 UNIT TOWNHOUSE BLOCK, BLDG C, 2 STOREY, DPN 6687, 6691, 6695, 6699, 6703, 6707. SOILS REPORT REQUIRED	6	2,217,974
THAMES VALLEY DISTRICT SCHOOL BOARD THAMES VALLEY DISTRICT SCHOOL BOARD	780 Dulaney Dr	(statcan) Alter - Schools Elementary, Kindergarten Interior electrical upgrades to fire alarm and washroom upgrade.	0	277,692
THAMES VALLEY DISTRICT SCHOOL BOARD THAMES VALLEY DISTRICT SCHOOL BOARD	782 Waterloo St	(statcan) Alter - Schools Elementary, Kindergarten Structural work to support electrical service replacement: wall removal and floor infill.	0	311,000
HOMESTEAD LAND HOLDINGS LTD. HOMESTEAD LAND HOLDINGS LTD.	80 Ridout St S	(statcan) Alter - Apartment Building ALTER TO REPAIR PARKING GARAGE AND WATERPROOFING.	0	125,000
LONDON HEALTH SCIENCES CENTRE LONDON HEALTH SCIENCES CENTRE	800 Commissioners Rd E	(statcan) Alter - Hospitals Interior alterations to LHSC VH MDR rooms D1-303 and D1-307.	0	225,000
Wcpt London Inc	817 Consortium Crt	(statcan) Alter - Offices Interior basic renovation to existing office	0	120,000
HOMESTEAD LAND HOLDINGS LTD. HOMESTEAD LAND HOLDINGS LTD.	82 Ridout St S	(statcan) Alter - Apartment Building ALTER TO REPAIR PARKING GARAGE AND WATERPROOFING.	0	125,000
MCDONALD'S RESTAURANTS OF CANADA LIMITED	959 Hamilton Rd	(statcan) Alter - Restaurant INTERIOR ALTERATIONS TO EXISTING MCDONALDS SHELL PERMIT ONLY: 1: Provide commercial cooking hood fire suppression shop drawing.	0	159,500



City of London - Building Division
Principal Permits Issued from July 1, 2023 to July 31, 2023

Owner	Project Location	Proposed Work	No. of Units	Construction Value
Dainty Solutions Limited	974 Dearness Dr	(statcan) Alter - Medical Offices TENANT FIT-UP FOR MEDICAL OFFICES	0	150,000

Total Permits 43 Units 27 Value 94,961,361

** Includes all permits over \$100,000, except for single and semi-detached dwellings.*

1914 Report to Planning & Environment Committee

To: Chair and Members
Planning & Environment Committee

From: Peter Kokkoros, P.Eng., B.A. (Econ)
Director Building & Chief Building Official

Subject: Building Division Monthly Report
August 2023

Date: December 4, 2023

Recommendation

That the report dated August 2023 entitled “Building Division Monthly Report August 2023”, **BE RECEIVED** for information.

Executive Summary

The Building Division is responsible for the administration and enforcement of the *Ontario Building Code Act* and the *Ontario Building Code*. Related activities undertaken by the Building Division include the processing of building permit applications and inspections of associated construction work. The Building Division also issues sign and pool fence permits. The purpose of this report is to provide Municipal Council with information related to permit issuance and inspection activities for the month of August 2023.

Linkage to the Corporate Strategic Plan

Growing our Economy

- London is a leader in Ontario for attracting new jobs and investments.

Leading in Public Service

- The City of London is trusted, open, and accountable in service of our community.
- Improve public accountability and transparency in decision making.

Analysis

1.0 Background Information

This report provides information on permit and associated inspection activities for the month of August 2023. Attached as Appendix “A” to this report is a “Summary Listing of Building Construction Activity for the Month of August 2023”, as well as respective “Principle Permits Reports”.

2.0 Discussion and Considerations

2.1 Building permit data and associated inspection activities – **August 2023**

Permits Issued to the end of the month

As of August 2023, a total of 2,509 permits were issued, with a construction value of \$777.7 million, representing 957 new dwelling units. Compared to the same period in 2022, this represents a 17% decrease in the number of building permits, with a 27.5% decrease in construction value and an 45.2% decrease in the number of dwelling units constructed.

Total permits to construct New Single and Semi-Dwelling Units

As of the end of August 2023, the number of building permits issued for the construction of single and semi-detached dwellings was 166, representing a 65.2% decrease over the same period in 2022.

Number of Applications in Process

As of the end of August 2023, 968 applications are in process, representing approximately \$916.6 million in construction value and an additional 1,167 dwelling units compared with 836 applications, with a construction value of \$820.2 million and an additional 1,119 dwelling units in the same period in 2022.

Rate of Application Submission

Applications received in August 2023 averaged to 16.8 applications per business day, for a total of 370 applications. Of the applications submitted 30 were for the construction of single detached dwellings and 71 townhouse units.

Permits issued for the month

In August 2023, 370 permits were issued for 201 new dwelling units, totaling a construction value of \$216.7 million.

Inspections – Building

A total of 1,914 inspection requests were received with 2,066 inspections being conducted.

In addition, 13 inspections were completed related to complaints, business licenses, orders and miscellaneous inspections.

Of the 1,914 inspections requested, 98% were conducted within the provincially mandated 48 hour period.

Inspections - Code Compliance

A total of 1,052 inspection requests were received, with 1,047 inspections being conducted.

An additional 120 inspections were completed relating to complaints, business licences, orders and miscellaneous inspections.

Of the 1,052 inspections requested, 99% were conducted within the provincially mandated 48 hour period.

Inspections - Plumbing

A total of 971 inspection requests were received with 1,264 inspections being conducted related to building permit activity.

An additional 6 inspections were completed related to complaints, business licenses, orders and miscellaneous inspections.

Of the 971 inspections requested, 100% were conducted within the provincially mandated 48 hour period.

2018 - 2020 Permit Data

Additional permit data has been provided in Appendix “A” to reflect 2018 – 2020 permit data.

Conclusion

The purpose of this report is to provide Municipal Council with information regarding the building permit issuance and building & plumbing inspection activities for the month of August 2023. Attached as Appendix “A” to this report is a “Summary Listing of Building Construction Activity” for the month of August 2023 as well as “Principle Permits Reports”.

Prepared by: Peter Kokkoros, P.Eng.
Director, Building and Chief Building Official
Planning and Economic Development

Submitted by: Scott Mathers, MPA, P.Eng.
Deputy City Manager
Planning and Economic Development

Recommended by: Scott Mathers, MPA, P.Eng.
Deputy City Manager
Planning and Economic Development

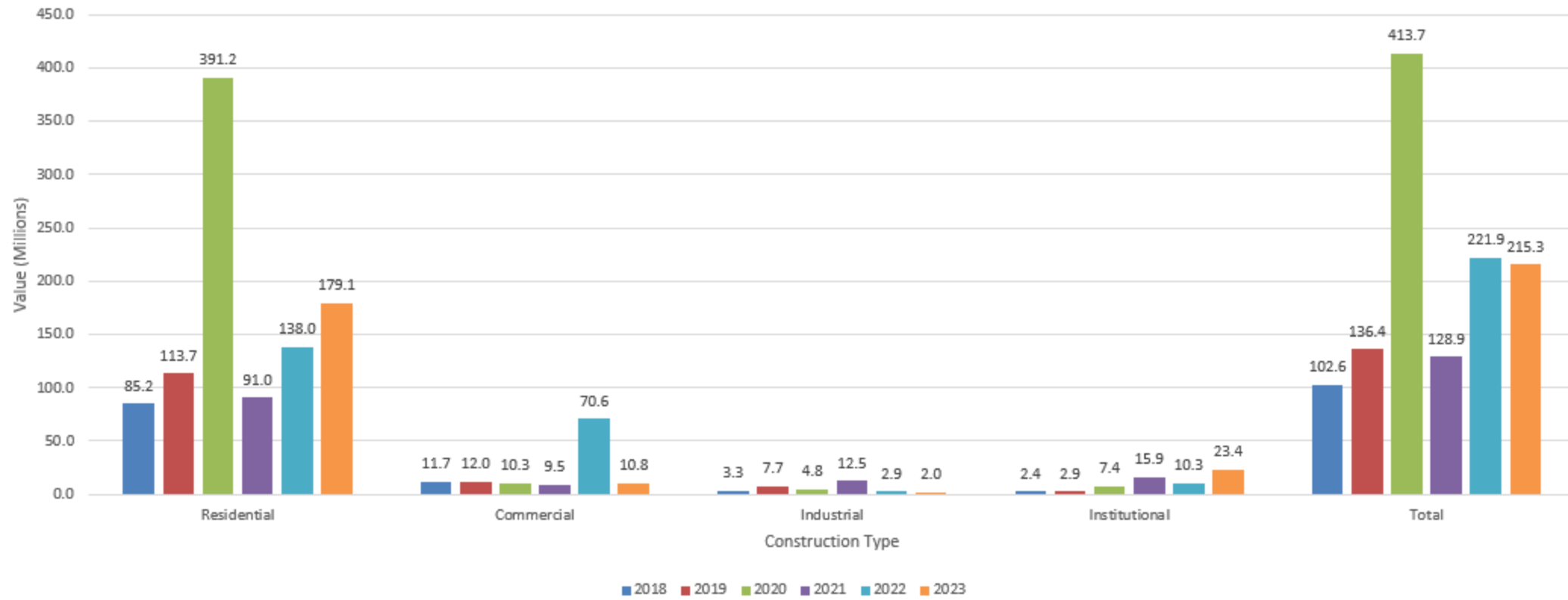
APPENDIX "A"

**CITY OF LONDON
SUMMARY LISTING OF BUILDING CONSTRUCTION ACTIVITY FOR THE MONTH OF August 2023**

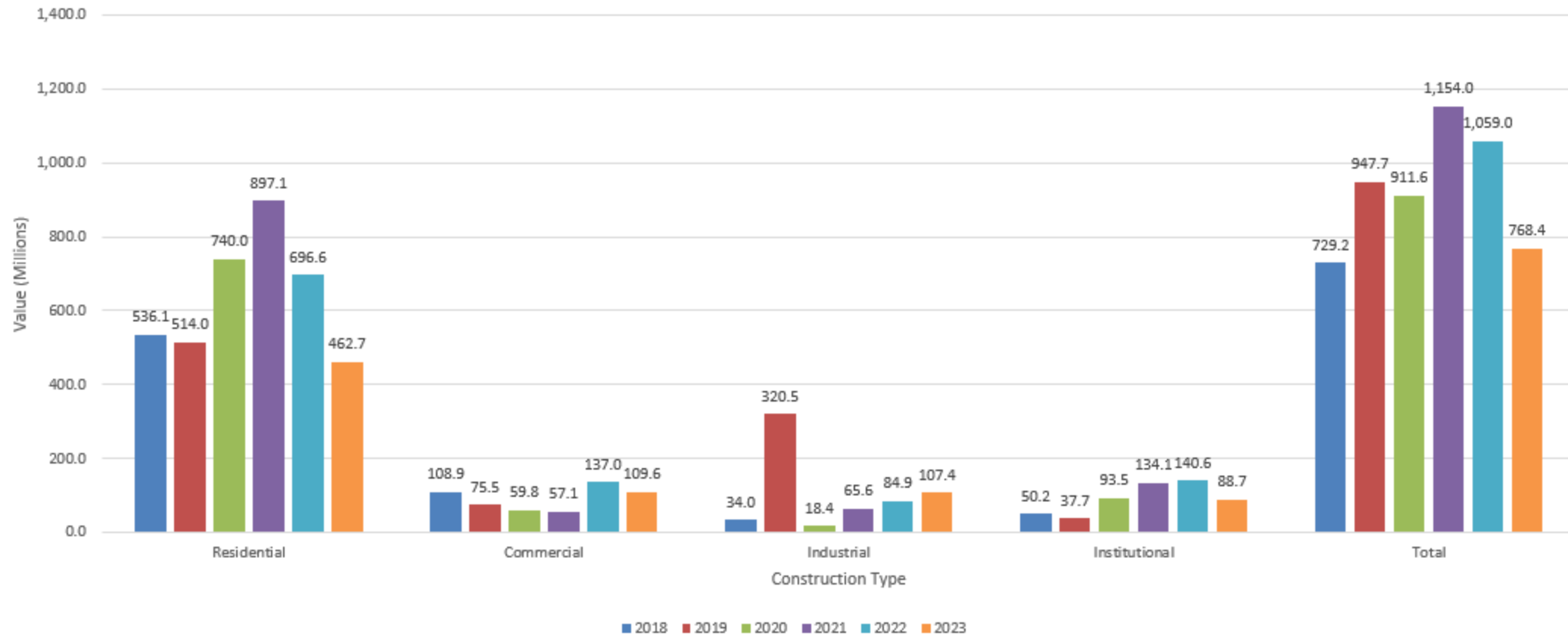
CLASSIFICATION	August 2023			to the end of August 2023			August 2022			to the end of August 2022			August 2021			to the end of August 2021		
	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS
SINGLE DETACHED DWELLINGS	30	16,976,648	30	164	90,256,055	164	58	30,238,847	58	480	243,461,189	480	70	34,137,150	70	780	356,736,550	780
SEMI DETACHED DWELLINGS	0	0	0	2	1,854,757	3	0	0	0	0	0	0	0	0	0	1	223,500	1
TOWNHOUSES	11	22,718,930	71	50	90,380,097	272	21	36,089,701	91	98	140,405,624	457	14	15,291,300	56	162	149,539,000	606
DUPLEX, TRIPLEX, QUAD, APT BLDG	2	49,696,221	74	13	128,611,951	361	4	59,165,143	175	12	244,442,763	675	1	36,000,000	124	11	333,596,000	1,485
RES-ALTER & ADDITIONS	208	89,709,703	25	1,284	151,626,714	156	228	12,535,924	26	1,351	68,291,507	134	165	5,596,716	14	1,249	57,017,596	88
COMMERCIAL - ERECT	0	0	0	6	38,580,370	0	2	1,600,000	0	11	39,508,279	0	0	0	0	21	11,424,400	0
COMMERCIAL - ADDITION	1	5,500,000	0	8	8,511,730	0	3	22,510,000	0	9	26,151,900	0	1	55,000	0	5	455,000	0
COMMERCIAL - OTHER	20	5,323,900	0	210	62,490,050	0	30	46,521,210	0	192	71,311,824	0	34	9,423,200	0	234	45,261,304	0
INDUSTRIAL - ERECT	0	0	0	4	46,321,468	0	1	1,199,606	0	2	29,580,184	0	5	12,349,909	0	12	45,227,409	0
INDUSTRIAL - ADDITION	0	0	0	7	45,228,428	0	0	0	0	7	52,657,871	0	0	0	0	5	3,386,560	0
INDUSTRIAL - OTHER	6	1,973,000	0	35	15,888,051	0	6	1,700,460	0	21	2,619,160	0	3	142,000	0	24	16,961,980	0
INSTITUTIONAL - ERECT	1	14,988,852	0	3	34,699,252	0	0	0	0	3	99,646,230	0	0	0	0	1	12,000,000	0
INSTITUTIONAL - ADDITION	2	2,005,000	0	14	12,209,743	0	0	0	0	2	2,379,000	0	1	660,000	0	6	47,273,386	0
INSTITUTIONAL - OTHER	20	6,451,800	0	149	41,744,016	0	40	10,311,000	0	108	38,547,536	0	15	15,227,800	0	99	74,854,950	0
AGRICULTURE	2	625,000	0	6	4,080,000	0	0	0	0	3	1,610,000	0	1	200,000	0	2	350,000	0
SWIMMING POOL FENCES	15	692,000	0	136	4,732,368	0	38	2,213,518	0	272	10,884,409	0	44	1,416,400	0	307	8,679,140	0
ADMINISTRATIVE	17	39,000	0	109	432,000	0	19	45,000	0	112	852,000	0	5	12,000	0	64	256,000	0
DEMOLITION	11	0	6	71	0	43	3	0	1	71	0	45	11	0	6	57	0	41
SIGNS/CANOPY - CITY PROPERTY	4	0	0	8	0	0	2	0	0	13	0	0	1	0	0	8	0	0
SIGNS/CANOPY - PRIVATE PROPERTY	20	0	0	230	0	0	40	0	0	259	0	0	38	0	0	223	0	0
TOTALS	370	216,700,054	200	2,509	777,647,049	956	495	224,130,409	350	3,026	1,072,349,476	1,746	409	130,511,475	270	3,271	1,163,242,774	3,001

CLASSIFICATION	August 2020			to the end of August 2020			August 2019			to the end of August 2019			August 2018			to the end of August 2018		
	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS
SINGLE DETACHED DWELLINGS	64	28,730,500	64	545	232,191,004	545	61	28,676,520	61	446	190,805,596	446	60	27,262,992	60	490	205,006,176	490
SEMI DETACHED DWELLINGS	0	0	0	1	354,000	2	0	0	0	0	0	0	0	0	0	0	0	0
TOWNHOUSES	7	7,150,200	24	80	72,293,685	266	14	14,780,360	54	105	92,114,230	418	19	14,219,480	61	164	122,479,869	490
DUPLEX, TRIPLEX, QUAD, APT BLDG	4	351,000,000	1,086	10	400,878,800	1,310	2	64,758,000	283	17	191,847,852	871	1	39,299,400	175	7	164,602,920	713
RES-ALTER & ADDITIONS	155	4,302,493	8	896	34,296,009	31	163	5,526,295	6	1,279	39,251,657	53	168	4,372,054	3	1,260	43,982,533	53
COMMERCIAL - ERECT	1	2,500,000	0	9	7,160,300	0	1	3,100,000	0	11	18,397,750	0	3	1,515,300	0	24	53,215,903	0
COMMERCIAL - ADDITION	0	0	0	2	791,800	0	1	8,000	0	12	8,627,000	0	0	0	0	12	9,650,718	0
COMMERCIAL - OTHER	29	7,806,692	0	235	51,812,827	0	46	8,889,330	2	364	48,449,410	2	45	10,186,252	0	308	46,082,989	0
INDUSTRIAL - ERECT	2	4,750,000	0	4	8,186,700	0	2	6,400,000	0	8	308,780,000	0	0	0	0	3	9,450,000	0
INDUSTRIAL - ADDITION	0	0	0	4	7,918,800	0	0	0	0	5	5,249,000	0	0	0	0	5	7,278,000	0
INDUSTRIAL - OTHER	2	5,300	0	25	2,277,907	0	8	1,329,200	0	56	6,468,520	0	5	3,346,736	0	42	17,242,804	0
INSTITUTIONAL - ERECT	0	0	0	3	32,825,000	0	0	0	0	1	9,816,800	0	0	0	0	0	0	0
INSTITUTIONAL - ADDITION	0	0	0	8	15,178,000	0	2	252,000	0	6	5,153,800	0	0	0	0	5	14,049,600	0
INSTITUTIONAL - OTHER	20	7,408,000	0	118	45,456,001	0	20	2,641,000	0	144	22,743,960	0	30	2,396,000	0	189	36,144,410	0
AGRICULTURE	0	0	0	1	100,000	0	0	0	0	5	15,640,000	0	2	160,000	0	3	210,000	0
SWIMMING POOL FENCES	58	1,637,071	0	278	6,724,570	0	24	512,050	0	171	3,684,667	0	31	663,590	0	175	3,757,520	0
ADMINISTRATIVE	8	5,000	0	34	100,000	0	19	52,000	0	112	307,000	0	21	33,750	0	125	404,100	0
DEMOLITION	12	0	11	52	0	39	9	0	7	61	0	35	4	0	1	62	0	26
SIGNS/CANOPY - CITY PROPERTY	0	0	0	2	0	0	2	0	0	26	0	0	0	0	0	10	0	0
SIGNS/CANOPY - PRIVATE PROPERTY	18	0	0	191	0	0	23	0	0	346	0	0	2	0	0	268	0	0
TOTALS	380	415,295,256	1,182	2,498	918,545,403	2,154	397	136,924,755	406	3,175	967,337,242	1,790	391	103,455,554	299	3,152	733,557,542	1,746

Construction Value of Building Permits (August)



Construction Value of Building Permits (Jan - August)





City of London - Building Division

Principal Permits Issued from August 1, 2023 to August 31, 2023

Owner	Project Location	Proposed Work	No. of Units	Construction Value
Wellington London Property Inc	1070 Wellington Rd	(statcan) Alter - Restaurant <= 30 People Interior alteration to existing mercantile group E store to a new pastry restaurant of 30 seats or less including new hood and exhaust equipment's	0	180,000
LONDON CITY LONDON CITY	110 Waterloo St	(statcan) Erect - Garage for Apartment Building Erect parking garage Foundation Permit – The foundation permit includes a ground floor hollow core garage precast slab and a suspended garage slab. Provide sealed precast stairs and landings, stair handrails, grading and parking ramp guards and the roof access shop drawings to the Building Division for review prior to work in these areas.	0	26,839,840
THAMES VALLEY DISTRICT SCHOOL BOARD THAMES VALLEY DISTRICT SCHOOL BOARD	1100 Victoria Dr N/S	(statcan) Alter - Schools Elementary, Kindergarten Structural work to support electrical service replacement: Wall removal and floor infill.	0	230,000
WESTERN UNIVERSITY WESTERN UNIVERSITY	1151 Richmond St	(statcan) Alter - University Upgrading interior facilities for students and faculty. Sprinkler Shop Drawings to be submitted, ITC form to be submitted.	0	1,600,000
THAMES VALLEY DISTRICT SCHOOL BOARD THAMES VALLEY DISTRICT SCHOOL BOARD	121 Ashley Cres	(statcan) Alter - Schools Elementary, Kindergarten REMOVAL OF THE EXISTING THERAPEUTIC POOL AND POOL MECHANICAL ROOM AND THE CONSTRUCTION OF A NEW SKILL CLASSROOM AND A MEETING ROOM .	0	361,000
Skyline Real Estate Holdings Inc	125 South Carriage Rd	(statcan) Erect - Apartment Building ERECT 4 STOREY APARTMENT BUILDING WITH 74 UNITS ***Foundation Permit Only*** BELOW GRADE WORKS ONLY	74	22,856,381
Webster Apts Inc	1270 Webster St	(statcan) Alter - Apartment Building Interior alterations to existing units.	0	500,000
SIFTON LIMITED SIFTON PROPERTIES LIMITED	1566 Ed Ervasti Lane	(statcan) Erect - Townhouse - Cluster SDD ERECT NEW CLUSTER SDD. 1 STOREY, 2 CAR GARAGE, 3 BEDROOM, PARTIALLY FINISHED BASEMENT, COVERED DECK, A/C, HOT2000, LOT 30 PLAN 33M-721, HRV & DWHR REQUIRED. SOILS REPORT REQUIRED. **CANNOT BE ISSUED UNTIL 22-025716 IS REVOKED**	1	428,872



City of London - Building Division
Principal Permits Issued from August 1, 2023 to August 31, 2023

Owner	Project Location	Proposed Work	No. of Units	Construction Value
SIFTON LIMITED SIFTON PROPERTIES LIMITED	1618 Ed Ervasti Lane	(statcan) Erect - Townhouse - Cluster SDD ERECT NEW SDD, 1 STOREY, 2 CAR + GOLF CART GARAGE, 2 BEDROOM, UNFINISHED BASEMENT, COVERED PORCH, A/C, HOT 2000, HRV & DWHR REQUIRED SOILS REPORT REQUIRED	1	414,753
SIFTON LIMITED SIFTON PROPERTIES LIMITED	1631 Ed Ervasti Lane	(statcan) Erect - Townhouse - Cluster SDD ERECT NEW SDD. 2 STOREY, 2 CAR GARAGE + BUGGY PARK, 4 BEDROOM, PARTIAL FINISHED BASEMENT, NO DECK, A/C, SB-12 A3, PART 11 PLAN 33M721, HRV & DWHR REQUIRED, SOILS REPORT REQUIRED	1	525,593
CF REALTY HLDG INC., FAIRVIEW CORP CF REALTY HLDG INC., C/O CADILLAC FAIRVIEW CORP	1680 Richmond St	(statcan) Alter - Retail Store CM - UNIT L081B - INTERIOR ALTER FOR JD SPORTS	0	674,300
2649548 Ontario Inc	1771 Oxford St E	Install - Automobile Repair Garage We will be installing a spray booth into an existing body shop.		300,000
Jysk Linen'N Furniture Inc	1960 Dundas St	(statcan) Alter - Retail Store Adding two new sliding doors, to North and South existing vestibule walls, to become new main entrance.	0	150,000
LUX HOMES DESIGN & BUILD INC.	Gate W	Townhouse - Condo ERECT NEW TOWNHOUSE BLOCK - BLDG W, 4 UNITS DPN 115,117,119,121, 2 STOREY, 1 CAR GARAGE, 3 BEDROOMS, UNFINISHED BASEMENT, A/C, HOT2000, HRV & DWHR REQUIRED SOILS REPORT REQUIRED	4	1,248,357
THAMES VALLEY DISTRICT SCHOOL BOARD THAMES VALLEY DISTRICT SCHOOL BOARD	230 Base Line Rd W	(statcan) Alter - Schools Secondary, High, Jr. High Washroom Upgrades - Fixtures and Finishes Replacements	0	900,000
Parklands Group Inc	2389 Main St	(statcan) Alter - Medical Offices Renovate interior of existing building Phase 1	0	150,000
Bluestone Properties Inc	2525 Sheffield Blvd	Install - Apartment Building Install Site Services		80,000,000
	278 Maurice St	(statcan) Alter - Triplex CONVERTING EXISTING INDOOR POOL AREA TO SECONDARY DWELLING UNIT.	1	138,244
THAMES VALLEY DISTRICT SCHOOL BOARD THAMES VALLEY DISTRICT SCHOOL BOARD	300 Clarke Rd	(statcan) Alter - Schools Secondary, High, Jr. High Interior alterations, updating finishes, lighting, mechanical and electrical. Creation of a new staff washroom and renovations to existing barrier free washroom and shower area.	0	268,400



City of London - Building Division
Principal Permits Issued from August 1, 2023 to August 31, 2023

Owner	Project Location	Proposed Work	No. of Units	Construction Value
LONDON DISTRICT CATHOLIC SCHOOL BOARD	3055 Buroak Dr	(statcan) Erect - Schools Elementary, Kindergarten ERECT 1 STOREY ELEMENTARY SCHOOL ***Foundation permit only*** NO MECHANICAL To go to full: 1. Provide roof access ladder shop drawings. 2. Provide stairs, guards, and handrails shop drawings.	0	14,988,852
WONDERLAND POWER CENTRE INC. WONDERLAND POWER CENTRE INC.	3100 Wonderland Rd S	(statcan) Add (Non-Residential) - Retail store and warehouse Proposed Retail + Warehouse expansion (approx. 1,977 s.m.) total, including interior alterations to finishes and layout for existing space. Includes new universal style washroom and finish upgrades to existing washrooms. Exterior improvements to existing facade for new finishes, online pick-up canopy and reconfigured exterior garden centre outdoor compound. Shell Permit Only – Provide sealed Stairs/Guardrails shop drawings to the Building Division for review prior to work in these areas.	0	5,500,000
Duchess Holdings Inc	319 Ridout St S	(statcan) Alter - Duplex Renovate existing two-storey brick Duplex.		308,537
WONDERLAND POWER CENTRE INC. WONDERLAND POWER CENTRE INC.	3209 Wonderland Rd S	(statcan) Alter - Dental Offices Tenant Improvements to Suit New Dental Office	0	200,000
CENTRAL OPTMETRY	330 Ridout St N	(statcan) Alter - Dental Offices Refitting existing Optometrist Office into a Dental Office	0	300,000
THAMES VALLEY DISTRICT SCHOOL BOARD THAMES VALLEY DISTRICT SCHOOL BOARD	371 Tecumseh Ave E	(statcan) Alter - Schools Secondary, High, Jr. High Interior Alteration to Existing Library/ Classroom ***SHELL PERMIT*** Provide sprinkler shop drawing and GRCC.	0	900,000
CONSELL SCOLAIRE VIAMONDE	40 Hunt Club Dr	(statcan) Alter - Schools Elementary, Kindergarten Alter interior to expand existing childcare space within existing elementary school. SHELL PERMIT ONLY. Provide Integration Testing - required for new smoke dampers connecting to existing FAS.	0	375,000
Four Fourteen Inc	414 Old Wonderland Rd A	(statcan) Erect - Street Townhouse - Condo ERECT NEW TOWNHOUSE BLOCK - BLDG A, 4 UNITS, DPN: 418,420,422,424, 2 STOREY, 1 CAR GARAGE, 3 BEDROOMS, UNFINISHED BASEMENT, NO A/C, UNCOVERED DECK, SB-12 A5, HRV & DWHR REQUIRED. **ENBRIDGE SETBACK REQUIREMENTS SHOULD BE DOUBLE CHECKED PRIOR TO ISSUING**	4	1,184,645



City of London - Building Division
Principal Permits Issued from August 1, 2023 to August 31, 2023

Owner	Project Location	Proposed Work	No. of Units	Construction Value
1301265 ONTARIO LIMITED 1301265 ONTARIO LIMITED	4166 Scotland Dr	(statcan) Erect - Farm Workshop ERECT 10,000 SQ. FT. PRE-ENGINEERED AGRICULTURAL SHED		450,000
Toromont Industries Ltd	50 Enterprise Dr	Install - Sanitary or Storm sewer - Workshop Scope of work to include resurfacing of the outside storage area from Granular to Asphalt. Install new catch basins that will be connected to the existing / onsite storm water system.		550,000
Masonville Square Inc Attn: Rocco Tullio	50 North Centre Rd	(statcan) Alter - Retail Store CM - INTERIOR ALTER FOR GIANT TIGER RETAIL STORE	0	2,770,700
THAMES VALLEY DISTRICT SCHOOL BOARD THAMES VALLEY DISTRICT SCHOOL BOARD	580 Grey St	(statcan) Add (Non-Residential) - Schools Elementary, Kindergarten ADD EXTENSION TO STAIR AREA (REMOVE OLD AND REPLACE WITH NEW). ADD NEW RAMP AND SOME ALTERATION TO INTERIOR ***Shell Permit Only***Provide Stairs shop drawings including handrails/guards design. Provide Handrails and guards shop drawings for exterior ramp. Provide sprinkler shop drawing and GRCC. Provide ITC report.	0	2,000,000
	651 Eagletrace Dr	Install Inground Pool Fence - Pool Fence Permit - Inground		130,000
	6602 White Oak Rd	(statcan) Erect - Storage Depot Erect agricultural building seed shed		175,000
MONTESSORI ACADEMY OF LONDON MONTESSORI ACADEMY OF LONDON	711 Waterloo St	(statcan) Alter - Private School - Elementary INTERIOR ALTERATIONS TO MONTESSORI ACADEMY OF LONDON Shell Permit Only To go to full permit: Provide guards/handrails shop drawings including loads used in the design.	0	650,000
785 Wonderland Road Inc C/O Mccor Management (East) Inc.	785 Wonderland Rd S	(statcan) Alter - Retail Store Interior alteration to create a bubble tea store (Real Fruit) with plumbing (including a washroom) and HVAC modifications.		150,000
CITY LONDON WESTERN FAIR ASSOCIATION	900 King St	(statcan) Alter - Food Processing Plant Creation of common area corridors and exit corridors for previous and future food production tenant spaces in the "Grove" area. Shell permit only. To go to full: provide handrails and guards shop drawings.	0	1,300,000
IRONSTONE COMPANY INC.	925 Deveron Cres A	(statcan) Erect - Townhouse - Condo ERECT NEW 14 UNIT STACKED TOWNHOUSE, BLDG A, 3 STOREY, DPNS 2, 4, 6, 8, 10, 12, 14, 16, 18, 20, 22, 24, 26, 28.	14	4,015,235



City of London - Building Division

Principal Permits Issued from August 1, 2023 to August 31, 2023

Owner	Project Location	Proposed Work	No. of Units	Construction Value
IRONSTONE COMPANY INC.	925 Deveron Cres B	(statcan) Erect - Townhouse - Condo ERECT NEW TOWNHOUSE BLOCK B, 10 UNITS, UNITS 30/32, 34/36, 38/40, 42/44, 46/48, 3 STOREYS, NO CAR GARAGE, 6 BEDROOMS, NO BASEMENT, NO A/C, UNCOVERED DECK, SB-12 A1, HRV & DWHR REQUIRED	10	3,595,164
IRONSTONE BUILDING COMPANY INC.	925 Deveron Cres C	(statcan) Erect - Townhouse - Condo ERECT NEW 10 UNIT STACKED TOWNHOUSE, BLDG C, 3 STOREY, DPNS 50, 52, 54, 56, 58, 60, 62, 64, 66, AND 68.	10	2,911,444
THAMES VALLEY DISTRICT SCHOOL BOARD THAMES VALLEY DISTRICT SCHOOL BOARD	927 Osgoode Dr	Install - Site Services Parking Lot and playground improvements. Works include replacement of existing storm sewer on the south side of the school along with asphalt rehabilitation of the parking lot area and playground area.	0	710,000
THAMES VALLEY DISTRICT SCHOOL BOARD THAMES VALLEY DISTRICT SCHOOL BOARD	951 Leathorne St	(statcan) Alter - Offices INTERIOR ALTERATIONS TO SECOND FLOOR OFFICES AND MEETING ROOMS.	0	110,000
DREWLO HOLDINGS INC DREWLO HOLDINGS INC	990 Deveron Cres A	(statcan) Erect - Street Townhouse - Condo ERECT NEW TOWNHOUSE BLOCK - BLDG A, 10 UNITS DPN 1,3,5,7,9,11,15,17,19,21, 3 STOREYS, NO GARAGE, 3 BEDROOMS, NO BASEMENT, NO A/C, COVERED PORCH & DECK, SB-12 A1, HRV & DWHR REQUIRED ***** REQUIRES SITE PLAN / PLANNING & DEVELOPMENT REVIEW***** *SOILS RETAINER REPORT REQUIRED*	10	3,231,393
DREWLO HOLDINGS INC DREWLO HOLDINGS INC	990 Deveron Cres B	(statcan) Erect - Townhouse - Condo ERECT NEW 8 UNIT STACKED TOWNHOUSE CONDO BLOCK B, 4 STOREY, NO GARAGE, 3 BED, SB12 A1, DWHR AND HRV REQUIRED. DPNS 23, 25, 27, 29, 31, 33, 35, 37	8	2,851,889



City of London - Building Division
Principal Permits Issued from August 1, 2023 to August 31, 2023

Owner	Project Location	Proposed Work	No. of Units	Construction Value
IRONSTONE COMPANY INC.	990 Deveron Cres C	(statcan) Erect - Townhouse - Condo ERECT NEW 8 UNIT STACKED TOWNHOUSE, BUILDING C, 3 STOREY, DPN's 39, 41, 43, 45, 47, 49, 51 & 53.	8	2,311,585

Total Permits 44 Units 147 Value 189,435,184

** Includes all permits over \$100,000, except for single and semi-detached dwellings.*

Report to Planning & Environment Committee

To: Chair and Members
Planning & Environment Committee

From: Peter Kokkoros, P.Eng., B.A. (Econ)
Director Building & Chief Building Official

Subject: Building Division Monthly Report
September 2023

Date: December 4, 2023

Recommendation

That the report dated September 2023 entitled “Building Division Monthly Report September 2023”, **BE RECEIVED** for information.

Executive Summary

The Building Division is responsible for the administration and enforcement of the *Ontario Building Code Act* and the *Ontario Building Code*. Related activities undertaken by the Building Division include the processing of building permit applications and inspections of associated construction work. The Building Division also issues sign and pool fence permits. The purpose of this report is to provide Municipal Council with information related to permit issuance and inspection activities for the month of September 2023.

Linkage to the Corporate Strategic Plan

Growing our Economy

- London is a leader in Ontario for attracting new jobs and investments.

Leading in Public Service

- The City of London is trusted, open, and accountable in service of our community.
- Improve public accountability and transparency in decision making.

Analysis

1.0 Background Information

This report provides information on permit and associated inspection activities for the month of September 2023. Attached as Appendix “A” to this report is a “Summary Listing of Building Construction Activity for the Month of September 2023”, as well as respective “Principle Permits Reports”.

2.0 Discussion and Considerations

2.1 Building permit data and associated inspection activities – **September 2023**

Permits Issued to the end of the month

As of September 2023, a total of 2,811 permits were issued, with a construction value of \$899.8 million, representing 1,131 new dwelling units. Compared to the same period in 2022, this represents a 17.1% decrease in the number of building permits, with a 22% decrease in construction value and an 39.9% decrease in the number of dwelling units constructed.

Total permits to construct New Single and Semi-Dwelling Units

As of the end of September 2023, the number of building permits issued for the construction of single and semi-detached dwellings was 185, representing a 64.4% decrease over the same period in 2022.

Number of Applications in Process

As of the end of September 2023, 891 applications are in process, representing approximately \$891.1 million in construction value and an additional 1,132 dwelling units compared with 970 applications, with a construction value of \$855.8 million and an additional 1,175 dwelling units in the same period in 2022.

Rate of Application Submission

Applications received in September 2023 averaged to 15.1 applications per business day, for a total of 302 applications. Of the applications submitted 16 were for the construction of single detached dwellings and 6 townhouse units.

Permits issued for the month

In September 2023, 302 permits were issued for 176 new dwelling units, totaling a construction value of \$122.1 million.

Inspections – Building

A total of 1,783 inspection requests were received with 2,001 inspections being conducted.

In addition, 24 inspections were completed related to complaints, business licenses, orders and miscellaneous inspections.

Of the 1,783 inspections requested, 100% were conducted within the provincially mandated 48 hour period.

Inspections - Code Compliance

A total of 858 inspection requests were received, with 998 inspections being conducted.

An additional 131 inspections were completed relating to complaints, business licences, orders and miscellaneous inspections.

Of the 858 inspections requested, 100% were conducted within the provincially mandated 48 hour period.

Inspections - Plumbing

A total of 881 inspection requests were received with 1,162 inspections being conducted related to building permit activity.

An additional 6 inspections were completed related to complaints, business licenses, orders and miscellaneous inspections.

Of the 881 inspections requested, 100% were conducted within the provincially mandated 48 hour period.

2018 - 2020 Permit Data

Additional permit data has been provided in Appendix “A” to reflect 2018 – 2020 permit data.

Conclusion

The purpose of this report is to provide Municipal Council with information regarding the building permit issuance and building & plumbing inspection activities for the month of September 2023. Attached as Appendix “A” to this report is a “Summary Listing of Building Construction Activity” for the month of September 2023 as well as “Principle Permits Reports”.

Prepared by: Peter Kokkoros, P.Eng.
Director, Building and Chief Building Official
Planning and Economic Development

Submitted by: Scott Mathers, MPA, P.Eng.
Deputy City Manager
Planning and Economic Development

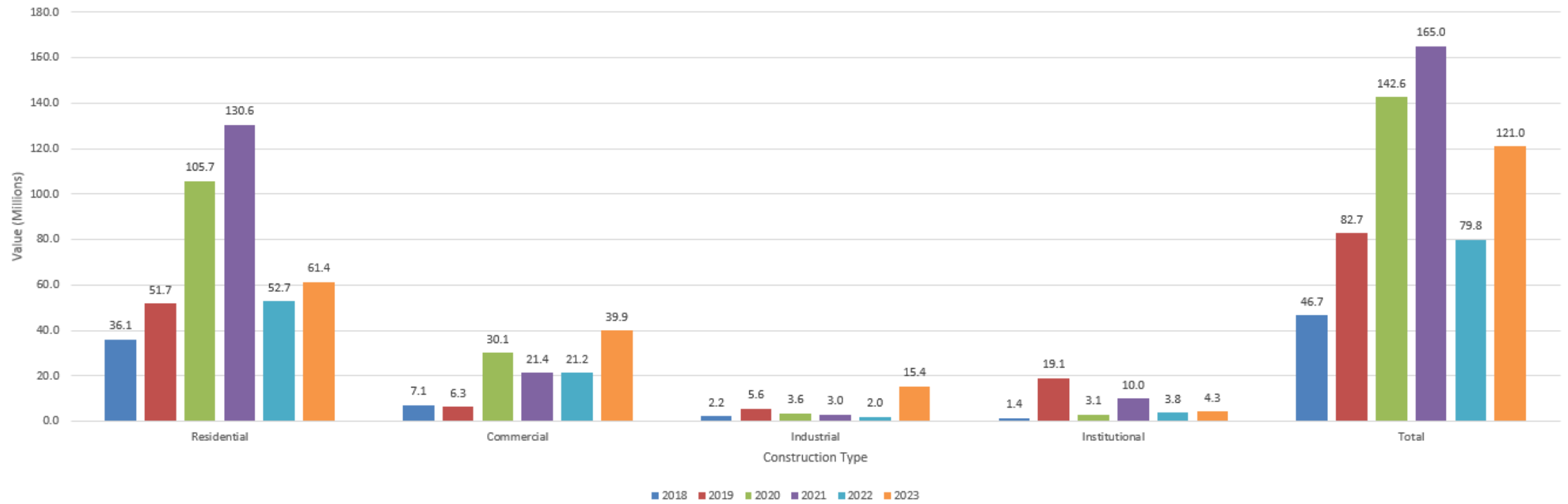
Recommended by: Scott Mathers, MPA, P.Eng.
Deputy City Manager
Planning and Economic Development

APPENDIX "A"

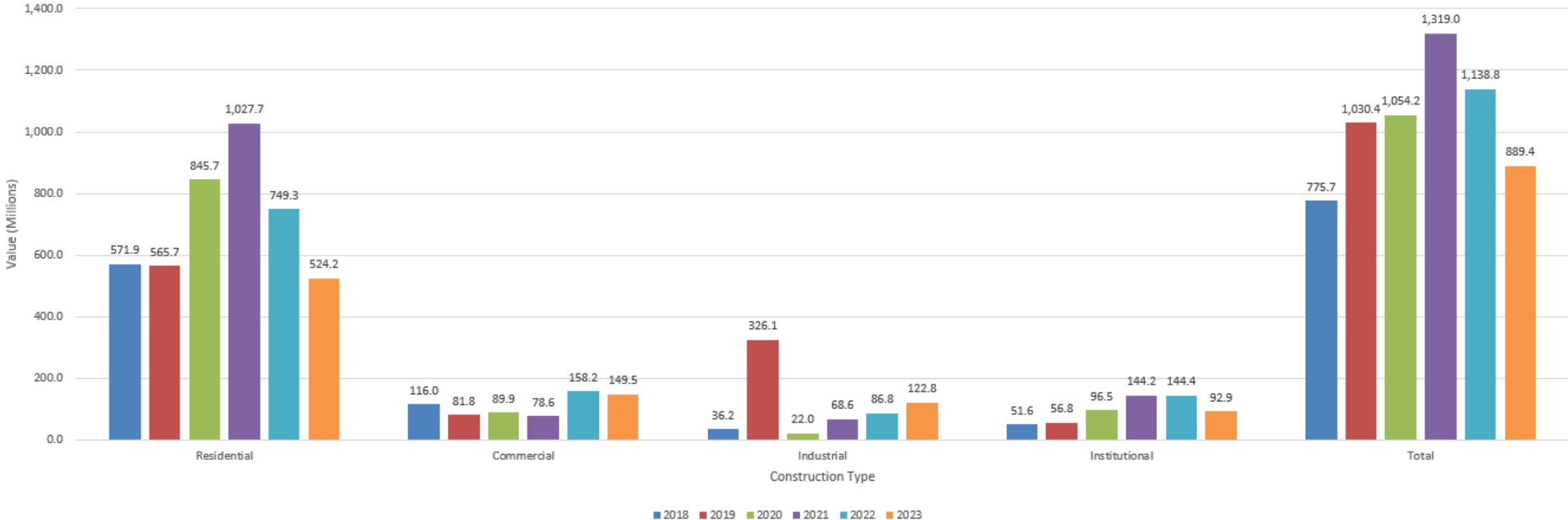
**CITY OF LONDON
SUMMARY LISTING OF BUILDING CONSTRUCTION ACTIVITY FOR THE MONTH OF September 2023**

CLASSIFICATION	September 2023			to the end of September 2023			September 2022			to the end of September 2022			September 2021			to the end of September 2021		
	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS
SINGLE DETACHED DWELLINGS	16	7,960,751	16	180	98,216,806	180	39	19,655,657	39	519	263,116,846	519	63	26,251,000	63	843	382,987,550	843
SEMI DETACHED DWELLINGS	3	2,326,269	6	5	4,181,026	9	0	0	0	0	0	0	0	0	0	1	223,500	1
TOWNHOUSES	2	2,078,814	6	52	92,458,911	278	4	2,634,380	7	102	143,040,004	464	17	20,110,900	69	179	169,649,900	675
DUPLEX, TRIPLEX, QUAD, APT BLDG	4	37,637,493	119	17	166,249,445	480	1	16,317,000	70	13	260,759,763	745	3	77,775,500	266	14	411,371,500	1,751
RES-ALTER & ADDITIONS	161	11,430,297	28	1,445	163,057,011	184	183	14,113,495	20	1,534	82,405,002	154	174	6,437,830	13	1,423	63,455,427	101
COMMERCIAL - ERECT	0	0	0	6	38,580,370	0	2	15,601,140	1	13	55,109,419	1	1	95,000	0	22	11,519,400	0
COMMERCIAL - ADDITION	1	50,000	0	9	8,561,730	0	1	850,000	0	10	27,001,900	0	1	3,171,500	0	6	3,626,500	0
COMMERCIAL - OTHER	30	39,879,710	0	240	102,369,760	0	27	4,788,370	0	219	76,100,194	0	33	18,158,011	0	267	63,419,315	0
INDUSTRIAL - ERECT	1	5,070,400	0	5	51,391,868	0	0	0	0	2	29,580,184	0	0	0	0	12	45,227,409	0
INDUSTRIAL - ADDITION	1	9,000,000	0	8	54,228,428	0	0	0	0	7	52,657,871	0	1	3,000,000	0	6	6,386,560	0
INDUSTRIAL - OTHER	4	1,314,000	0	39	17,202,051	0	5	1,959,000	0	26	4,578,160	0	0	0	0	24	16,961,980	0
INSTITUTIONAL - ERECT	0	0	0	3	34,699,252	0	0	0	0	3	99,646,231	0	0	0	0	1	12,000,000	0
INSTITUTIONAL - ADDITION	0	0	0	14	12,209,743	0	0	0	0	2	2,397,000	0	0	0	0	6	47,273,386	0
INSTITUTIONAL - OTHER	5	4,295,000	0	154	46,039,016	0	29	3,841,300	0	137	42,388,836	0	9	10,042,500	0	108	84,897,450	0
AGRICULTURE	1	72,600	0	7	4,152,600	0	0	0	0	3	1,610,000	0	0	0	0	2	350,000	0
SWIMMING POOL FENCES	17	957,000	0	153	5,689,368	0	25	1,022,500	0	297	11,906,909	0	30	1,037,018	0	337	9,716,158	0
ADMINISTRATIVE	12	49,000	0	121	481,000	0	9	23,000	0	121	875,000	0	18	34,000	0	82	290,000	0
DEMOLITION	6	0	3	77	0	46	11	0	10	82	0	55	3	0	0	60	0	41
SIGNS/CANOPY - CITY PROPERTY	2	0	0	10	0	0	2	0	0	15	0	0	0	0	0	8	0	0
SIGNS/CANOPY - PRIVATE PROPERTY	36	0	0	266	0	0	27	0	0	286	0	0	44	0	0	267	0	0
TOTALS	302	122,121,334	175	2,811	899,768,384	1,131	365	80,805,842	137	3,391	1,153,173,319	1,883	397	166,113,259	411	3,668	1,329,356,034	3,412
CLASSIFICATION	September 2020			to the end of September 2020			September 2019			to the end of September 2019			September 2018			to the end of September 2018		
	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS
SINGLE DETACHED DWELLINGS	112	46,697,350	112	658	278,888,354	658	62	24,838,835	62	508	215,644,431	508	44	19,413,669	44	533	224,219,845	533
SEMI DETACHED DWELLINGS	1	669,000	2	2	1,023,000	4	3	884,400	6	3	884,400	6	0	0	0	0	0	0
TOWNHOUSES	17	21,226,500	75	97	93,520,185	341	20	20,297,380	98	125	112,411,610	516	12	11,865,000	58	176	134,344,869	548
DUPLEX, TRIPLEX, QUAD, APT BLDG	2	30,000,000	104	12	430,878,800	1,414	1	639,000	4	18	192,486,852	875	0	0	0	7	164,602,920	713
RES-ALTER & ADDITIONS	191	7,118,000	9	1,087	41,414,009	40	146	5,036,160	3	1,425	44,287,817	56	176	4,786,404	9	1,436	48,768,937	62
COMMERCIAL - ERECT	0	0	0	9	7,160,300	0	3	2,264,330	0	14	20,662,080	0	3	2,231,200	0	27	55,447,103	0
COMMERCIAL - ADDITION	1	5,000	0	3	796,800	0	3	927,000	0	15	9,554,000	0	0	0	0	12	9,650,718	0
COMMERCIAL - OTHER	44	30,126,500	0	279	81,939,327	0	33	3,111,000	0	397	51,560,410	2	49	4,837,650	0	357	50,920,639	0
INDUSTRIAL - ERECT	1	100,000	0	5	8,286,700	0	3	3,986,000	0	11	312,766,000	0	0	0	0	3	9,450,000	0
INDUSTRIAL - ADDITION	0	0	0	4	7,918,800	0	3	1,064,100	0	8	6,313,100	0	1	32,000	0	6	7,310,000	0
INDUSTRIAL - OTHER	7	3,536,500	0	32	5,814,407	0	5	578,800	0	61	7,047,320	0	3	2,173,000	0	45	19,415,804	0
INSTITUTIONAL - ERECT	0	0	0	3	32,825,000	0	1	17,640,000	0	2	27,456,800	0	0	0	0	0	0	0
INSTITUTIONAL - ADDITION	0	0	0	8	15,178,000	0	1	180,000	0	7	5,333,800	0	0	0	0	5	14,049,600	0
INSTITUTIONAL - OTHER	14	3,083,000	0	132	48,539,001	0	7	1,245,000	0	151	23,988,960	0	12	1,382,282	0	201	37,526,692	0
AGRICULTURE	0	0	0	1	100,000	0	1	60,000	0	6	15,700,000	0	0	0	0	3	210,000	0
SWIMMING POOL FENCES	39	1,042,321	0	317	7,766,891	0	22	454,600	0	193	4,139,267	0	21	400,452	0	196	4,157,972	0
ADMINISTRATIVE	7	6,000	0	41	106,000	0	12	13,000	0	124	320,000	0	30	30,250	0	155	434,350	0
DEMOLITION	5	0	4	57	0	43	10	0	4	71	0	39	9	0	8	71	0	34
SIGNS/CANOPY - CITY PROPERTY	0	0	0	2	0	0	1	0	0	27	0	0	1	0	0	11	0	0
SIGNS/CANOPY - PRIVATE PROPERTY	37	0	0	228	0	0	51	0	0	397	0	0	3	0	0	271	0	0
TOTALS	478	143,610,171	302	2,977	1,062,155,574	2,457	388	83,219,605	173	3,563	1,050,556,847	1,963	364	47,151,907	111	3,515	780,509,449	1,856

Construction Value of Building Permits (Septemeber)



Construction Value of Building Permits (Jan - Septemeber)





City of London - Building Division

Principal Permits Issued from September 1, 2023 to September 30, 2023

Owner	Project Location	Proposed Work	No. of Units	Construction Value
FANSHAWE COLLEGE OF APPLIED ARTS & TECHNOLOGY FANSHAWE COLLEGE OF APPLIED ARTS & TECHNOLOGY	1001 Fanshawe College Blvd	(statcan) Alter - Community College Removal and replacement of existing heat pumps and light fixtures on Level 1 of Building "E" including emergency lighting. Minor electrical and mechanical upgrades to suit new heat pumps and light fixtures.	0	1,600,000
BORIS REZVAN 1972242 ONTARIO LIMITED	1100 Commissioners Rd E	(statcan) Alter - Retail Store UNIT 7 - INTERIOR ALTERATION FOR GROCERY STORE	0	148,000
REMBRANDT MEADOWLILLY INC REMBRANDT MEADOWLILLY INC	1224 Blackwell Blvd I	(statcan) Erect - Townhouse - Condo ERECT NEW CONDO STREET TOWNHOUSE BLOCK I, 2 STOREY 1 CAR, 3 BED, FINISHED BASEMENT, W/ DECK, W/ A/C, ENERGY STAR, HRV AND DWHR. REQUIRED - DPN 21, 23, 25, 27, 29	5	1,435,915
Peoplecare Inc	1242 Oakcrossing Rd	(statcan) Alter - Nursing Homes Add heating/cooling hydronic fan coils to long term care resident rooms. Primary source of heating/cooling is from rooftop air to glycol heat pumps pumped into existing hydronic heating system. Backup heating is provided by existing natural gas heating boiler system which will remain.	0	2,500,000
Calloway Reit (London N) Inc Canadian Property Holdings	1280 Fanshawe Park Rd W	(statcan) Alter - Retail Store Remove existing roof mounted refrigeration equipment and replace with new, Replace RTU's as needed to suit renovation, Replace exterior vestibule sliding doors with new, New exterior entrance for Medical Clinic & Fitness Centre, Replace lighting as needed, Replace exit and emergency lighting throughout, Raise overhead doors from 8' to 9' at receiving area, Raise all crash doors from 7' to 9', Full renovation to existing washrooms, Demise current space to add 2 separate suites, 1 for medical clinic and 1 for fitness centre ***Shell permit only*** To go to full permit: 1.Submit guardrail shop drawings at top of retaining wall. 2.Submit Pre-fabricated canopy shop drawings. (See section 4 on S2.3) 3.Provide steel studs shop drawings including connection at the following location: 3.1.Parapets 3.2.Exterior walls 3.3.Exterior cornice 3.4.Vestibule walls	0	8,100,000



City of London - Building Division

Principal Permits Issued from September 1, 2023 to September 30, 2023

Owner	Project Location	Proposed Work	No. of Units	Construction Value
	1280 King St	(statcan) Alter - Four-Plex ALTER TO REPAIR TO EXISTING 4PLEX DUE TO FIRE DAMAGE, REPLACINF PLUMBING, FIRE SEPARATIONS AND (2) FLOOR JOISTS *****TO BE REBUILT AS PER EXISTING*****	0	120,000
OXBURY CENTRE (C/O WESTDELL DEVELOPMENT CORPORATION)	1299 Oxford St E	(statcan) Alter - Retail Store Interior alteration -Tenant Fit-Up	0	232,410
PERPETUALLY INNOVATIVE DEVELOPMENTS INC	1328 Bush Hill Link	(statcan) Erect - Duplex ERECT NEW SFD WITH ADDITIONAL DWELLING UNIT IN BASEMENT, 2 STOREY, 2 CAR GARAGE, FINISHED BASEMENT, 5 BEDROOMS, NO DECK, NO A/C, SB12 A1,LOT16 33M-819 , HRV AND DWHR REQUIRED	2	439,037
2560334 ONTARIO INC 2560334 ONTARIO INC	135 Villagewalk Blvd	Install - Sanitary or Storm sewer - Site Services Install services only for commercial plaza		281,000
1803494 ONTARIO INC	142 Wharncliffe Rd N	(statcan) Alter - Triplex	0	300,000
London City	1484 Wilton Grove Rd	(statcan) Alter - Municipal Buildings REPAIR EXISTING SALT DOME	0	130,000
1600 Hp Inc	1600 Hyde Park Rd	(statcan) Alter - Dental Offices CHANGE OF USE FROM GROUPS C AND E TO D FOR A TENANT FIT UP OF DENTAL OFFICES Shell Permit Only - Provide the Building Division with sealed stair and stair guard shop drawings, including design loads and connection between structural members, for review before starting work in these areas. **NO OCCUPANCY PERMIT ISSUED FOR BASE BUILDING 20-023709**	0	151,000
5005515 ONTARIO LIMITED 5005515 ONTARIO LIMITED	1619 Haig St	(statcan) Erect - Duplex ERECT NEW SFD W/ BASEMENT ADDITIONAL DWELLING UNIT, 1 STOREY, 1 CAR, 4 BED, FINISHED BASEMENT, NO DECK, W/ A/C, SB12 A5, INFILL LOT, HRV AND DWHR REQUIRED, SOILS REQUIRED	2	452,628
HABITAT FOR HUMANITY HEARTLAND ONTARIO HABITAT FOR HUMANITY HEARTLAND ONTARIO	1697 Highbury Ave N A	Install - Sanitary or Storm sewer - Townhouse - Condo Install standalone site servicing		550,000
RICHMOND HYLAND CENTRE INC RICHMOND HYLAND CENTRE INC	1701 Richmond St	(statcan) Alter - Offices ALTER INTERIOR FOR GROUP D TENANT FIT-UP, 2ND FLOOR SUITE 2B (UNIT 3) *** DEMISING WALL ON SEPARATE PERMIT - 23-011616 UNDER REVIEW ***	0	128,500



City of London - Building Division

Principal Permits Issued from September 1, 2023 to September 30, 2023

Owner	Project Location	Proposed Work	No. of Units	Construction Value
THE RIDGE AT BYRON INC. THE RIDGE AT BYRON INC.	1710 Ironwood Rd 9	(statcan) Erect - Townhouse - Cluster SDD ERECT CLUSTER SDD, 1 STOREY, 2 CAR GARAGE, 3 BEDROOM, FINISHED BASEMENT, REAR DECK, A/C, SB-12 A5, MVLCP 903 Level 1 Unit 5, HRV & DWHR REQUIRED, SOILS REPORT REQUIRED MODEL: THE BIRMINGHAM	1	642,899
Takawy Properties Ltd	1828 Blue Heron Dr	(statcan) Alter - Recreation Centre Tenant fit-up interior alteration of indoor golf	0	192,500
DANCOR OXFORD INC DANCOR PHASE II OXFORD INC	2146 Oxford St E	(statcan) Alter - Plant for Manufacturing The interior fit-up for the existing shell building for the main office in the North-East corner and a warehouse/shipping office. SHELL - No occupancy until the main building completed.	0	1,000,000
Bruder Realty Ltd	2323 Scanlan St	(statcan) Erect - Plant for Manufacturing ERECY 1 STOREY MANUFACTURING FACILITY (PHASE 1)	0	5,070,400
LONDON & MIDDLESEX COMMUNITY HOUSING LONDON & MIDDLESEX COMMUNITY HOUSING INC	241 Simcoe St	(statcan) Alter - Apartment Building Unit 211,311,411 and 511 limited tearout and repairs for smoke and water damages due to fire, interior finishes, fire separation on party walls, exterior walls and ceilings. Unit 611,711,811,911,1011 full gut and repairs for smoke and water damages due to fire, interior finishes, fire separation on party walls, exterior walls and ceilings. *****TO BE REBUILT AS PER EXISTING*****	0	500,000
MOHAMED ABDULREHMAN CARDINAL HOMES INC.	2500 Main St	Install - Townhouse - Condo INSTALL SITE SERVICES		3,700,000
THE CANADA LIFE ASSURANCE COMPANY	255 Dufferin Ave	(statcan) Alter - Retail Store Interior alteration to exist. Group E Retail outlet within a Group D Office Building. No change in Use, no structural work req'd. New equip., finishes and millwork.	0	170,000
WONDERLAND POWER CENTRE INC. WONDERLAND POWER CENTRE INC.	3120 Wonderland Rd S Maxwell, Chantal (cmaxwell@london.ca) is signed in	(statcan) Alter - Retail Store Interior Alteration for IKEA design	0	375,000
FIRST LONDON EAST DEVELOPMENTS INC	320 Clarke Rd	(statcan) Alter - Restaurant INTERIOR ALTERATIONS TO STACKED PANCAKE & BREAKFAST HOUSE	0	297,800
G Belisle Holdings (London) Limited	4231 Blakie Rd	Install - Water service/main - Contractors office and warehouse Connect new water main to City of London's take off. The current building is fed from a well. Now that the City of London has extended the watermain in the street, Tornado Insulation would like to use this water instead of the well .		225,000



City of London - Building Division

Principal Permits Issued from September 1, 2023 to September 30, 2023

Owner	Project Location	Proposed Work	No. of Units	Construction Value
Dr. Michael J Dasilva Dentistry Professional Corp	4342 Colonel Talbot Rd	(statcan) Alter - Dental Offices RENOVATION TO DENTAL OFFICE BUILDING	0	400,000
Wcpt London Inc	649 Consortium Crt	(statcan) Alter - Industrial Laboratory Interior alteration inside an existing unit, creating a waste room, food lab and a corridor.	0	150,000
GERALD SLEMKO BARVEST REALTY INC	758 Baransway Dr	(statcan) Add (Non-Residential) - Plant for Manufacturing Addition 2,883 sq.m. to the existing Trudell Healthcare facility. SHELL PERMIT - Provide Integrated Testing Plan and Coordinator. Provide sealed shop drawings final version for retaining walls guardrails (at gridlines 2 and 7) and access ladder to the Building Division for review prior to work in these areas.	0	9,000,000
780 WONDERLAND HOLDINGS LIMITED	780 Wonderland Rd S	(statcan) Alter - Apartment Building This is a high rise multi unit building - Stairwell exterior walls are to be replaced due to structural deficiencies noted by engineer. new walls will be built and new steel cladding installed	0	200,000
TALU PROPERTIES INC. TALU PROPERTIES INC.	820 Chelton Rd	(statcan) Erect - Apartment Building Erect 6 storey apartment building	62	17,567,658
CHRIS LEIGH 89 RIDOUT SOUTH LTD.	89 Ridout St S	(statcan) Alter - Apartment Building Balcony Repair.	0	250,000
CITY LONDON WESTERN FAIR ASSOCIATION	900 King St	(statcan) Alter - Amusement Games Establishment Major Renovation to the building interior London Gateway Casino. Shell permit only. No site services. Provide sprinkler shop drawing and GRCC, fire alarm shop drawing, integrated testing plan and Coordinator, commercial cooking hood suppression shop drawing. Provide interior/exterior stair and guards/handrails shop drawings.	0	28,000,000
CITY LONDON WESTERN FAIR ASSOCIATION	900 King St	(statcan) Alter - Food Processing Plant Construction of Greenhouse within existing Grove space at Western Fair. Structure is meant to be used as a demonstration piece for marketing of future similar products. SHELL permit. Provide sprinkler shop drawing and GRCC.	0	150,000
ROSS OF LONDON HOLDINGS INC	91 Enterprise Dr	(statcan) Alter - Automobile Repair Garage ALTERATIONS FOR FOUNTAIN TIRE Shell permit only - Provide sealed stair guards/handrails, access ladder and stairs shop drawings to the Building Division for review prior to work in these areas.	0	578,000
LONDON & MIDDLESEX COMMUNITY HOUSING LONDON & MIDDLESEX COMMUNITY HOUSING INC	931 Southdale Rd E	(statcan) Erect - Apartment Building ERECT 6 STOREY APARTMENT BUILDING ***FOUNDATION PERMIT***	53	19,178,171



City of London - Building Division

Principal Permits Issued from September 1, 2023 to September 30, 2023

Owner	Project Location	Proposed Work	No. of Units	Construction Value
	9345 Elviage Dr	Install Inground Pool Fence - Pool Fence Permit - Inground		140,000
96-98 BESSEMER HOLDINGS LIMITED	96 Bessemer Crt	(statcan) Alter - Office Complex (Retail/Office) Structural Repair to exterior masonry wall (+ asphalt work which does not require permit)	0	200,000

Total Permits 37 Units 125 Value 104,555,918

** Includes all permits over \$100,000, except for single and semi-detached dwellings.*

Report to Planning & Environment Committee

To: Chair and Members
Planning & Environment Committee

From: Peter Kokkoros, P.Eng., B.A. (Econ)
Director Building & Chief Building Official

Subject: Building Division Monthly Report
October 2023

Date: December 4, 2023

Recommendation

That the report dated October 2023 entitled “Building Division Monthly Report October 2023”, **BE RECEIVED** for information.

Executive Summary

The Building Division is responsible for the administration and enforcement of the *Ontario Building Code Act* and the *Ontario Building Code*. Related activities undertaken by the Building Division include the processing of building permit applications and inspections of associated construction work. The Building Division also issues sign and pool fence permits. The purpose of this report is to provide Municipal Council with information related to permit issuance and inspection activities for the month of October 2023.

Linkage to the Corporate Strategic Plan

Growing our Economy

- London is a leader in Ontario for attracting new jobs and investments.

Leading in Public Service

- The City of London is trusted, open, and accountable in service of our community.
- Improve public accountability and transparency in decision making.

Analysis

1.0 Background Information

This report provides information on permit and associated inspection activities for the month of October 2023. Attached as Appendix “A” to this report is a “Summary Listing of Building Construction Activity for the Month of October 2023”, as well as respective “Principle Permits Reports”.

2.0 Discussion and Considerations

2.1 Building permit data and associated inspection activities – **October 2023**

Permits Issued to the end of the month

As of October 2023, a total of 3,092 permits were issued, with a construction value of \$1.04 billion, representing 1,244 new dwelling units. Compared to the same period in 2022, this represents a 15.3% decrease in the number of building permits, with a 23.1% decrease in construction value and an 43.4% decrease in the number of dwelling units constructed.

Total permits to construct New Single and Semi-Dwelling Units

As of the end of October 2023, the number of building permits issued for the construction of single and semi-detached dwellings was 204, representing a 63.2% decrease over the same period in 2022.

Number of Applications in Process

As of the end of October 2023, 973 applications are in process, representing approximately \$774.3 million in construction value and an additional 1,083 dwelling units compared with 1,469 applications, with a construction value of \$754 million and an additional 1,469 dwelling units in the same period in 2022.

Rate of Application Submission

Applications received in October 2023 averaged to 13.5 applications per business day, for a total of 283 applications. Of the applications submitted 19 were for the construction of single detached dwellings and 13 townhouse units.

Permits issued for the month

In October 2023, 282 permits were issued for 113 new dwelling units, totaling a construction value of \$140.2 million.

Inspections – Building

A total of 1,903 inspection requests were received with 1,899 inspections being conducted.

In addition, 27 inspections were completed related to complaints, business licenses, orders and miscellaneous inspections.

Of the 1,903 inspections requested, 98% were conducted within the provincially mandated 48 hour period.

Inspections - Code Compliance

A total of 994 inspection requests were received, with 1,117 inspections being conducted.

An additional 184 inspections were completed relating to complaints, business licences, orders and miscellaneous inspections.

Of the 994 inspections requested, 98% were conducted within the provincially mandated 48 hour period.

Inspections - Plumbing

A total of 905 inspection requests were received with 1,158 inspections being conducted related to building permit activity.

An additional 9 inspections were completed related to complaints, business licenses, orders and miscellaneous inspections.

Of the 905 inspections requested, 100% were conducted within the provincially mandated 48 hour period.

2018 - 2020 Permit Data

Additional permit data has been provided in Appendix “A” to reflect 2018 – 2020 permit data.

Conclusion

The purpose of this report is to provide Municipal Council with information regarding the building permit issuance and building & plumbing inspection activities for the month of October 2023. Attached as Appendix “A” to this report is a “Summary Listing of Building Construction Activity” for the month of October 2023 as well as “Principle Permits Reports”.

Prepared by: Peter Kokkoros, P.Eng.
Director, Building and Chief Building Official
Planning and Economic Development

Submitted by: Scott Mathers, MPA, P.Eng.
Deputy City Manager
Planning and Economic Development

Recommended by: Scott Mathers, MPA, P.Eng.
Deputy City Manager
Planning and Economic Development

APPENDIX "A"

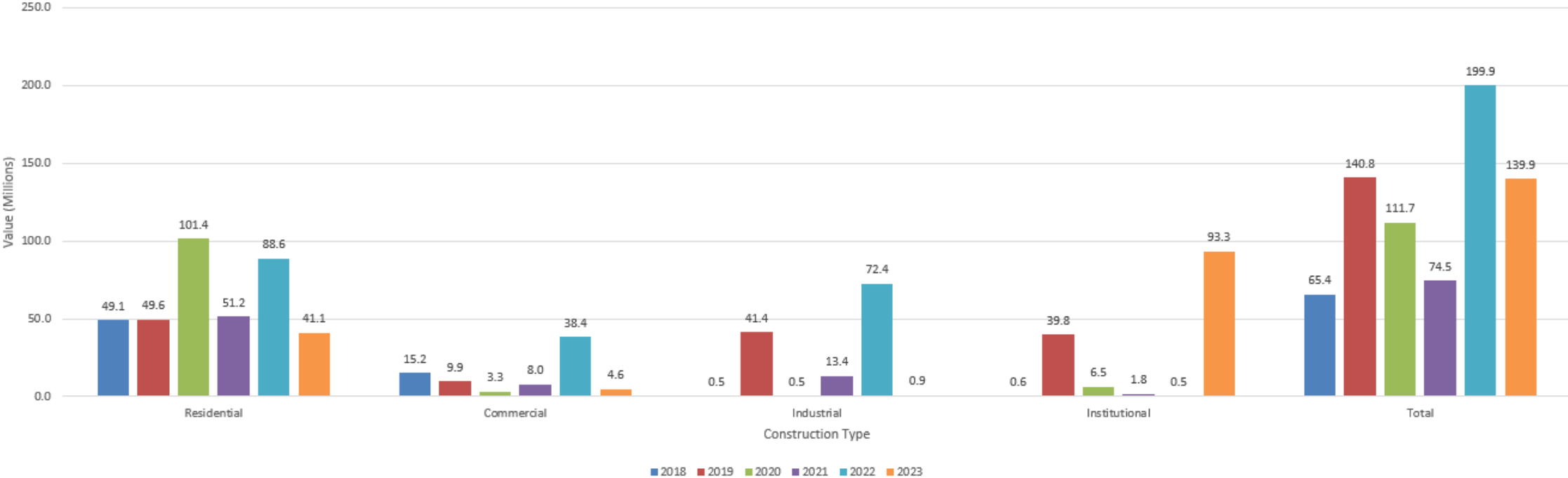
CITY OF LONDON

SUMMARY LISTING OF BUILDING CONSTRUCTION ACTIVITY FOR THE MONTH OF OCTOBER 2023

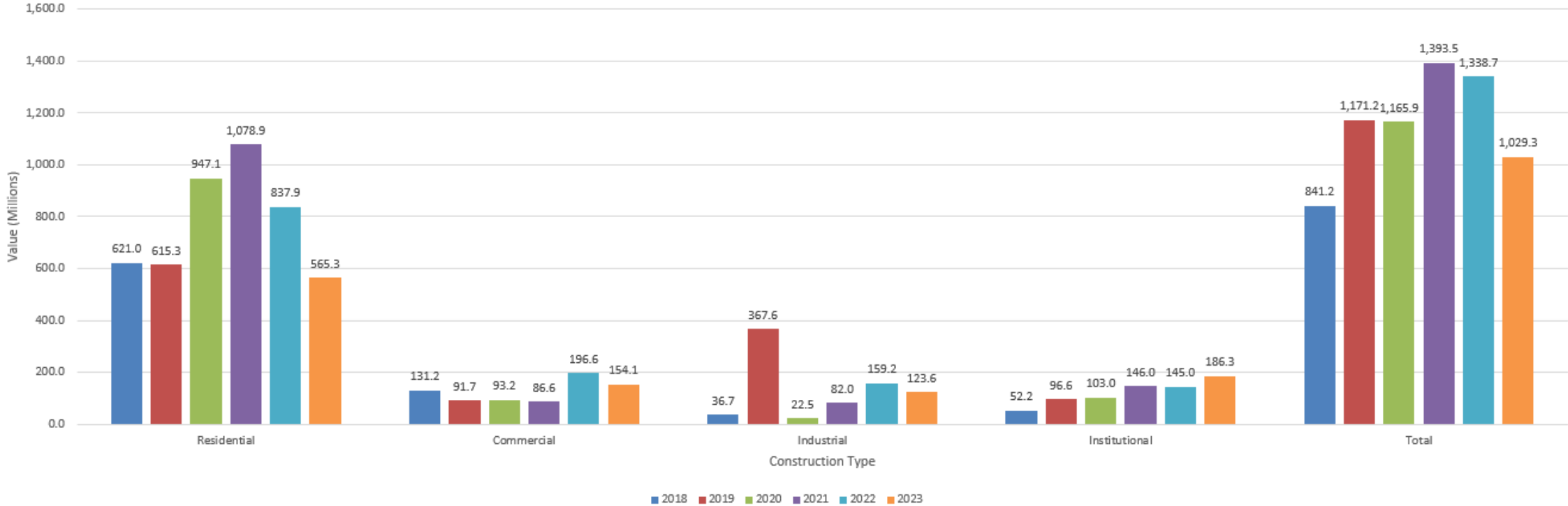
CLASSIFICATION	October 2023			to the end of October 2023			October 2022			to the end of October 2022			October 2021			to the end of October 2021		
	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS
SINGLE DETACHED DWELLINGS	19	10,926,021	19	199	109,142,827	199	36	20,100,228	36	555	283,217,074	555	71	31,069,000	71	914	414,056,550	914
SEMI DETACHED DWELLINGS	0	0	0	5	4,181,026	9	0	0	0	0	0	0	1	526,000	1	2	749,500	2
TOWNHOUSES	3	6,232,023	13	55	98,690,934	291	3	3,682,524	12	105	146,722,528	476	14	15,022,300	73	193	184,672,200	748
DUPLEX, TRIPLEX, QUAD, APT BLDG	1	11,711,772	42	18	177,961,217	522	2	56,049,372	250	15	316,809,135	995	0	0	0	14	411,371,500	1,751
RES-ALTER & ADDITIONS	157	12,219,716	39	1,602	175,276,727	223	155	8,728,583	16	1,688	91,115,585	170	154	4,575,225	13	1,577	68,030,652	114
COMMERCIAL - ERECT	1	800,000	0	7	39,380,370	0	2	36,671,668	0	15	91,781,087	1	2	1,955,000	0	24	13,474,400	0
COMMERCIAL - ADDITION	2	70,000	0	11	8,631,730	0	0	0	0	10	27,001,900	0	0	0	0	6	3,626,500	0
COMMERCIAL - OTHER	19	3,707,438	0	259	106,077,198	0	18	1,734,916	0	237	77,835,110	0	32	6,094,900	0	299	69,514,215	0
INDUSTRIAL - ERECT	0	0	0	5	51,391,868	0	1	69,793,395	0	3	99,373,579	0	2	1,115,000	0	14	46,342,409	0
INDUSTRIAL - ADDITION	1	10,000	0	9	54,238,428	0	0	0	0	7	52,657,871	0	1	10,500,000	0	7	16,886,560	0
INDUSTRIAL - OTHER	5	862,000	0	43	18,004,051	0	3	2,601,700	0	29	7,179,860	0	7	1,820,500	0	31	18,782,480	0
INSTITUTIONAL - ERECT	1	86,243,053	0	4	120,942,305	0	0	0	0	3	99,646,231	0	0	0	0	1	12,000,000	0
INSTITUTIONAL - ADDITION	0	0	0	13	12,159,743	0	0	0	0	2	2,379,000	0	0	0	0	6	47,273,386	0
INSTITUTIONAL - OTHER	12	7,093,109	0	167	53,182,125	0	5	543,000	0	142	42,931,836	0	8	1,829,500	0	116	86,726,950	0
AGRICULTURE	0	0	0	7	4,152,600	0	0	0	0	3	1,610,000	0	2	207,000	0	4	557,000	0
SWIMMING POOL FENCES	8	305,000	0	161	5,994,368	0	10	600,600	0	307	12,507,509	0	18	737,138	0	355	10,453,296	0
ADMINISTRATIVE	7	12,000	0	128	493,000	0	6	6,000	0	127	881,000	0	5	8,000	0	87	298,000	0
DEMOLITION	10	0	24	87	0	70	3	0	3	85	0	58	7	0	4	67	0	45
SIGNS/CANOPY - CITY PROPERTY	2	0	0	12	0	0	2	0	0	17	0	0	0	0	0	8	0	0
SIGNS/CANOPY - PRIVATE PROPERTY	34	0	0	300	0	0	16	0	0	302	0	0	43	0	0	310	0	0
TOTALS	282	140,192,132	113	3,092	1,039,900,516	1,244	262	200,511,986	314	3,652	1,353,649,305	2,197	367	75,459,563	158	4,035	1,404,815,597	3,529

CLASSIFICATION	October 2020			to the end of October 2020			October 2019			to the end of October 2019			October 2018			to the end of October 2018		
	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS	NO. OF PERMITS	CONSTRUCTION VALUE	NO. OF UNITS
SINGLE DETACHED DWELLINGS	98	42,392,100	98	756	321,280,454	756	55	22,933,225	55	564	238,577,565	564	64	26,364,180	64	597	250,584,025	597
SEMI DETACHED DWELLINGS	0	0	0	2	1,023,000	4	0	0	0	3	884,400	6	0	0	0	0	0	0
TOWNHOUSES	22	22,359,800	97	119	115,879,985	438	21	21,781,520	90	146	134,193,130	606	11	9,727,400	39	187	144,072,269	587
DUPLEX, TRIPLEX, QUAD, APT BLDG	1	29,586,000	116	13	460,464,800	1,530	0	0	0	18	192,486,852	875	2	6,753,600	27	9	171,356,520	740
RES-ALTER & ADDITIONS	156	7,028,754	5	1,243	48,442,763	45	161	4,916,980	2	1,586	49,204,797	58	163	6,250,322	12	1,599	55,021,259	74
COMMERCIAL - ERECT	0	0	0	9	7,160,300	0	1	320,000	0	15	20,982,080	0	4	3,537,200	0	31	58,984,303	0
COMMERCIAL - ADDITION	1	1,386,000	0	4	2,182,800	0	0	0	0	15	9,554,000	0	1	1,130,000	0	13	10,780,718	0
COMMERCIAL - OTHER	24	1,953,842	0	303	83,893,169	0	42	9,609,793	0	439	61,170,203	2	48	10,552,700	0	405	61,473,339	0
INDUSTRIAL - ERECT	1	366,700	0	6	8,653,400	0	1	6,000,000	0	12	318,766,000	0	0	0	0	3	9,450,000	0
INDUSTRIAL - ADDITION	1	12,500	0	5	7,931,300	0	2	35,390,000	0	10	41,703,100	0	1	20,000	0	7	7,330,000	0
INDUSTRIAL - OTHER	3	101,000	0	35	5,915,407	0	3	43,000	0	64	7,090,320	0	10	500,500	0	55	19,916,304	0
INSTITUTIONAL - ERECT	1	30,000	0	4	32,855,000	0	0	0	0	2	27,456,800	0	0	0	0	0	0	0
INSTITUTIONAL - ADDITION	0	0	0	8	15,178,000	0	2	33,900,000	0	9	39,233,800	0	0	0	0	5	14,049,600	0
INSTITUTIONAL - OTHER	13	6,458,000	0	145	54,997,001	0	16	5,902,000	0	167	29,890,960	0	7	607,250	0	208	38,133,942	0
AGRICULTURE	1	162,000	0	2	262,000	0	0	0	0	6	15,700,000	0	0	0	0	3	210,000	0
SWIMMING POOL FENCES	23	788,400	0	340	8,555,291	0	12	263,000	0	205	4,402,267	0	20	762,000	0	216	4,919,972	0
ADMINISTRATIVE	4	3,000	0	45	109,000	0	13	29,000	0	137	349,000	0	18	19,500	0	173	453,850	0
DEMOLITION	8	0	1	65	0	44	7	0	7	78	0	46	2	0	1	73	0	35
SIGNS/CANOPY - CITY PROPERTY	3	0	0	5	0	0	1	0	0	28	0	0	3	0	0	14	0	0
SIGNS/CANOPY - PRIVATE PROPERTY	33	0	0	261	0	0	31	0	0	428	0	0	2	0	0	273	0	0
TOTALS	393	112,628,096	317	3,370	1,174,783,670	2,817	368	141,088,518	147	3,932	1,191,645,274	2,111	356	66,224,652	142	3,871	846,736,100	1,998

Construction Value of Building Permits (October)



Construction Value of Building Permits (Jan - Oct)





City of London - Building Division
Principal Permits Issued from October 1, 2023 to October 31, 2023

Owner	Project Location	Proposed Work	No. of Units	Construction Value
Wallmac Wood Industries C/O Reitter Management Corp	1 Cuddy Crt	(statcan) Alter - Freight Depots Installation of 4 Electric Vehicle charger with overhead structural support	0	140,000
RONGTIAN CHAI Meadowlily Developments Inc	101 Meadowlily Rd S	Install - Water service/main - Townhouse - Cluster SDD Servicing Permit Application		1,653,000
MCC 91 Middlesex Condominium Corporation No. 91	1096 Jalna Blvd	(statcan) Alter - Apartment Building This is a multi-unit apartment building - looking to repair existing balconies		706,675
Ingredion Canada Corporation	1100 Green Valley Rd	(statcan) Alter - Food Processing Plant Installing a new tank base / retaining wall in place of an existing one to suite a larger tank requirement.	0	120,000
KAMATO HOLDINGS LIMITED	115 Cherryhill Blvd	Install - Backflow Preventer - Apartment Building install 1 1/4" RP backflow device installation in the mechanical room on the roof, serving the hydronic heating system replacing the 1" existing. device.		160,000
WESTERN UNIVERSITY WESTERN UNIVERSITY	1151 Richmond St	(statcan) Alter - University WU RRI 15.2T MRI Renovation	0	1,298,900
WESTERN UNIVERSITY WESTERN UNIVERSITY	1151 Richmond St	(statcan) Alter - University Interior alternation on 4th floor area of Social Sciences Building.	0	3,668,749
WESTERN UNIVERSITY WESTERN UNIVERSITY	1151 Richmond St	(statcan) Alter - University ALTER TO FIRST FLOOR NURSING BUILDING RM 1350 **SHELL PERMIT ONLY** PROVIDE SPRINKLER SHOP DRAWINGS FOR FULL	0	250,000
REMBRANDT MEADOWLILLY INC REMBRANDT MEADOWLILLY INC	1224 Blackwell Blvd G	(statcan) Erect - Townhouse - Condo ERECT NEW CONDO STREET TOWNHOUSE BLOCK G, 2 STOREY 1 CAR, 3 BED, FINISHED BASEMENT, W/ DECK, W/ A/C, ENERGY STAR, HRV AND DWHR REQUIRED, DPN 52, 54, 56, 58, 60, 62	6	1,667,523
SIFTON LIMITED SIFTON PROPERTIES LIMITED	1325 Riverbend Rd	(statcan) Alter - Restaurant ALTER TO EXPAND RESTAURANT (unit 130) INTO EXISTING VACANT TENANT SPACE/SHELL SPACE (unit 140)	0	200,000
ALEXANDRE DA COSTA SOUTHBRIDGE HEALTH CARE GP INC.	1390 Dundas St	(statcan) Erect - Nursing Homes 256 Long-term care beds, 5-storey tall Group B, Division 2 together with Group D tenancy at the ground level	0	86,243,053
AMARELO PROPERTIES GP INC	140 Cherryhill Pl	(statcan) Alter - Apartment Building Concrete repairs to parking structure		103,000
MARTIN PETERKA ARBOR MEMORIAL INC	2001 Dundas St	(statcan) Erect - Non-Residential Accessory Building To construct a new columbarium/ accessory building on an existing site.	0	800,000



City of London - Building Division
Principal Permits Issued from October 1, 2023 to October 31, 2023

Owner	Project Location	Proposed Work	No. of Units	Construction Value
	2079 Trafalgar St	Install - Sanitary or Storm sewer - Duplex Install only sewer/storm services. **See open plumbing permit 23-026099, 23-027769, & 23-027772. To be inspected at same time.**		130,000
	2083 Trafalgar St	Install - Sanitary or Storm sewer - Duplex Install only sewer/ storm services. **See open plumbing permit 23-026099, 23-027764, & 23-027772. To be inspected at same time.**		130,000
	2087 Trafalgar St	Install - Sanitary or Storm sewer - Duplex Install only sewer/storm services. **See open plumbing permit 23-026099, 23-027764, & 23-027769. To be inspected at same time.**		130,000
SIFTON LIMITED SIFTON PROPERTIES LIMITED	235 Kennington Way A	(statcan) Erect - Townhouse - Condo ERECT 3 STOREY, 6 UNIT TOWNHOUSE BLOCK BLDG A, DPNS 1, 3, 5, 7, 9, 11	6	3,683,350
Sifton Properties Limited The Canada Life Assurance Company	255 Queens Ave	(statcan) Alter - Offices Interior alterations to existing full floor tenant at One London Place	0	762,000
SIFTON LIMITED SIFTON PROPERTIES LIMITED	2550 Sheffield Blvd	Install - Sanitary or Storm sewer - Site Services Install Sanitary Sewer, Storm Sewer and Water Services for the 20-Unit Townhouse Development as per Site Plan Number SPA21-069.		525,479
SKYLINE REAL ESTATE HOLDINGS INC SKYLINE REAL ESTATE HOLDINGS INC	301 Base Line Rd W	(Statcan) Add- Res. Accessory Struct - Apartment Building New 2-storey accessory building to replace existing pool house (to be demolished) *****NOT TO BE USED AS A DWELLING UNIT*****	0	1,000,000
CRAWFORD PROPERTIES INC	3101 Gore Rd	Install - Warehousing INSTALL NEW EXTERIOR CRANE	0	500,000
WONDERLAND POWER CENTRE INC. WONDERLAND POWER CENTRE INC.	3165 Wonderland Rd S 1	(statcan) Alter - Retail Store PRO HOCKEY LIFE INTERIOR FIT UP Shell Permit Only – Provide the details of the grille door's attachment to the HSS beam (to the non-combustible element) and the sealed shop drawings for the new folding grille door to the Building Division for review prior to work in these areas.	0	750,000
Sabra Canadian Gp I Inc	350 North Centre Rd	(statcan) Alter - Apartment Building ALTER FOR STRUCTURAL REPAIRS TO BOILER ROOM	0	110,650



City of London - Building Division

Principal Permits Issued from October 1, 2023 to October 31, 2023

Owner	Project Location	Proposed Work	No. of Units	Construction Value
	353 Queens Ave	(statcan) Alter - Duplex ALTER TO DIVIDE EXISTING SPACE TO CREATE A NEW ADDITIONAL RESIDENTIAL UNIT ON THE SECOND AND THIRD FLOORS *****ADDITIONAL RESIDENTIAL UNITS AS PER SEC 4.37 OF ZONING BY-LAW Z-1***** *****TOTAL OF 3 BEDROOMS FOR BOTH UNITS COMBINED*****	2	197,676
DREWLO HOLDINGS INC DREWLO HOLDINGS INC	530 Gatestone Rd	Install - Site Services install site services		1,300,000
DREWLO HOLDINGS INC	680 Waterloo St	(statcan) Alter - Offices Interior renovation work for 1st floor and 2nd floor office lease tenants.	0	1,000,000
C/O INFRASTRUCTURE ONTARIO MANAGEMENT BOARD SECRETARIAT	711 Exeter Rd	(statcan) Alter - Correctional & Detention Centre Repairs to the DWD, HWD and Sanitary Systems	0	639,800
Cranbrook Estates Inc	759 Adelaide St N	(statcan) Alter - Retail Store Dollar Tree - Interior Fit up. Work includes Mechanical, Electrical & Structural (Structural: opening in exterior wall for new egress door ***SHELL PERMIT*** Provide exterior stair shop drawings including guards/handrail	0	585,000
LONDON HEALTH SCIENCES CENTRE LONDON HEALTH SCIENCES CENTRE	800 Commissioners Rd E	(statcan) Alter - Hospitals INTERIOR ALTERATIONS TO D1-450 - D1-459, D1-301A AND D1-C01. SHELL - Provide sprinkler shop drawing and GRCC.	0	1,000,000
TALU PROPERTIES INC. TALU PROPERTIES INC.	840 Chelton Rd	(statcan) Erect - Apartment Building Erect 4 storey apartment building	42	11,711,772

Total Permits 30 Units 57 Value 121,166,627

** Includes all permits over \$100,000, except for single and semi-detached dwellings.*

Report to Planning and Environment Committee

To: Chair and Members
Planning and Environment Committee

From: Scott Mathers, MPA, P.Eng.
Deputy City Manager, Planning and Economic Development

Subject: Royal Premier Homes
1350 Wharncliffe Road South
File Number: Z-9611/39T-23501, Ward 12
Public Participation Meeting

Date: December 4, 2023

Recommendation

That, on the recommendation of the Director, Planning and Development, the following actions be taken with respect to the application of Royal Premier Homes relating to the property located at 1350 Wharncliffe Road South:

- (a) the proposed by-law attached hereto as Appendix "A" **BE INTRODUCED** at the Municipal Council meeting December 19th, 2023, to amend Zoning By-law No. Z.-1, in conformity with the Official Plan, *The London Plan*, for the City of London to change the zoning of the subject property **FROM** a Holding Urban Reserve UR6 Special Provision (h-17*h-42*UR6(1)) Zone **TO** a Holding Residential R1 Special Provision (h*h-100*R1-13(7)) Zone; Holding Residential R1 Special Provision (h*h-100*R1-13(_)) Zone; Holding Residential R1 Special Provision (h*h-100*R1-13(_)) Zone; Holding Residential R4 Special Provision (h*h-17*h-18*h-100*h-149*R4-6(_)) Zone; and, a Holding Residential R6 Special Provision (h*h-17*h-18*h-100*h-149*R6-5(_)) Zone;
- (b) the Planning and Environment Committee **REPORT TO** the Approval Authority the issues, if any, raised at the public meeting;
- (c) The Site Plan Approval Authority **BE REQUESTED** to consider the provision of short-term public bicycle parking in the development of each block through the site plan process; and
- (d) The Approval Authority **BE ADVISED** that Municipal Council supports issuing draft approval of the proposed plan of residential subdivision, submitted by Royal Premier Homes (File No. 39T-23501), prepared by Stantec, Project No. 16141212, March 17th 2022, which shows a draft plan of subdivision consisting of three (3) medium density residential blocks, twenty-eight (28) single-detached lots, one (1) reserve block and one (1) road widening blocks servicing by the extension on Southbridge Avenue and a new Neighbourhood Street (Street A).

Executive Summary

Summary of Request

The applicant has requested an amendment to the Zoning By-law Z.-1 to rezone the property from a Holding Urban Reserve UR6 Special Provision (h-17*h-42*UR6(1)) Zone to a Holding Residential R1 Special Provision (h*h-100*R1-13(7)) Zone; Holding Residential R1 Special Provision (h*h-100*R1-13(_)) Zone; Holding Residential R1 Special Provision (h*h-100*R1-13(_)) Zone; Holding Residential R4 Special Provision (h*h-17*h-18*h-100*h-149*R4-6(_)) Zone; and, a Holding Residential R6 Special Provision (h*h-17*h-18*h-100*h-149*R6-5(_)) Zone.

Staff have proposed holding provisions to form part of the zone to ensure the following:

- h: orderly development and adequate provision of municipal services through an approved Development Agreement;

- h-17: orderly development of the lands and adequate provision of municipal services through the provision of full municipal sanitary sewer and water services;
- h-18: the required archaeological studies have been completed and accepted, and any recommendations implemented;
- h-100: there is adequate water services and appropriate access, a looped watermain system must be constructed and a second public access must be available to the satisfaction of the City Engineer; and,
- h-149: sanitary and stormwater servicing reports have been prepared and confirmation that sanitary and stormwater management systems are implemented to the satisfaction of the City Engineer.

The Zoning By-law Amendment and Draft Plan of Subdivision based on the development proposal will contribute to approximately **108 residential units**.

Purpose and the Effect of Recommended Action

The purpose and effect of the recommended action is for Municipal Council to approve the recommended Zoning By-law Amendments to permit the development of a residential subdivision comprised of single-detached, cluster townhouse and back-to-back townhouse dwelling units.

Rationale of Recommended Action

Approval of the recommended Zoning By-law Amendment because:

1. The recommended Zoning By-law Amendment is consistent with the *Provincial Policy Statement 2020*.
2. The recommended zoning conforms to the policies of *The London Plan*, including, but not limited to, the Neighbourhoods Place Type, City Building and Design, Our Tools, and all other applicable policies of *The London Plan*.
3. The zoning will permit development that is considered appropriate and compatible with the existing and future land uses surrounding the subject lands.
4. The proposed and recommended amendments are consistent with the *Provincial Policy Statement 2020*, which promotes a compact form of development in strategic locations to minimize land consumption and servicing costs, provide for and accommodate an appropriate affordable and market-based range and mix of housing type and densities to meet the projected requirements of current and future residents.
5. The proposed and recommended zoning amendments will support the proposed Draft Plan of Subdivision and facilitate an appropriate form and mix of low and medium density residential development that conforms to *The London Plan*.

Linkage to the Corporate Strategic Plan

The recommendation supports the following Strategic Areas of Focus:

- **Housing and Homelessness**, by ensuring London's growth and development is well-planned and considers use, intensity, and form; and,
- **Wellbeing and Safety**, by promoting neighbourhood planning and design that creates safe, accessible, diverse, walkable, healthy, and connected communities.

Analysis

1.0 Background Information

1.1 Previous Reports Related to this Matter

PEC Report – Application by Weldwood Farms (John Arroyas) 1350 Wharncliffe Road South Z-5716 – August 30, 1999

PEC Report – Application by the City of London 1350 Wharncliffe Road South Z-7498 – April 7, 2008

1.2 Planning History

The subject lands were annexed into the City of London in 1993. Prior to annexation, Council for the Town of Westminster approved a Zoning By-law Amendment to add a Montessori School, an antique shop and a wood working shop as permitted uses. In September 1999, Council approved an Official Plan Amendments to add the following permitted uses:

- An antique shop;
- A restaurant associated with the antique shop;
- Woodwork shop;
- A self-storage building;
- A private school;
- General offices

At the same time, a Zoning By-law Amendment was approved to remove Montessori School from the permitted uses and add the following additional uses:

- General office;
- A private school;
- A restaurant associated with an antique shop;
- A self-storage building; and,
- The sale of seasonal nursery and garden stock

In June of 2005, the Annexed Area Zoning By-law Amendment (Z.-1-051390) was approved by Council. This inadvertently zoned the subject lands as Urban Reserve UR6, which permitted a limited range of recreation uses and any legally existing uses. The property owner brought this matter to the attention of Planning Staff in 2007, and in February of 2008, the City of London initiated a Zoning By-law Amendment to reinstate the uses that were permitted prior the adoption of Annexed Area Zoning By-law Amendment.

1.3 Property Description and Location

The subject property is located directly adjacent to the southeast of the intersection of Wharncliffe Road South and the future Bradley Avenue Extension, described as Part of Lots 34 of Concession 2 in the former Westminster Township. There is a mix of built or proposed low and medium density residential surrounding the subject lands with light industrial uses further west and restricted service commercial uses to the east.

The subject lands are listed on the *City of London Register of Cultural Heritage Resources* as the Weldwood Farm. Council passed a by-law to designate the building under the *Ontario Heritage Act* on October 17th, 2023, and the appeal period ended on December 2, 2023. The rationale for the designation is because of the history of the subject lands. The lands were purchased in 1910 by John Weld, the publisher of the widely circulated *Farmer's Advocate*. Weld built an experimental farm on the property to test new crop varieties and agricultural methods. The main building was constructed

between 1911 and 1918 to support the experimental farm, and is proposed to be maintained as a part of the proposed subdivision.

The site is 4.05 hectares in size, 430 metres deep and has a frontage of 122 metres. The site will have access to municipal services and is within the Urban Growth Boundary.

Site Statistics:

- Current Land Use: Residential and Commercial Uses
- Frontage: 122 metres (400 feet)
- Depth: 430 metres (1410 feet)
- Area: 4.05 hectares (10.01 acres)
- Shape: regular (rectangle)
- Located within the Built Area Boundary: No
- Located within the Primary Transit Area: No

Surrounding Land Uses:

- North: Future Mid-Rise Residential
- East: Future Low-Rise and Mid-Rise Residential
- South: Low-Rise and Mid-Rise Residential
- West: Low-Rise and Mid-Rise Residential

Existing Planning Information:

- Existing *The London Plan* Place Type: Neighbourhoods
- Existing Zoning: Holding Urban Reserve UR6 Special Provision (h-17*h-42*UR6(1)) Zone

Additional site information and context is provided in Appendix B.

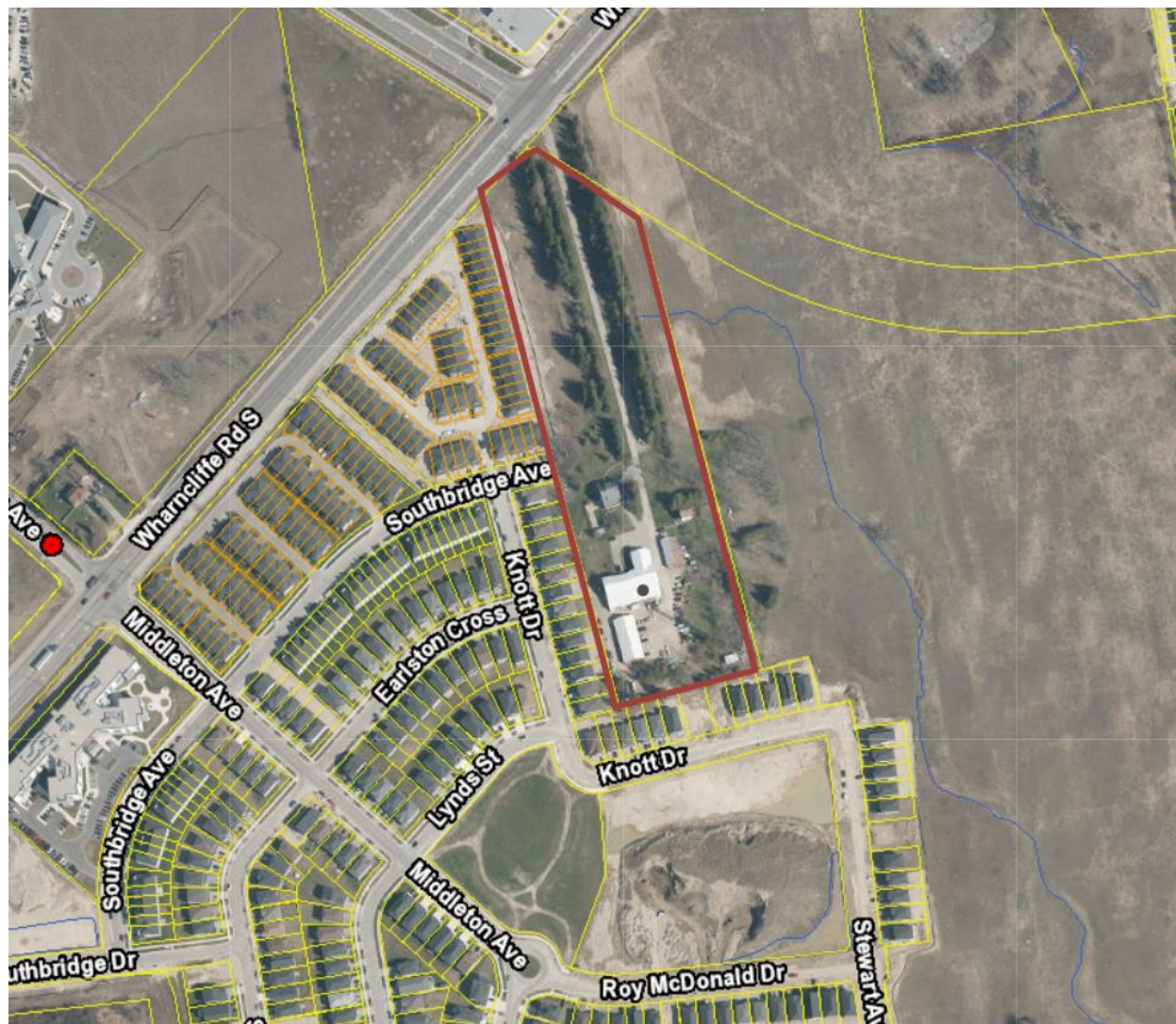
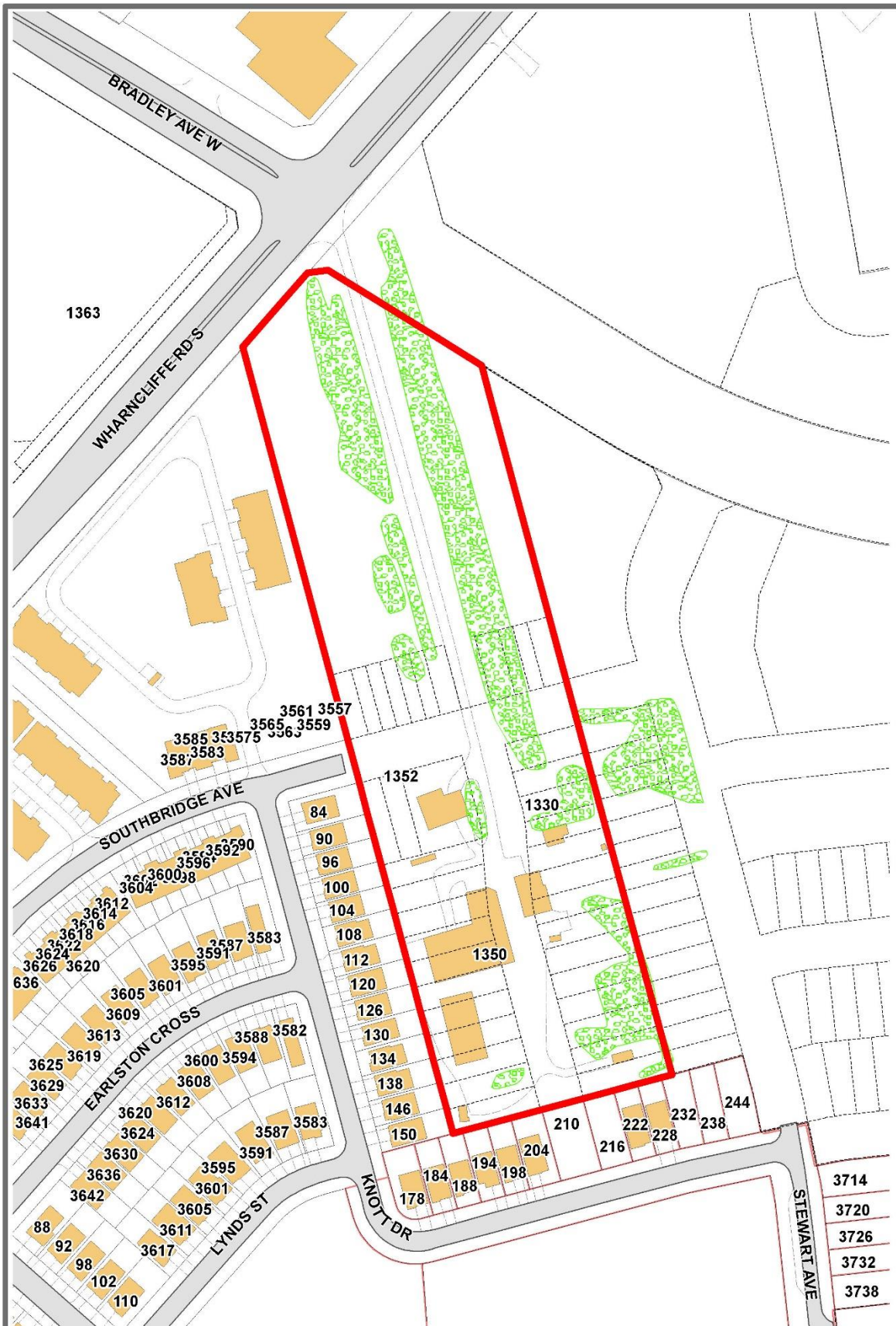


Figure 1- Aerial Photo of 1350 Wharncliffe Road South and surrounding lands



Figure 2 - Streetview of 1350 Wharnclyffe Road South (view looking South)

Location Map



LOCATION MAP

Address: 1350 Wharnclyffe Road South
 File Number: Z-9611
 Planner: Alison Curtis
 Date: 2023/5/26

Corporation of the City of London
 Prepared By: Planning and Development



Scale 1:2000

Legend

- Subject Site
- Draft Approved Subdivisions
- Buildings
- Vegetation

2.0 Discussion and Considerations

2.1 Development Proposal

The Zoning By-law amendment will facilitate the development of a Draft Plan of Subdivision that provides for the following:

- three (3) medium density blocks (approx. 80 units)
 - Blocks 29 to 31 are proposed for townhouse units in a mixed form, which includes back-to-back, cluster and freehold street townhouse units.
 - Block 31, which fronts onto both Wharncliffe Road South and the Bradely Avenue Extension, will transition to the low and medium density residential land uses to the south.
- twenty-eight (28) lots for single-detached dwellings, and
- one (1) block for road widening.

The heritage building is proposed to be retained as one of the twenty-eight (28) single detached lots. The proposed Draft Plan will be served by the extension of Southbridge Avenue (Neighbourhood Street) and Street A (Neighbourhood Street). Please note that the Draft Plan of Subdivision, seen below, may be further refined and reviewed prior to Draft Approval by Civic Administration.

The proposed development includes the following features:

- Residential development that is within the Urban Growth Boundary and adjacent to existing development within the Built Area Boundary;
- Medium density, multiple-attached residential dwellings that will provide a more intensive scale of development that supports a compact urban form, area commercial uses to the northeast, and transit services, as well as serving as a transition between low density residential to the south and northeast;
- Single detached dwelling lots that may be developed with four (4) units through the Additional Residential Unit (ARU) requirements of the Z.-1 Zoning By-law, which allows for an additional 84 units on these lots for a total of 112 units; and,
- Extension of Southbridge Avenue and provision of Street A, improving pedestrian and vehicle connectivity within the subdivision and to the adjacent lands.

Additional information on the development proposal is provided in Appendix B.

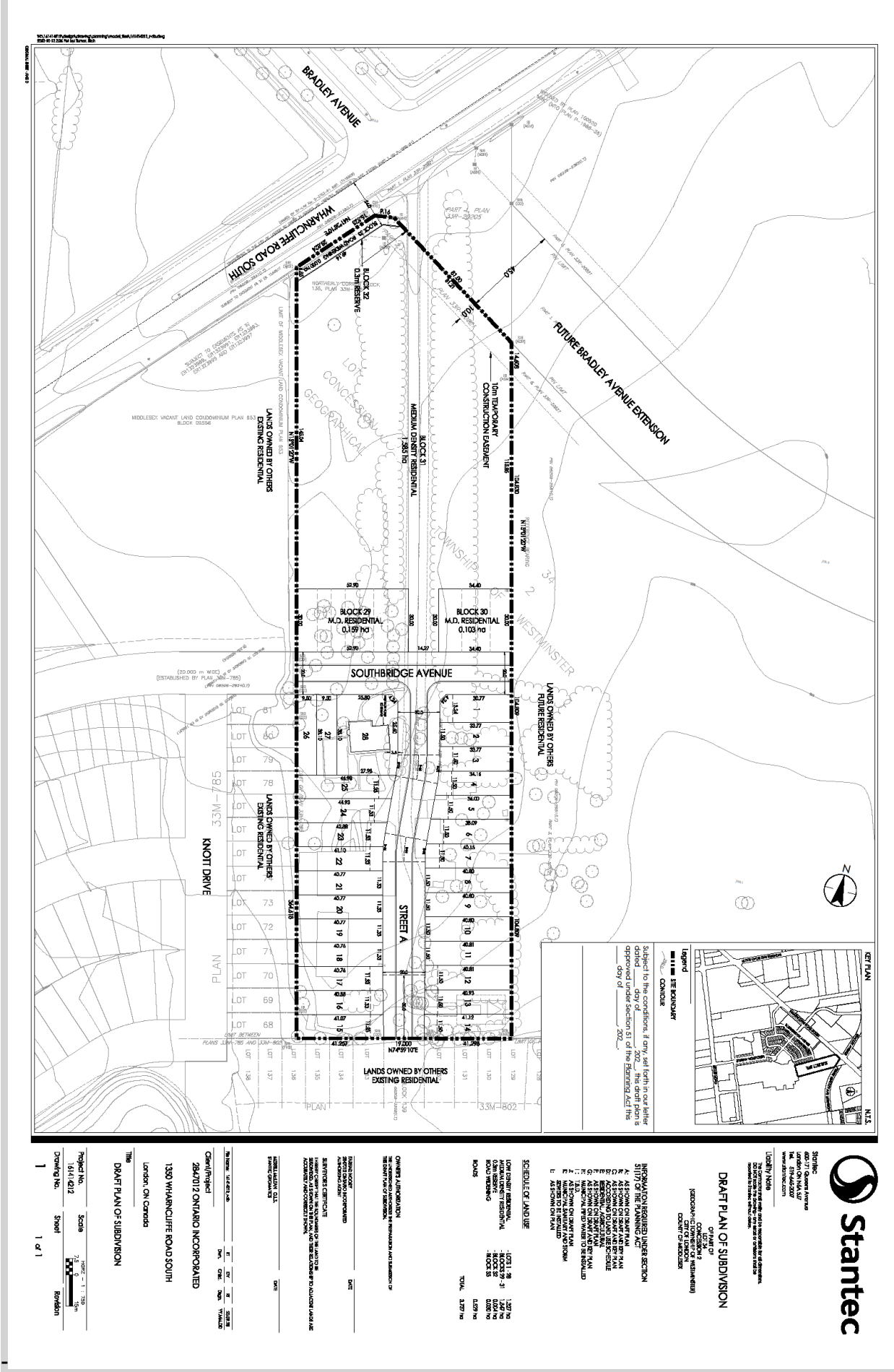


Figure 3 – Draft Plan of Subdivision (March 2023)

Additional plans and drawings of the development proposal are provided in Appendix C.

2.2 Requested Amendment

The applicant has requested an amendment to the Zoning Bylaw Z.-1 to rezone the property from a Holding Urban Reserve UR6 Special Provision (h-17*h-42*UR6(1)) Zone to a Holding Residential R1 Special Provision (h*h-100*R1-13(7)) Zone; Holding Residential R1 Special Provision (h*h-100*R1-13(_)) Zone; Holding Residential R1 Special Provision (h*h-100*R1-13(_)) Zone; Holding Residential R4 Special Provision (h*h-17*h-18*h-100*h-149*R4-6(_)) Zone; and, a Holding Residential R6 Special Provision (h*h-17*h-18*h-100*h-149*R6-5(_)) Zone.

The following table summarizes the special provisions that have been proposed by the applicant and those that are being recommended by Staff.

Lots	Zone String	Special Provisions Requested
Lots 1 to 26	h*h-100*R1-13(7)	<ul style="list-style-type: none"> Existing, see section 5.4 of the R1 Residential Zone
Lot 27	h*h-100*R1-13(_)	<ul style="list-style-type: none"> Minimum front yard setback of 8.75 metres; Minimum rear yard setback of 6 metres; and, Garages shall not project beyond the façade of the dwellings or façade (front face) of any porch, and shall not occupy more than 50% of lot frontage.
Lot 28 (existing dwelling)	h*h-18*h-100*R1-13(_)	<ul style="list-style-type: none"> Minimum front yard setback of 3.5 metres; Minimum rear yard setback of 4.5 metres; and, Garages shall not project beyond the façade of the dwellings or façade (front face) of any porch, and shall not occupy more than 50% of lot frontage
Blocks 29 and 30	h*h-17*h-18*h-100*h-149*R4-6(_)	<ul style="list-style-type: none"> Minimum lot frontage of 6.7 meters; Maximum lot coverage of 50%; Maximum height of 12 metres; and, Garages shall not project beyond the façade of the dwellings or façade (front face) of any porch, and shall not occupy more than 50% of lot frontage.
Block 31	h*h-17*h-18*h-100*h-149*R6-5(_)	<ul style="list-style-type: none"> Notwithstanding section 10.2, single-detached dwellings shall not be permitted; Minimum front and exterior side yard depth of 1.5 metres; and, Minimum density of 30 units per hectare and a maximum height of 75 units per hectare.

2.3 Internal and Agency Comments

The application and associated materials were circulated for internal comments and public agencies to review. Comments received were considered in the review of this application and are addressed in Section 4.0 of this report.

Key issues identified by staff and agencies included:

- Retention and integration of the heritage building;
- Street orientation within medium density blocks; and,
- Inclusion of holding provision in the zoning to ensure orderly development.

Detailed internal and agency comments are included in Appendix D of this report.

2.4 Public Engagement

On June 8, 2023, Notice of Application was sent to 167 property owners and residents in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on June 8, 2023. A planning application sign was also placed on the site.

There were no responses received during the public consultation period.

Detailed public notice is included in Appendix E of this report.

2.5 Policy Context

The Planning Act and the Provincial Policy Statement, 2020

The Provincial planning policy framework is established through the *Planning Act* (Section 3) and the *Provincial Policy Statement, 2020 (PPS)*. The *Planning Act* requires that all municipal land use decisions affecting planning matters shall be consistent with the *PPS*.

The mechanism for implementing Provincial policies is through the Official Plan, The London Plan. Through the preparation, adoption, and subsequent Ontario Land Tribunal (OLT) approval of The London Plan, the City of London has established the local policy framework for the implementation of the Provincial planning policy framework. As such, matters of provincial interest are reviewed and discussed in The London Plan analysis below.

As the application for a Zoning By-law Amendment complies with The London Plan, it is staff's opinion that the application is consistent with the *Planning Act* and the *PPS*.

Important policy objectives to highlight are those within Sections 1.1, 1.4 and 1.6. These policies require land use within settlement areas to effectively use the land and resources through appropriate densities, range of uses and the efficient use of infrastructure. Section 2 *PPS* sets out policies for the protection of significant built heritage resources and significant cultural heritage landscapes to ensure they are conserved, and development or site alteration shall not be permitted adjacent to protected heritage property, except where the proposed development or site alteration has been evaluated and demonstrated that the heritage attributes of the protected property will be conserved (Sections 2.6.1 and 2.6.3). Heritage Assessments have been completed for the property and a separate process to designate the building under the *Ontario Heritage Act* is currently underway. Archaeological Assessments are being conducted on the lands to determine if there are additional cultural resources.

The proposal does not direct development towards any natural human hazards and is of a sufficient distance away from human made hazards, satisfying Section 3 – Protecting Public Health and Safety of the *PPS*. The requested amendment has been reviewed for consistency with the *PPS*. Staff are of the opinion that the zoning amendment and draft plan of subdivision are consistent with the *PPS*.

The London Plan, 2016

The London Plan (TLP) includes evaluation criteria for all planning and development applications with respect to use, intensity and form, as well as with consideration of the following (TLP 1577-1579):

1. Consistency with the *Provincial Policy Statement 2020* and all applicable legislation.
2. Conformity with the Our City, Our Strategy, City Building, and Environmental policies.
3. Conformity with the Place Type policies.
4. Consideration of applicable guideline documents.
5. The availability of municipal services.
6. Potential impacts on adjacent and nearby properties in the area and the degree

to which such impacts can be managed and mitigated.

7. The degree to which the proposal fits within its existing and planned context.

Staff are of the opinion that all the above criteria have been satisfied.

The subject lands are currently designated within the Neighbourhoods Place Type along a Civic Boulevard (Wharnccliffe Road South) and an Urban Thoroughfare (Bradley Avenue). This Place Type at this location, based on Street Classification, permits single-detached, semi-detached, duplex, converted dwellings, townhouses, low-rise apartments and group homes (Table 10). A minimum height of two (2) stories, a standard maximum height of four (4) stories and an upper maximum of six (6) stories is permitted at the intersection of the Civic Boulevard and an Urban Thoroughfare (Table 11). Permitted heights along a Neighbourhood Street are a minimum of one (1), a standard maximum of three (3) and an upper maximum of four (4). The proposed Zoning By-Law Amendment is in keeping with these policies of *The London Plan*.

The vision for the Neighbourhoods Place Type is to ensure that neighbourhoods are vibrant and exciting places that contribute to community well-being and quality of life. This vision is supported by key elements, some of which include strong neighbourhood character; attractive streetscapes; diverse housing choices; well-connected neighbourhoods; alternatives for mobility; employment opportunities close to where people live; and parks and recreational opportunities. The proposal is in keeping with the vision for the Neighbourhoods Place Type and its key elements. It contributes to neighbourhood character, attractive streetscapes, and a diversity of housing choices. The proposed development is near to lands designated within the Shopping Area and Commercial Industrial Place Types, providing for amenities and employment opportunities within a distance appropriate for active transportation.

An excerpt from *The London Plan* Map 1 – Place Types is found in Appendix F.

Southwest Area Secondary Plan (SWAP)

The *Southwest Area Secondary Plan* has been reviewed in its entirety and it is Staff's opinion that the proposed Zoning By-law Amendment is consistent with these policies.

The following are key policies that relate to this proposal.

The site forms part of the 'Central Longwoods Neighbourhood' within the greater *Secondary Plan*. This *Secondary Plan* sets out policy and guidance to create neighbourhoods that have the following features: a mix of uses and diverse mix of residential housing; an emphasis on design parameters with placemaking features; walkability within and between neighbourhoods; an integration of the Natural Heritage System as an opportunity for residents to enjoy; and, Neighbourhood Central Activity Nodes as destination places in the neighbourhood.

The proposal will contribute to a range of dwelling types in the area and could contribute to creating opportunities for affordable housing, as required in section 50.5.3.1 Housing, in a compact form of development, which could contribute to a reduction of land and energy, as set out in section 20.5.3.2 Sustainable/Green Development.

3.0 Financial Impact/Considerations

Through the completion of the works associated with this application, fees, development charges and taxes will be collected. There will be increased operating and maintenance costs for works being assumed by the City.

4.0 Key Issues and Considerations

4.1 Land Use

The proposed stacked-townhouse and single detached-dwellings in this development would provide a mix of housing choices in compact form that are street oriented, which contributes to a safe pedestrian environment that promotes connectivity to adjacent lands within the Commercial Industrial and Shopping Area Place Types (TLP 285, 286, 916 and 1578).

There are lands located west and northwest of the subject lands, from the intersection of Wharncliffe Road South and Bradley Avenue, designated within the Shopping Area Place Type that provide for amenities and employment opportunities within a distance appropriate for active transportation (TLP 285, 286, 916 and 1578). The proximity of parks and other open space lands to the southeast provides for recreational opportunities and attractive alternatives for mobility (TLP 916). There are school sites located north of Southdale Road East and West, and east of White Oaks Road. Lands within the Neighbourhoods Place Type are located directly to the north, south, east, and west, and there are additional lands further east within the Neighbourhoods Place Type (TLP 916).

The London Plan provides direction for growth and development that is compact in form and directed to strategic locations, taking into consideration the required infrastructures and services required to support growth. “Inward and upward” growth is emphasized in *The London Plan* to achieve a compact urban form, and residential intensification is identified as playing a large role in achieving this goal (TLP 79 and 80). The proposed Zoning By-law Amendment and Draft Plan of Subdivision is located close to the border of the Built Area Boundary, but represents infill development of a vacant and underutilized lot within the Urban Growth Boundary in a use that is in keeping with the surrounding existing and proposed development.

The Residential R1, R4 and R6 Zone have been requested by the Applicant in order to facilitate their proposed development. The Residential R1 zone permits single-detached dwellings, and the Residential R4 Zone street townhouses dwellings Permitted uses with the R6 Zone include clustered: single detached dwelling; semi-detached dwelling; duplex dwelling; triplex dwelling; townhouse dwelling; stacked townhouse dwelling; apartment building; and, fourplex dwelling. The recommended zoning and holding provisions are considered an appropriate use that is generally consistent with Zoning By-law Z.-1 and *The London Plan* and surrounding residential and commercial development.

4.2 Intensity

The subject lands are sufficient in size and configuration to accommodate the development of street townhouses and single-detached dwellings. The Residential R1-13 Zone Variation requires a minimum lot area of 270 square metres per single detached lot, which is satisfied. The R4-6 requires a minimum of 145 square metres while the Residential R6-5 Zone Variation requires a minimum lot area of 850 square metres for cluster townhouse developments. Blocks 29, 30 and 31 of the Draft Plan of Subdivision satisfy these zoning requirements. Directly west of the subject lands there are townhouse dwellings fronting onto Wharncliffe Road South, and lands to the east are Draft Approved for townhouses of a similar intensity.

The Residential R4-6 Zone Variation does not specify a density maximum as it provides regulations based on one unit per lot, and the Residential R6-5 Zone Variation permits a maximum density of 35 units per hectare. Blocks 29 and 30 of the Draft Plan of Subdivision satisfy these zoning requirements for the R4-6 Zone. A special provision for a minimum density of 30 units per hectare and a maximum of 75 units per hectare has been required for the R6-5 Zone for Block 31. Similar densities have been considered and permitted in the R6-5 Zone Variation, and similar densities are permitted on lands directly to the east and west. The proposed size of Block 31 far

exceeds the minimum lot size and is sufficient to accommodate the increased density requested.

Building heights within the Neighbourhoods Place Type, at this location, shall not exceed four (4) storeys. Heights above this, to a maximum of six (6) storeys, may be permitted in conformity with the Our Tools policies of this Plan relating to Zoning to the Upper Maximum Height (Policies 1638 to 1641). The Residential R4-6 Zone Variation permits a height of twelve (12) metres, which has also been requested as a special provision, and the Residential R6-5 Zone Variation permits twelve (12) metres. Development proposed for Blocks 29, 30 and 31 are compliant with these Official Plan requirements and zone regulations.

The street and cluster townhomes proposed for Blocks 29, 30 and 31 will serve as a transition in densities, buffering the proposed single-detached dwellings to the south and southeast. Lands further to the north and northwest, in the Neighbourhoods and Shopping Area Place Types are currently being developed for, or are planned to accommodate, greater heights and densities appropriate for their location. The height and density proposed for Blocks 29, 30 and 31 will help serve as a transition between the higher density lands to the west and lower density lands to the east.

4.3 Form and Zoning Provisions

As previously noted, the recommended zoning would permit medium density residential development in the form of townhouses and single-detached dwellings, which can be accommodated on the lands. The recommended zoning would facilitate the development of mid-rise development, which aligns with the form identified as appropriate in *The London Plan* and is designed with street and pedestrian orientation in mind to promote connectivity. This connectivity could contribute to walkability to support lands to the northwest and northeast in the Shopping Area and Commercial Industrial Place Types. The Residential R4-6 Zone Variation requires a minimum lot area of 145 square metres per townhouse dwellings unit, while the Residential R6-5 Zone Variation requires a minimum lot area of 850 square metres for cluster townhouse developments. Blocks 29, 30 and 31 of the Draft Plan of Subdivision satisfy these zoning requirements and the subject lands can accommodate the proposed development.

Policies for the street network require the following: the configuration of streets planned for new neighbourhoods will be a grid or modified grid; cul-de-sacs and dead ends will be limited; new neighbourhood streets will be designed to have multiple direct connections to existing and future neighbourhoods; street patterns will be easy and safe to navigate by walking and cycling and will be supportive of transit services; and blocks within a neighbourhood should be of a size and configuration that supports connections to transit and other neighbourhood amenities, typically within a ten minute walk (212, 213, 218 and 228). The proposed subdivision maintains a grid pattern of the surrounding context and will provide connections to adjacent subdivisions. No dead-ends or cul-de-sacs are included in the Draft Plan of Subdivision. The proposed blocks are of a size and configuration that supports connections to transit services in the neighbourhood on Wharncliffe Road South, as well as provide for safe and easy walking and cycling on Wharncliffe Road South. To support walkability, sidewalks shall be located on both sides of all streets (349). The proposed Draft Plan of subdivision includes sidewalks on both sides of Southbridge Avenue and Street A.

The policies relating to buildings promote an active street front at a human scale to support pedestrian activity and safety (285 and 286). The built form, site layout, key entrances and streetscape should be designed to establish a sense of place and character consistent with the planning vision of the place type and the surrounding area (197, 202, 221 and 252). These policies are addressed through the proposed Draft Plan of Subdivision as the requested reduced front yard and exterior side yard setbacks would site the proposed development close to the street to create an active street front at a human scale.

The subject lands are currently zoned Holding Urban Reserve UR6 Special Provision (h-17*h-42*UR6(1)). This Zone is applied to areas where industrial development is anticipated over the long term, and permitted uses include: existing dwellings, agricultural uses, with exceptions conservation lands, managed woodlots, wayside pits, passive recreation uses, farm gate sales, kennels, private outdoor recreation clubs, riding stables, and existing defined industrial uses.

The Applicant has requested zone changes to facilitate residential development, which is consistent with Neighbourhoods Place Type in *The London Plan* and surrounding development. Staff are recommending the following zones: Holding Residential R1 Special Provision (h*h-100*R1-13(7)) Zone; Holding Residential R1 Special Provision (h*h-100*R1-13(_)) Zone; Holding Residential R1 Special Provision (h*h-100*R1-13(_)) Zone; Holding Residential R4 Special Provision (h*h-17*h-18*h-100*h-149*R4-6(_)) Zone; and, a Holding Residential R6 Special Provision (h*h-17*h-18*h-100*h-149*R6-5(_)) Zone.

The Holding Provisions that are proposed to form part of the zone are to ensure the following:

- h: orderly development and adequate provision of municipal services through an approved Development Agreement;
- h-17: orderly development of the lands and adequate provision of municipal services through the provision of full municipal sanitary sewer and water services;
- h-18: the required archaeological studies have been completed and accepted, and any recommendations implemented;
- h-100: there is adequate water services and appropriate access, a looped watermain system must be constructed and a second public access must be available to the satisfaction of the City Engineer; and,
- h-149: sanitary and stormwater servicing reports have been prepared and confirmation that sanitary and stormwater management systems are implemented to the satisfaction of the City Engineer.

The h-42 holding provision, included in the current zoning, is not applicable to the proposed development, and is not included in Staff's recommended zoning.

A number of Special Provision Zones have been requested; they are as follows:

Reduced Lot Frontage 6.7 metres (Minimum) – Blocks 29 and 31

Lot frontage is defined in the Zoning By-law Z.-1 as the horizontal distance between the side lot lines measured along the front lot line but where the front lot line is not a straight line, or where the side lot lines are not parallel, the frontage is to be measured by a line 6 m (19.7 ft.) back from and parallel to the chord of the frontage and for the purpose of this paragraph, the chord of the frontage is a straight line joining the two points where the side lot lines intersect the front lot line. The requested special provision increases the required minimum lot frontage to meet servicing standards.

Reduced Front Yard Setback and Exterior Side Yard Setbacks– Blocks 31 and Lot 28

Front yard setbacks and exterior side yard setbacks are intended to ensure adequate space between buildings and lot lines to ensure there are adequate sight lines, landscaping, space to accommodate future road-widening, should it be required, and ensure there is sufficient separation between new and existing development. A reduced setback of 1.5 meters has been requested for Block 31, and a reduced front yard setback of 3.5 metres has been requested for Lot 28 to reflect the siting of the heritage structure. The requested reduced front and exterior side yard setback helps to facilitate development that is street and pedestrian oriented by helping to establish a strong street edge and an active street front, while still allowing sufficient space for sight lines and landscaping.

Reduced Rear Side Yard Setback – Lots 1 to 28

Rear Yard Setbacks are intended to ensure there is sufficient separation between new and existing development, to potentially mitigate negative impacts while also providing access to the interior yard space. The requested reduced rear yard setback of six metres for Lots 1-17 helps to facilitate development that is street and pedestrian oriented by helping to establish a strong street edge and an active street front, while still allowing sufficient space for sight lines and landscaping, along the future street. The reduced rear yard setback of 4.5 metres for Lot 28 to reflect the existing heritage structure.

Maximum Lot Coverage of 50 per cent – Blocks 29 and 30

Lot coverage is defined as percentage of a lot covered by the first storey of all buildings and structures on the lot including the principal building or structure, all accessory buildings or structures and all buildings or structures attached to the principal building or structure, excluding balconies, canopies and overhanging eaves which are 2.0 metres (6.6 ft.) or more in height above finished grade. The R4-6 Zone Variation sets a maximum of 45 per cent and a special provision for a maximum of 50 per cent has been requested, which is sufficient to ensure the site functions properly.

Maximum Height – Blocks 29 and 30

As previously noted, building heights within the Shopping Area Place Type shall not exceed the standard maximum four (4) storeys. Heights above this, to an upper maximum of six (6) storeys, may be permitted in conformity with the Our Tools policies of this plan relating to Zoning to the Upper Maximum Height (878). *The London Plan* requires applications to exceed the standard maximum height will be reviewed on a site-specific basis and will not require an amendment to the *Plan* (1638). These requests will be reviewed through a site-specific zoning by-law amendment (1640), and will be permitted where the resulting intensity and form represent good planning within its context (1641).

Residential development south of Wharncliffe Road South and north of Bradley Avenue consists of townhouses and single-detached dwelling units at a maximum height of two (2) storeys. The lands at the southwest corner of Bradley Avenue and Morgan Avenue are currently being developed for a twelve (12) storey apartment building, a fourteen (14) storey apartment building, and two (2) storey townhouse dwellings. The requested height of twelve (12) metres for Blocks 29 and 30 are considered an appropriate form that is generally consistent with the existing and proposed future development.

Minimum Density of 30 units per hectare and Maximum Density of 75 units per hectare

The Residential R4-6 Zone Variation does not specify a density maximum as it provides regulations based on one unit per lot, and the Residential 6-5 Zone Variation permits a maximum density of 35 units per hectare. Blocks 29 and 30 of the Draft Plan of Subdivision satisfy these zoning requirements for the R4-6 Zone. A special provision for a minimum density of 30 units per hectare and a maximum of 75 units per hectare has been required for the R6-5 Zone for Block 31. Similar densities have been considered and permitted in the R6-5 Zone Variation, and similar densities are permitted on lands directly to the east and west. The proposed size of Block 31 far exceeds the minimum lot size and is sufficient to accommodate the increased density requested.

Staff support the requested special provisions, discussed above, and they are included in the recommendation.

4.4 Heritage and Archaeology

As previously noted, the subject lands are listed on the *City of London Register of Cultural Heritage Resources* as the Weldwood Farm. The main building was constructed between 1911 and 1918 to support the experimental farm, and is proposed to be maintained as a part of the proposed subdivision.

A separate process to designate the building under the *Ontario Heritage Act* is currently underway, and additional Archaeological Studies are being conducted to assess the lands for any additional archaeological resources.

Conclusion

The applicant has requested an amendment to the Zoning By-law Z.-1 to rezone the property from a Holding Urban Reserve UR6 Special Provision (h-17*h-42*UR6(1)) Zone to a Holding Residential R1 Special Provision (h*h-100*R1-13(7)) Zone; Holding Residential R1 Special Provision (h*h-100*R1-13(_)) Zone; Holding Residential R1 Special Provision (h*h-100*R1-13(_)) Zone; Holding Residential R4 Special Provision (h*h-17*h-18*h-100*h-149*R4-6(_)) Zone; and, a Holding Residential R6 Special Provision (h*h-17*h-18*h-100*h-149*R6-5(_)) Zone.

The development proposal, as recommended by Staff, provides for a mix of housing affordability that will meet the projected requirements of current and future residents, and preserves a historical building in this neighbourhood. The application is consistent with *The London Plan, the Southwest Area Secondary Plan*, and the Zoning By-law Z.-1 to redevelop a vacant and underutilized site with a range of housing options. The recommended zoning and special provisions of the zoning amendment will permit townhouse units and single-detached dwellings that are considered appropriate and compatible with existing and future land uses in the surrounding area. Therefore, staff are satisfied that the proposal represents good planning in the broad public interest and recommend approval of this development application.

Prepared by: Alison Curtis, MA
Planner, Subdivision Planning

Reviewed by: Bruce Page
Manager, Subdivision Planning

Recommended by: Heather McNeely, MICP, RPP
Director, Planning and Development

Submitted by: Scott Mathers, MPA, P.Eng.
Deputy City Manager, Planning and Economic
Development

CC: Peter Kavcic, Manager, Subdivisions and Development Inspections
Michael Pease, Manager, Site Plans
Ismail Abushehada, Manager, Subdivision Engineering

HM//BP/AC/ac
Y:\Shared\DEVELOPMENT SERVICES\4 - Subdivisions\2023\39T-23501 - 1350 Wharncliffe
Road South (AC)

Appendix A – Zoning Bylaw Amendment

Bill No. (number to be inserted by Clerk's Office)
2023

By-law No. Z.-1-

A by-law to amend By-law No. Z.-1 to
rezone an area of land located at 1350
Wharnccliffe Road South

THEREFORE the Municipal Council of The Corporation of the City of London enacts as follows:

1. Schedule "A" to By-law No. Z.-1 is amended by changing the zoning applicable to lands located at 1350 Wharnccliffe Road South as shown on the attached map comprising part of Key Map No. A111, **FROM** a Holding Urban Reserve UR6 Special Provision (h-17*h-42*UR6(1)) Zone **TO** a Holding Residential R1 Special Provision (h*h-100*R1-13(7)) Zone; Holding Residential R1 Special Provision (h*h-100*R1-13(_)) Zone; Holding Residential R1 Special Provision (h*h-100*R1-13(_)) Zone; Holding Residential R4 Special Provision (h*h-17*h-18*h-100*h-149*R4-6(_)) Zone; and, a Holding Residential R6 Special Provision (h*h-17*h-18*h-100*h-149*R6-5(_)) Zone.

2. Section Number 5.4 of the R1 Zone is amended by adding the following Special Provisions:

R1-13(*) 1350 Wharnccliffe Road South

a. Regulations

- i) Front Yard Setback (Minimum) 8.75 m
- ii) Rear Yard Setback (Minimum) 6 m
- iii) Garages shall not project beyond the façade of the dwellings or façade (front face) of any porch, and shall not occupy more than 50% of lot frontage.

R1-13(**) 1350 Wharnccliffe Road South

a. Regulations

- i) Front Yard Setback (Minimum) 3.5 m
- ii) Rear Yard Setback (Minimum) 4.5 m
- iii) Garages shall not project beyond the façade of the dwellings or façade (front face) of any porch, and shall not occupy more than 50% of lot frontage.

3. Section Number 8.4 of the R4 Zone is amended by adding the following Special Provisions:

R4-6(_) 1350 Wharnccliffe Road South

a. Regulations

- i) Lot Frontage (Minimum) – 6.7 m
- ii) Lot Coverage (Maximum) (%) – 50 %
- iii) Height (Maximum) – 12 m
- iv) Garages shall not project beyond the façade of the dwellings or façade (front face) of any porch, and shall not occupy more than 50% of lot frontage.

4. Section Number 10.4 of the R6 Zone is amended by adding the following Special Provisions:

R6-5() 1350 Wharnccliffe Road South

a. Regulations

- i) Front and Exterior Side Yard Setback (Minimum) – 1.5 m
- ii) Density (Minimum) – 30 units per hectare
Density (Maximum) – 75 units per hectare
- iii) Notwithstanding section 10.2, single-detached dwellings shall not be permitted

The inclusion in this By-law of imperial measure along with metric measure is for the purpose of convenience only and the metric measure governs in case of any discrepancy between the two measures.

This By-law shall come into force and be deemed to come into force in accordance with Section 34 of the *Planning Act, R.S.O. 1990, c. P13*, either upon the date of the passage of this by-law or as otherwise provided by the said section.

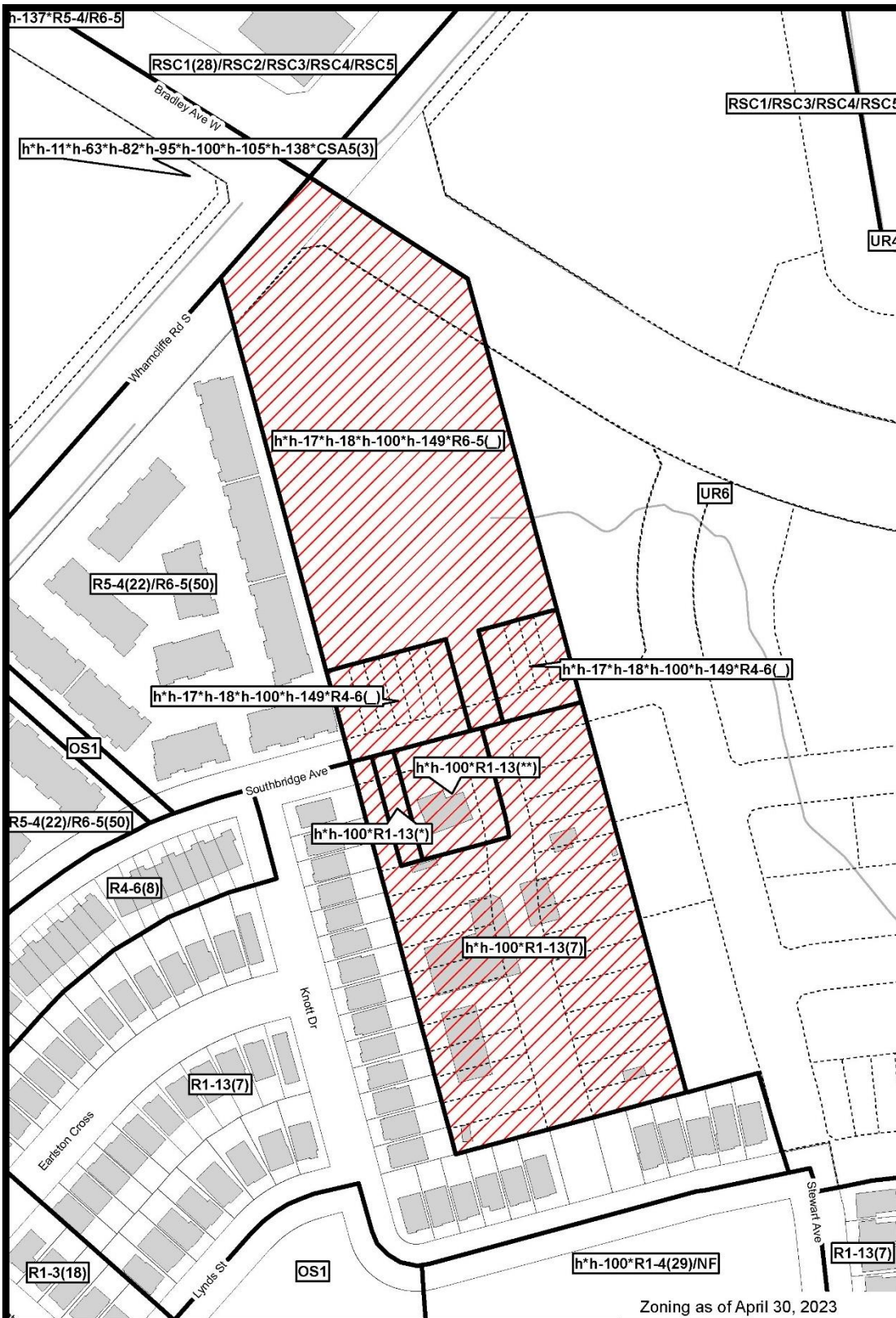
PASSED in Open Council on December 19, 2023

Josh Morgan
Mayor


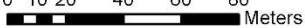

Michael Schulthess
City Clerk

First Reading – December 19, 2023
Second Reading – December 19, 2023
Third Reading – December 19, 2023

AMENDMENT TO SCHEDULE "A" (BY-LAW NO. Z.-1)



Zoning as of April 30, 2023

<p>File Number: Z-9611 Planner: AC Date Prepared: 2023/11/1 Technician: RC By-Law No: Z.-1-</p>	<p>SUBJECT SITE </p> <p>1:2,000</p> <p>0 10 20 40 60 80 Meters </p> <p></p>
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Geodatabase

Appendix B - Site and Development Summary

A. Site Information and Context

Site Statistics

Current Land Use	Residential and Commercial Uses
Frontage	122 metres (400 feet)
Depth	430 metres (1410 feet)
Area	4.05 hectares (10 637 acres)
Shape	Regular (rectangle)
Within Built Area Boundary	No
Within Primary Transit Area	No

Surrounding Land Uses

North	Future Mid-Rise Residential
East	Future Low- and Mid-Rise Residential
South	Low- and Mid-Rise Residential
West	Low- and Mid-Rise Residential

B. Planning Information and Request

Current Planning Information

Current Place Type	Neighbourhoods, Urban Thoroughfare (Bradley Avenue) and Civic Boulevard (Wharnccliffe Road South)
Current Special Policies	N/A
Current Zoning	Holding Urban Reserve UR6 Special Provision (h-17*h-42*UR6(1)) Zone

Requested Designation and Zone

Requested Place Type	Neighbourhoods Place Type
Requested Special Policies	N/A
Requested Zoning	Holding Residential R1 Special Provision (h*h-100*R1-13(7)) Zone; Holding Residential R1 Special Provision (h*h-100*R1-13(_)) Zone; Holding Residential R1 Special Provision (h*h-100*R1-13(_)) Zone; Holding Residential R4 Special Provision (h*h-17*h-18*h-100*h-149*R4-6(_)) Zone; and, a Holding Residential R6 Special Provision (h*h-17*h-18*h-100*h-149*R6-5(_)) Zone

Requested Special Provisions

Lots	Zone String	Special Provisions Requested
Lots 1 to 26	h*h-100*R1-13(7)	<ul style="list-style-type: none"> Existing, see section 5.4 of the R1 Residential Zone
Lot 27	h*h-100*R1-13(_)	<ul style="list-style-type: none"> Minimum front yard setback of 8.75 metres; and, Garages shall not project beyond the façade of the dwellings or façade (front face) of any porch, and shall not occupy more than 50% of lot frontage.
Lot 28	h*h-18*h-100*R1-13(_)	<ul style="list-style-type: none"> Minimum front yard setback of 3.5 metres; Minimum rear yard setback of 4.5 metres; and, Garages shall not project beyond the façade of the dwellings or façade (front

Lots	Zone String	Special Provisions Requested
		face) of any porch, and shall not occupy more than 50% of lot frontage
Blocks 29 and 30	h*h-17*h-18*h-100*h-149*R4-6()	<ul style="list-style-type: none"> • Minimum lot frontage of 6.7 meters; • Maximum lot coverage of 50%; • Maximum height of 12 metres; and, • Garages shall not project beyond the façade of the dwellings or façade (front face) of any porch, and shall not occupy more than 50% of lot frontage.
Block 40	h*h-17*h-18*h-100*h-149*R6-5()	<ul style="list-style-type: none"> • Notwithstanding section 10.2, single-detached dwellings shall not be permitted; • Minimum front and exterior side yard depth of 1.5 metres; and, • Minimum density of 30 units per hectare and a maximum height of 75 units per hectare.

C. Development Proposal Summary

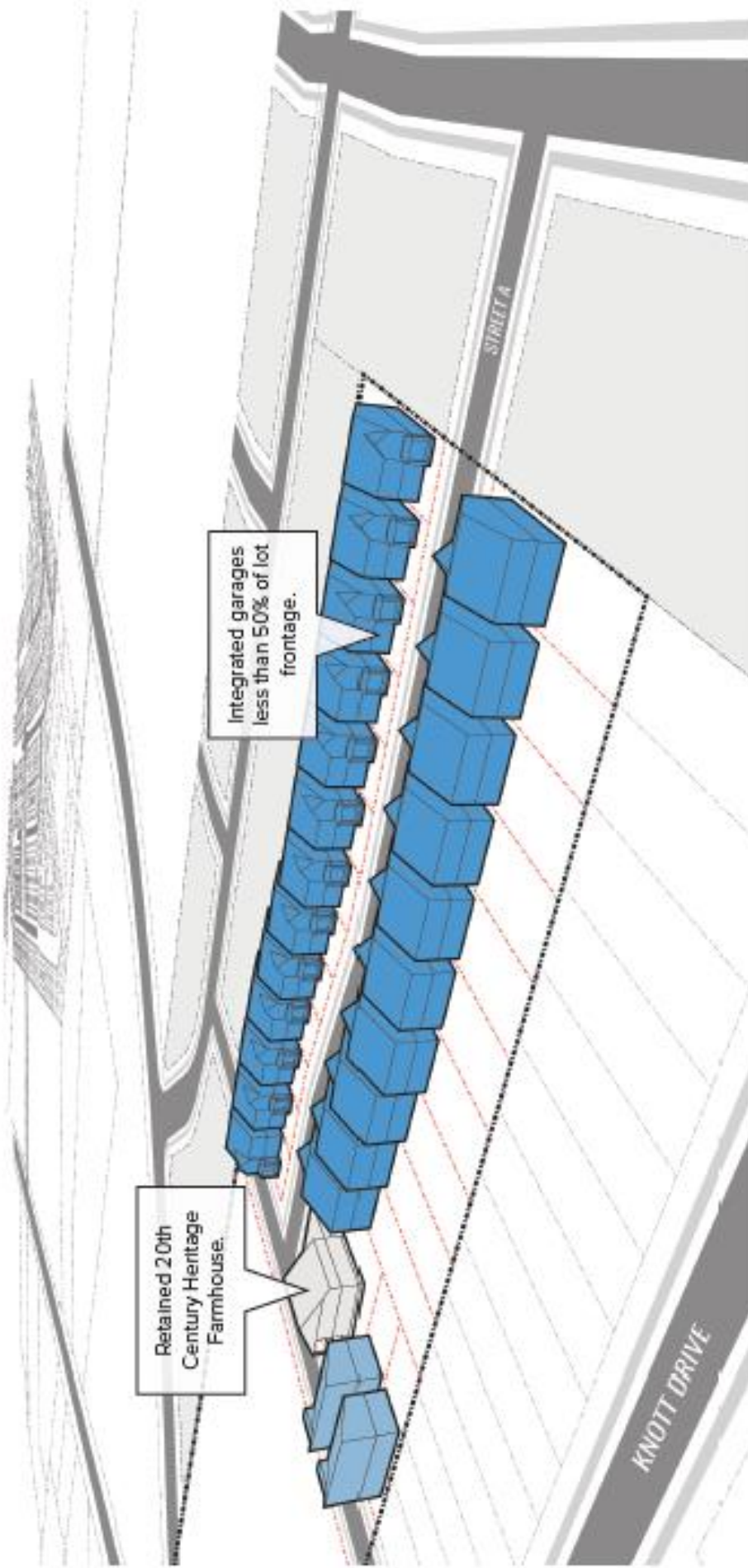
Development Overview

The Draft Plan of Subdivision provides for three (3) medium density blocks, twenty-eight (28) lots for single-detached dwellings, and one (1) block for road widening. Blocks 29 to 31 are proposed for approximately eighty (80) townhouse units in a mixed form, included back-to-back, cluster and freehold street townhouse units. Block 31, which fronts onto both Wharncliffe Road South and the Bradely Avenue Extension, will transition to the low and medium density residential land uses to the south. The heritage building is proposed to be retained as one of the twenty-eight (28) single detached lots. The proposed Draft Plan will be served by the extension of Southbridge Avenue (Neighbourhood Street) and Street A (Neighbourhood Street). Please note that the Draft Plan of Subdivision may be further refined and reviewed prior to Draft Approval.

Proposal Statistics

Land use	Residential
Form	Single-detached, Townhouses, Cluster Townhouses, and Back-to-Back Townhouses
Height	Varies
Residential units	~97
Density	Varies
Parkland	Cash-in lieu

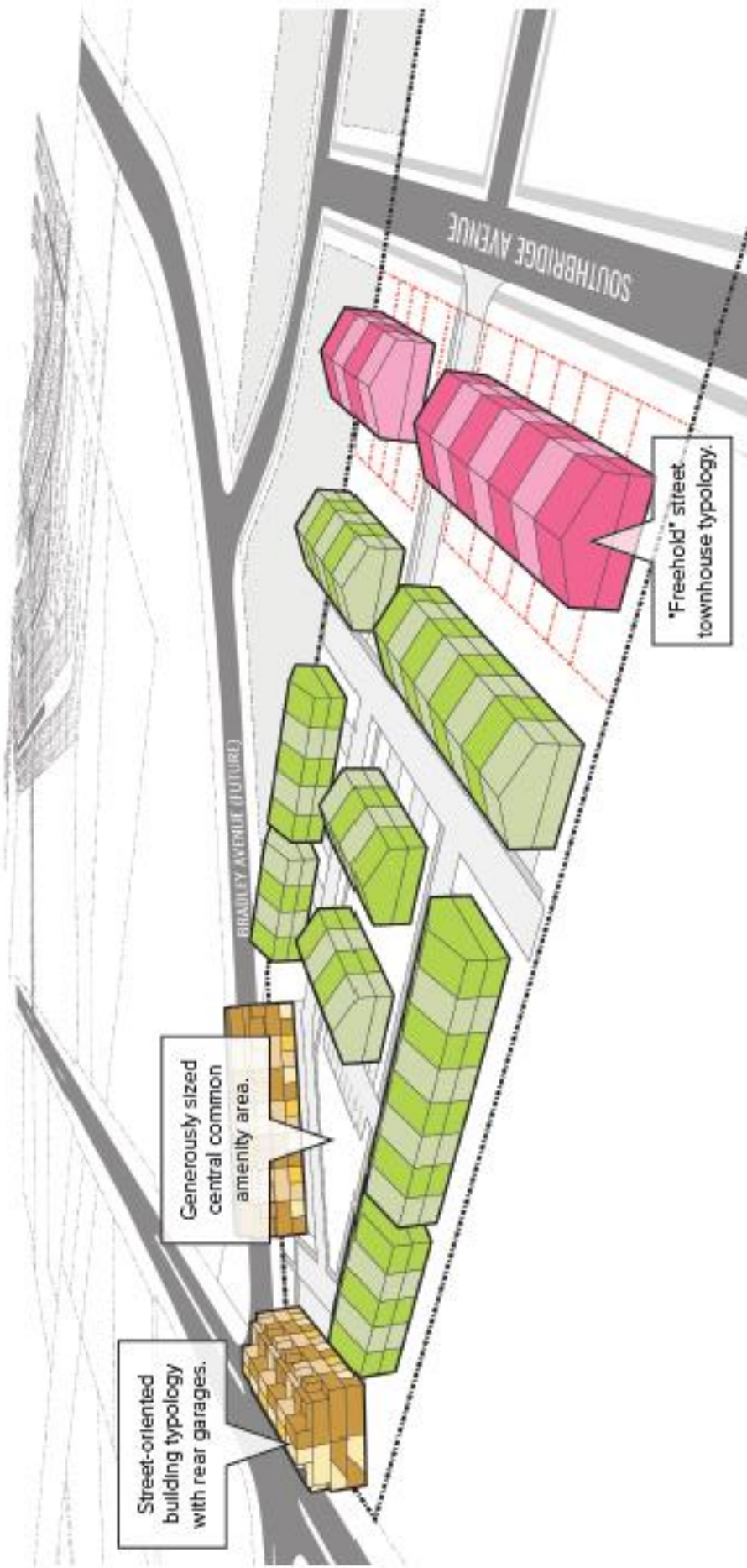
/ Low Density (South of Southbridge Avenue)



- LEGEND**
- Project Site Boundary
 - New Lot Boundaries
 - Future Street 'A'

NOTE: The missing diagrams and illustrations contained within this brief for the are representative of developer's future intention for the built form and site design. They are an illustration of the permissions sought through the proposed Zoning By-law Amendment outlined in Section 6.2 below.

/ Medium Density (North of Southbridge)



- LEGEND**
- █ Project Site Boundary
 - █ New Lot Boundaries
 - █ Future Southbridge Ave

NOTE: The massing diagrams and illustrations contained within this brief for the are representative of developer's future intention for the built form and site design. They are an illustration of the permissions sought through the proposed Zoning By-law Amendment outlined in Section 6.2 below.

Appendix D – Internal and Agency Comments

Internal Department Comments

Parks Planning and Design

Parks Planning and Design staff have reviewed the submitted Notice of Application for draft plan of subdivision and zoning by-law amendments for the above noted development and offer the following comments:

- Parkland dedication for this development is required. The City has no need for parkland within this development. Cash in lieu as per By-law CP-25 will be required for the proposed residential, lots and blocks.

Urban Design

Please find below the Urban Design comments for the Draft Plan of Subdivision at **1350 Wharncliffe Road South (39T-23501)**:

Comments for the Subdivision Design

The following features of the Draft Plan of Subdivision are supported and should be carried forward:

1. The continuation of the existing street pattern in the form of proposed Street 'A' and extension of Southbridge Avenue
2. Sidewalks along both sides of the proposed 'Street A' and 'Southbridge Avenue'
 - Include wide grass boulevards to support tree planting along the streets.
3. The pedestrian connection through Block 40 to the Future Bradley Avenue Extension.
 - Ensure the connection includes a minimum 50% built edge and active uses are oriented towards it, such as windows and wrap around building features such as porches, as opposed to privacy fencing and blank side facades.
4. The orientation of the single-detached lots (1-27) and the townhouse lots (29-39)
5. The transition in height from north along the arterial roads towards the south
 - Consider designing an alternative built form (e.g., mid-rise apartment) adjacent to and oriented towards arterial roads and having a similar level of active façade to the internal side facing the amenity space and the community as opposed to a non-active façade with continuous garages at grade.

Matters for Zoning

Zoning provision for all lots:

1. Corner lots (1, 35 & 36) should be of appropriate size for providing enhanced facades on street-flanking elevations and emphasizing the intersection.
2. Noise walls and non-transparent fencing (i.e., board on board) shall not be permitted around common outdoor amenity spaces and along the street frontages. Refer to the London Plan, Policy 241, Refer to SWASP 20.5.3.9. ii). f)
 - Fencing will be limited to only decorative transparent fencing with a maximum height of 4ft (1.2m) or landscaping with provision for pedestrian access along public streets, amenity spaces and the open space block.
3. Zoning should specify the front yard setback for lot 27 to provide a transition in the form of increasing front yard setbacks from lot 26 to lot 28 and have regard for the listed heritage property. Refer to London Plan, Policy 257

Zoning provision for all blocks:

1. Zoning should address a minimum and maximum setback from the proposed Street A, Southbridge Avenue and the ultimate Right-of-Way of Wharncliffe Road South and Future Bradley Avenue Extension
2. The front façade and primary entrance of dwelling units shall be oriented to adjacent public streets and/or open spaces with direct pedestrian connections to the public sidewalk.
3. Ensure any garages are not the dominant feature in the streetscape by not occupying more than 50% of the unit width and not projecting beyond the façade of the dwelling or the façade of any porch. Refer to The London Plan, Policy 222_A, SWASP 20.5.3.9.iii.e.
4. All medium density blocks shall not permit single detached dwellings.

Matters for Site Plan

General Comments

1. All buildings and dwelling units shall front the highest order street and/or open space with primary entrances and active building elements with enhanced articulation (i.e., windows or openings, porches, canopies, architectural details and materials) along the street and/or open space and direct pedestrian connections to the public sidewalk.
2. The development should maintain and incorporate the existing topography and natural features into the design of the site and the buildings
3. Design of the side elevation of the corner units that is facing a public street, drive aisle or a shared pedestrian access with enhanced detail, such as wrap-around porches and a similar number of windows as is found on the front elevation to establish the same relationship with the street or public realm and offer passive surveillance

Comments for Block 40

1. The following site layout and built form features have been acknowledged and should be carried forward:
 - The gridded internal street pattern with pedestrian connectivity throughout the block and shared connection to the public sidewalk along the arterial roads
 - The principal entrances and direct pedestrian access from Wharncliffe Road South and Bradley Avenue Future extension
 - The transition in height from 4 to 2 storeys from the arterial roads towards the lower intensity residential uses on the south
 - Provision of a centralised amenity space well-connected by pedestrian walkways
 - For providing minimum surface parking on the site
2. Stacked townhouses with below grade units should be avoided along the arterial roads
3. Ensure an enhanced view terminus is created while approaching from Southbridge Avenue into Block 40. Refer to the London Plan, Policy 257, SWASP 20.5.3.3.iv)d)

Condition for the Subdivision Agreement:

1. The owner agrees to register on title and include in all Purchase and Sale Agreements for Lot 1 a requirement that the purchaser/home builder shall provide concept plans and elevations prior to the application for a building permit which demonstrate that both elevations facing the streets (the front and exterior side elevations) are designed as front elevations with entrances facing Street 'A' and with connections to the future public sidewalk. Both elevations should be constructed to have a similar level of architectural details (materials, windows (size and amount) and design features, such as but not limited to porches, wrap-around materials and features, or other architectural elements that provide for street-oriented design) and limited chain link or decorative fencing along no more

than 50% of the exterior side-yard abutting the exterior side-yard frontage, to the satisfaction of the City.

Ecology

SAR Memo and FPR - Ecology Comments

39T-23501 Notice of Application & ZBA (File #Z-9611) – 1350 Wharncliffe Road South

This email is to confirm that a Species At Risk (SAR) and Significant Wildlife Habitat (SWH) Screening Memo dated March 13, 2023, has been reviewed, and accepted as complete. Ecology has no further concerns related to the application.

The following recommendations are to be followed and included in future construction plans as an outcome of the study.

- Avoid tree removal within the active bat roosting period (April 30 – September 1) to reduce potential interactions with Endangered bat species, to avoid contravention of the Endangered Species Act.
- Avoid vegetation removal within the active breeding bird period (April 1 – August 30) to avoid disturbing nesting birds and contravening the Migratory Bird Convention Act.

Landscape Architecture

Landscape Architecture has requested the following conditions be included in the Draft Plan Approval Conditions:

- XX. As part of the Focused Design Studies, the Owner shall have a Tree Preservation Report and Plan prepared for lands within the proposed draft plan of subdivision as required by the Tree Inventory. Tree preservation shall be established prior to grading/servicing design to accommodate maximum tree preservation. The Tree Preservation Report and Plan shall focus on the preservation of quality specimen trees within Lots and Blocks and shall be completed in accordance with the current City of London Guidelines for the preparation of Tree Preservation Reports and Tree Preservation Plans to the satisfaction of the Director, Planning and Economic Development. The Owner shall incorporate the approved Tree Preservation Plan on the accepted grading plans.
- XX. In conjunction with Focused Design Studies, the Owner's qualified consultant shall undertake a Hazard Tree Assessment Study. The study will undertake a tree risk assessment to identify hazard trees or hazardous parts of any trees within falling distance of lot lines and provide recommendations to abate the hazard, to the satisfaction of the City.

Heritage and Archaeology Comments

Heritage

The property at 1350 Wharncliffe Road South is listed on the Register of Cultural Heritage Resources. Due to changes to the *Ontario Heritage Act* as a result of Bill 108, if a "Prescribed Event" takes place on a heritage-listed property, Council has 90-days to issue a Notice of Intention to Designate a property under the *Ontario Heritage Act*. A "Prescribed Event" is defined as a Notice of Application for a Official Plan Amendment, Zoning By-Law Amendment, or Draft Plan of Subdivision. As the Notice of Application for this Draft Plan of Subdivision and ZBA (39T-21501 & Z-9611) was issued on June 1, 2023, the 90-day timeline for the Prescribed Event expires on August 30, 2023. Further, due to changes to the *Ontario Heritage Act* as a result of Bill 23 heritage listed properties may only remain on the Register for a maximum of 2 years after which they must be removed. As the cultural heritage resources of the property are being retained as a part of the proposed development staff will be recommending that Council issue a Notice of Intention to Designate (NOID) the property pursuant to Part IV of the *Ontario Heritage Act*.

Heritage staff have reviewed the following and find the report's evaluation, assessment, and recommendations sufficient to fulfill the heritage impact assessment requirements for this application:

Stantec, *Heritage Impact Assessment: 1350 Wharncliffe Road South, London, Ontario*, March 16, 2023.

Please be advised that heritage planning staff recognize the following recommendations included within the Heritage Impact Assessment.

- Design Guidelines – Incorporate materials to clad new residential that harmonize with the existing main residence which will be conserved in situ. Sympathetic materials include brick. These recommended materials include elements of the existing residence and therefore will be compatible with its overall character and heritage attributes.
 - Staff suggest that the setbacks used for Lot 26 and 27 should ensure a proper transition to the retained heritage dwelling.
- Site Plan Controls – A qualified person(s) should be retained to complete a pre-construction vibration assessment to determine acceptable levels of vibration given the site-specific conditions (including soil conditions, equipment proposed to be used and building characteristics). Should the residence be determined to be within the zone of influence, additional steps should be taken to secure the building from experiencing negative vibration effects.
- Commemoration Plan – Preparation of a Commemoration Plan is recommended to recognize the identified CHVI within the Study Area. The Commemoration Plan should include site-specific history, a landscaping component through plantings, and possible commemoration through the naming of roadways and amenity spaces. Any planting program or commemorative activity should be developed in conjunction with the City of London and follow adherence to crime prevention through environmental design approaches.
 - Staff note that at present “Street A” does not have a proposed street name. Given the history of the property, it may be appropriate to contemplate a named connected to the history of the property such as “Weldwood Avenue”.

Heritage staff agree with the recommendations provided within the HIA.

In addition, heritage staff also recommend that front yard setbacks adjacent to the heritage property be increased in order to provide an appropriate transition from Lots 26-28. Typical infill applications adjacent to heritage properties often utilize an approach that uses setbacks at half of the distance between the existing heritage properties in order to provide a transition to a heritage property. Using this approach, a front yard setback of 4.5m for Lot 26, and a front yard setback of 8.75m for Lot 27 could help to transition to Lot 28 which will have a front yard setback of approximately 13m (see attached example).



The Community Advisory Committee on Planning (CACCP) will be consulted on a staff recommendation designation of this property pursuant to the *Ontario Heritage Act* at their meeting to be held on August 9, 2023.

Archaeology

Please be advised that the City is in receipt of the Stage 1-2 Archaeological Assessment submitted as a part of this application as well as a Stage 1-2 Archaeological Assessment submitted for the Bradley Avenue Extension Detailed Design. Archaeological requirements for this property are currently under review. Under the review is complete, archaeological conditions associated with this property should not be cleared.

Engineering Comments

These conditions represent the consolidated comments of the Planning and Development (engineering) division, the Transportation Planning and Design division, the Sewer Engineering division, the Water Engineering division, and the Stormwater Engineering division.

Zoning By-law Amendment

Planning and Development (engineering) and the above-noted engineering divisions have no objection to the proposed Zoning By-law Amendment for the proposed draft plan of subdivision subject to the following:

1. 'h' holding provision is implemented with respect to servicing, including sanitary, stormwater and water, to the satisfaction of the Deputy City Manager, Environment and Infrastructure and the entering of a subdivision agreement.
2. 'h-100' holding provision is implemented with respect to water services and appropriate access that no more than 80 units may be developed until a looped watermain system is constructed and there is a second public access is available, to the satisfaction of the Deputy City Manager, Environment and Infrastructure.
3. Holding provision is implemented until the regional stormwater management pond is constructed and operational for the north portion on lands to the east.
6. A holding provision shall be implemented on the R4-6 zone (street townhouse) until the Deputy City Manager, Environment and Infrastructure is satisfied with the servicing arrangements to provide adequate separation between services and avoid conflicts with City services. A minimum lot frontage of 6.7 metres as per SW-

- 7.1 will be required to accommodate street townhouses within this draft plan of subdivision.
7. A holding provision on lands north of Southbridge Avenue (Block 40) and including Southbridge Avenue until lands to the east develop for servicing and White Oak SWMF P3 is available.

Required Revisions to the Draft Plan

Note: Revisions are required to the draft plan as follows:

- i) Identify road widening on Wharncliffe Road, if necessary, as per the draft plan conditions.
- ii) Remove sidewalk locations from the draft plan; these are identified in the conditions.
- iii) Remove pavement widths from the draft plan; these are identified in the conditions.
- iv) Identify the road centreline radii on Street 'A'.
- v) Revise Street 'A' right-of-way width to taper over 30 metres from 19.0 metres at the southern limit of this plan to a width of 20.0 metres.
- vi) Provide 3m x 3m daylighting triangle at the intersection of Street A and Southbridge Avenue
- vii) Identify frontage of Lot 15.
- viii) Revise all lots that are zoned as street townhouses (Lots 29 to 39) to be medium density Blocks.
- vii) The Owner shall align the right-of-way of Southbridge Avenue in this plan with future Southbridge Avenue to the east and west of this plan, to the satisfaction of the Deputy City Manager, Environment and Infrastructure.
- viii) Revise right-of-way widths, tapers, bends, intersection layout, daylighting triangles, etc., and include any associated adjustments to the abutting lots, if necessary.

Note that any changes made to this draft plan will require a further review of the revised plan prior to any approvals as the changes may necessitate revisions to our comments.

External Agency Comments

Bell Canada

We have reviewed the circulation regarding the above noted application. The following paragraphs are to be included as a condition of approval:

Bell Canada Condition(s) of Approval

- 1) The Owner acknowledges and agrees to convey any easement(s) as deemed necessary by Bell Canada to service this new development. The Owner further agrees and acknowledges to convey such easements at no cost to Bell Canada.
- 2) The Owner agrees that should any conflict arise with existing Bell Canada facilities where a current and valid easement exists within the subject area, the Owner shall be responsible for the relocation of any such facilities or easements at their own cost.

Upon receipt of this comment letter, the Owner is to provide Bell Canada with servicing plans/CUP at their earliest convenience to planninganddevelopment@bell.ca to confirm the provision of communication/telecommunication infrastructure needed to service the development.

It shall be noted that it is the responsibility of the Owner to provide entrance/service duct(s) from Bell Canada's existing network infrastructure to service this development. In the event that no such network infrastructure exists, in accordance with the Bell Canada Act, the Owner may be required to pay for the extension of such network infrastructure.

If the Owner elects not to pay for the above noted connection, Bell Canada may decide not to provide service to this development.

Concluding Remarks:

To ensure that we are able to continue to actively participate in the planning process and provide detailed provisioning comments, we note that we would be pleased to receive circulations on all applications received by the Municipality and/or recirculations.

If you believe that these comments have been sent to you in error or have questions regarding Bell's protocols for responding to municipal circulations and enquiries, please contact planninganddevelopment@bell.ca directly.

We note that WSP operates Bell Canada's development tracking system, which includes the intake and processing of municipal circulations. **However, all responses to circulations and requests for information, such as requests for clearance, will come directly from Bell Canada, and not from WSP.** WSP is not responsible for Bell's responses and for any of the content herein.

UTRCA

London Hydro

Servicing the above proposal should present no foreseeable problems, any new and/or relocation of existing infrastructure will be at the applicant's expense, maintaining safe clearances from L.H. infrastructure is mandatory. A blanket easement will be required. **Note:** Transformation lead times are minimum 16 weeks. Contact Engineering Dept. to confirm requirements and availability.

London Hydro has no objection to this proposal or possible official plan and/or zoning amendment. However, London Hydro will require a blanket easement.

Hydro One

We are in receipt of your Plan of Subdivision application, 39T-23501 dated July 18th, 2023. We have reviewed the documents concerning the noted Plan and have no comments or concerns at this time. **Our preliminary review considers issues affecting Hydro One's 'High Voltage Facilities and Corridor Lands' only.**

For proposals affecting 'Low Voltage Distribution Facilities' the Owner/Applicant should consult their local area Distribution Supplier. Where Hydro One is the local supplier the Owner/Applicant must contact the Hydro subdivision group at subdivision@Hydroone.com or 1-866-272-3330.

Enbridge Gas (Union Gas)

Thank you for your correspondence with regards to draft plan of approval for the above noted project.

It is Enbridge Gas Inc.'s request that as a condition of final approval that the owner/developer provide to Enbridge the necessary easements and/or agreements required by Enbridge for the provision of gas services for this project, in a form satisfactory to Enbridge.

Should you require any further information, please contact the undersigned.

Appendix E – Public Engagement

Community Engagement

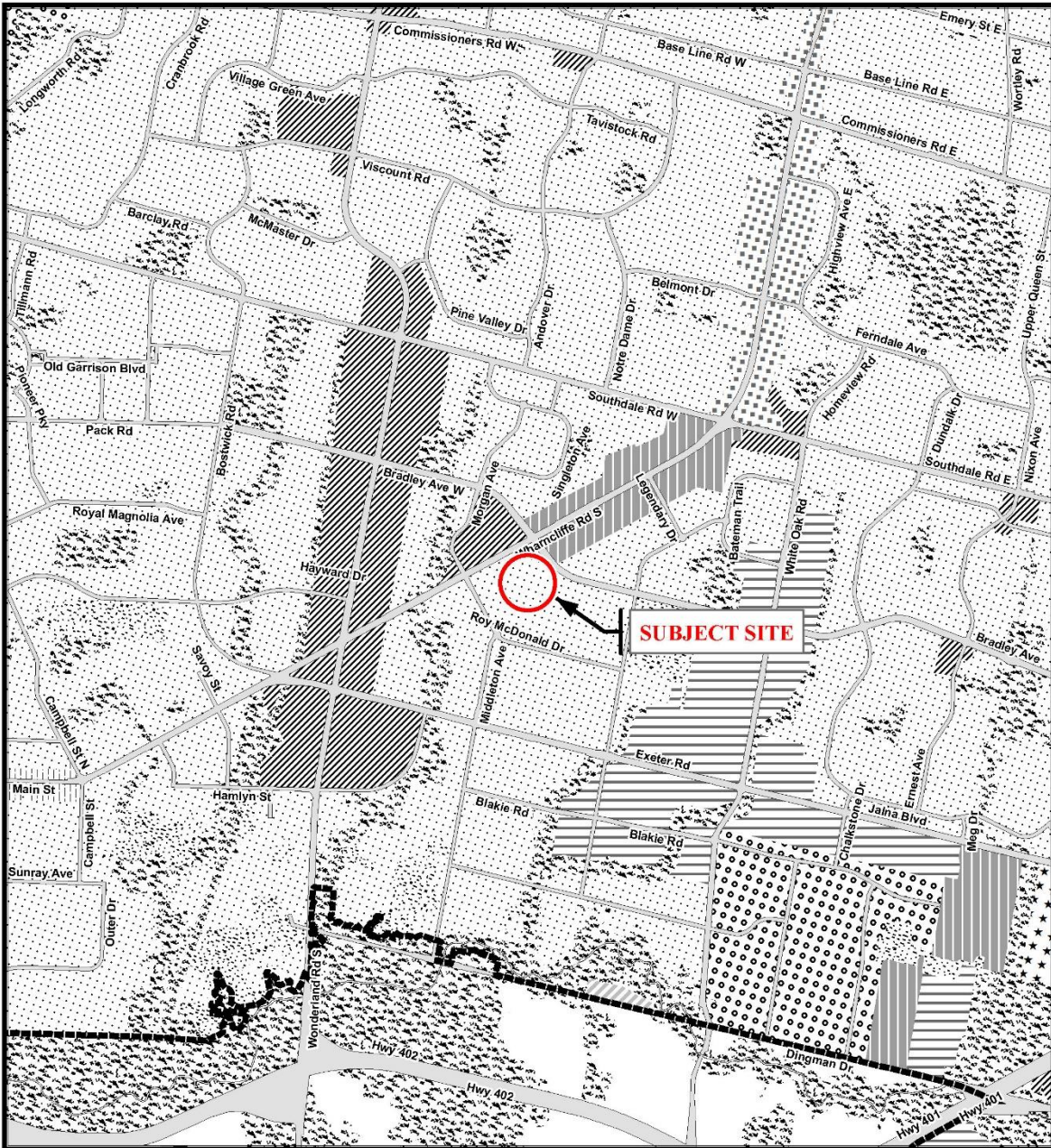
Public Liaison: Information regarding the requested Zoning By-law Amendment application and opportunities to provide comments were provided to the public as follows:

- Notice of Public Participation Meeting was sent to property owners within 120 metres of the subject property and on published in the Public Notices and Bidding Opportunities section of The Londoner November 14, 2023.
- Notice of Application was sent to property owners within 120 metres of the subject property on July 9, 2023, 2022.
- Information about the Application were posted on the website on July 8, 2023.

Londoner Notice: 1350 Wharncliffe Road South; located northeast of Middleton Avenue and south of Bradley Avenue – The purpose and effect of this application is to consider a proposed Draft Plan of Subdivision and Zoning By-law Amendment to allow for a residential subdivision. **Draft Plan of Subdivision** - Consideration of a proposed Draft Plan of Subdivision to allow for: twenty-eight (28) single detached lots, eleven (11) lots for townhouse units and one (1) medium density block, one (1) reserve block, and two (2) streets (Southbridge Avenue and Street A). **Zoning By-law Amendment** – Consideration of an amendment to the Zoning By-law Z.-1 to change the zoning from Holding Urban Reserve (h-17*h-42*UR6(1)), to: Residential R1 Special Provision (R1-13(7)) Zone for Lots 1 through 28 to permit single-detached lots with a minimum lot area of 270 square metres and a minimum lot frontage of 9 metres; Residential R4 Special Provision (R4-6(_)) Zone for lots 29 through 39 to permit street townhouses with special provisions for minimum lot frontage of 6.7 meters, a maximum lot coverage of 50 per cent, and a height of 12 metres; and, Residential R6 Special Provision (R6-5(_)) Zone to permit cluster housing with special provisions for a minimum front and exterior side yard of 1.5 meters, and a minimum density of 30 units per hectare and a maximum density of 75 units per hectare. The City may also consider applying Holding Provisions in the zoning.

File: 39T-22501 & OZ-9502 Planner: A. Curtis x. 4497

Appendix F – The London Plan and Zoning By-law Excerpts



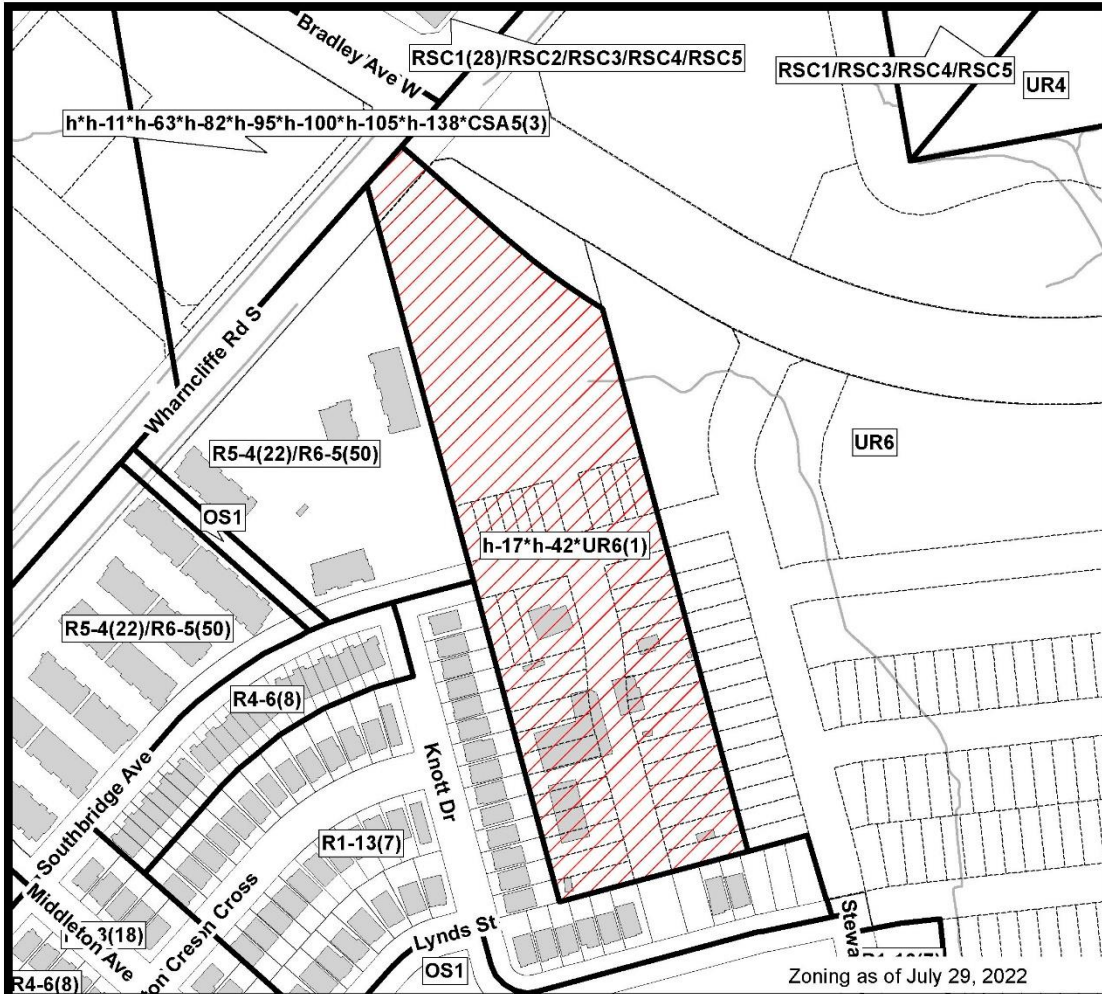
Legend

- | | | |
|------------------------|--------------------------|---|
| Downtown | Future Community Growth | Environmental Review |
| Transit Village | Heavy Industrial | Farmland |
| Shopping Area | Light Industrial | Rural Neighbourhood |
| Rapid Transit Corridor | Future Industrial Growth | Waste Management Resource Recovery Area |
| Urban Corridor | Commercial Industrial | Urban Growth Boundary |
| Main Street | Institutional | |
| Neighbourhood | Green Space | |

This is an excerpt from Planning & Development's working consolidation of Map 1 - Place Types of the London Plan, with added notations.

<p>CITY OF LONDON Official Plan</p> <p>LONDON PLAN MAP 1 - PLACE TYPES -</p> <p>PREPARED BY: Planning & Development</p>	<p>Scale 1:30,000</p> <p>Meters</p>	<p>File Number: Z-9611</p> <p>Planner: AC</p> <p>Technician: RC</p> <p>Date: 2023/5/29</p>
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Project Location: E:\Planning\Projects\p_officialplan\workconsol00\excerpts_LondonPlan\mxds\Z-9611_Map1_PlaceTypes.mxd



COUNCIL APPROVED ZONING FOR THE SUBJECT SITE: R1-11/AG1/OS4

1) *LEGEND FOR ZONING BY-LAW Z-1*

- | | |
|---|-----------------------------------|
| R1 - SINGLE DETACHED DWELLINGS | RF - REGIONAL FACILITY |
| R2 - SINGLE AND TWO UNIT DWELLINGS | CF - COMMUNITY FACILITY |
| R3 - SINGLE TO FOUR UNIT DWELLINGS | NF - NEIGHBOURHOOD FACILITY |
| R4 - STREET TOWNHOUSE | HER - HERITAGE |
| R5 - CLUSTER TOWNHOUSE | DC - DAY CARE |
| R6 - CLUSTER HOUSING ALL FORMS | OS - OPEN SPACE |
| R7 - SENIOR'S HOUSING | CR - COMMERCIAL RECREATION |
| R8 - MEDIUM DENSITY/LOW RISE APTS. | ER - ENVIRONMENTAL REVIEW |
| R9 - MEDIUM TO HIGH DENSITY APTS. | |
| R10 - HIGH DENSITY APARTMENTS | OB - OFFICE BUSINESS PARK |
| R11 - LODGING HOUSE | LI - LIGHT INDUSTRIAL |
| DA - DOWNTOWN AREA | GI - GENERAL INDUSTRIAL |
| RSA - REGIONAL SHOPPING AREA | HI - HEAVY INDUSTRIAL |
| CSA - COMMUNITY SHOPPING AREA | EX - RESOURCE EXTRACTIVE |
| NSA - NEIGHBOURHOOD SHOPPING AREA | UR - URBAN RESERVE |
| BDC - BUSINESS DISTRICT COMMERCIAL | |
| AC - ARTERIAL COMMERCIAL | AG - AGRICULTURAL |
| HS - HIGHWAY SERVICE COMMERCIAL | AGC - AGRICULTURAL COMMERCIAL |
| RSC - RESTRICTED SERVICE COMMERCIAL | RRC - RURAL SETTLEMENT COMMERCIAL |
| CC - CONVENIENCE COMMERCIAL | TGS - TEMPORARY GARDEN SUITE |
| SS - AUTOMOBILE SERVICE STATION | RT - RAIL TRANSPORTATION |
| ASA - ASSOCIATED SHOPPING AREA COMMERCIAL | |
| OR - OFFICE/RESIDENTIAL | "h" - HOLDING SYMBOL |
| OC - OFFICE CONVERSION | "D" - DENSITY SYMBOL |
| RO - RESTRICTED OFFICE | "H" - HEIGHT SYMBOL |
| OF - OFFICE | "B" - BONUS SYMBOL |
| | "T" - TEMPORARY USE SYMBOL |

CITY OF LONDON

PLANNING SERVICES / DEVELOPMENT SERVICES

**ZONING
BY-LAW NO. Z.-1
SCHEDULE A**



THIS MAP IS AN UNOFFICIAL EXTRACT FROM THE ZONING BY-LAW WITH ADDED NOTATIONS

FILE NO:

Z-9611

AC

MAP PREPARED:

2023/05/26

RC

1:3,000

0 15 30 60 90 120

Meters

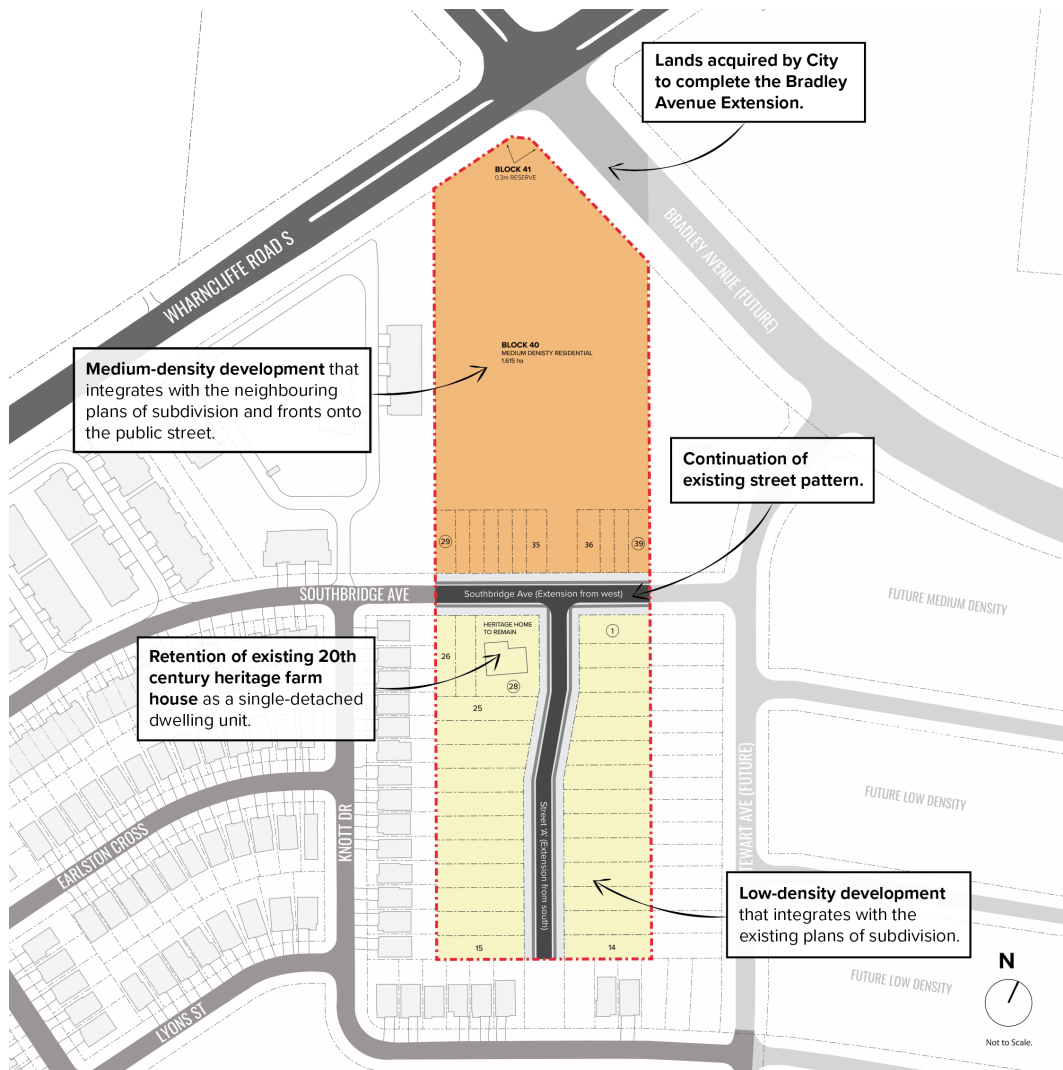
1350 WHARNCLIFFE ROAD S.

PROJECT SUMMARY

www.siv-ik.ca/1350ws | **Developer:** Royal Premier Developments

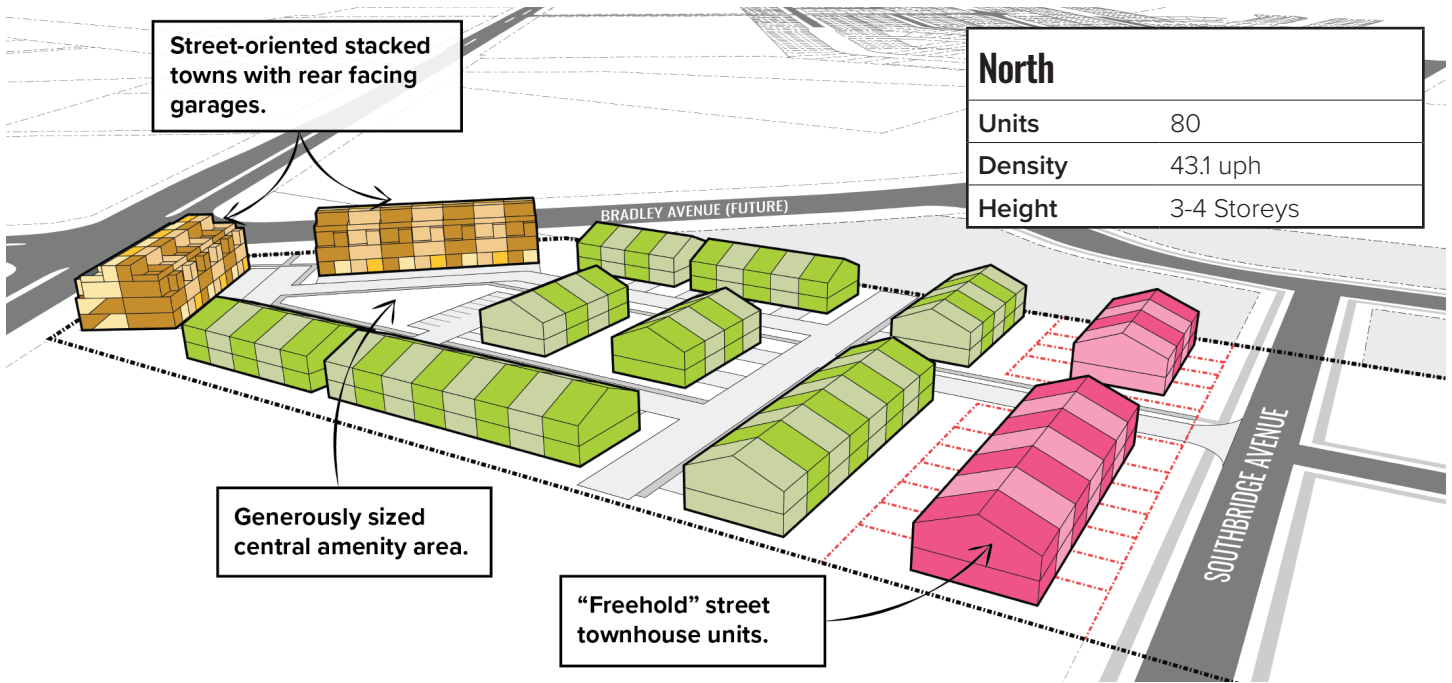
Subdivision At-A-Glance

Royal Premier Developments is the developer of this 3.74-hectare block of land in the actively developing Central Longwoods neighbourhood in southwest London. The plan seeks to “infill” a remnant gap in the area subdivision pattern, thereby “completing” this important segment of the Central Longwoods Neighbourhood. The plan centers around minor extensions of two existing local streets which create a block/lot framework for the development of single detached dwellings on the south portion of the site (south of Southbridge Avenue), street townhouses facing the north side of Southbridge Avenue and a Medium Density Residential block adjacent to the intersection of Wharncliffe Road S. and Bradley Avenue. The plan has been designed to integrate seamlessly with its surroundings and preserve the original Weldwood Farmhouse as a focal point for the project.

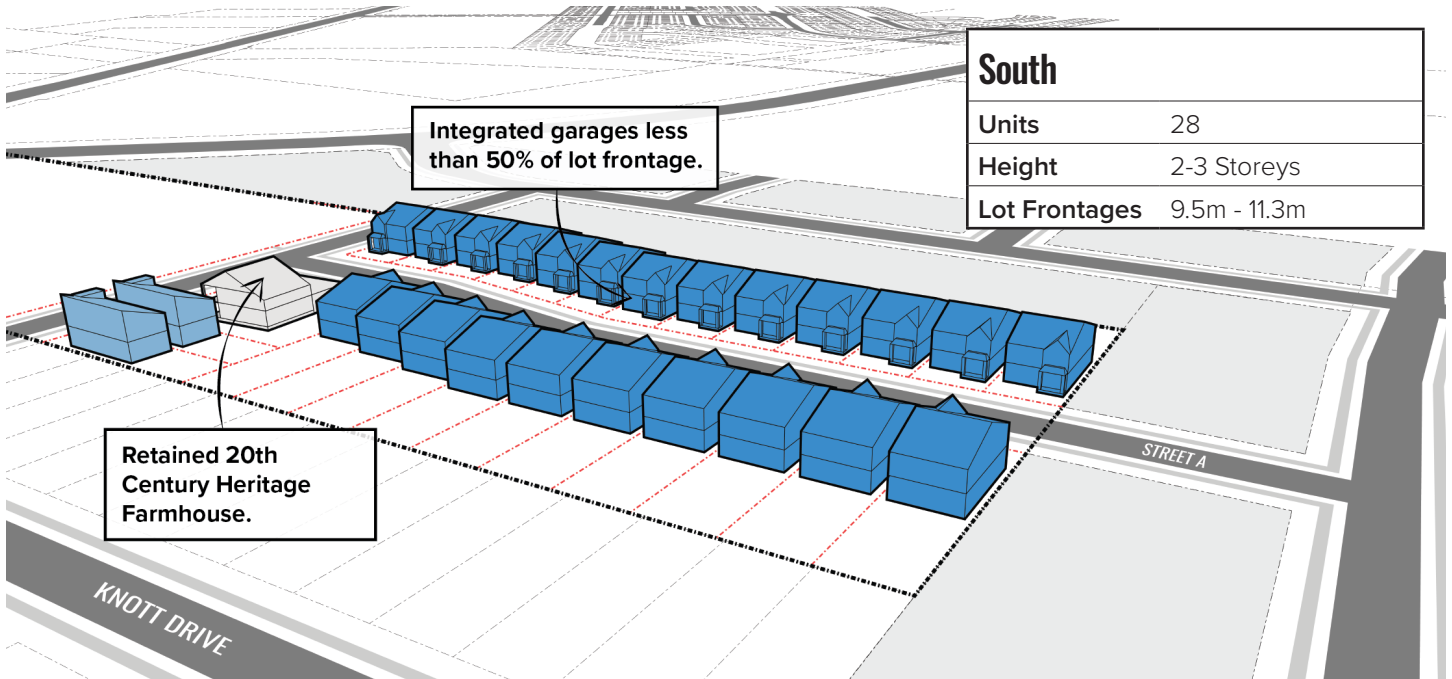


Development Concept

North of Southbridge Avenue



South of Southbridge Avenue



Contact Us

www.siv-ik.ca | info@siv-ik.ca

Report to Planning and Environment Committee

To: Chair and Members
Planning and Environment Committee

From: Scott Mathers, MPA, P.Eng.
Deputy City Manager, Planning and Economic Development

Subject: Cadillac Fairview Corporation Ltd.
1680 Richmond Street
File Number: Z-9667, Ward 5
Public Participation Meeting

Date: December 4, 2023

Recommendation

That, on the recommendation of the Director, Planning and Development, the following action be taken with respect to the application of Cadillac Fairview Corporation Ltd. relating to the property located at 1680 Richmond Street. The proposed by-law attached hereto as Appendix "A" **BE INTRODUCED** at the Municipal Council meeting December 19, 2023 to amend Zoning By-law No. Z.-1, in conformity with the Official Plan, The London Plan, to amend the zoning of the subject property **FROM** a Regional Shopping Area Special Provision RSA1(1) **TO** a Regional Shopping Area Special Provision RSA1(1) Zone.

IT BEING NOTED, that the above noted amendment is being recommended for the following reasons:

- i) The recommended amendment is consistent with the *Provincial Policy Statement, 2020*;
- ii) The recommended amendment conforms with the policies of The London Plan, including but not limited to the Key Directions and the Transit Village Place Type
- iii) The recommended amendment would provide access to automobile sales boutique in a convenient and accessible location.

Executive Summary

Summary of Request

The applicant has requested an amendment to the Zoning By-law Z.-1 to permit an *Automobile Sales Boutique* as a permitted use on the subject lands.

Staff are recommending approval of the requested Zoning Bylaw amendment.

Purpose and the Effect of Recommended Action

The recommended action is to rezone the subject site to permit an *Automobile Sales Boutique* as an additional permitted use under the RSA1(1) Zone. The proposed use would occupy a vacant unit within the enclosed shopping centre known as CF Masonville Place.

Rationale of Recommended Action

1. The recommended amendment is consistent with the *PPS 2020*;
2. The recommended amendment conforms to *The London Plan*, including, but not limited to the Place Type and most relevant sections; and
3. The recommended amendment would provide access to an automobile sales boutique in a convenient and accessible location to meet the needs of the community.

Linkage to the Corporate Strategic Plan

This recommendation supports the following Strategic Areas of Focus:

- **Economic Growth, Culture, and Prosperity** by supporting small and growing businesses, entrepreneurs and non-profits to be successful.

Analysis

1.0 Background Information

1.1 Previous Reports Related to this Matter

Z-8652 - October 2016

1.2 Planning History

In 2016 the subject site was rezoned to permit a “Place of Entertainment” in association with an “Amusement Games Establishment” use in an existing regional shopping area centre.

The subject site is located in the Masonville Secondary Plan study area. The Masonville Secondary Plan was approved on June 29, 2022, with all items not under appeal coming into force and effect. The final version with all appeals settled came into effect on July 11, 2023.

1.3 Property Description and Location

The subject site is located in the north end of the City of London, at the south-east corner of Fanshawe Park Road East and Richmond Street, within the Masonville planning district. The subject site has an area of 19.45 hectares, with the enclosed shopping center centrally located within the property. CF Masonville Place shopping center has operated on the subject site since its opening in 1985, and contains 130 retail stores, restaurants and social amenities. The building housing the shopping center has 61,591 square metres of leasable area, with 3,461 parking spaces which are contained in a number of surface parking areas surrounding the existing building. The subject site also provides for a significant transit terminal (bus) within the northwestern portion of the site, connecting a number of bus routes providing service across the City of London.

Additional shopping amenities are located to the immediate north, east and west of the subject site with residential neighbourhood parks and green spaces in the surrounding area. Designated and shared cycle lanes are located along both Fanshawe Park Road East, and Richmond Street.

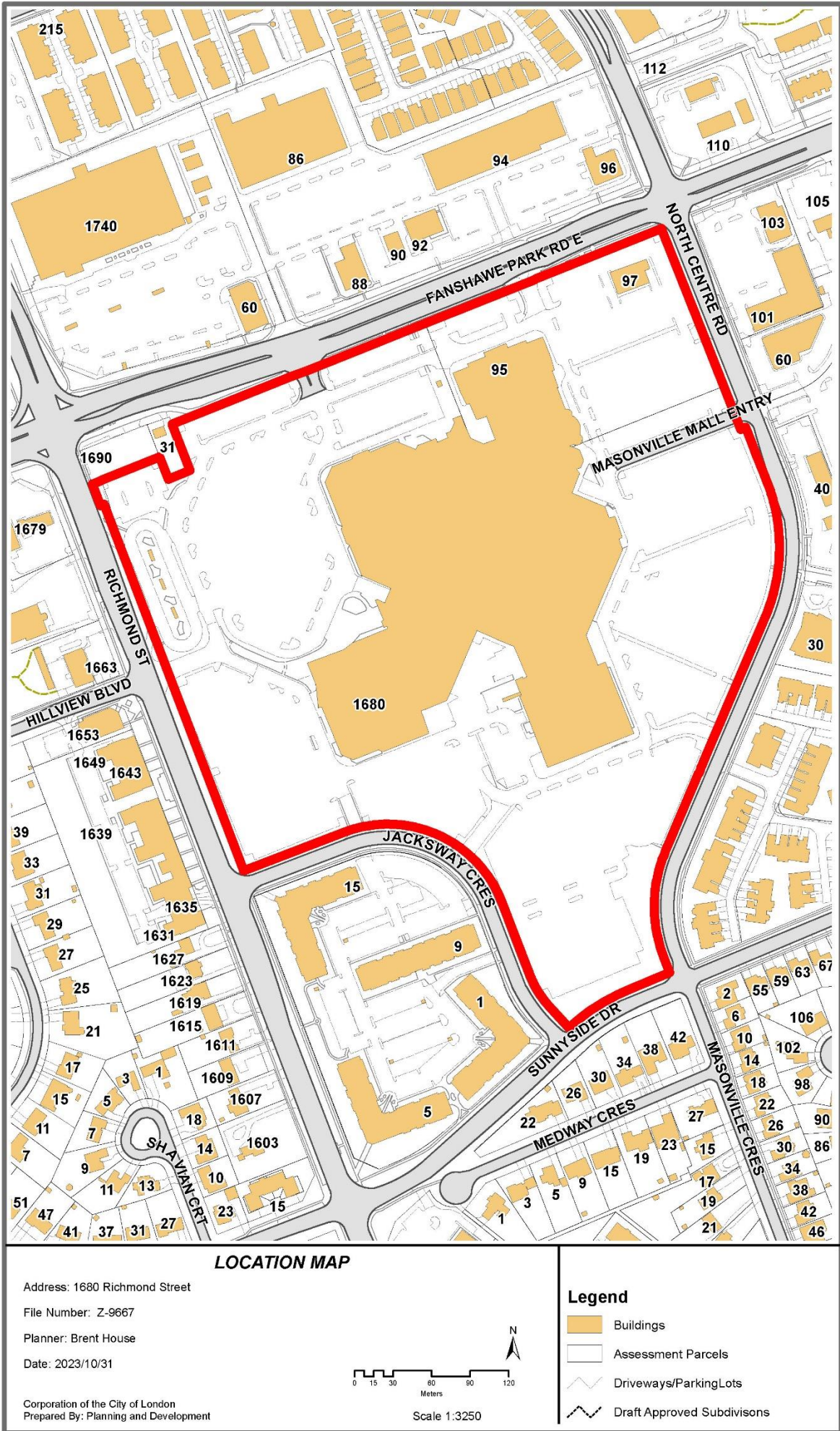


Figure 1. Aerial Photo of 1680 Richmond and surrounding lands.



Figure 2. Streetview of the subject lands (facing east from Richmond Street).

Site Statistics:

- Current Land Use: Regional Shopping Centre
- Frontage: 323 metres (1059.7 feet)
- Depth: 489.3metres (1605.3 feet)
- Area: 19.45 hectares (48.06 Acres)
- Shape: Irregular
- Located within the Built Area Boundary: Yes
- Located within the Primary Transit Area: Yes

Surrounding Land Uses:

- North: to the north across Fanshawe Park Road East is a commercial plaza containing stand-alone commercial buildings including “big box” retailers with associated surface parking lots. Further to the north is medium and high-density residential development and a residential retirement complex.
- East: to the east across North Centre Road is a commercial plaza containing smaller scale strip plaza retailers and restaurants as well as larger format “big box” retailers with associated surface parking lots. Beyond the commercial plaza are single detached residential dwellings, generally in cul-de-sac formations.
- South: to the immediate south is medium and low-density residential development.
- West: to the west, across Richmond Street is a seven-storey residential building, along with single detached dwellings fronting Richmond Street to the southwest. To the north-west is a commercial plaza and public school. Further to the north-west is a commercial plaza containing “big box” retailers beyond which is higher density residential development and townhomes.

Existing Planning Information:

- Existing The London Plan Place Type: Transit Village
- Existing Special Policies: Masonville Secondary Plan
- Existing Zoning: RSA1(1)

Additional site information and context is provided in Appendix “B”.

2.0 Discussion and Considerations

2.1 Development Proposal

The recommended action will permit the establishment of an *Automobile Sales Boutique* as an additional permitted use under the RSA1(1) Zone on the Subject site at 1680

Richmond Street, within an existing vacant unit of the enclosed shopping centre known as CF Masonville Place (the “subject site”).

The proposed development includes the following features:

- Land use: Regional Shopping Area
- Form: CF Masonville Place
- Gross floor area: 61,591 m²
- Building coverage: 32%
- Parking spaces: 3,461 surface parking spaces

Additional information on the development proposal is provided in Appendix “B”.

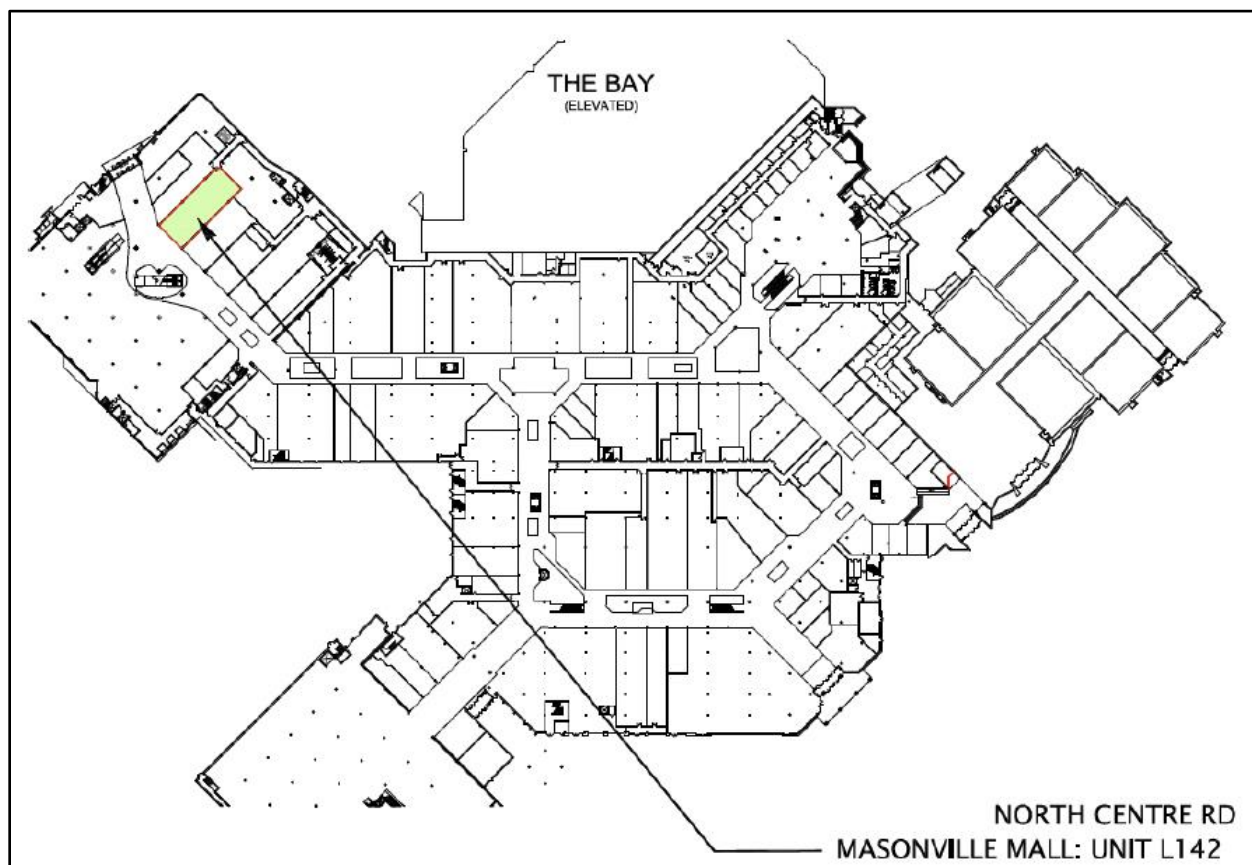


Figure 3 - Conceptual Site Plan (2023)

Additional plans and drawings of the development proposal are provided in Appendix “C”.

2.2 Requested Amendment

The applicant has requested an amendment to the Zoning Bylaw Z.-1 to add an additional special provision to the current RSA1(1) Zone. The request will permit *Automobile Sales Boutique* as a permitted use under the RSA1(1) Zone.

2.3 Internal and Agency Comments

The application and associated materials were circulated for internal comments and public agencies to review. Comments received were considered in the review of this application and are addressed in Section 4.0 of this report.

There were no key issues identified by staff and agencies.

Detailed internal and agency comments are included in Appendix “D” of this report.

2.4 Public Engagement

On October 30, 2023, Notice of Application was sent to 464 property owners and residents in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on November 2nd. A “Planning Application” sign was also placed on the site.

There were zero responses received during the public consultation period.

2.5 Policy Context

The Planning Act and the Provincial Policy Statement, 2020

The Provincial planning policy framework is established through the *Planning Act* (Section 3) and the *Provincial Policy Statement, 2020 (PPS)*. The *Planning Act* requires that all municipal land use decisions affecting planning matters shall be consistent with the *PPS*.

The mechanism for implementing Provincial policies is through the Official Plan, *The London Plan*. Through the preparation, adoption and subsequent Ontario Land Tribunal (OLT) approval of *The London Plan*, the City of London has established the local policy framework for the implementation of the Provincial planning policy framework. As such, matters of provincial interest are reviewed and discussed in *The London Plan* analysis below.

As the application for a Zoning By-law amendment complies with *The London Plan*, it is staff’s opinion that the application is consistent with the *Planning Act* and the *PPS*.

The London Plan, 2016

The London Plan (TLP) includes evaluation criteria for all planning and development applications with respect to use, intensity and form, as well as with consideration of the following (TLP 1577-1579):

1. Consistency with the Provincial Policy Statement and all applicable legislation.
2. Conformity with the Our City, Our Strategy, City Building, and Environmental policies.
3. Conformity with the Place Type policies.
4. Consideration of applicable guideline documents.
5. The availability of municipal services.
6. Potential impacts on adjacent and nearby properties in the area and the degree to which such impacts can be managed and mitigated.
7. The degree to which the proposal fits within its existing and planned context.

Staff are of the opinion that all the above criteria have been satisfied.

Masonville Secondary Plan

The purpose of the *Masonville Secondary Plan* is to establish a vision, principles, and detailed policies for the *Masonville Secondary Plan* area that provide a consistent framework to evaluate future developments and public realm improvements. The intent of the policies is to provide direction and guidance to ensure the Secondary Plan area continues to evolve into a vibrant, connected and mixed-use community that enhances the human-scale quality of streetscapes, and integrates new and existing development, people and open spaces in a compatible and cohesive way.

The *Masonville Secondary Plan* has been reviewed in its entirety and it is staff’s opinion that the proposed Zoning Bylaw amendment is consistent with it.

3.0 Financial Impact/Considerations

None

4.0 Key Issues and Considerations

4.1 Land Use

The London Plan identifies that part of the vision for the Transit Village Place Type is to become high-density mixed-use urban neighbourhood connected by rapid transit to the Downtown and each other. They will be occupied by extensive retail and commercial services contributing to complete communities (The London Plan, 806). Transit Villages are major mixed-use destinations with centrally located rapid transit stations (The London Plan, 807).

The subject site is located within the Transit Village Place Type on Map 1 fronting a Rapid Transit Boulevard on Map 3, in The London Plan. The Transit Village Place Type contemplates a wide range of residential, retail, service, office, cultural, institutional, hospitality, entertainment, and recreational uses. The proposed automobile sales boutique will help contribute to the vision of the Transit Village and the use will align with the range of permitted uses contemplated within the Transit Village Place Type.

4.2 Intensity

The existing regional shopping centre and associated surface parking are in keeping with the intensity contemplated in The London Plan which requires a minimum of two storeys in height and the efficient use of infrastructure and services (The London Plan, 813.1-2). Given no new development, no exterior changes and no changes to the site layout are proposed as part of this zoning application, staff are satisfied that the subject site continues to be an appropriate shape and size to accommodate the existing and proposed new uses. The automobile sales boutique is not anticipated to have any negative impacts on the surrounding context.

4.3 Form

No additional development or site alterations are contemplated as part of this application.

4.4 Zoning

The subject lands are zoned Regional Shopping Area Special Provision (RSA1(1)) in the City of London's Zoning By-law. The RSA1(1) Zone permits a wide range of regional-scale, specialized, comparison-shopping retail and personal service uses, as well as some office, commercial recreation and community facilities uses, which are suited to a location within an enclosed shopping centre building. The applicant has requested to add an automobile sales boutique as an additional permitted use under the RSA1(1) Zone. Under the current Zoning By-Law for the City of London, an "Automobile Sales Boutique" is defined as *an enclosed retail store where vehicles are displayed in a showroom internal to the premises, for the purpose of sale, hire or lease but shall not include the outside display or storage of vehicles or the repair and service of vehicles* (Z.-1-112010).

Planning and Development Staff are of the opinion that the recommended addition of an automobile sales boutique as a new use under the RSA1(1) Zone is appropriate and contributes to the broad range of uses that are considered compatible within the context of the subject site.

Conclusion

The recommended action is consistent with the PPS 2020, conforms to The London Plan and permits a new use that is appropriate within the surrounding context and will not conflict with current uses within the CF Masonville Place shopping centre. The new use will facilitate access to an automobile sales boutique in a convenient and accessible location to meet the needs of community.

Prepared by: Brent House, Planner

Reviewed by: Mike Corby, MCIP, RPP
Manager, Planning Implementation

Recommended by: Heather McNeely, MCIP, RPP
Director, Planning and Development

Submitted by: Scott Mathers, MPA, P.Eng.
Deputy City Manager, Planning and Economic
Development

Copy:
Britt O'Hagan, Manager, Current Development
Michael Pease, Manager, Site Plans
Brent Lambert, Manager, Development Engineering

Appendix A – Zoning Bylaw Amendment

Bill No. (number to be inserted by Clerk's Office)
2023

By-law No. Z.-1-

A by-law to amend By-law No. Z.-1 to rezone an area of land located at 1680 Richmond Street.

WHEREAS this amendment to the Zoning By-law Z.-1 conforms to the Official Plan;

THEREFORE the Municipal Council of The Corporation of the City of London enacts as follows:

1. Schedule "A" to By-law No. Z.-1 is amended by changing the zoning applicable to lands located at 1680 Richmond, as shown on the attached map comprising part of Key Map No. A.102, to permit an *Automobile Sales Boutique* as a permitted use under the RSA1(1) Zone.
2. Section Number 21.4 of the RSA1(1) Zone is amended by adding the following Special Provisions:

RSA1(1) 1680 Richmond Street

a. Additional Permitted Uses:

1. Automotive Sales Boutique
2. This Amendment shall come into effect in accordance with Section 34 of the *Planning Act, R.S.O. 1990, c. P13*, either upon the date of the passage of this by-law or as otherwise provided by the said section.

The inclusion in this By-law of imperial measure along with metric measure is for the purpose of convenience only and the metric measure governs in case of any discrepancy between the two measures.

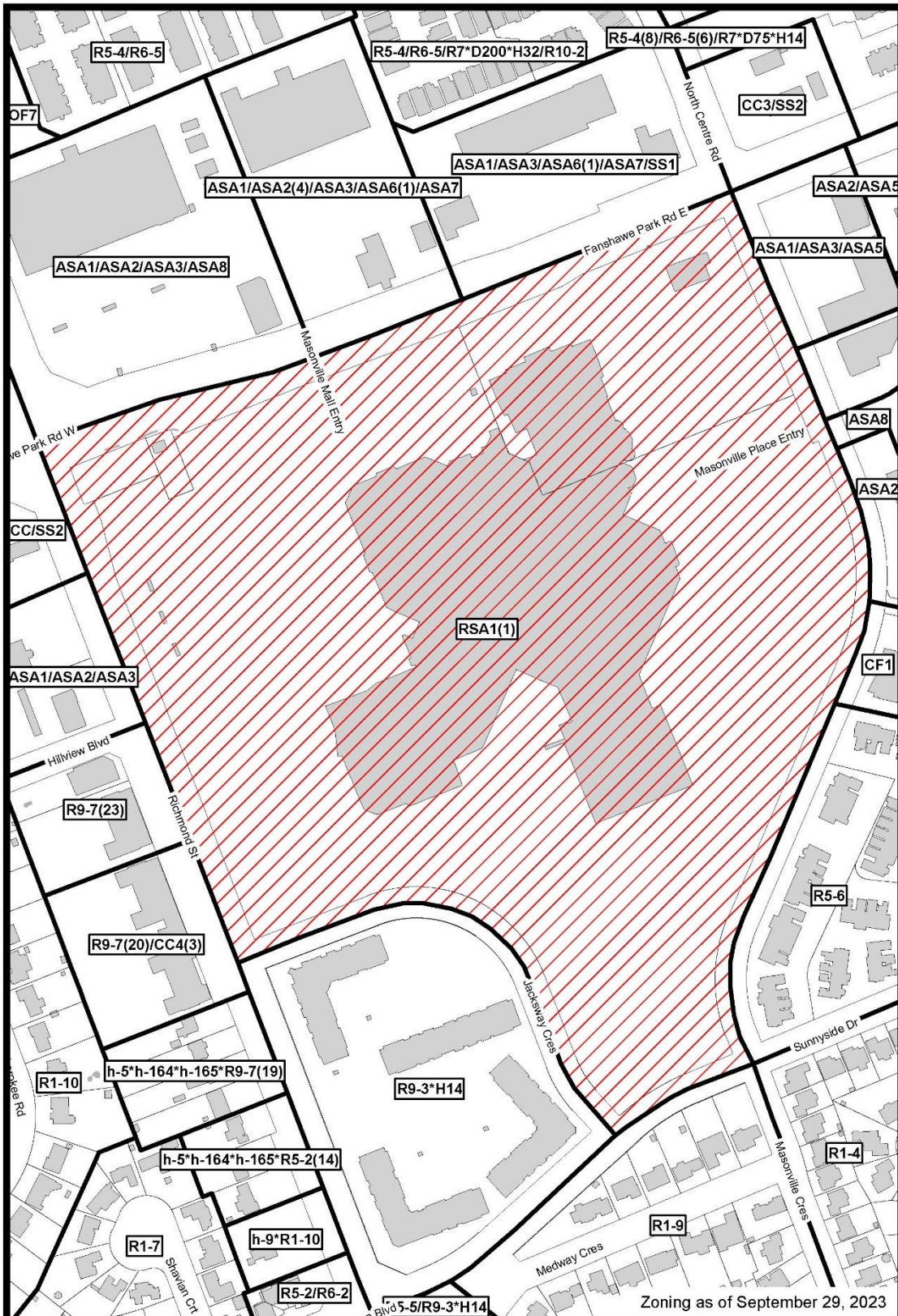
PASSED in Open Council on December 19, 2023 subject to the provisions of PART VI.1 of the *Municipal Act, 2001*.

Josh Morgan
Mayor


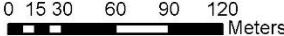

Michael Schulthess
City Clerk

First Reading – December 19, 2023
Second Reading – December 19, 2023
Third Reading – December 19, 2023

AMENDMENT TO SCHEDULE "A" (BY-LAW NO. Z.-1)



Zoning as of September 29, 2023

<p>File Number: Z-9667 Planner: BH Date Prepared: 2023/10/31 Technician: RC By-Law No: Z.-1-</p>	<p>SUBJECT SITE </p> <p>1:3,250</p> <p> Meters</p> <p></p>
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Geodatabase

Appendix B - Site and Development Summary

A. Site Information and Context

Site Statistics

Current Land Use	CF Masonville Place Shopping Centre
Frontage	323 Metres (1059.7 feet)
Depth	489.3 Metres (1605.3 feet)
Area	19.45 Hectares (48.06 acres)
Shape	Irregular
Within Built Area Boundary	Yes
Within Primary Transit Area	Yes

Surrounding Land Uses

North	commercial plaza containing stand-alone commercial buildings including “big box” retailers with associated surface parking lots. Further to the north is medium and high density residential development and a residential retirement complex.
East	commercial plaza containing smaller scale strip plaza retailers and restaurants as well as larger format “big box” retailers with associated surface parking lots. Beyond the commercial plaza are single detached residential dwellings, generally in cul-de-sac formations.
South	high, medium and low density residential development. Further south, across Windermere Road is the Western University campus
West	on the Subject Site is a bus transit hub with access from Richmond Street, which connects to routes across the City of London. To the north-west abutting the Subject Site are a LondonHydro facility and retail operations not associated with the shopping centre lands. Across Richmond Street is a seven-storey residential building, along with single detached dwellings fronting Richmond Street to the southwest. To the north-west is a commercial plaza and public school. Further to the north-west is a commercial plaza containing “big box” retailers beyond which is higher density residential development and townhomes

Proximity to Nearest Amenities

Major Intersection	Fanshawe Park Road East at Richmond Street
Dedicated cycling infrastructure	Fanshawe Park Road East (9.2m), Richmond Street (4m)
London Transit stop	Richmond Street (0m)
Public open space	Uplands Trail/Gibbons Wetland (380m)

B. Planning Information and Request

Current Planning Information

Current Place Type	Transit Village, Rapid Transit Blvd
Current Special Policies	Masonville Secondary Plan
Current Zoning	RSA1(1)

Requested Designation and Zone

Requested Place Type	No Changes Proposed
Requested Special Policies	No Changes Proposed
Requested Zoning	Add use to current zoning to permit <i>Automobile Sales Boutique</i> as a permitted use under the RSA1(1) Zone

C. Development Proposal Summary

Development Overview

The proposed Zoning By-Law Amendment is to establish an *Automobile Sales Boutique* as an additional permitted use under the RSA1(1) Zone on the Subject site at 1680 Richmond Street, within an existing vacant unit of the enclosed shopping centre known as CF Masonville Place (the “subject site”).

Proposal Statistics

Land use	CF Masonville Place Shopping Centre
Form	Regional Shopping Area
Height	2 Storeys (metres)
Density	Units per hectare
Gross floor area	61,591 Metres ²
Building coverage	32%
Landscape open space	N/A
Functional amenity space	N/A
New use being added to the local community	Yes

Mobility

Parking spaces	3,461 surface
Vehicle parking ratio	Spaces per unit
New electric vehicles charging stations	0
Secured bike parking spaces	Number
Secured bike parking ratio	Spaces per unit
Completes gaps in the public sidewalk	NA
Connection from the site to a public sidewalk	Yes
Connection from the site to a multi-use path	Yes

Environmental Impact

Tree removals	N/A
Tree plantings	N/A
Tree Protection Area	No
Loss of natural heritage features	NA
Species at Risk Habitat loss	NA
Minimum Environmental Management Guideline buffer met	NA
Existing structures repurposed or reused	Yes
Green building features	Unknown

Appendix C – Additional Plans and Drawings

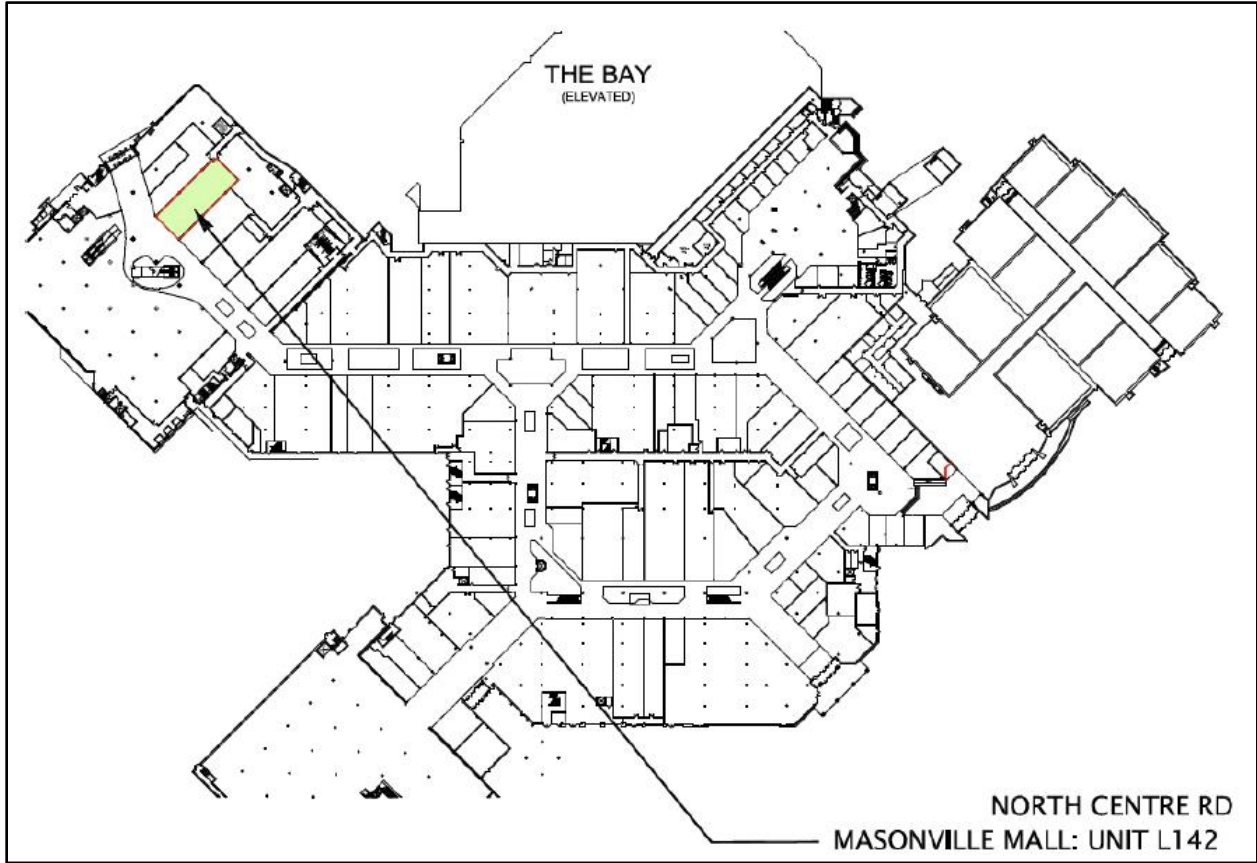
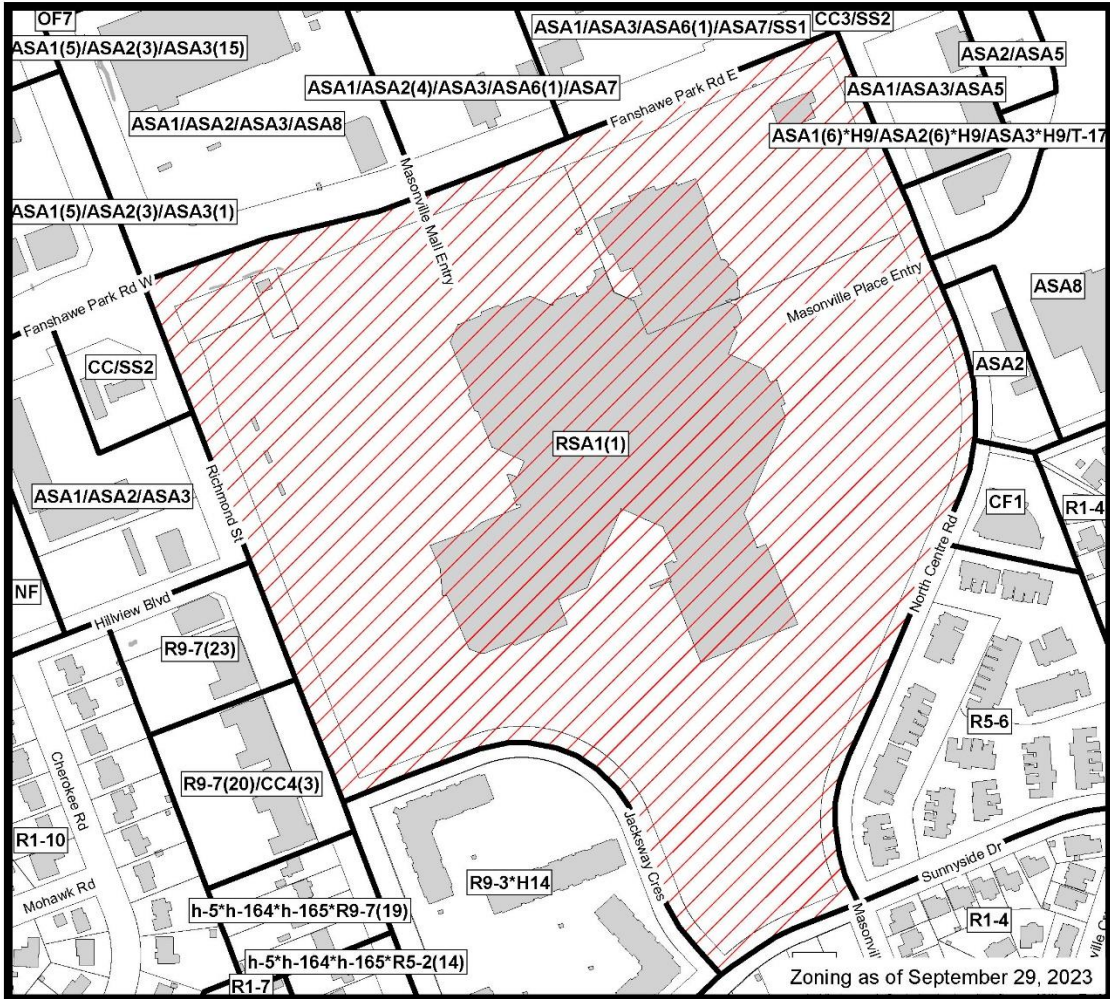


Figure 4 - Conceptual Site Plan (2023)



Zoning as of September 29, 2023

COUNCIL APPROVED ZONING FOR THE SUBJECT SITE:

1) *LEGEND FOR ZONING BY-LAW Z-1*

- | | |
|---|-----------------------------------|
| R1 - SINGLE DETACHED DWELLINGS | RF - REGIONAL FACILITY |
| R2 - SINGLE AND TWO UNIT DWELLINGS | CF - COMMUNITY FACILITY |
| R3 - SINGLE TO FOUR UNIT DWELLINGS | NF - NEIGHBOURHOOD FACILITY |
| R4 - STREET TOWNHOUSE | HER - HERITAGE |
| R5 - CLUSTER TOWNHOUSE | DC - DAY CARE |
| R6 - CLUSTER HOUSING ALL FORMS | |
| R7 - SENIOR'S HOUSING | OS - OPEN SPACE |
| R8 - MEDIUM DENSITY/LOW RISE APTS. | CR - COMMERCIAL RECREATION |
| R9 - MEDIUM TO HIGH DENSITY APTS. | ER - ENVIRONMENTAL REVIEW |
| R10 - HIGH DENSITY APARTMENTS | |
| R11 - LODGING HOUSE | OB - OFFICE BUSINESS PARK |
| | LI - LIGHT INDUSTRIAL |
| DA - DOWNTOWN AREA | GI - GENERAL INDUSTRIAL |
| RSA - REGIONAL SHOPPING AREA | HI - HEAVY INDUSTRIAL |
| CSA - COMMUNITY SHOPPING AREA | EX - RESOURCE EXTRACTIVE |
| NSA - NEIGHBOURHOOD SHOPPING AREA | UR - URBAN RESERVE |
| BDC - BUSINESS DISTRICT COMMERCIAL | |
| AC - ARTERIAL COMMERCIAL | AG - AGRICULTURAL |
| HS - HIGHWAY SERVICE COMMERCIAL | AGC - AGRICULTURAL COMMERCIAL |
| RSC - RESTRICTED SERVICE COMMERCIAL | RRC - RURAL SETTLEMENT COMMERCIAL |
| CC - CONVENIENCE COMMERCIAL | TGS - TEMPORARY GARDEN SUITE |
| SS - AUTOMOBILE SERVICE STATION | RT - RAIL TRANSPORTATION |
| ASA - ASSOCIATED SHOPPING AREA COMMERCIAL | |
| OR - OFFICE/RESIDENTIAL | "h" - HOLDING SYMBOL |
| OC - OFFICE CONVERSION | "D" - DENSITY SYMBOL |
| RO - RESTRICTED OFFICE | "H" - HEIGHT SYMBOL |
| OF - OFFICE | "B" - BONUS SYMBOL |
| | "T" - TEMPORARY USE SYMBOL |

CITY OF LONDON

PLANNING SERVICES / DEVELOPMENT SERVICES

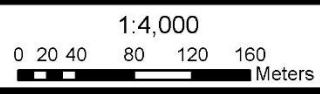
ZONING BY-LAW NO. Z-1 SCHEDULE A

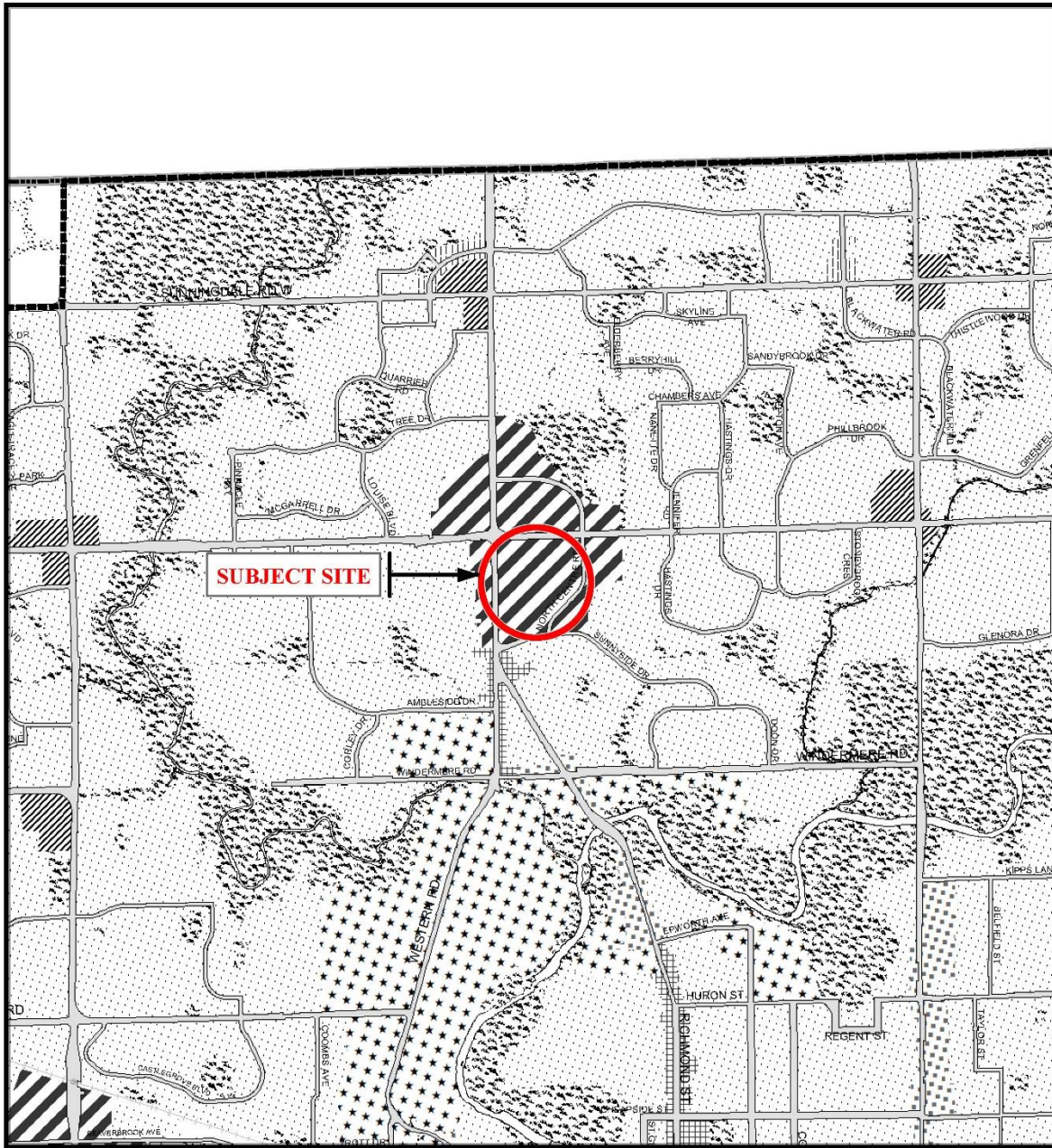


THIS MAP IS AN UNOFFICIAL EXTRACT FROM THE ZONING BY-LAW WITH ADDED NOTATIONS

FILE NO:
Z-9667 BH

MAP PREPARED:
2023/10/31 RC





Legend

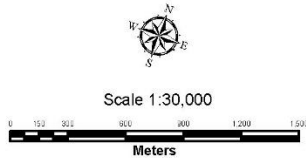
- | | | |
|------------------------|--------------------------|---|
| Downtown | Future Community Growth | Environmental Review |
| Transit Village | Heavy Industrial | Farmland |
| Shopping Area | Light Industrial | Rural Neighbourhood |
| Rapid Transit Corridor | Future Industrial Growth | Waste Management Resource Recovery Area |
| Urban Corridor | Commercial Industrial | Urban Growth Boundary |
| Main Street | Institutional | |
| Neighbourhood | Green Space | |

This is an excerpt from Planning & Development's working consolidation of Map 1 - Place Types of the London Plan, with added notations.

**CITY OF LONDON
Official Plan**

**LONDON PLAN MAP 1
- PLACE TYPES -**

PREPARED BY: Planning & Development



File Number: Z-9667
Planner: BH
Technician: RC
Date: 2023/10/31

Project Location: E:\Planning\Projects\p_officialplan\workconsol\00\excerpts_LondonPlan\mxds\Z-9667-Map1-PlaceTypes.mxd

Appendix D – Internal and Agency Comments

Site Plan

- No Comments. SP not required.

UTRCA

- The subject lands are not affected by any regulations (Ontario Regulation 157/06) made pursuant to Section 28 of the Conservation Authorities Act.

Urban Design

- As there are no exterior changes to the existing building and its interaction with the public realm, Urban Design has no comment.

Engineering

- Engineering has no comments on the additional use of automotive boutique.

Appendix E – Public Engagement

No public responses received.

Report to Planning and Environment Committee

To: Chair and Members
Planning and Environment Committee

From: Scott Mathers, MPA, P.Eng.
Deputy City Manager, Planning and Economic Development

Subject: Alora Homes
130 Southdale Road West
File Number: Z-9663, Ward 10
Public Participation Meeting

Date: December 4, 2023

Recommendation

That, on the recommendation of the Director, Planning and Development, the following actions be taken with respect to the application of Alora Homes relating to the property located at 130 Southdale Road West. The proposed by-law attached hereto as Appendix "A" **BE INTRODUCED** at the Municipal Council meeting December 19, 2023 to amend Zoning By-law No. Z.-1, in conformity with the Official Plan, The London Plan, to change the zoning of the subject property **FROM** a Residential R1 (R1-9) Zone, **TO** a Residential R3 Special Provision (R3-1(_)) Zone;

IT BEING NOTED, that the above noted amendment is being recommended for the following reasons:

- i) The recommended amendment is consistent with the PPS 2020;
- ii) The recommended amendment conforms to The London Plan, including, but not limited to the Neighbourhoods Place Type and Key Directions; and
- iii) The recommended amendment facilitates the development of vacant residential lands within the Built Area Boundary and Primary Transit Area with an appropriate form of infill development that provides choice and diversity in housing options.

Executive Summary

Summary of Request

The applicant has requested an amendment to the Zoning By-law Z.-1 to rezone the property from a Residential R1 (R1-9) Zone to a Residential R3 Special Provision (R3-1(_)) Zone.

Purpose and the Effect of Recommended Action

Staff are recommending approval with special provisions that will facilitate reduced yards (front, rear and east interior side), increased height, reduced driveway width, and increased parking area coverage.

The recommended action will permit a fourplex residential dwelling for a total of **four (4)** units.

Linkage to the Corporate Strategic Plan

This recommendation supports the following Strategic Areas of Focus:

- **Housing and Homelessness**, by ensuring London's growth and development is well-planned and considers use, intensity, and form.
- **Housing and Homelessness**, by supporting faster/ streamlined approvals and increasing the supply of housing with a focus on achieving intensification targets.

Analysis

1.0 Background Information

1.1 Previous Reports Related to this Matter

None.

1.2 Planning History

The subject lands were subject to a Site Plan Approval application in 2021 (SPA21-085) for the infill construction of a single detached dwelling. Upon the passing of provincial legislation, More Homes Built Faster Act 2022 (Bill 23), the application was cancelled as the development would no longer require Site Plan Approval under the new regulation in the *Planning Act*.

1.3 Property Description and Location

The subject lands are located on the north side of Southdale Road West, between Wharncliffe Road South to the east and Notre Dame Drive to the west. The lands are currently vacant. The lands are relatively flat in topography and contain some trees along the southeast corner of the lot, as well as other small trees and bushes along the west and north edges of the lot.

The subject lands are located within a mixed use area, being surrounded by a townhouse condominium development to the north and west sides and by single detached dwellings to the east side. Uses vary to the south, ranging from residential to institutional (place of worship) and commercial uses (furniture store, automotive dealerships) along Southdale Road West and Wharncliffe Road South.

Site Statistics:

- Current Land Use: Vacant residential land
- Frontage: 20m
- Area: 594m²
- Shape: regular (rectangle)
- Located within the Built Area Boundary: Yes
- Located within the Primary Transit Area: Yes

Surrounding Land Uses:

- North: Townhouse condominium
- East: Single detached dwelling
- South: Vacant
- West: Townhouse condominium

Existing Planning Information:

- Existing The London Plan Place Type: Neighbourhoods Place Type along a Civic Boulevard
- Existing Special Policies: None
- Existing Zoning: Residential R1 (R1-9)

Additional site information and context is provided in Appendix B.

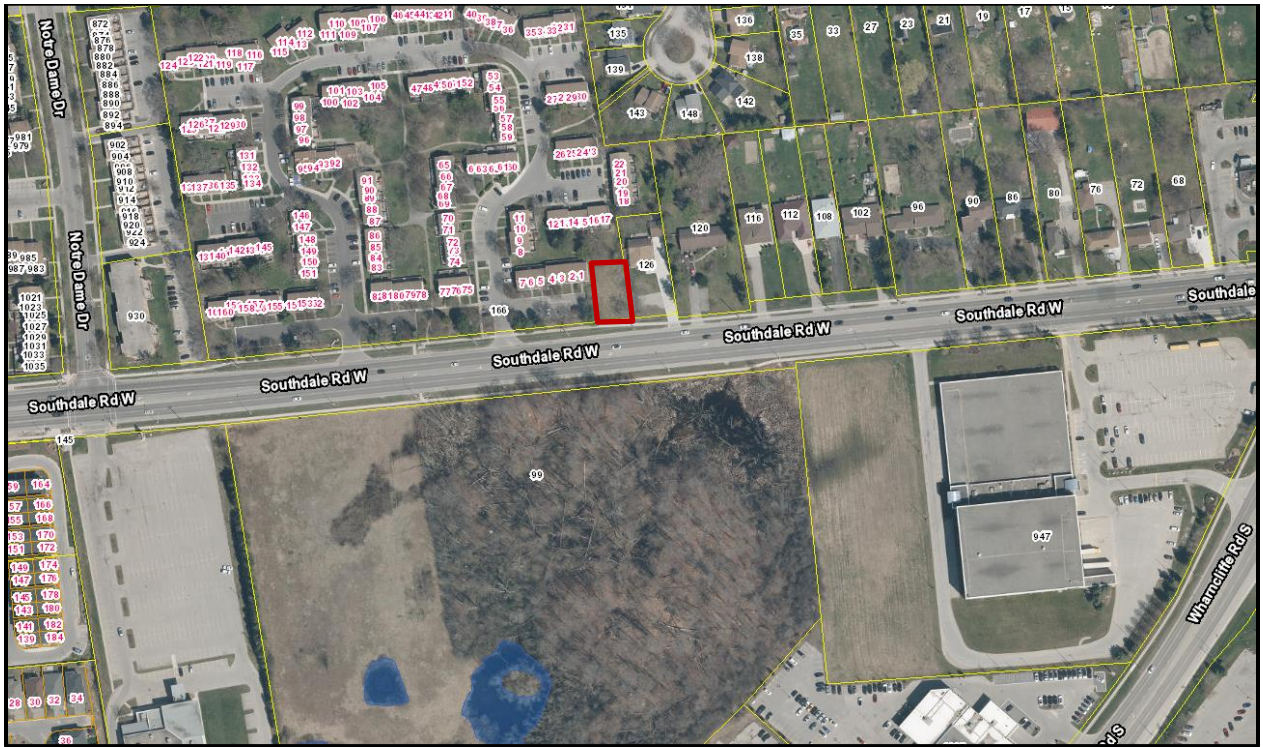


Figure 1 - Aerial Photo of 130 Southdale West and surrounding lands



Figure 2 - Streetview of 130 Southdale Road West (view looking North from Southdale Road West)

2.0 Discussion and Considerations

2.1 Development Proposal

The applicant is proposing a 3.5-storey fourplex dwelling on the subject lands. The building is located on the southeastern side of the lot, and parking is provided at the rear of the lot, accessed by a single-lane driveway on the west side of the proposed building. An internal sidewalk runs along the proposed driveway and connects the parking to the front entrance of the building as well as to the main public sidewalk.

The proposed development includes the following features:

- Land use: Residential
- Form: Fourplex Dwelling
- Height: 3.5 storeys (11.5m)
- Residential units: 4
- Building coverage: 25%
- Parking spaces: 5 (surface)
- Landscape open space: 30%

Additional information on the development proposal is provided in Appendix B.

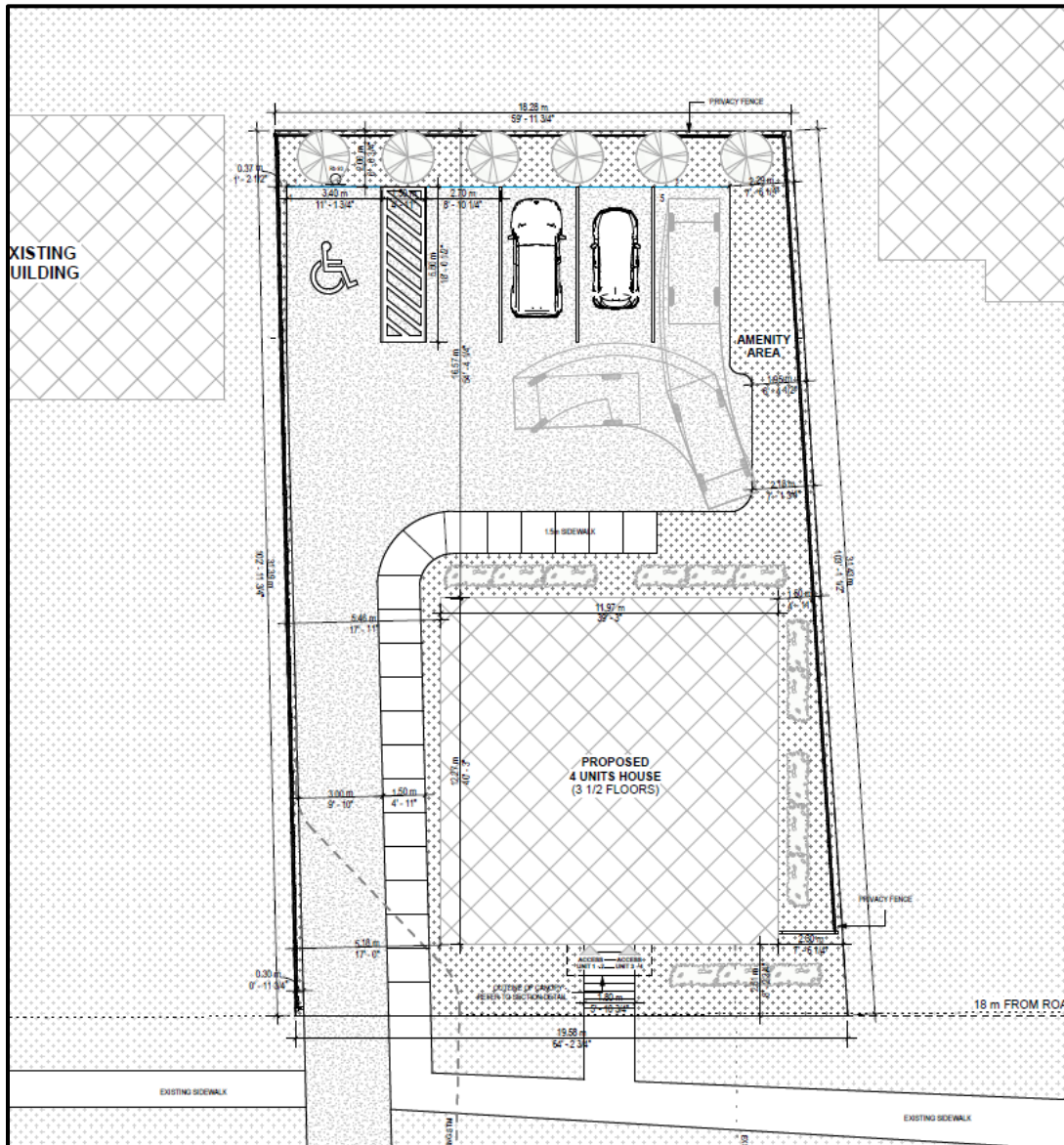


Figure 3 - Conceptual Site Plan (September 2023)

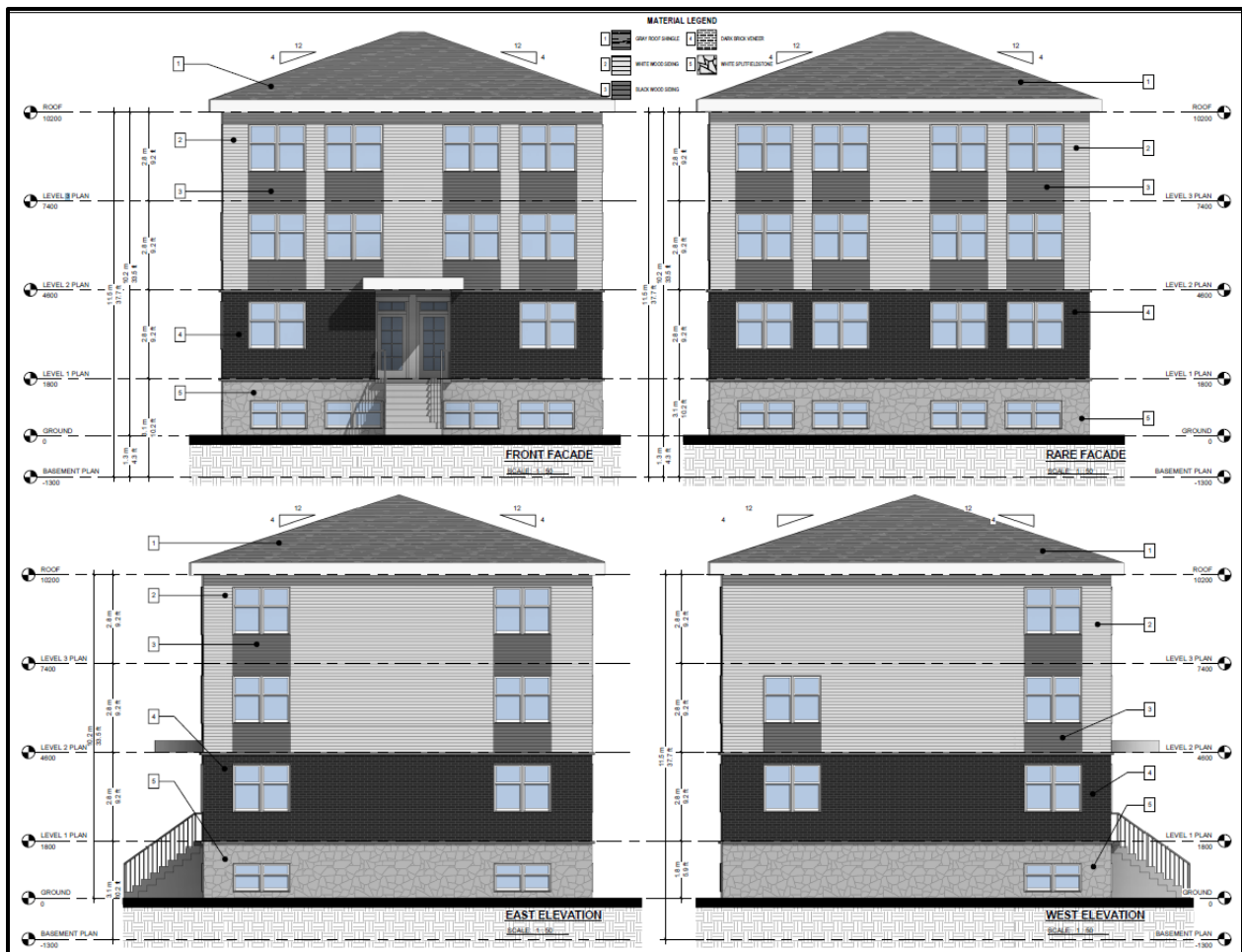


Figure 4 – Elevations (September 2023)

Additional plans and drawings of the development proposal are provided in Appendix C.

2.2 Requested Amendment(s)

The applicant has requested an amendment to Zoning Bylaw Z.-1 to rezone the property from a Residential R1 (R1-9) Zone to a Residential R3 Special Provision (R3-1()) Zone.

The following table summarizes the special provisions that have been proposed by the applicant and those that are being recommended by staff.

Regulation (R3-1())	Required	Proposed	Recommended
Minimum front yard setback	4.5m	2.5m	2.5m
Minimum interior side yard setback	2.4m	1.5m to 2.3m	1.5m to 2.3m
Maximum height	12.0m	13.0m	13.0m
Maximum parking area coverage	35%	41%	41%
Minimum driveway width	6.7m	3.0m	3.0m
Minimum parking area setback (from rear lot line)	3.0m	2.0m	2.0m
Building orientation	N/A	N/A	Towards Southdale Road West

2.3 Internal and Agency Comments

The application and associated materials were circulated for internal comments and public agencies to review. Comments received were considered in the review of this application and are addressed in Section 4.0 of this report.

Key issues identified by staff and agencies included:

- Setbacks from adjacent lots
- Building orientation

Detailed internal and agency comments are included in Appendix D of this report.

2.4 Public Engagement

On October 11, 2023, Notice of Application was sent to 175 property owners and residents in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on October 19, 2023. A “Planning Application” sign was also placed on the site.

There were 2 responses received during the public consultation period. Comments received were considered in the review of this application and are addressed in Section 4.0 of this report.

Concerns expressed by the public relate to:

- Building form
- Privacy
- Parking

Detailed public comments are included in Appendix E of this report.

2.5 Policy Context

The Planning Act and the Provincial Policy Statement, 2020

The Provincial planning policy framework is established through the *Planning Act* (Section 3) and the *Provincial Policy Statement, 2020 (PPS)*. The *Planning Act* requires that all municipal land use decisions affecting planning matters shall be consistent with the *PPS*.

The mechanism for implementing Provincial policies is through the Official Plan, The London Plan. Through the preparation, adoption and subsequent Ontario Land Tribunal (OLT) approval of The London Plan, the City of London has established the local policy framework for the implementation of the Provincial planning policy framework. As such, matters of provincial interest are reviewed and discussed in The London Plan analysis below.

As the application for a Zoning By-law amendment complies with The London Plan, it is staff’s opinion that the application is consistent with the *Planning Act* and the *PPS*.

The London Plan, 2016

The London Plan (TLP) includes evaluation criteria for all planning and development applications with respect to use, intensity and form, as well as with consideration of the following (TLP 1577-1579):

1. Consistency with the Provincial Policy Statement and all applicable legislation.
2. Conformity with the Our City, Our Strategy, City Building, and Environmental policies.
3. Conformity with the Place Type policies.
4. Consideration of applicable guideline documents.
5. The availability of municipal services.
6. Potential impacts on adjacent and nearby properties in the area and the degree to which such impacts can be managed and mitigated.
7. The degree to which the proposal fits within its existing and planned context.

Staff are of the opinion that all the above criteria have been satisfied.

3.0 Financial Impact/Considerations

None.

4.0 Key Issues and Considerations

4.1 Land Use

The proposed residential use is supported by the policies of the Provincial Policy Statement and contemplated in the Neighbourhoods Place Type in The London Plan (TLP 921_). The site is located on a Civic Boulevard (Southdale Road West) which would permit a range of low-rise residential uses including single detached, semi-detached, duplex, triplex, and fourplex dwellings, townhouses, stacked townhouses, and low-rise apartments (Table 10 – Range of Permitted Uses in Neighbourhoods Place Type).

Consistent with The London Plan, the fourplex dwelling will contribute to the existing range and mix of housing types in the area, currently comprised of single detached dwellings to the east, as well as townhouse developments to the north and west. The proposed 3.5-storey building containing 4 units will provide choice and diversity in housing options for both current and future residents. No new roads are required to service the site. The property has suitable access to open space, community facilities and shopping areas as further detailed in Appendix B of this report. In Staff's opinion the proposed use is considered appropriate given its location on a higher order road and proximity to other similar (townhouse) residential developments.

4.2 Intensity

The proposed intensity is consistent with the policies of the PPS that encourage residential intensification (PPS 1.1.3.3 and 1.4.3), an efficient use of land (PPS 1.1.3.2) and a range and mix of housing options (PPS 1.4.3).

Consistent with the PPS, the recommended amendment facilitates the redevelopment of a site within a settlement area. The increased intensity of development on the site will make use of existing and planned transit services, local and regional institutional uses, and shopping, entertainment and service uses.

The London Plan contemplates residential intensification where appropriately located and provided in a way that is sensitive to and a good fit with existing neighbourhoods (83_, 937_, 939_ 2. and 5., and 953_ 1.). The London Plan directs that intensification may occur in all place types that allow for residential uses (84_). Subject to the City Structure Plan and Residential Intensification policies in the Neighbourhoods Place Type, infill and intensification in a variety of forms will be supported to increase the supply of housing in areas where infrastructure, transit, and other public services are available and accessible (506_). The Plan identifies appropriate locations and promotes opportunities for intensification and redevelopment, to specific areas such as higher order streets.

The London Plan uses height as a measure of intensity in the Neighbourhoods Place Type. A minimum height of 2 storeys and a maximum height of 4 storeys, with an upper maximum height up to 6 storeys, is contemplated within the Neighbourhoods Place Type where a property has frontage on a Civic Boulevard. (Table 11 – Range of Permitted Heights in the Neighbourhoods Place Type). The intensity of development must be appropriate for the size of the lot (953_3.).

The subject lands have frontage on Southdale Road West, a Civic Boulevard, which is a higher-order street, to which higher-intensity uses are directed. The subject site is considered underutilized as it is currently vacant but, is of a size and configuration capable of accommodating the proposed development which represents a form of intensification through infill redevelopment. The proposed 3.5 storeys is in keeping with the permissions of The London Plan; however, the reduced side yard setback to the east was identified as a concern by Staff and members of the public. Staff are

recommending an increase from a 1.2 metre side yard setback to a 1.5 metre setback, as well as a reorientation of the building to line up with Southdale Road West, which further increases the setback from the lot line at the front of the building. The recommended provisions would mitigate the impacts of the reduced side yard to adjacent properties.

4.3 Form

During the review process, some concerns were raised by the public and staff. Notable issues relating to form included building orientation and setbacks.

The proposed building is oriented generally toward Southdale Road West but is parallel to the east interior lot lines. While this creates an even 1.2 metre setback for the length of the building, the building is at an angle from the street. Staff are recommending a provision to require the building to be oriented directly toward Southdale Road West to better align the development with the public realm and setting a positive precedent for future development along the corridor.

Staff also identified the proposed east interior side yard setback and parking area setback from the rear lot line as concerns. The reduced east interior side yard setback created privacy concerns expressed by the public, and the reduced setback from the parking area to the rear lot line limited space for tree planting behind the parking area. As a result of re-orienting the building, Staff are amenable to recommend an increased 1.5 metre setback from the interior side lot line and a 2.0 metre setback from the rear lot line to the parking area. The re-orientation toward the street, sets the building further from the east lot line and the adjacent lot, mitigating privacy concerns while the setback from the parking area to the rear lot line increases the proposed 1.0 metre setback to 2.0 metres, which gives more adequate space for tree plantings.

Conclusion

The applicant has requested an amendment to the Zoning By-law Z.-1 to rezone the property from a Residential R1 (R1-9) Zone to a Residential R3 Special Provision (R3-1(_)) Zone. Staff are recommending approval of the requested Zoning Bylaw amendment with special provisions.

The recommended action is consistent with the PPS 2020, conforms to The London Plan and will permit a fourplex dwelling.

Prepared by: Noe O'Brien
Planner, Planning Implementation

Reviewed by: Mike Corby, MCIP, RPP
Manager, Planning Implementation

Recommended by: Heather McNeely, MCIP, RPP
Director, Planning and Development

Submitted by: Scott Mathers, MPA, P.Eng.
Deputy City Manager, Planning and Economic
Development

Appendix A – Zoning Bylaw Amendment

Bill No. (number to be inserted by Clerk's Office)
2023

By-law No. Z.-1-

A by-law to amend By-law No. Z.-1 to
rezone an area of land located at 130
Southdale Road West

WHEREAS this rezoning conforms to the Official Plan;

THEREFORE the Municipal Council of The Corporation of the City of London enacts as follows:

1. Schedule "A" to By-law No. Z.-1 is amended by changing the zoning applicable to lands located at 130 Southdale Road West as shown on the attached map comprising part of Key Map No. A107 **FROM** a Residential R1 (R1-9) Zone **TO** a Residential R3 Special Provision (R3-1(_)) Zone.
2. Section Number 7.4 of the R3 Zone is amended by adding the following Special Provisions:

R3-1(_) 130 Southdale Road West

a. Regulations

- | | |
|---|-------------|
| i) Front yard setback
(Minimum) | 2.5 metres |
| ii) Interior side yard setback (east)
(Minimum) | 1.5 metres |
| iii) Height
(Maximum) | 13.0 metres |
| iv) Parking area coverage
(Maximum) | 41% |
| v) Driveway and drive
aisle width
(Minimum) | 3.0m |
| vi) Parking area setback
from rear lot line
(Minimum) | 2.0m |
| vii) Buildings shall be oriented towards Southdale Road West. | |

The inclusion in this By-law of imperial measure along with metric measure is for the purpose of convenience only and the metric measure governs in case of any discrepancy between the two measures.

This By-law shall come into force and be deemed to come into force in accordance with Section 34 of the *Planning Act, R.S.O. 1990, c. P13*, either upon the date of the passage of this by-law or as otherwise provided by the said section.

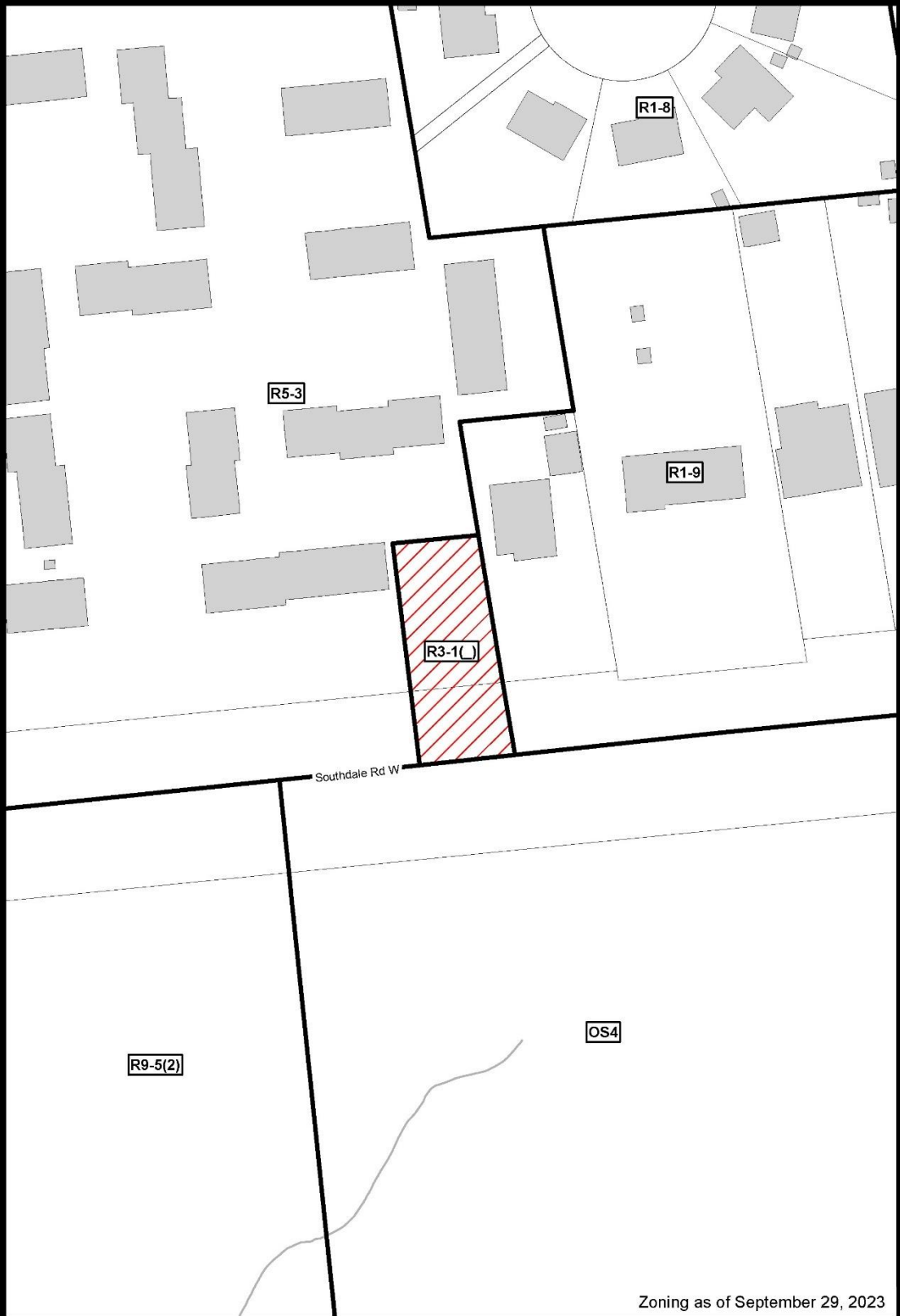
PASSED in Open Council on December 19, 2023


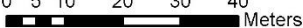

Josh Morgan
Mayor

Michael Schulthess
City Clerk

First Reading – December 19, 2023
Second Reading – December 19, 2023
Third Reading – December 19, 2023

AMENDMENT TO SCHEDULE "A" (BY-LAW NO. Z.-1)



<p>File Number: Z-9663 Planner: NO Date Prepared: 2023/11/02 Technician: RC By-Law No: Z.-1-</p>	<p>SUBJECT SITE </p> <p>1:1,000</p> <p>0 5 10 20 30 40 Meters </p> <p></p>
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Geodatabase

Appendix B - Site and Development Summary

A. Site Information and Context

Site Statistics

Current Land Use	Vacant residential land
Frontage	20 metres
Area	594m ²
Shape	Regular
Within Built Area Boundary	Yes
Within Primary Transit Area	Yes

Surrounding Land Uses

North	Townhouse condominium
East	Single detached dwelling
South	Vacant
West	Townhouse condominium

Proximity to Nearest Amenities

Major Intersection	Southdale Road West and Wharncliffe Road South, 400m
Dedicated cycling infrastructure	Southdale Road West, adjacent
London Transit stop	Southdale Road West, 130m
Public open space	Westbury Park, 950m
Commercial area/use	Southdale Road West and Wharncliffe Road South, 400m
Food store	Berries Market, 500m
Community/recreation amenity	Westbury Park, 950m

B. Planning Information and Request

Current Planning Information

Current Place Type	Neighbourhood Place Type, along a Civic Boulevard
Current Special Policies	None
Current Zoning	R1-9

Requested Designation and Zone

Requested Place Type	No changes proposed
Requested Special Policies	No changes proposed
Requested Zoning	R3-1()

Requested Special Provisions

Regulation (R3-1())	Required	Proposed	Recommended
Minimum front yard setback	4.5m	2.5m	2.5m
Minimum interior side yard setback	2.4m	1.5m to 2.3m	1.5m to 2.3m
Maximum height	12.0m	13.0m	13.0m
Maximum parking area coverage	35%	41%	41%
Minimum driveway width	6.7m	3.0m	3.0m
Minimum parking area setback (from rear lot line)	3.0m	2.0m	2.0m
Building orientation	N/A	N/A	Towards

Regulation (R3-1(_))	Required	Proposed	Recommended
			Southdale Road West

C. Development Proposal Summary

Development Overview

3.5 storey fourplex development. The building is situated along Southdale Road West, with a drive aisle on the west side of the lot leading to a parking area in the rear of the site.
--

Proposal Statistics

Land use	Residential
Form	Fourplex dwelling
Height	3.5 Storeys (11.5m)
Residential units	4
Building coverage	25%
Landscape open space	30%
New use being added to the local community	No

Mobility

Parking spaces	5
Vehicle parking ratio	1.25
New electric vehicles charging stations	Unknown
Completes gaps in the public sidewalk	N/A
Connection from the site to a public sidewalk	Yes
Connection from the site to a multi-use path	N/A

Environmental Impact

Tree removals	1
Tree plantings	Unknown
Tree Protection Area	No
Loss of natural heritage features	N/A
Species at Risk Habitat loss	N/A
Minimum Environmental Management Guideline buffer met	N/A
Existing structures repurposed or reused	N/A
Green building features	Unknown

Appendix C – Additional Plans and Drawings

SITE STATISTICS

SITE AREA: 584 sq ft (53.80 sq m)
 PROPOSED ZONE: R2-1
 BUILDING AREA: TOWNHOUSE - 4/2 FLOOR
 OFFICE AREA: 208.27 sq ft
 LANDSCAPE AREA: 182.27 sq ft
 TOTAL UNITS: 4 UNITS

ZONAL REGULATIONS		
PROPOSED ZONE AND R2-1-1		
	REGULATED	PROPOSED
LOT AREA	584 sq ft	584 sq ft
LOT FRONTAGE	15.5 m	15.5 m
LOT DEPTH	36.9 m	37.4 m
FRONT YARD SETBACK	4.5 m	2.0 m
REAR YARD SETBACK (E)	2.4 m	1.5 m to 2.2 m
REAR YARD SETBACK (W)	3.0 m	3.0 m to 3.0 m
COVERED AREA	25%	25%
COVERED (S)	4%	2%
HEIGHT (M)	15.5 m	15.5 m
PERMITTED COVERED AREA	20%	40.8%
NUMBER OF UNITS PER LOT	4	4
MINIY SPACE	NA	85 sq ft

WASTE REMOVAL
 WASTE TO BE STORED INTERNALLY AND PICKUP PROVIDED BY THE CITY OF LONDON


PARKING REQUIREMENTS
 MINIMUM PARKING SPACE DIMENSIONS TO BE: 2.1m x 5.5m x 3.0m, TYPE B2, 8.2m x 5.5m
 ACCESSIBLE TYPE: 1 SPACE

PARKING PROVIDED:
 STACKED TOWNHOUSE: 4 RESIDENTIAL UNITS + 4 SPACES (1 SPACE PER UNIT)
 TOTAL REQUIRED PARKING: 4 SPACES
 TOTAL PROVIDED PARKING: 4 SPACES (INC. BARRIER FREE PARKING SPACES)
 BARRIER FREE: 0 SPACES

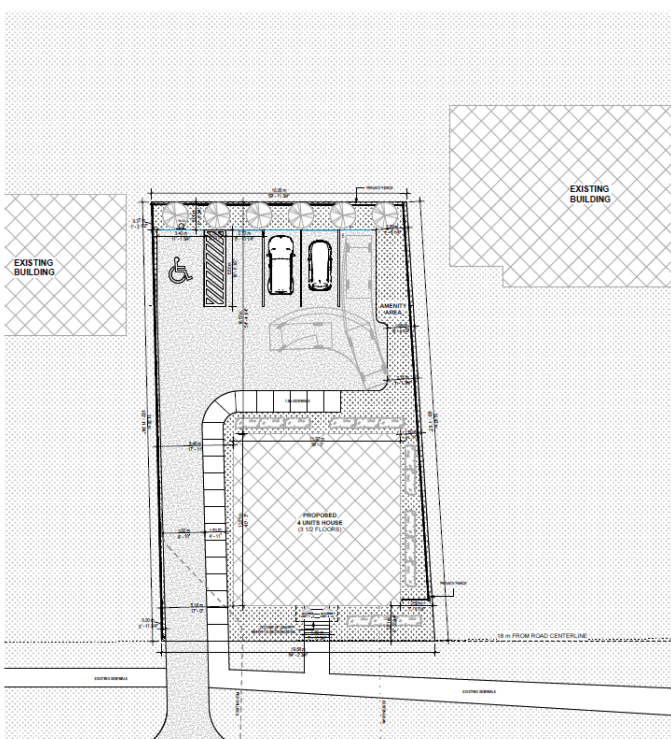
BARRIER FREE PARKING:
 REQUIRED: 0% TOTAL PARKING REQUIRED + 1 SPACES
 PROVIDED: 0% TOTAL PARKING PROVIDED + 0 SPACES

BUILDING CLASS
 GROUP C1 (NET 4-HOUR FIRE RATED BUILDING) - STACKED TOWNHOUSE

REFERENCE DOCUMENTS
 1. CONCEPTUAL PLAN FOR SUBJECT OF PART OF LOT 22 CONVEYANCE TO WIFE OF WIFE LAND SUBDIVISION, DATED JAN 11, 2023



ORTHO BASEMAP BY THE CITY OF LONDON - 2022



EXISTING BUILDING

EXISTING BUILDING

EXISTING BUILDING


PROPOSED 4 UNITS HOUSE (3 TO 4 FLOORS)

15 m FROM ROAD CENTERLINE

LINES LEGEND

- PROJECT LINE
- OTHER PROPERTIES
- SETBACK ROAD CENTERLINE
- EXISTING STM
- EXISTING SAN

* STM AND SAN INFORMATION ARE FROM ENGINEERING PLAN DONE BY MTE JULY 2021



CREATIVE STRUCTURES

1000 SHEPPARD AVENUE EAST, SUITE 100, SCARBOROUGH, ONTARIO M1S 1T6
 TEL: (416) 291-1111
 WWW.CREATIVESTRUCTURES.COM

PRELIMINARY PLAN FOR REVIEW ONLY NOT FOR OFFICIAL SUBMISSIONS, PRICING OR CONSTRUCTION

NO.	DESCRIPTION	DATE
1	ISSUED FOR REVIEW	2023-02-01
2	REVISION	2023-02-15
3	REVISION	2023-03-01
4	REVISION	2023-03-15
5	REVISION	2023-04-01
6	REVISION	2023-04-15
7	REVISION	2023-05-01
8	REVISION	2023-05-15
9	REVISION	2023-06-01
10	REVISION	2023-06-15
11	REVISION	2023-07-01
12	REVISION	2023-07-15
13	REVISION	2023-08-01
14	REVISION	2023-08-15
15	REVISION	2023-09-01
16	REVISION	2023-09-15
17	REVISION	2023-10-01
18	REVISION	2023-10-15
19	REVISION	2023-11-01
20	REVISION	2023-11-15

ISSUED/REVISION TABLE

STACKED 4 UNITS HOUSE - ALORA HOMES

1000 SHEPPARD AVENUE EAST, SUITE 100, SCARBOROUGH, ONTARIO M1S 1T6

PRELIMINARY SITE PLAN PROPOSAL

Project No: 2023-02
 Project Name: ALORA HOMES
 Date: 2023-02-01
 Scale: A-1.0

A-1.0 20

Appendix D – Internal and Agency Comments

Heritage – October 17

- There are no cultural heritage comments on this application. Archaeological matters were previously addressed.

Ecology – October 18

- There are currently no ecological planning issues related to this property and/or associated study requirements.

Engineering – Pending

-

UTRCA – October 13

- The UTRCA has no objections to the application and has no Section 28 approval requirements.

Urban Design – October 18

The following site layout and design features are acknowledged and should be carried forward through zoning:

1. Siting the building close to Southdale Road West with a direct walkway connection and orientation towards the Civic Boulevard. *Refer to The London Plan, Policy [TLP] 259, 268*
2. Locating surface parking in the rear yard, away from the street frontage. *[TLP 272]*

Matters for Zoning

1. The following zoning provisions for a setback along the boundaries of the subject site should be provided:
 - A minimum front yard setback from the ultimate right of way along Southdale Road West to ensure building elements such as footing, and canopies do not encroach into the right-of-way. *[TLP 259]*
 - A minimum east side yard setback to allow for privacy and not hinder the redevelopment of adjacent properties. *[TLP 253]*
 - A minimum west and rear side yard setback between the property line and the drive aisle/parking lot to allow space for landscaping and avoid any negative impact on the adjacent properties. *[TLP 278, 253]*
3. Noting Site Plan is not required for the application, zoning provisions should ensure that the built form is oriented towards Southdale Road West with principal entrances and window openings to face the public street for visual amenity and allowing passive surveillance.

Parks Planning – October 18

- Parkland dedication for this development is required and will be taken in form of cash-in-lieu in accordance with By-law CP-25 through the building permit process.
-

Landscape Architecture – October 13

- Major Issues
 - No potential grounds for refusal, or issues that could require significant changes to the proposal.
- Matters for OPA/ZBA
 - One boundary tree has been identified for removal. A Boundary trees as defined in the Province's Forestry Act, 1998, c. 18, Sched. I, s. 21, is any tree growing on the boundary between adjoining lands. The trees are

considered the common property of the owners of the adjoining lands and are protected by the province's Forestry Act. These trees can't be removed without written consent from co-owner. Every person who injures or destroys a tree growing on the boundary between adjoining lands without the consent of the land owners is guilty of an offence under this Act. If consent from neighbour cannot be obtained, a minimum 5 metre setback is required to protect the tree.

Site Plan

- Site Plan not required.

London Hydro – October 16

- Servicing the above proposal should present no foreseeable problems. Any new and/or relocation of existing infrastructure will be at the applicant's expense, maintaining safe clearances from L.H. infrastructure is mandatory. Note: Transformation lead times are minimum 16 weeks. Contact the Engineering Dept. to confirm requirements & availability.
- London Hydro has no objection to this proposal or possible official plan and/or zoning amendment. Any new or relocation of the existing service will be at the expense of the owner.

Appendix E – Public Engagement

Public Comment #1 – Received October 19, 2023

Contact Method – Email

Hi there.

I oppose to changing the current zoning for this lot and for any development of a fourplex dwelling in this area. It does not align with all of the single-dwelling homes in this area. How many residences will this complex house? I see that there is no parking – because there is NO ROOM for parking and cars will be towed if they try to park anywhere at 166 Southdale Road W. This is a very small lot. What is the developer thinking? Are they out-of-town developers? Is the owner out-of-town as well? Is this complex designed for low-income dwellers? The height will obstruct our view to the forest across the street. If the plan is to have parking for cars, this will play havoc on an already congested street. Having more density will increase traffic and safety concerns where Southdale currently is already a “trap” in trying to enter and exit our complex at 166 Southdale Road W. Drivers are always exceeding the speed limit. Let’s put it this way “not in my backyard” is the message I am conveying.

Note that there were protests against the previous rezoning for a single dwelling of increased height and depth.

Public Comment #2 – Received October 20, 2023

Contact Method – Email

I am contacting you regarding the recent proposal for 130 Southdale Rd W.

We spoke about this early last year in regards to a 2 story 2 garage house being built there, and the concerns we as well as many in the neighborhood shared, as expressed by those of us who spoke during the council meeting as well as those who signed the petition. Thank you for your support at that time.

Once again I ask for your help in this matter. The newest proposal is to change the zoning from a house on the small lot, to a 3.5 story fourplex dwelling, with special provisions for every single category as well as requiring the property to have;

" increased maximum height, again additional reduced front interior setbacks and increased parking area for coverage for not 1-2 but 5 vehicles and decreased drive isle width." This effectively changes the lot from R1-9 to R3-1 zoning and then requests special provisions even within that zoning. The construction hasn't even begun and we have already had people trespassing on our property multiple times, measuring our trees to determine if they need to be chopped down (without our permission or easement).

We understand that London is a growing city and that it is difficult to stop a lot from being utilized as housing, especially with the city giving additional permissions with the new regulations passed by the federal government. However this should be done in a fair non intrusive manner that does not disrupt other residents' lives and doesn't dismiss normal policy i.e. respecting lot size and the impact such a proposed property with that much parking, would pose. So far their conduct has not been done in good faith, as we only received a notice for the change this week, not even 4 full days before the October 20th deadline for comments. This is a process that has been expedited far too quickly and clearly bends to meet the proposals beneficial to the builder / developer while disregarding the residents surrounding & impacted by the 130 lot development. There are zoning, privacy, trespassing and safety concerns with this amended proposal as well as concerns raised with the conduct in which the residents have been informed re

this matter and how drastic the minimum requirements proposed are, some of which are a 1/4 of the regular minimum requirements.

We are sincerely hoping for your support in addressing these concerns and stopping these amendments.

Thank you for your time and response in this matter.

Appendix A – Zoning Bylaw Amendment

Bill No. (number to be inserted by Clerk's Office)
2023

By-law No. Z.-1-

A by-law to amend By-law No. Z.-1 to rezone
an area of land located at 130 Southdale
Road West

WHEREAS this rezoning conforms to the Official Plan;

THEREFORE the Municipal Council of The Corporation of the City of London enacts as follows:

1. Schedule "A" to By-law No. Z.-1 is amended by changing the zoning applicable to lands located at 130 Southdale Road West as shown on the attached map comprising part of Key Map No. A107 **FROM** a Residential R1 (R1-9) Zone **TO** a Residential R3 Special Provision (R3-1(_)) Zone.
2. Section Number 7.4 of the R3 Zone is amended by adding the following Special Provisions:

R3-1(_) 130 Southdale Road West

a. Regulations

- | | |
|--|-------------|
| i) Front yard setback
(Minimum) | 2.5 metres |
| ii) Interior side yard setback (east)
(Minimum) | 1.5 metres |
| iii) Height
(Maximum) | 13.0 metres |
| iv) Parking area coverage
(Maximum) | 41% |
| v) Driveway and drive
aisle width
(Minimum) | 3.0m |
| vi) Parking area setback
from rear lot line
(Minimum) | 2.0m |
| vii) Parking area setback
from east lot line
(Minimum) | 1.9m |
| viii) Buildings shall be oriented towards Southdale Road West. | |

The inclusion in this By-law of imperial measure along with metric measure is for the purpose of convenience only and the metric measure governs in case of any discrepancy between the two measures.

This By-law shall come into force and be deemed to come into force in accordance with Section 34 of the *Planning Act, R.S.O. 1990, c. P13*, either upon the date of the passage of this by-law or as otherwise provided by the said section.

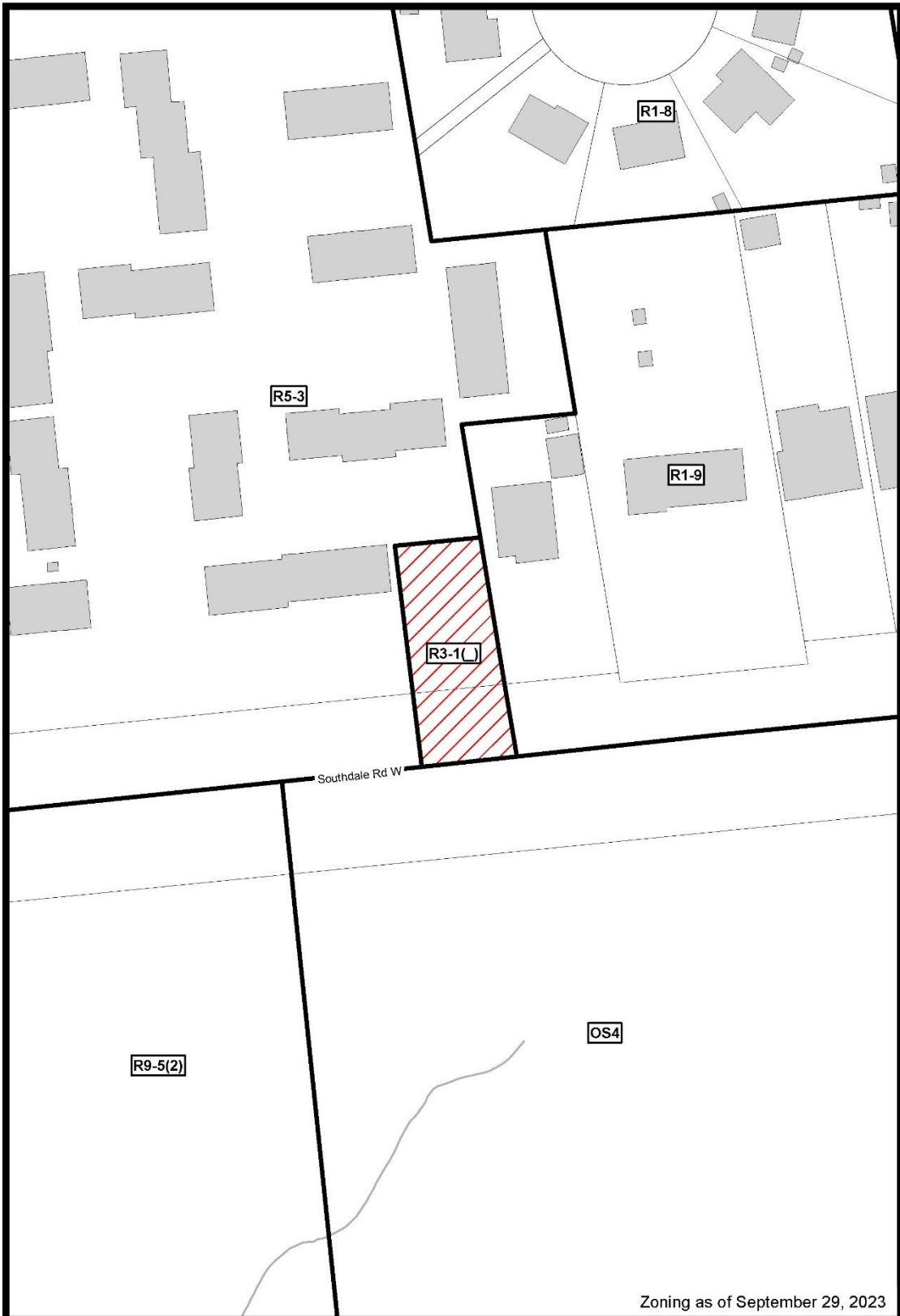
PASSED in Open Council on December 19, 2023

Josh Morgan
Mayor

Michael Schulthess
City Clerk

First Reading – December 19, 2023
Second Reading – December 19, 2023
Third Reading – December 19, 2023

AMENDMENT TO SCHEDULE "A" (BY-LAW NO. Z.-1)



Zoning as of September 29, 2023

File Number: Z-9663

Planner: NO

Date Prepared: 2023/11/02

Technician: RC

By-Law No: Z.-1-

SUBJECT SITE 

1:1,000

0 5 10 20 30 40 Meters



Geodatabase

Report to Planning and Environment Committee

To: Chair and Members
Planning and Environment Committee

From: Scott Mathers, MPA, P.Eng.
Deputy City Manager, Planning and Economic Development

Subject: Oxbury Centre Ltd. (c/o Westdell Development Corporation)
625 Mornington Avenue (1299 Oxford Street East)
File Number: Z-9589, Ward 4
Public Participation Meeting

Date: December 4, 2023

Recommendation

That, on the recommendation of the Director, Planning and Development, the following actions be taken with respect to the application of Oxbury Centre Ltd. (c/o Westdell Development Co.) relating to the property located at 625 Mornington Avenue and 1299, 1303, 1307, & 1323 Oxford Street East:

- (a) the proposed by-law attached hereto as Appendix "A" **BE INTRODUCED** at the Municipal Council meeting December 19, 2023 to amend Zoning By-law No. Z.-1, in conformity with the Official Plan, The London Plan, to change the zoning of the subject property **FROM** a Community Shopping Area (CSA4) Zone, **TO** a Holding Residential R9 Special Provision (h-_*R9-7(_)*H45) Zone and a Community Shopping Area Special Provision (CSA4(_)) Zone;
- (b) The Site Plan Approval Authority **BE REQUESTED** to consider the following design issues through the site plan process:
 - i) The recommendations of the Noise Study be implemented;
 - ii) Provide an adequately sized and centrally located outdoor amenity space, either at-grade or rooftop, or a combination of both;
 - iii) Details regarding garbage storage and collection be determined.
- (c) pursuant to Section 34(17) of the Planning Act, as determined by the Municipal Council, no further notice **BE GIVEN** in respect of the proposed by-law as the special provisions to the CSA4 Zone relate to changes to existing conditions resulting from the recommended zone boundary and do not significantly alter the proposed development circulated in the Notice of Revised Application and Notice of Public Meeting.

IT BEING NOTED, that the above noted amendment is being recommended for the following reasons:

1. The recommended amendment is consistent with the *PPS 2020*;
2. The recommended amendment conforms to The London Plan, including, but not limited to the Transit Village Place Type and Protected Major Transit Station Area policies; and
3. The recommended amendment facilitates intensification of an underutilized site.

Executive Summary

Summary of Request

The applicant has requested an amendment to the Zoning By-law Z.-1 to rezone a portion of the property from a Community Shopping Area (CSA4) Zone to a Residential R9 Special Provision (R9-7(_)*H45) Zone and Community Shopping Area Special Provision (CSA4(_)) Zone.

Purpose and the Effect of Recommended Action

Staff are recommending approval of the requested Zoning Bylaw amendment with additional special provisions that will facilitate a human-scale, pedestrian-oriented

development with mitigation measures to minimize impacts on the adjacent low density residential neighbourhood. Staff are further recommending a holding provision that will ensure the development will not occur until such time as there is an accepted water strategy and adequate capacity available.

The recommended action will permit a 15-storey, **177-unit** residential apartment building.

Rationale of Recommended Action

1. The recommended amendment is consistent with the *PPS 2020*;
2. The recommended amendment conforms to The London Plan, including, but not limited to the Transit Village Place Type and Protected Major Transit Station Area policies; and
3. The recommended amendment facilitates intensification of an underutilized site.

Linkage to the Corporate Strategic Plan

This recommendation supports the following Strategic Areas of Focus:

- **Housing and Homelessness**, by ensuring London's growth and development is well-planned and considers use, intensity, and form.
- **Housing and Homelessness**, by increasing access to a range of quality, affordable, and supportive housing options that meet the unique needs of Londoners.
- **Wellbeing and Safety**, by promoting neighbourhood planning and design that creates safe, accessible, diverse, walkable, healthy, and connected communities.

Analysis

1.0 Background Information

1.1 Previous Reports Related to this Matter

None.

1.2 Planning History

None.

1.3 Property Description and Location

The subject lands are located on the southwest corner of Highbury Avenue North and Oxford Street East, in the Carling Planning District. The subject lands have frontages on Highbury Avenue North, Oxford Street East, and Mornington Avenue. The subject site is currently developed with an existing mall, three freestanding commercial buildings, and a parking garage accessed from Mornington Avenue. The applicant is proposing to demolish a portion of the existing parking garage to facilitate redevelopment along Mornington Avenue.

The site in its entirety has an area of 3.968 hectares, a lot frontage of 125.76 metres (Highbury Avenue North), and a lot depth of 277.83 metres. Surrounding land uses consist of low density residential and commercial to the north, the London Psychiatric Hospital lands to the east, the Canada Post facility to the south, and low density residential to the east. Further south are apartment buildings ranging from 7 to 11 storeys in height.

Site Statistics:

- Current Land Use: Commercial mall
- Frontage: 125.76 metres (412.6 feet)
- Depth: 277.83 metres (911.5 feet)
- Area: 3.968 hectares (9.8 acres)
- Shape: Irregular
- Located within the Built Area Boundary: Yes
- Located within the Primary Transit Area: Yes

Surrounding Land Uses:

- North: Low rise residential and commercial
- East: London Psychiatric Hospital Lands
- South: Industrial
- West: Low rise residential

Existing Planning Information:

- Existing The London Plan Place Type: Transit Village Place Type
- Existing Special Policies: Protected Major Transit Station Area
- Existing Zoning: Community Shopping Area (CSA4) Zone

Additional site information and context is provided in Appendix “B”.



Figure 1 - Aerial Photo of 625 Mornington Avenue and 1299, 1303, 1307, & 1323 Oxford Street East and surrounding lands (approximate area proposed for development delineated with dashed line)



Figure 2 - Streetview of 625 Mornington Avenue (view looking SE)

2.0 Discussion and Considerations

2.1 Development Proposal

The applicant is proposing to demolish a portion of the existing parking garage on Mornington Avenue to facilitate the development of a 15-storey, 177-unit apartment building. The original development concept proposed an 18-storey, 187-unit apartment building and was revised to the current concept following discussions with staff through the review of the application.

The proposed development includes the following features:

- Land use: Residential
- Form: High-rise apartment building
- Height: 15 storeys (44.10 m)
- Residential units: 177
- Density: 475 units per hectare
- Gross floor area: proposed apartment building: 15,577.86 m²; extension to parking deck: 351.48 m²
- Building coverage: 80.25%
- Parking spaces: 103 structured / 37 surface
- Bicycle parking spaces: 180 long-term / 4 short-term
- Landscape open space: 12.22%

Additional information on the development proposal is provided in Appendix “B”.

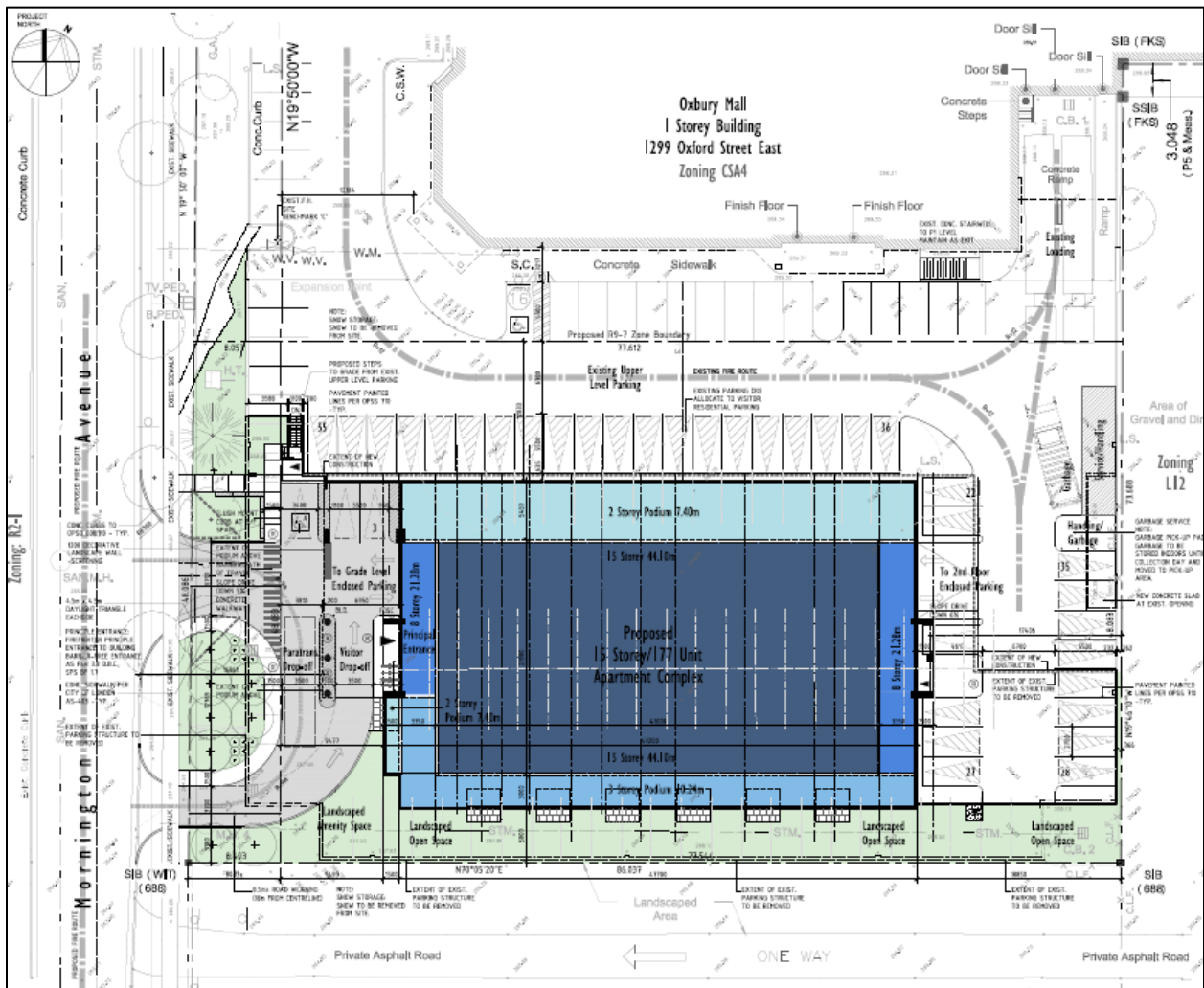


Figure 3 - Conceptual Site Plan (November 2023)

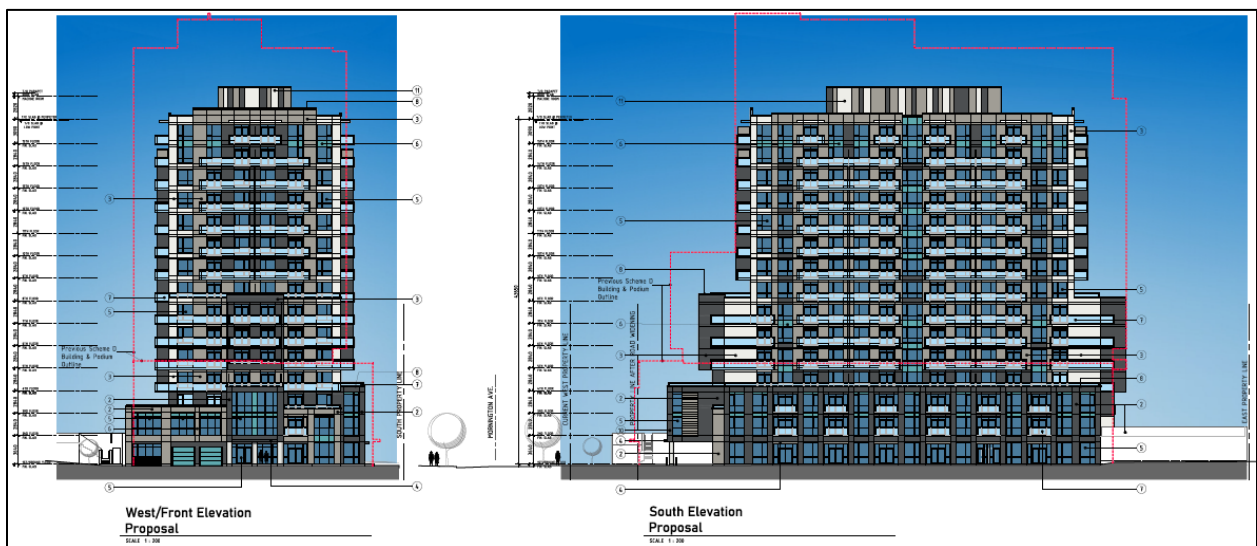


Figure 4 – West and South Elevations (October 2023)

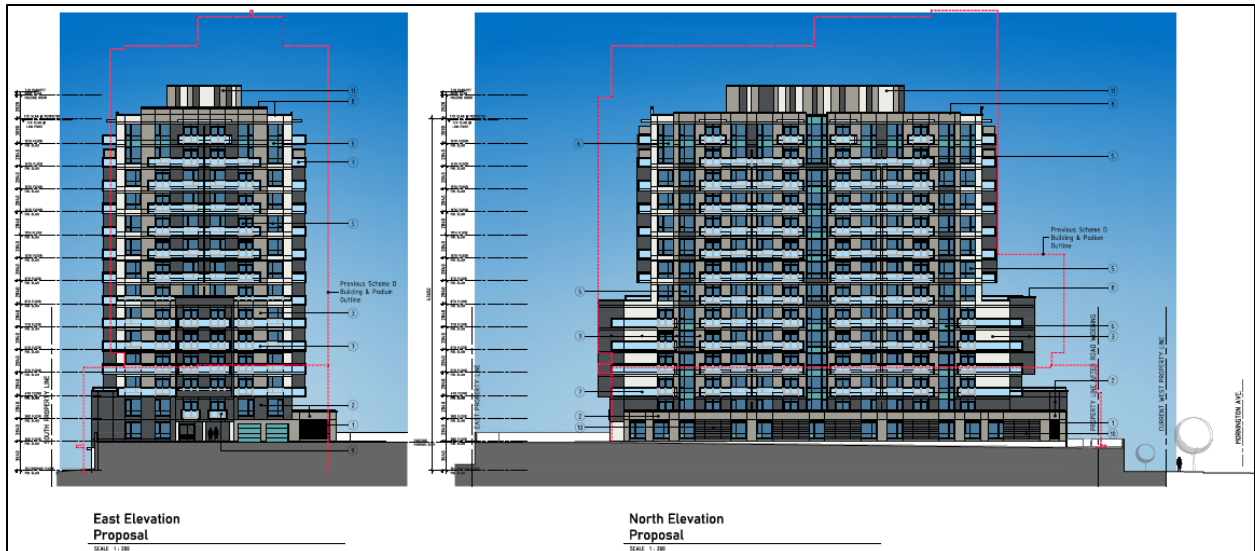


Figure 5 – East and North Elevations (October 2023)

Additional plans and drawings of the development proposal are provided in Appendix “C”.

2.2 Requested Amendment

The applicant has requested an amendment to the Zoning Bylaw Z.-1 to rezone the property from a Community Shopping Area (CSA4) Zone to a Residential R9 Special Provision (R9-7(_)*H45) Zone and Community Shopping Area Special Provision (CSA4(_)) Zone.

The following table summarizes the special provisions that have been proposed by the applicant and those that are being recommended by staff.

Regulation (R9-7 Zone)	Required	Proposed
Building Height (Maximum)	N/A	45 metres
Density (Maximum)	150 units per hectare	475 units per hectare
Front Yard Depth (Minimum)	10.11 metres	3.81 metres to the main building and 0.0 metres to the parking garage
Interior Side Yard Depth – Northerly (Minimum)	17.64 metres	12.83 metres to the main building and 0.0 metres to the parking garage
Interior Side Yard Depth – Southerly (Minimum)	10.11 metres	5.0 metres
Rear Yard Depth (Minimum)	17.64 metres	17.4 metres (to apartment building) and 0.36 metres to the parking garage
Lot Coverage (Maximum)	30%	81% (inclusive of the parking garage)
Landscaped Open Space (Minimum)	30%	12%
Regulation (CSA4 Zone)	Required	Proposed
Interior Side Yard Depth – Southerly (Minimum)	10.0 metres abutting a residential zone	8.5 metres to the main building and 0.0 metres to the parking garage
Rear Yard Depth (Minimum)	10.0 metres abutting a residential zone	0.0 metres to the parking garage

2.3 Internal and Agency Comments

The initial application and associated materials were circulated for internal comments and public agencies to review. Comments received were considered in the review of this application and are addressed in Section 4.0 of this report.

Key issues identified by staff and agencies included:

- The intensity of the apartment building and its siting adjacent to the Neighbourhoods Place Type and existing single-storey residential buildings;
- The building design and lack of measures to mitigate impacts of the intensity on the adjacent low-rise residential neighbourhood;
- The initial request to rezone the entire property and to add residential permissions to the existing CSA4 Zone;
- Sanitary servicing constraints.

Detailed internal and agency comments are included in Appendix “D” of this report.

2.4 Public Engagement

On February 8, 2023, Notice of Application was sent to 951 property owners and residents in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on February 9, 2023. A “Planning Application” sign was also placed on the site.

10 responses (six phone calls and four emails) from 9 interested parties were received during the public consultation period. Comments received were considered in the review of this application and are addressed in Section 4.0 of this report.

Concerns expressed by the public relate to:

- The proposed density and height of the building, given its proximity to existing low density residential;
- Traffic, citing pre-existing traffic issues and concerns that they would be exacerbated by the proposed development;
- Suggestion to consider 3-storey walk-ups that are affordable, but not a high-rise as proposed;
- Wind impacts, birds flying into windows, and conflicts with helicopters and airplanes;
- Removal of the existing parking garage as some neighbours use the garage regularly.

Detailed public comments are included in Appendix “E” of this report.

2.5 Policy Context

The Planning Act and the Provincial Policy Statement, 2020

The Provincial planning policy framework is established through the *Planning Act* (Section 3) and the *Provincial Policy Statement, 2020 (PPS)*. The *Planning Act* requires that all municipal land use decisions affecting planning matters shall be consistent with the *PPS*.

The mechanism for implementing Provincial policies is through the Official Plan, The London Plan. Through the preparation, adoption and subsequent Ontario Land Tribunal (OLT) approval of The London Plan, the City of London has established the local policy framework for the implementation of the Provincial planning policy framework. As such, matters of provincial interest are reviewed and discussed in The London Plan analysis below.

As the application for a Zoning By-law amendment complies with The London Plan, it is staff’s opinion that the application is consistent with the *Planning Act* and the *PPS*.

The London Plan, 2016

The London Plan (TLP) includes evaluation criteria for all planning and development applications with respect to use, intensity and form, as well as with consideration of the following (TLP 1577-1579):

1. Consistency with the Provincial Policy Statement and all applicable legislation.
2. Conformity with the Our City, Our Strategy, City Building, and Environmental policies.
3. Conformity with the Place Type policies.
4. Consideration of applicable guideline documents.
5. The availability of municipal services.
6. Potential impacts on adjacent and nearby properties in the area and the degree to which such impacts can be managed and mitigated.
7. The degree to which the proposal fits within its existing and planned context.

Staff are of the opinion that all the above criteria have been satisfied.

3.0 Financial Impact/Considerations

None

4.0 Key Issues and Considerations

4.1 Land Use

The proposed residential use is supported by the policies of the *Provincial Policy Statement* and is contemplated in the Transit Village Place Type in The London Plan (TLP 811_1, 815E_).

4.2 Intensity

The proposed intensity is consistent with the policies of the PPS that encourage residential intensification (PPS 1.1.3.3 and 1.4.3), an efficient use of land (PPS 1.1.3.2) and a range and mix of housing options (PPS 1.4.3). The proposed intensity conforms to the policies of the Transit Village Place Type in The London Plan, which contemplates a standard maximum height of 15 storeys and an upper maximum height of 22 storeys subject to the policies in Our Tools (TLP 813_1). Policy 813_3 requires permitted building heights to step down from the core of the Transit Village to any adjacent Neighbourhoods Place Type. The portion of the site proposed to be developed is located adjacent to the Neighbourhoods Place Type across Mornington Avenue, which is currently developed with single detached dwellings and semi-detached dwellings. Staff are satisfied that the proposed building height of 15 storeys provides for a transition in height from the upper maximum height of 22 storeys, while also ensuring an adequate level of intensity is provided to support the goals of the Transit Village Place Type, per policy 813_2.

The site is also within a Protected Major Transit Station Area (PMTSA), which establishes minimum intensification targets and contemplates a maximum height of 22 storeys (TLP 815C_). Within the Transit Village PMTSAs, the minimum density is 45 units per hectare for residential uses or a floor area ratio of 0.5 for non-residential uses (TLP 815B_). The proposed development provides a residential density of 475 units per hectare.

A Watermain Feasibility Report, prepared by Driven Engineering Inc. dated January 9, 2023, was submitted as part of the complete application. A Hydrant Flow Test Report, prepared by C&H Fire Suppression Systems Inc. dated September 20, 2023, was subsequently submitted following Engineering staff's review of the Watermain Feasibility Report. An updated Watermain Feasibility Report, prepared by Driven Engineering Inc. dated October 26, 2023, was also submitted which factored in the analysis of the Hydrant Flow Test Report. The reports concluded that the existing 150mm diameter watermain along Mornington Avenue is not sufficient to provide adequate fire flows for

the proposed building. As such, the following holding provision is recommended to ensure adequate water capacity is available prior to development:

h- Purpose: To ensure the adequate provision of municipal services, the holding provision shall not be removed until such time as there is an accepted water strategy and adequate capacity available.

Staff have reviewed the range of existing holding provisions in Zoning By-law Z.-1 however none specifically address water capacity. As such, staff are recommending a new holding provision which addresses the specific water capacity issue for this site while also using generic language that can be applied to other sites in the future, if needed.

A Sanitary Servicing Brief, prepared by Driven Engineering Inc. dated January 9, 2023, was also submitted as part of the complete application. Engineering staff have confirmed that while there is sufficient capacity to support the number of units proposed in this development, there are known sanitary capacity constraints which will affect future development of this site and others in the drainage area.

No other concerns regarding traffic, noise, or parking were raised by staff. A Noise Study, prepared by Akoustik Engineering Limited dated August 4, 2022, was submitted as part of the complete application. Engineering staff have reviewed the report and accepted the recommendations. Implementation of mitigation measures are recommended as a consideration to the Site Plan Approval Authority. A Transportation Impact Assessment, prepared by BT Engineering dated October 22, 2022, was submitted as part of the complete application, and accepted by City Transportation staff.

4.3 Form

In accordance with policy 814_, the following form policies apply within the Transit Village Place Type and are relevant to the proposed development:

- High-quality architectural design.
- Buildings and public realm to be designed to be pedestrian, cycling, and transit-supportive through building orientation, location of entrances, clearly marked pedestrian pathways, widened sidewalks, cycling infrastructure, and general site layout that reinforces pedestrian safety and easy navigation.
- Convenient pedestrian access to transit facilities.
- Publicly accessible pedestrian connections through development sites.
- The base of all buildings will be designed to establish and support a high-quality pedestrian environment.
- Massing and architecture within the Transit Village should provide for articulated façades and rooflines, accented main entry points, and generous use of glazing and other façade treatments along sidewalk areas such as weather protection features to support a quality pedestrian environment.
- Surface parking areas should be located in the rear and interior side yard. Underground parking and structured parking integrated within the building design is encouraged.
- Planning and development applications will be required to demonstrate how the proposed development can be coordinated with existing, planned and potential development on surrounding lands within the Transit Village Place Type.

In addition to the form policies of the Transit Village Place Type, all planning and development applications will conform with the City Design policies of The London Plan (841_1). These policies direct all planning and development to foster a well-designed building form, and ensure development is designed to be a good fit and compatible within its context (193_1 and 193_2). The site layout of new development should be designed to respond to its context, the existing and planned character of the surrounding area, and to minimize and mitigate impacts on adjacent properties (252_ and 253_).

In accordance with policy 289_, high and mid-rise buildings should be designed to express three defined components: a base, middle, and top. Alternative design solutions that address the following intentions may be permitted:

1. The base should establish a human-scale façade with active frontages including, where appropriate, windows with transparent glass, forecourts, patios, awnings, lighting, and the use of materials that reinforce a human scale.
2. The middle should be visually cohesive with, but distinct from, the base and top.
3. The top should provide a finishing treatment, such as roof or a cornice treatment, to hide and integrate mechanical penthouses into the overall building design.

The applicant met with Planning and Development staff through the review of the application to address several form-based concerns and to provide for a design solution that justifies the proposed building height. The following is a summary of the changes made to the site and building design:

- Reduced height from 18 storeys to 15 storeys.
- Reduced apartment building GFA from 21,319 m² to 16,037 m².
- Reduced podium height from 4 storeys to 2 storeys on the north façade and 3 storeys at the south façade.
- Increased south side yard setback from 3.95 m to 5 m, plus an additional 3 m to tower.
- Increased setbacks at 4th to 7th floors.
- Reduced floorplate areas – less than 900m² at 8th floor and beyond.
- Removed all parking from south and southwest side and relocated to the north side of building.
- Significant south façade changes – replaced parking with dwelling units on the first 4 storeys; also provided increased glazing, patios, balconies and terraces on this side of the building.
- Changes to the podium – dwelling units now on the first 4 storeys with storeys 3 and 4 being exclusively comprised of dwelling units.
- Although underground parking is not proposed in the revised submission, the ground floor plan drawing reference to a “new parking deck” with an area of 351.48m² reflects an emphasis on trying to shift parking inside the building to better accommodate dwelling units within the podium and repurpose/add to the existing parking deck, where feasible.
- Shifted unit orientation, balconies for all westerly units to west/street façade.
- Increased the amount of at-grade landscaped open space from 227.3 square metres to 455.5 square metres.

Staff recommend the following additional special provisions to ensure conformity with the policy framework, to implement the revised proposal, and that serves to mitigate impacts of the proposed development on the existing surrounding residential neighbourhood:

- Minimum and maximum podium heights of 2 storeys and 3 storeys, respectively.
- Minimum front yard stepbacks of 3 metres above the 2nd, 3rd and 7th storeys.
- Minimum 55% glazing on the west (front) façade of the podium.
- Minimum 40% glazing on the west (front) and east (rear) façades of the tower.
- Minimum 45% on the north and south façades of the tower.
- Minimum ground floor height of 3.6m.
- Principal building entrance for the residential lobby along Morning Avenue.
- Maximum floorplate size of 1,000m² above the eighth storey.

The intent of the recommended special provisions for height and step-backs is to mitigate negative impacts on the existing and planned neighbourhood and to provide a human-scale environment along the building’s active edges (TLP 253_). These design features are especially important given the proximity of the existing single-storey, low density residential neighbourhood located immediately across Mornington Avenue. A minimum ground floor to second floor height of 3.6m and a minimum percentage of transparent glazing is also recommended to facilitate an active ground floor along Mornington Avenue that reinforces a human scale (TLP 289_1 and 291_).

With the inclusion of the above noted special provisions, staff are satisfied the revised development concept is consistent with the form policies of the Transit Village Place Type policies and the City Design Policies.

Conclusion

The applicant has requested an amendment to the Zoning By-law Z.-1 to rezone the property from a Community Shopping Area (CSA4) Zone to a Residential R9 Special Provision (R9-7(_)) Zone and a Community Shopping Area Special Provision (CSA4(_)) Zone. Staff are recommending approval of the requested Zoning By-law Amendment with additional special provisions and a holding provision.

The recommended action is consistent with the PPS 2020, conforms to The London Plan and will permit a 15-storey, 177-unit apartment building.

Prepared by: Catherine Maton, MCIP, RPP
Senior Planner, Planning Implementation

Reviewed by: Mike Corby, MCIP, RPP
Manager, Planning Implementation

Recommended by: Heather McNeely, MCIP, RPP
Director, Planning and Development

Submitted by: Scott Mathers, MPA, P.Eng.
Deputy City Manager, Planning and Economic
Development

Copy: Britt O'Hagan, Manager, Current Development
Michael Pease, Manager, Site Plans
Brent Lambert, Manager, Development Engineering

Appendix A – Zoning Bylaw Amendment

Bill No. (number to be inserted by Clerk's Office)
2023

By-law No. Z.-1-

A by-law to amend By-law No. Z.-1 to rezone an area of land located at 625 Mornington Avenue and 1299, 1303, 1307, & 1323 Oxford Street East

WHEREAS this amendment to the Zoning By-law Z.-1 conforms to the Official Plan;

THEREFORE the Municipal Council of The Corporation of the City of London enacts as follows:

1. Schedule "A" to By-law No. Z.-1 is amended by changing the zoning applicable to lands located at 625 Mornington Avenue and 1299, 1303, 1307, & 1323 Oxford Street East, as shown on the attached map comprising part of Key Map No. A108, **FROM** a Community Shopping Area (CSA4) Zone **TO** a Holding Residential R9 Special Provision (h-_*R9-7(_)*H45) Zone and a Community Shopping Area Special Provision (CSA4(_)) Zone.

2. Section Number 3.8 2) of the Holding "h" Zone is amended by adding the following Holding Provisions:

h-(_) Purpose: To ensure the adequate provision of municipal services, the holding provision shall not be removed until such time as there is an accepted water strategy and adequate capacity available.

3. Section Number 13.4.g) of the Residential R9 (R9-7) Zone is amended by adding the following Special Provisions:

R9-7(_) 625 Mornington Avenue and 1299, 1303, 1307, & 1323 Oxford Street East

a. Regulations

- i) Front Yard Depth (Minimum) – 3.8 metres to the podium, 9.4 metres to the main building, and 0.0 metres to the parking garage
 - ii) North Interior Side Yard Depth (Minimum) – 12.8 metres to the main building and 0.0 metres to the parking garage
 - iii) South Interior Side Yard Depth (Minimum) – 5.0 metres
 - iv) Rear Yard Depth (Minimum) – 17.4 metres to the main building and 0.3 metres to the parking garage
 - v) Building Stepback Above the 2nd, 3rd, and 7th Storeys (Minimum) – 3.0 metres
 - vi) Podium Height (Minimum) – 2 Storeys
 - vii) Podium Height (Maximum) – 3 Storeys
 - viii) Ground Floor to Second Floor Height (Minimum) – 3.6 metres
 - ix) Glazing (Minimum) – 55% on the west (front) façade of the podium; 40% on the west (front) and east (rear) façades of the tower; and 45% on the north and south façades of the tower
 - x) Floorplate Area 8th Storey and Above (Maximum) – 1,000 square metres
 - xi) Density (Maximum) – 475 units per hectare
 - xii) Lot Coverage (Maximum) – 81%
 - xiii) Landscaped Open Space (Minimum) – 12%
 - xiv) The main building entrance shall be oriented to Mornington Avenue
4. Section Number 22.4.d) of the Community Shopping Area (CSA4) Zone is amended by adding the following Special Provisions:

CSA4() 625 Mornington Avenue and 1299, 1303, 1307, & 1323 Oxford Street East

a. Regulations

- i) South Interior Side Yard Depth (Minimum) – 8.5 metres to the main building and 0.0 metres to the parking garage
- ii) Rear Yard Depth (Minimum) – 0.0 metres to the parking garage
- iii) Landscaped Open Space (Minimum) – 10% or as existing on the date of the passing of this by-law, whichever is less
- iv) Lot Coverage (Maximum) – 30% or as existing on the date of the passing of this by-law, whichever is greater
- v) Highbury Avenue North shall be deemed to be the front lot line

The inclusion in this By-law of imperial measure along with metric measure is for the purpose of convenience only and the metric measure governs in case of any discrepancy between the two measures.

This By-law shall come into force and be deemed to come into force in accordance with Section 34 of the *Planning Act, R.S.O. 1990, c. P13*, either upon the date of the passage of this by-law or as otherwise provided by the said section.

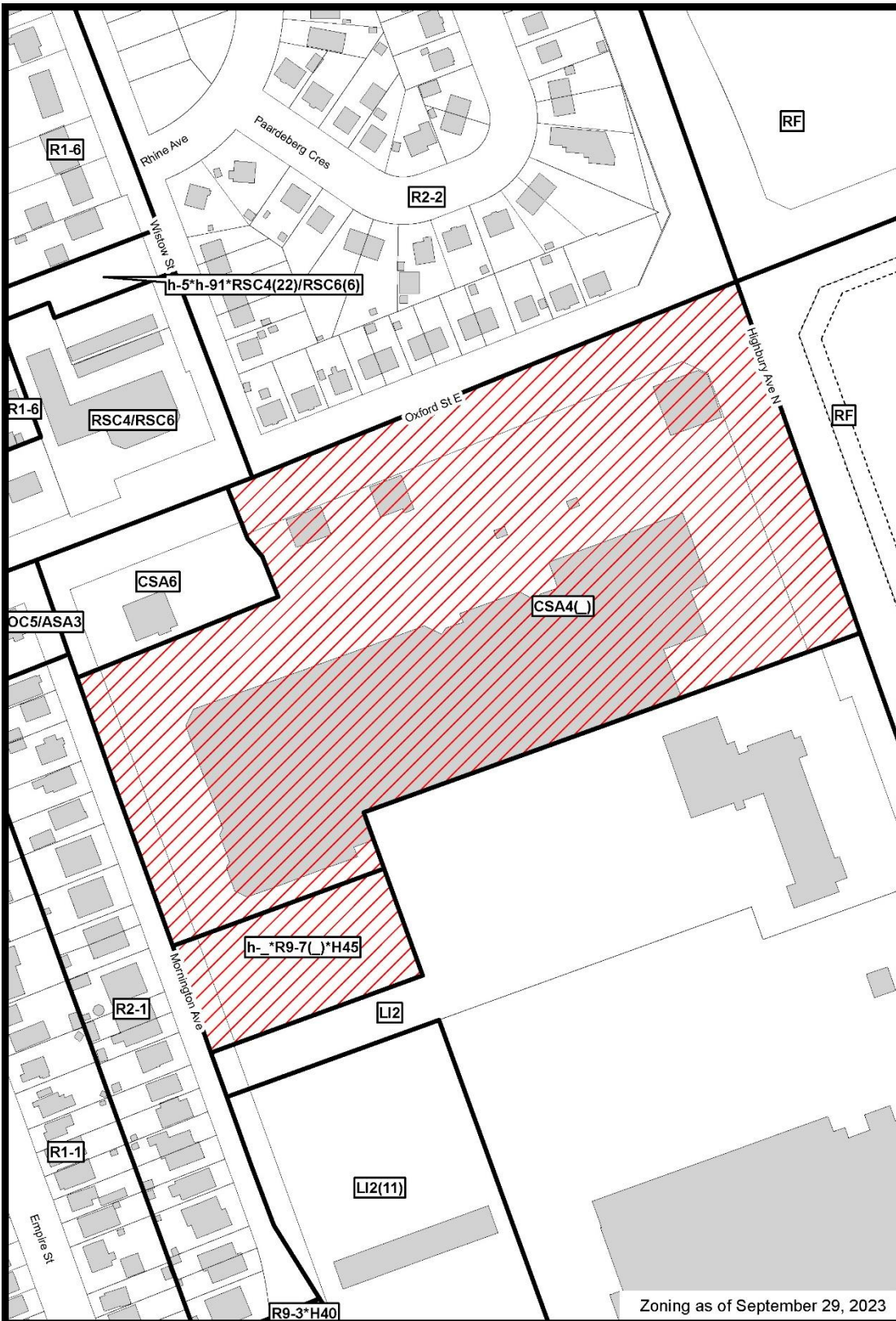
PASSED in Open Council on December 19, 2023

Josh Morgan
Mayor


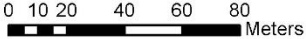

Michael Schulthess
City Clerk

First Reading – December 19, 2023
Second Reading – December 19, 2023
Third Reading – December 19, 2023

AMENDMENT TO SCHEDULE "A" (BY-LAW NO. Z.-1)



Zoning as of September 29, 2023

<p>File Number: Z-9589 Planner: CM Date Prepared: 2023/11/09 Technician: RC By-Law No: Z.-1-</p>	<p>SUBJECT SITE </p> <p>1:2,000</p> <p> Meters</p> <p></p>
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Geodatabase

Appendix B - Site and Development Summary

A. Site Information and Context

Site Statistics

Current Land Use	Commercial mall
Frontage	125.76 metres (412.6 feet)
Depth	277.83 metres (911.5 feet)
Area	3.968 hectares (9.8 acres)
Shape	Irregular
Within Built Area Boundary	Yes
Within Primary Transit Area	Yes

Surrounding Land Uses

North	Low rise residential and commercial
East	London Psychiatric Hospital Lands (undeveloped)
South	Industrial (Canada Post)
West	Low rise residential

Proximity to Nearest Amenities

Major Intersection	Highbury Avenue North and Oxford Street East
Dedicated cycling infrastructure	Oxford Street East, 385 metres
London Transit stop	Mornington Avenue, 95 metres
Public open space	Mornington Park, 800 metres
Commercial area/use	Oxbury Mall, on-site
Food store	Food Basics (Oxbury Mall), on-site
Community/recreation amenity	Boyle Memorial Community Centre, 1.9 km

B. Planning Information and Request

Current Planning Information

Current Place Type	Transit Village Place Type
Current Special Policies	Protected Major Transit Station Area
Current Zoning	Community Shopping Area (CSA4) Zone

Requested Designation and Zone

Requested Place Type	N/A
Requested Special Policies	N/A
Requested Zoning	N/A

Requested Special Provisions

Regulation (R9-7 Zone)	Required	Proposed
Building Height (Maximum)	N/A	45 metres
Density (Maximum)	150 units per hectare	475 units per hectare
Front Yard Depth (Minimum)	10.11 metres	3.81 metres to the main building and 0.0 metres to the parking garage
Interior Side Yard Depth – Northerly (Minimum)	17.64 metres	12.83 metres to the main building and 0.0 metres to the parking garage
Interior Side Yard Depth – Southerly	10.11 metres	5.0 metres

Regulation (R9-7 Zone)	Required	Proposed
(Minimum)		
Rear Yard Depth (Minimum)	17.64 metres	17.4 metres (to apartment building) and 0.36 metres to the parking garage
Lot Coverage (Maximum)	30%	81% (inclusive of the parking garage)
Landscaped Open Space (Minimum)	30%	12%
Regulation (CSA4 Zone)	Required	Proposed
Interior Side Yard Depth – Southerly (Minimum)	10.0 metres abutting a residential zone	8.5 metres to the main building and 0.0 metres to the parking garage
Rear Yard Depth (Minimum)	10.0 metres abutting a residential zone	0.0 metres to the parking garage

C. Development Proposal Summary

Development Overview

The applicant is proposing to demolish a portion of the existing parking garage to facilitate the development of a 15-storey, 177-unit apartment building. The initial development concept proposed an 18-storey, 187-unit apartment building and was revised to the current concept following discussions with staff through the review of the application.

Proposal Statistics

Land use	Residential (apartment building)
Form	High-rise
Height	15 Storeys (44.10 metres)
Residential units	177
Density	475 units per hectare
Gross floor area	15,577.86 m ² ; extension to parking deck: 351.48 m ²
Building coverage	80.25%
Landscape open space	12.22%
New use being added to the local community	No

Mobility

Parking spaces	103 structured, 37 surface
Vehicle parking ratio	0.79 spaces per unit
New electric vehicles charging stations	0
Secured bike parking spaces	180 long-term spaces (+ 4 short-term spaces)
Secured bike parking ratio	>1 space per unit
Completes gaps in the public sidewalk	N/A
Connection from the site to a public sidewalk	Yes
Connection from the site to a multi-use path	N/A

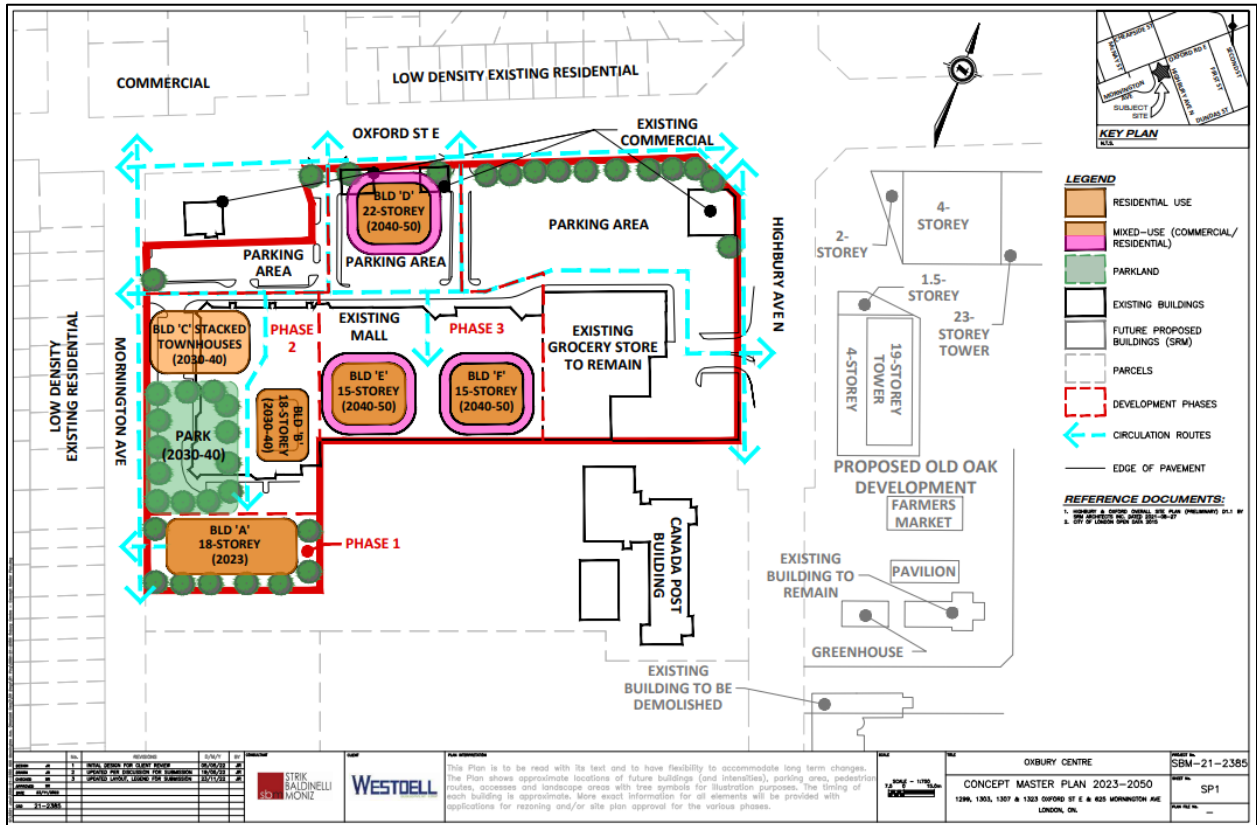
Environmental Impact

Tree removals	TBD
Tree plantings	TBD
Tree Protection Area	No

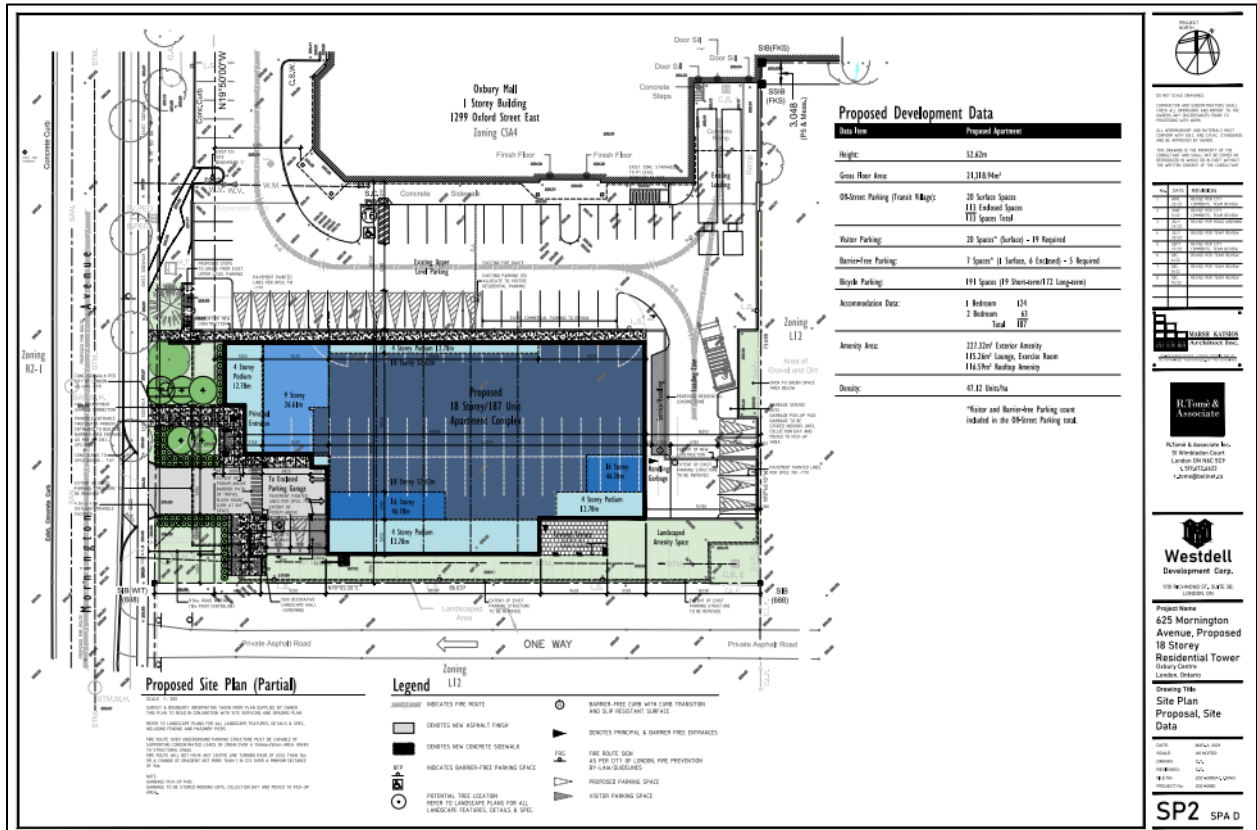
Loss of natural heritage features	N/A
Species at Risk Habitat loss	N/A
Minimum Environmental Management Guideline buffer met	N/A
Existing structures repurposed or reused	N/A
Green building features	Unknown

Appendix C – Additional Plans and Drawings

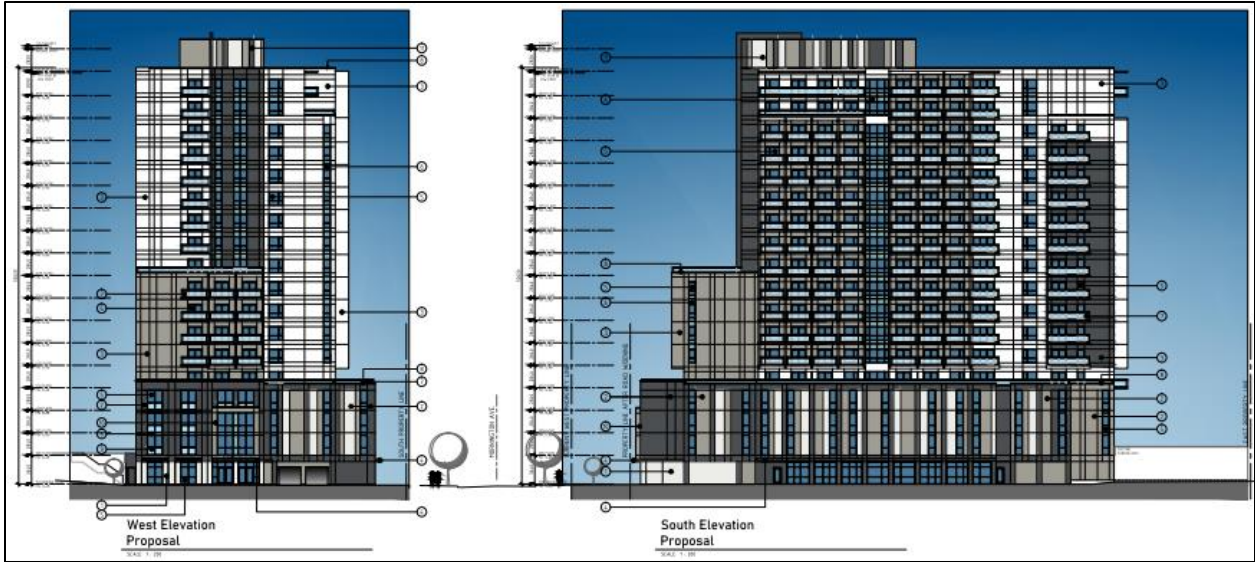
Original Proposal (January 2023)



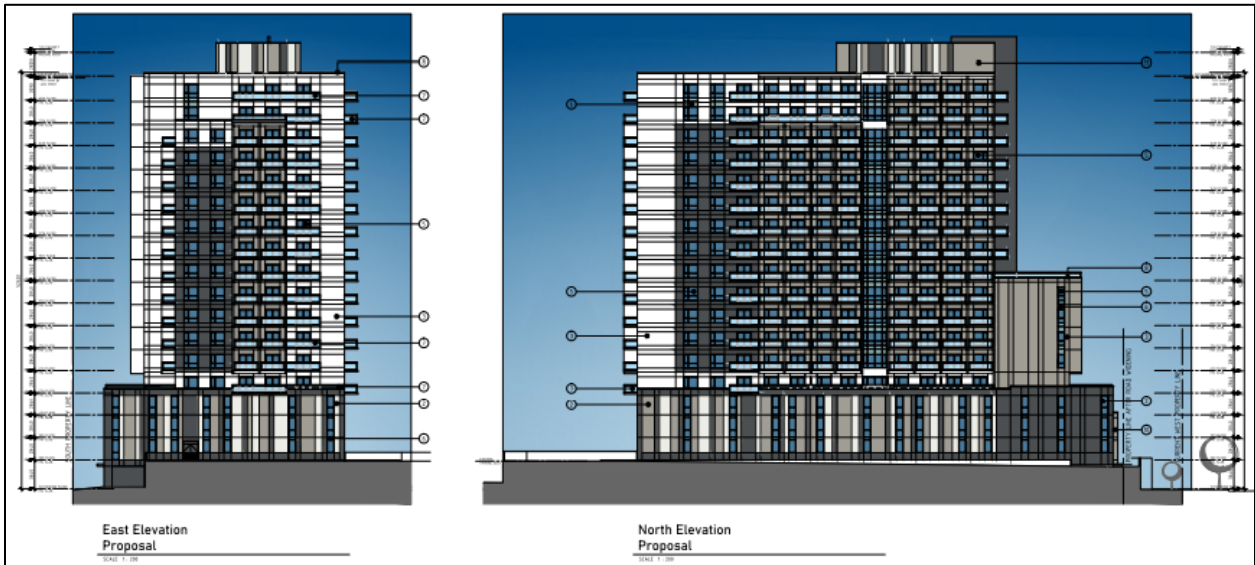
Concept Master Plan



Concept Site Plan



West and South Elevations



East and North Elevations



Renderings

Shadow Study Images – Original Proposal



March 21st 9:00 AM



March 21st 12:00 PM



March 21st 4:00 PM



June 21st 9:00 AM



June 21st 12:00 PM



June 21st 4:00 PM



September 21st 9:00 AM



September 21st 12:00 PM



September 21st 4:00 PM



December 21st 9:00 AM



December 21st 12:00 PM



December 21st 4:00 PM

Shadow Study Images – Revised Proposal



March 21st 9:00 AM



March 21st 12:00 PM



March 21st 4:00 PM



June 21st 9:00 AM



June 21st 12:00 PM



June 21st 4:00 PM



September 21st 9:00 AM



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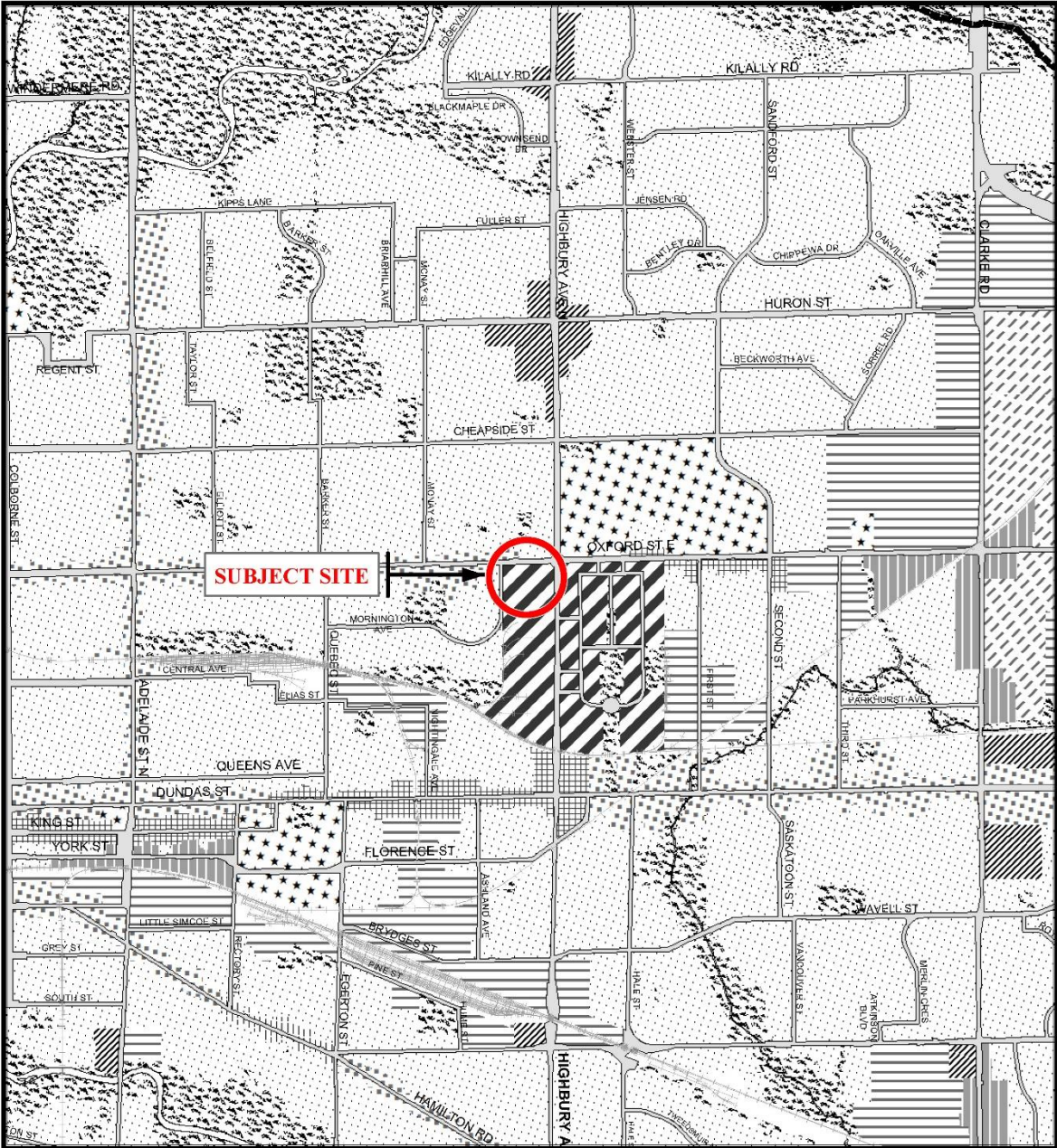
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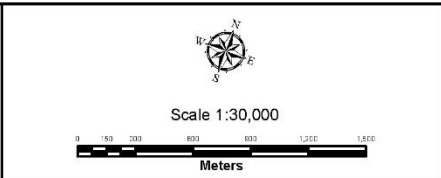


Legend

- | | | |
|------------------------|--------------------------|---|
| Downtown | Future Community Growth | Environmental Review |
| Transit Village | Heavy Industrial | Farmland |
| Shopping Area | Light Industrial | Rural Neighbourhood |
| Rapid Transit Corridor | Future Industrial Growth | Waste Management Resource Recovery Area |
| Urban Corridor | Commercial Industrial | Urban Growth Boundary |
| Main Street | Institutional | |
| Neighbourhood | Green Space | |

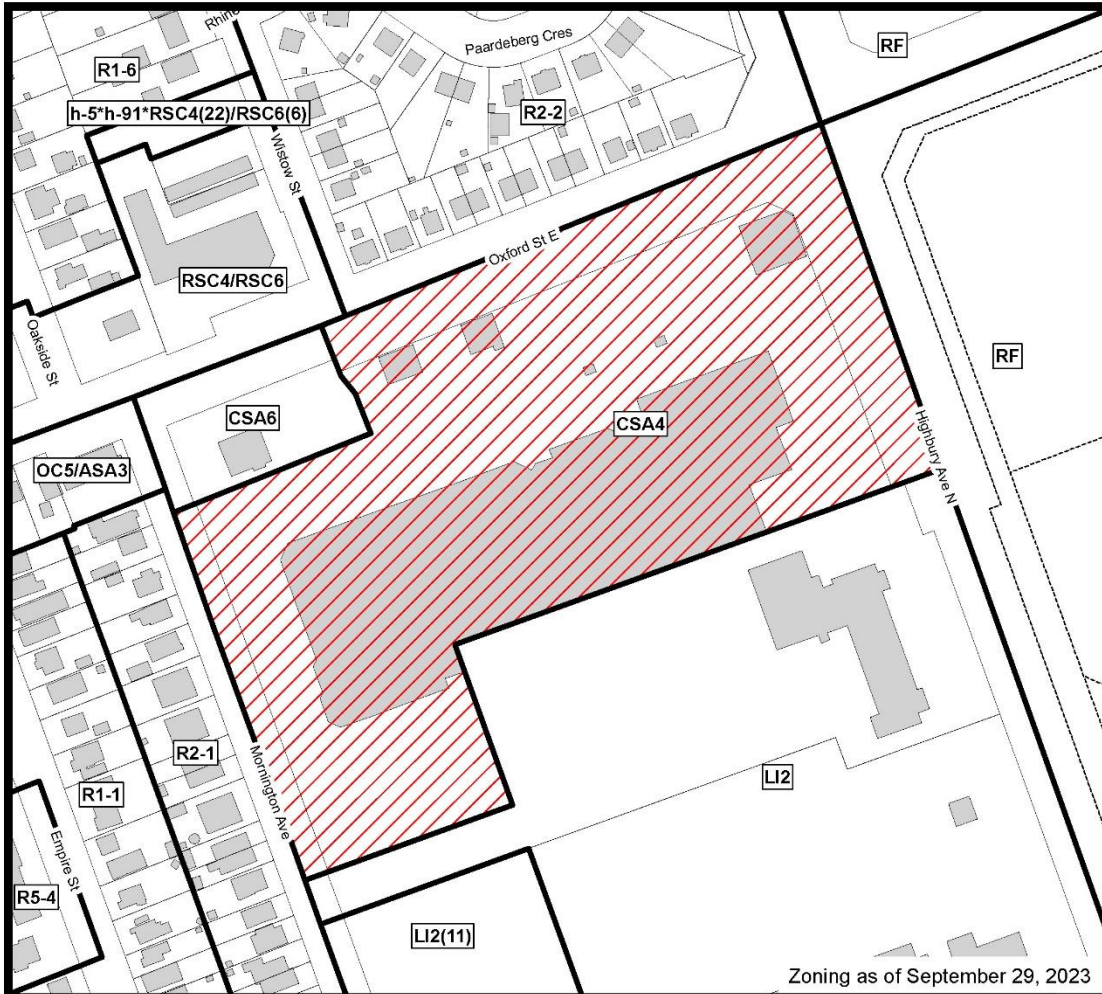
This is an excerpt from Planning & Development's working consolidation of Map 1 - Place Types of the London Plan, with added notations.

CITY OF LONDON
Official Plan
LONDON PLAN MAP 1
- PLACE TYPES -
 PREPARED BY: Planning & Development



File Number: Z-9589
Planner: CM
Technician: RC
Date: 2023/11/09

Project Location: E:\Planning\Projects\p_officialplan\workconsol00\excerpts_LondonPlan\mxd\Z-9589-Map1-PlaceTypes.mxd



COUNCIL APPROVED ZONING FOR THE SUBJECT SITE:

1) *LEGEND FOR ZONING BY-LAW Z-1*

- R1 - SINGLE DETACHED DWELLINGS
- R2 - SINGLE AND TWO UNIT DWELLINGS
- R3 - SINGLE TO FOUR UNIT DWELLINGS
- R4 - STREET TOWNHOUSE
- R5 - CLUSTER TOWNHOUSE
- R6 - CLUSTER HOUSING ALL FORMS
- R7 - SENIOR'S HOUSING
- R8 - MEDIUM DENSITY/LOW RISE APTS.
- R9 - MEDIUM TO HIGH DENSITY APTS.
- R10 - HIGH DENSITY APARTMENTS
- R11 - LODGING HOUSE

- DA - DOWNTOWN AREA
- RSA - REGIONAL SHOPPING AREA
- CSA - COMMUNITY SHOPPING AREA
- NSA - NEIGHBOURHOOD SHOPPING AREA
- BDC - BUSINESS DISTRICT COMMERCIAL
- AC - ARTERIAL COMMERCIAL
- HS - HIGHWAY SERVICE COMMERCIAL
- RSC - RESTRICTED SERVICE COMMERCIAL
- CC - CONVENIENCE COMMERCIAL
- SS - AUTOMOBILE SERVICE STATION
- ASA - ASSOCIATED SHOPPING AREA COMMERCIAL

- OR - OFFICE/RESIDENTIAL
- OC - OFFICE CONVERSION
- RO - RESTRICTED OFFICE
- OF - OFFICE

- RF - REGIONAL FACILITY
- CF - COMMUNITY FACILITY
- NF - NEIGHBOURHOOD FACILITY
- HER - HERITAGE
- DC - DAY CARE

- OS - OPEN SPACE
- CR - COMMERCIAL RECREATION
- ER - ENVIRONMENTAL REVIEW

- OB - OFFICE BUSINESS PARK
- LI - LIGHT INDUSTRIAL
- GI - GENERAL INDUSTRIAL
- HI - HEAVY INDUSTRIAL
- EX - RESOURCE EXTRACTIVE
- UR - URBAN RESERVE

- AG - AGRICULTURAL
- AGC - AGRICULTURAL COMMERCIAL
- RRC - RURAL SETTLEMENT COMMERCIAL
- TGS - TEMPORARY GARDEN SUITE
- RT - RAIL TRANSPORTATION

- "H" - HOLDING SYMBOL
- "D" - DENSITY SYMBOL
- "H" - HEIGHT SYMBOL
- "B" - BONUS SYMBOL
- "T" - TEMPORARY USE SYMBOL

CITY OF LONDON

PLANNING SERVICES / DEVELOPMENT SERVICES

**ZONING
BY-LAW NO. Z.-1
SCHEDULE A**



THIS MAP IS AN UNOFFICIAL EXTRACT FROM THE ZONING BY-LAW WITH ADDED NOTATIONS

FILE NO:

Z-9589

CM

MAP PREPARED:

2023/11/9

RC

1:2,500

0 12.525 50 75 100 Meters

Appendix D – Internal and Agency Comments

Staff/Agency Comments on 2nd Submission Development Concept:

Site Plan – September 26, 2023

1. Major Issues

- The proposed “drop off” area does not meet the intent of the Site Plan Control By-law to provide for a paratransit drop off area. The location proposed would utilize the entire one-way access. As noted in the Site Plan Control By-law, the “building entrance facility shall include a **separate** hard surface loading pad for use by paratransit and other service vehicles with minimum dimensions of 3.5 metres wide by 12.0 metres” and its to be “accessible from the internal driveway system and shall not conflict with other parking or manoeuvring facilities”. The applicant is to provide a separate lay-by as per the Site Plan Control By-law (Section 6.8.1).
- There are discrepancies between the “site plan” and the “ground floor plan” as it pertains to the drop off area, parking, access aisles, barrier-free stalls etc. Ensure all plans match moving forward.
- There are no pedestrian connections from the existing parking structure to the proposed residential development. Revise accordingly.

2. Matters for OPA/ZBA

- Dimensions are required for the internal drive-aisle widths for the parking area as well as dimensions for the standard parking stalls proposed within the building.
- Revise the internal parking layout. The entrance at the front only provides for access to the at-grade parking area with the access at the rear only providing access to the 2nd floor enclosed parking area. Provide for a continuous flow of traffic from one level to the next.
- Given the grading on site, there are no direct pedestrian connections internal for users to access the existing mall and uses on site. Explore opportunities to provide these connections.

3. Matters for Site Plan

- Landscape planting is required to be provided along the property boundaries to meet the Site Plan Control By-law.

Urban Design – September 29, 2023

1. Matters for Zoning

- The applicant is commended for providing additional glazing along the active edges of the proposed development. Urban Design staff encourage the applicant to retain this design feature through the development process.
- **The following Special Provisions are recommended** to be applied to the zoning for Building A:
 - Maximum height;
 - Minimum and maximum podium height;
 - Minimum step-back above the podium;
 - Minimum percentage of glazing on the podium;
 - Minimum percentage of glazing on the tower;
 - Minimum ground floor height of 4.5m;
 - Principal building entrance for the residential lobby along Morning Avenue;
 - Maximum floorplate size of 1000m² above the eight storey;
- Include zoning provisions for the height and step-backs to mitigate negative impacts on the existing and planned neighbourhood and to provide a human-scale environment along Building A’s active edges (TLP_253).
- Include provisions for a minimum ground floor height of 4.5m and a minimum percentage of transparent glazing to facilitate an active ground floor along Mornington Avenue and ensure any proposed above grade parking structures are wrapped in active uses along Building A’s active edges (TLP, 291).
- Clarify the floorplate size of each storey. To mitigate shadow impact on the neighbouring properties and promote a slender point tower with a distinct base,

middle, and cap, include a zoning provision for a maximum floorplate size of 1000m² above the eight storey (TLP, 289).

- To foster an active streetscape, include a zoning provision for the primary entrance to be located on the street-facing (west) building façade along Mornington Avenue (TLP, 268 & 291).

2. Matters for Site Plan

- Urban Design is supportive of the proposed landscaped amenity space along Mornington Avenue. Clarify if this element will be retained. Ensure that adequate outdoor amenity is provided for the number of anticipated residents (TLP_295).
- Rotate the south-west ground floor unit entrance to front onto Mornington Avenue.
 - Provide a walkway from the ground floor unit to the sidewalk.
- Clarify the relationship between the existing parking garage and Building A in addition to vehicular circulation through Building A. Remove the new parking garage access provided along Morning Avenue and relocate the entrance along the northern elevation (TLP, 269 & 270).
- Clarify cyclist circulation through Building A. Relocate or provide additional entrances to the *Bicycle Lock* room to mitigate vehicle and cyclist conflicts (TLP_255).
- Clarify pedestrian circulation through the structured parking garage to Building A's communal residential entrances. To mitigate pedestrian and vehicular conflicts, delineate pedestrian pathways throughout the parking garage to the internal communal entrances (TLP_255).

Engineering – October 3, 2023

Engineering comments remain the same, we are acceptable to the one building being rezoned, not the entire property.

The Water Feasibility Report submitted to support the rezoning concluded that the existing 150mm diameter watermain along Mornington Ave. is not sufficient to provide adequate fire flows for the proposed building.

We are requesting a holding provision to ensure upgrades are completed as part of the site plan application. Please include a holding provision:

h-(): Purpose: To ensure the adequate provision of municipal services, the holding provision shall not be removed until such time as there is an accepted water strategy and adequate capacity available.

Staff/Agency Comments on 1st Submission Development Concept:

Landscape Architecture – February 13, 2023

- I don't see any potential conflicts with trees on this application and I would not require a TPP. There are some trees growing in the Mornington blvd but they can be addressed at Site Plan.

UDPRP – February 15, 2023

General Comments

- The conceptual master site plan appears to be counter productive for achieving the vision of a Transit Village. As a general principle, taller buildings with higher density should be located closer to the transit station. Height and density should gradually reduce as one moves away from the transit station.

Conceptual Master Plan

- Developing on a brownfield site of the excess parking structure for residential use is to be commended, however the master plan as presented lacks conviction and the future parkette shown to the north of the development does not seem resolved with the topography.
- The conceptual master plan should include a clear vision and framework for the public realm that can support pedestrian activities and place-making.

- The proposed development (the current phase) does not support the proposed park. The park is going to be faced with a parking ramp and in the shadow of the proposed tower.

Site Plan, Building Massing and Design

- 18 stories is out of context with the single story residences across the street to the west and the step backing of the structure as it rises does not significantly soften the impact. Fewer stories should be considered.
- The proposed tower floor plate is quite overwhelming for either the low-rise neighbourhood to the west or the future park to the north.
- Despite being in the Transit Village, much of the ground floor of the proposed building is dedicated to parking and vehicular circulation.
- The proposed at grade amenity area is isolated and hardly attractive for residents.
- The proposed outdoor amenity space can be enhanced and expanded. At ground level, it should be integrated with the indoor amenity space. If the tower floor plate size gets reduced, more outdoor amenity space can be gained on the roof of the podium.
- Within both the planned and existing context, the property appears to be a mid-rise site.
- The podium of the building has no life as it is 4 levels of parking.
- The building will require more than just the use of brick. The use of painted concrete is not recommended.

Concluding comments:

- It is critical to develop a comprehensive master plan/block plan for a future urban community. A master plan for the entire block will assist in developing a walkable urban community in this transit village. The block plan should include:
 - streets, parks, driveway, servicing, and outdoor amenity spaces;
 - the conceptual building location, orientation, massing and separation distance with other development;
 - the building front entrances and their connections to the pedestrian/cycling routes;
 - phasing plan;
 - the existing and planned schools, community center or library in the block and how to link the proposed developments to these existing and future facilities with comfortable pedestrian walkways or bicycle trails.
- Without an appropriate block plan;
 - It is difficult to determine whether the proposed building setbacks are appropriate. Especially to the south property line, the proposed 4 storey podium setbacks 3.95m to the south property line and the tower setbacks 9m to the south property line. Are these setbacks sufficient if there is a future tower south of this?
 - It is difficult to plan the use both inside and outside of the building without knowing if there are any future park, public street or mid-block pedestrian walkways in the adjacent context.
- The proponent should work towards a conceptual master plan which address the site as a major transit hub.
- The proponent should reconsider the placement, height and massing of the building in relation to its context.
- The proponent should consider providing a more animated podium which contain active uses which address the street and adjacent lands.
- The use of painted concrete cladding should be reconsidered, for example, a combination of brick and metal panel.

UTRCA – February 23, 2023

- No objections.

London Hydro – February 23, 2023

- Servicing the above proposal should present no foreseeable problems. Any new and/or relocation of existing infrastructure will be at the applicant's expense,

maintaining safe clearances from L.H. infrastructure is mandatory. **Note:** Transformation lead times are minimum 16 weeks. Contact the Engineering Dept. to confirm requirements & availability.

- London Hydro has no objection to this proposal or possible official plan and/or zoning amendment. Any new or relocation of the existing service will be at the expense of the owner.

Parks Long Range Planning & Design – February 28, 2023

- Parkland dedication is required in the form of cash in lieu, pursuant to By-law CP-25 and will be finalized at the time of site plan approval.

Ecology – March 1, 2023

- There are currently no ecological planning issues related to this property and/or associated study requirements.

Site Plan – March 6, 2023

1. A lay-by is required given that there are more than 24-units proposed. Refer to Section 6.8.1 of the Site Plan Control By-law for lay-by requirements. The rear servicing and loading does not function for this purpose as the lay-by is to be 15 metres from the main building entrance
2. For underground parking stalls, please refer to Section 6.2 of the Site Plan Control By-law for required dimensions.
3. The accessible parking stalls are to be sized in accordance with the Zoning By-law Z.-1 and located in close proximity to the building access points.
4. The proposed outdoor common amenity space does not appear large enough to accommodate all units on site. Staff typically look for an amenity space of 5m² per unit. Clarify the programming for the roof-top amenity space.
5. Identify the proposed snow-storage area on the site plan.
6. The proposal does not provide for adequate spacing between the building and property lines to provide for the required landscape strip between the property lines (refer to Table 9.4 of the Site Plan Control By-law).
7. I've reached out to Solid Waste Management regarding the proposed garbage collection.

Based on the current concept plan, site plan staff have concerns with how the site will function.

Urban Design – March 7, 2023 (revised March 22, 2023)

Provide a response to the **February 2023 UDPRP memo**. After attending the UDPRP, the applicant received a formal memo from the UDPRP Chair, or their designate. A Comment Response Table outlining in detail the applicant's response to the UDPRP is required.

- Provide updated drawings that reflect the revisions made to address the UDPRP comments.

Comments for Zoning

Urban Design does not support the proposed built-form due to its height and location. The proposed 18-storey residential building does not provide an effective transition in height between the adjacent neighbourhood. The proposed built form is located away from the rapid transit route along Highbury Avenue North and is on the edge of the Transit Village Place Type next to Mornington Avenue, a neighbourhood connector street. We recommend that a low- or mid-rise building is more appropriate in the location of the proposed 18-storey built form.

If the intensity and form is deemed to be appropriate, the following matters should be addressed through zoning:

- Zoning provisions for the height, setbacks, & step back should be provided to mitigate negative impacts on the existing neighbourhood. Refer to the London Plan, Policy 253.
 - Provide a minimum 5m step-back above the 3rd to 5th storey along Mornington Avenue and the southern private asphalt road to provide a human-scale environment along the public/private street(s).

- The step-back provided above the 4th storey along Mornington Avenue is acknowledged.
- If a high-rise form is deemed to be appropriate, design the tower portion (the 8th storey and above) as a slender tower (maximum floor plate size of up to 1000 square meters and a length to width ratio of 1:1.5) to reduce the "slab-like" appearance of the tower. Which will mitigate shadow impacts, and obstruction of sky views to be less imposing on the neighbouring properties and public spaces. The floor plate size proposed above 8 storeys is large and elongated (approximately 1175,41 sqm).
 - A provision of a side-yard setback of 5m along the southern elevation to provide adequate space and sight lines to the amenity space at the rear of the proposed development.
 - A provision for the primary entrance to be located on the street-facing (west) building façade.
- Include provisions for a minimum ground floor height of 4m and a minimum percentage of transparent glazing on the building's podium/base and front elevation to facilitate an active ground floor and façade along Mornington Avenue. This will ensure that any above ground parking structures are wrapped in active uses along the public and private street(s).
- Include a provision for a minimum size shared amenity space on site. This could be at grade and/or on a rooftop.

Items to be Addressed at Site Plan

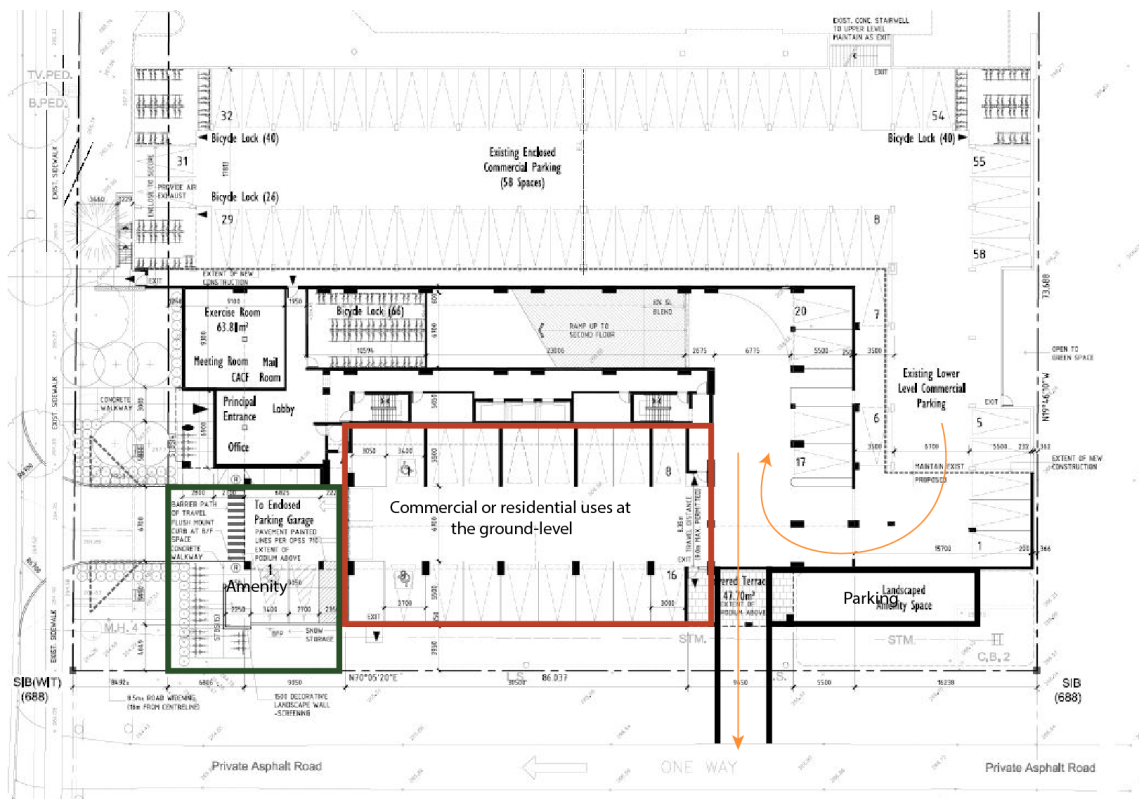
- Ensure that the elevations and renderings match the site plan. Ensure that the surrounding context is included in the elevations, site plans, and renderings. Provide a rooftop plan that outlines the placement and screening of any mechanical equipment. Further Urban Design comments may follow upon receipt.

Site Design:

- Clarify the location of the primary entrance along Mornington Avenue and provide signage to identify the main public lobby entrance.
- Clarify the condition between the proposed building and the existing parking structure along the northern elevation. Is the walkway enclosed or open-air? Ensure that the width of the walkway is a minimum of 2m.
 - CPTED Principles should be integrated into the design.
- If the outdoor amenity space cannot be located closer to an active frontage, provide a direct pathway to the proposed amenity area at the rear of the subject site. Relocate a few of the proposed temporary bicycle parking spaces and some of the landscaping to create clear sightlines from the front of the subject site to the rear.
 - CPTED Principles should be integrated into the design.
- Consider including a modest parking bay area along Mornington Ave for convenient vehicle drop-offs and deliveries in close proximity to the main entrance.
- The addition of temporary bicycle parking is acknowledged. Provide for pedestrian and transit-oriented amenities including benches and bike racks close to the principal entrance.

Building Design:

- Consider providing an underground parking facility to reduce the height of the proposed built form. Refer to the London Plan, Policy 814.11.
- Consider removing the parking garage access from Mornington Avenue. Consider a joint access / access easement for the parking entrance from the private asphalt road (Canada Post) to the south or through the existing commercial parking garage.



Proposed Ground Floor Plan Floor Plate 1522.16m² (16384.93ft²)

SCALE: 1:200
PARKING SPACES:
 2 Surface
 20 Proposed Enclosed Residential
 58 Existing Enclosed Commercial

- Rerouting the access to the parking garage will provide opportunities to provide an active frontage along Mornington Avenue and the southern private asphalt road.
- The proposed built form should be designed to establish and support a high-quality pedestrian environment with active frontage along Mornington and any internal streets (public/private) and driving aisles (i.e, 1.8m walkways, benches, street trees, ground level active windows, balconies at the 2nd storey and above, ground level patios) . Refer to the London Plan, Policy, 814.7.
 - CPTED Principles should be integrated into the design.
 - Consider extending the awning to the covered terrace to provide weather protection for subject site users accessing the amenity space from Mornington Avenue.

Engineering – March 7, 2023

Comments related to Rezoning:

- The watermain feasibility report will need to be updated to address the following:
 - Hydrant flow tests were taken from the hydrant at 616 Mornington Ave and the hydrant at 560 Mornington Ave. The flow volume used to justify the fire protection at 625 Mornington Ave was taken from the fire hydrant further down the street at 560 Mornington Ave which is located on a 250mm Municipal watermain. The flow volume should be taken from the hydrant nearest the subject property at 616 Mornington Ave to determine accurate fire flow availability at the site.
- Sewer Engineering comments related to the proposed 18 Storey Tower off of Mornington Ave.:
 - Based on a previous sanitary brief recently circulated that included an 18-storey tower (187 units) along with the existing commercial buildings that are expected to remain, SED acknowledges we are amicable, however we further noted there is no additional available capacity for further intensification beyond this 18-storey tower and current land use.
- Sewer Engineering comments related to the proposed conceptual Master Plan:
 - The master plan that was subsequently provided did not provide sufficient detail as far as the applicant's ultimate intensification beyond including projected building heights and did not include servicing assumptions for the

conceptual master plan nor the maximum build out per the zone or the maximum population.

- Based on record drawings and the 2016 IRP sanitary area plans and design sheets, the downstream sewer system does not have adequate surplus capacity available to support the proposed master plan and intensification sought for the subject lands. Additionally, there are no future projects or plans to upgrade the downstream sewer system which were part of a recent 2016 IRP. This increase in growth, density and build form being suggested exceeds current standards and was not contemplated.
- SED is not supportive of the proposed intensification referenced in the master conceptual plan as there is no available surplus capacity in the downstream sewer system.

The following items are to be considered during a future site plan application stage:

Transportation:

- Right of way dedication of 24.0m from centre line on Oxford Street East and Highbury Ave N. within 150m of an intersection.
- Right of way dedication of 18.0m from centre line on Oxford Street East outside of the 24.0m from centre line.
- Right of way dedication of 20.0m from centre line on Highbury Ave. N. outside of the 24.0m from centre line.
- Right of way dedication of 18.0m from centre line required along Mornington Avenue.
- Revised 6.0m x 6.0m daylight triangles at all intersection corners.
- Detailed comments regarding access design and location will be made through the site plan process.
- TIA prepared by BT Engineering, dated October 24th, 2022 has been accepted.

Stormwater:

- As per the City of London's Design Requirements for Permanent Private Systems, the proposed application falls within the Central Subwatershed (case 4), therefore the following design criteria should be implemented:
 - the flow from the site must be discharged at a rate equal to or less than the existing condition flow;
 - the discharge flow from the site must not exceed the capacity of the stormwater conveyance system;
 - the design must account the sites unique discharge conditions (velocities and fluvial geomorphological requirements);
 - "normal" level water quality is required as per the MECP guidelines and/or as per the EIS field information; and
 - shall comply with riparian right (common) law.

The consultant shall provide a servicing report and drawings to present calculations, recommendations and details to address these requirements.

- As per City as-constructed 23606, the site is tributary to the existing 675mm storm sewer on Mornington Avenue. For proposed development that increases the existing C-value of the site, the development is to store volumes in excess of the existing condition flow. On-site SWM controls design should include, but not be limited to required storage volume calculations, flow restrictor sizing, bioswales, etc.
- As per the Drainage By-law, the consultant would be required to provide for a storm pdc ensuring existing peak flows from the 2 through 100 year return period storms are maintained pre to post development with any increase in flow being managed onsite. It appears that the site may already be serviced by an existing maintenance hole and 250mm sewer per as-con 23606, consultant is to confirm.
- The consultant is encouraged to implement LID solutions to provide a reduction to the site's existing peak flows. If undertaken, this site may be eligible to qualify for a Stormwater Rate Reduction as outlined in Section 6.5.2.1 of the Design Specifications and Requirements manual. Interested applicants can find more information and an application form at the following: <https://london.ca/living-london/water-environment/your-water-bill>.

- The consultant may note that downstream quality control is provided by the Mornington Pond SWMF therefore no on-site quality treatment is required.

General comments for sites within Central Thames Subwatershed

- The subject lands are located within a subwatershed without established targets. City of London Standards require the Owner to provide a Storm/Drainage Servicing Report demonstrating compliance with SWM criteria and environmental targets identified in the Design Specifications & Requirements Manual. This may include but not be limited to, quantity control, quality control (70% TSS), erosion, stream morphology, etc.
- The Developer shall be required to provide a Storm/drainage Servicing Report demonstrating that the proper SWM practices will be applied to ensure the maximum permissible storm run-off discharge from the subject site will not exceed the peak discharge of storm run-off under pre-development conditions up to and including 100-year storm events.
- The Owner agrees to promote the implementation of SWM Best Management Practices (BMP's) within the plan, including Low Impact Development (LID) where possible, to the satisfaction of the City Engineer. It shall include water balance.
- The owner is required to provide a lot grading plan for stormwater flows and major overland flows on site, ensuring that stormwater flows are self-contained and that grading can safely convey up to the 250 year storm event, all to be designed by a Professional Engineer for review.
- The Owner shall allow for conveyance of overland flows from external drainage areas that naturally drain by topography through the subject lands.
- Stormwater run-off from the subject lands shall not cause any adverse effects to adjacent or downstream lands.
- An erosion/sediment control plan that will identify all erosion and sediment control measures for the subject site and that will be in accordance with City of London and MECP (formerly MOECC) standards and requirements, all to the specification and satisfaction of the City Engineer. This plan is to include measures to be used during all phases of construction. These measures shall be identified in the Storm/Drainage Servicing Report.

Heritage – March 15, 2023

This memo is to confirm that I have reviewed the following and find the report's (analysis, conclusions, and recommendations) to be sufficient to fulfill the heritage impact assessment requirements for (Z-9589):

- AECOM Canada Ltd. (2022, December). *Heritage Impact Assessment Memo, Proposed Development of 625 Mornington Avenue, Adjacent to 955 Highbury Avenue North, a Heritage Listed Property, City of London.*

Specific supportable conclusions of the heritage impact assessment (HIA) are as follows:

[T]he proposed development at 625 Mornington Avenue will not have any direct or indirect impacts on the heritage attributes of 955 Highbury Avenue North, due to the location of the proposed development and its separation. (p8)

Based on the impact assessment completed for this HIA Memo, no mitigation measures are required since the heritage attributes of listed property at 955 Highbury Avenue North will not be impacted by the proposed development at 625 Mornington Avenue. (p7)

Based on the review of the HIA, heritage staff is satisfied that there will be no adverse impacts to the adjacent LISTED property at 955 Highbury Avenue North as a result of development at 625 Mornington Avenue. It has been sufficiently demonstrated that significant heritage attributes will be conserved, and the HIA can be accepted to meet heritage requirements for (Z-9589).

Appendix E – Public Engagement

From: noemi perzia

Sent: Wednesday, March 1, 2023 4:56 PM

To: Maton, Catherine <cmaton@london.ca>; Stevenson, Susan <sstevenson@london.ca>

Subject: [EXTERNAL] Re: Zoning By Law Amendment-625 Mornington Av

Planning and Development; Ward Councillor:

Dear Madam:

Further to our phone discussion on the Month Of February ;After receiving a letter from the city about the above development:

This Development 18 -Storey building ,187 unit apartment building 625 Mornington Av. is the wrong place as this area is single families.

The proposed building is to close and narrow to single residential families.

This building will destroy the privacy and enjoyment of this families.

The traffic in Mornington Av. will be chaotic as 187 families -400 people will use the road Mornington already congested by high traffic and used for emergencies: fire dept., ambulances and police cruisers to short cut to downtown ;Speeding is high in this area. Hope you have into consideration this building project that will affect the life of neighbors of Mornington Av.

Best Regards

Noemi Perzia

Hope you have this into consideration for this Zoning

From: chfunn

Sent: Saturday, February 25, 2023 8:14 PM

To: Maton, Catherine <cmaton@london.ca>

Cc: Cathy Heal

Subject: [EXTERNAL] File Z-9589 625 Mornington Ave

Hello Catherine Maton,

I live at [REDACTED], here are my comments/questions regarding this application.

- 18 stories seems quite high. The apartments further south on Mornington have 11 floors and are well set back from the road. Noise & light pollution are contained close to the buildings. If the new building is close to the road, and overlooking the area, this will negatively affect houses nearby. Garbage pick up should not be close to Mornington to reduce noise. The building possibly needs to be shorter.

On windy days when there is an east wind, it is already quite strong. Any idea how this will be affected by the building ? I don't want any roof damage.

There are currently many birds in the area. Canada geese, ducks, sea gulls & smaller birds. Are the glass windows being used going to prevent the birds from flying into them?

Also, Orange Air helicopters frequently fly over this area and the pilots from the air show at London Airport.

- Traffic & parking

There is no street parking on Mornington. Currently there is not sufficient parking for the neighbours. Some are parking regularly in the parking building.

Please ensure there are enough parking spots and visitor parking for the building.

Is there any solution for current neighbours? Street parking doesn't seem feasible on such a busy road.

The exit on Mornington, from the Canada Post building on Highbury Ave, is quite busy when the delivery trucks exit to their routes in the AM. If the exit from the apt building is in the same area, it will cause congestion and could be dangerous to oncoming traffic, cars exiting their driveways and any pedestrians.

Suggest a third lane up to the mall entrance from the Canada Post exit may need to be added.

I would appreciate your taking these points into consideration. If some items are for other departments, please pass along.

I look forward to your reply by email.

Regards
Cathy Heal

From: Judy White
Sent: Sunday, February 19, 2023 3:12 PM
To: Maton, Catherine <cmaton@london.ca>
Subject: [EXTERNAL] File Z-9589

At present the red light at Mornington and Oxford is one of the longest in the city to make a left hand turn on to Oxford St.

I can't imagine how it can possibly work with more traffic!

I hope this is being taken into consideration.

Thanks for your time.

Judy

From: Helena Fernandes
Sent: Friday, February 17, 2023 4:32 PM
To: Maton, Catherine <cmaton@london.ca>
Subject: [EXTERNAL] It's about the building that is going to be built In 625 Mornington Avenue this area is not for business like that if the owner of the property wants to do it he can built in front of his house to how we feel about that no way that building ...

Report to Planning and Environment Committee

To: Chair and Members
Planning and Environment Committee

From: Scott Mathers, MPA, P.Eng.
Deputy City Manager, Planning and Economic Development

Subject: Willow Bridge Homes Ltd./o Zelinka Priamo Ltd.
488-492 Pond Mills Road
File Number: Z-9625, Ward 14
Public Participation Meeting

Date: December 4, 2023

Recommendation

That, on the recommendation of the Director, Planning and Development, the following actions be taken with respect to the application of Willow Bridge Homes Ltd. c/o Zelinka Priamo Ltd. relating to the property located at 488-492 Pond Mills Road:

- (a) The proposed by-law attached hereto as Appendix "A" **BE INTRODUCED** at the Municipal Council meeting December 19, 2023, to amend Zoning By-law No. Z.-1, in conformity with the Official Plan, The London Plan, to change the zoning of the subject property **FROM** a Residential R1 (R1-3) Zone **TO** a Residential R9 Special Provision (R9-3(_)*H15) Zone;
- (b) The Site Plan Approval Authority **BE REQUESTED** to consider the following issues through the site plan process:
 - i. The possible addition of a public pathway/easement for pedestrian access to Pond Mills Road from Glenroy Crescent;
 - ii. Additional landscaping to be implemented along the eastern property boundary adjacent to Glenroy Crescent;
 - iii. Enhanced tree planting;

IT BEING NOTED that the above noted amendment is being recommended for the following reasons:

- i. The recommended amendment is consistent with the *Provincial Policy Statement, 2020 (PPS)*, which encourages the regeneration of settlement areas and land use patterns within settlement areas that provide for a range of uses and opportunities for intensification and redevelopment. The *PPS* directs municipalities to permit all forms of housing required to meet the needs of all residents, present and future;
- ii. The recommended amendment conforms to The London Plan, including but not limited to the Key Directions, City Design and Building policies, and the Neighbourhoods Place Type policies;
- iii. The recommended amendment would permit an appropriate form of development at an intensity that is appropriate for the site and the surrounding neighbourhood; and
- iv. The recommended amendment facilitates an infill development on an underutilized site and provides a broader range and mix of housing options within the area.

Executive Summary

Summary of Request

The applicant has requested an amendment to the Zoning By-law Z.-1 to rezone the subject site from a Residential R1 (R1-3) Zone to a Residential R9 Special Provision (R9-3(_)*H15) Zone.

Purpose and the Effect of Recommended Action

Staff are recommending approval of the requested Zoning Bylaw amendment with additional special provisions that will permit a 4-storey residential apartment building with a maximum density of 100 units per hectare (uph). Special provisions requested by the applicant and recommended by staff include: consider Pond Mills Road as the front lot line; a minimum front yard depth of 3.0 metres whereas 10.0 metres is required; a minimum north interior side yard depth of 4.4 metres whereas 5.6 metres is required; a minimum parking lot setback of 3.0 metres from the rear lot line; a building orientation and entrance to Pond Mills Road; and a maximum height of 15 metres (4 storeys).

The recommended action will permit a 4-storey, **39-unit**, residential apartment building.

Linkage to the Corporate Strategic Plan

This recommendation supports the following Strategic Areas of Focus:

- a. **Wellbeing and Safety**, by promoting neighbourhood planning and design that creates safe, accessible, diverse, walkable, healthy, and connected communities.
- b. **Housing and Homelessness**, by supporting faster/streamlined approvals and increasing the supply of housing with a focus on achieving intensification targets.
- c. **Climate Action and Sustainable Growth** by ensuring waterways, wetlands, watersheds, and natural areas are protected and enhanced.

Analysis

1.0 Background Information

1.1 Property Description and Location

The subject site is located on the east side of Pond Mills Road, north of Southdale Road East, within the Glen Cairn Planning District. The site has a total area of approximately 0.4 ha, with 68.4m of frontage along Pond Mills Road, and a maximum lot depth of approximately 59.1m. The site is a through lot with a one-foot reserve along Glenroy Crescent. The lands are vacant (a previous single detached dwelling and workshop were demolished in 2015) and generally slope toward the property lines, which are bordered by perimeter trees. A portion of the south side of the subject site is subject to an easement (Instrument No. 139822) related to an existing gas line (Enbridge).

The subject site is part of an established low-density residential community with a mix of single and multi-unit dwellings, ranging in built form and height in the immediate surrounding area from one to three storeys. Adjacent properties to the north, west, and east consist of single-detached dwellings. Abutting the southern lot line is a 3-storey townhouse community known as Miller's Cove (with vehicular access from Pond Mills Road and Pond View Road). There are also several small-scale commercial plazas uses to the north and northeast. Westminster Ponds is in close proximity to the site.

1.2 Site Statistics

- Current Land Use: Vacant Residential
- Frontage: 68.6 metres (225 feet)
- Depth: 59.3 metres (194.5 feet)
- Area: 0.4 hectares (1 acre)
- Shape: Rectangular
- Located within the Built Area Boundary: Yes
- Located within the Primary Transit Area: Yes

Surrounding Land Uses

- North: Residential
- East: Residential
- South: Residential/ Open Space
- West: Residential/ Open Space

Existing Planning Information

- Existing The London Plan Place Type: Neighbourhoods fronting a Civic Boulevard
- Existing Zoning: Residential R1 (R1-3) Zone

Additional site information and context is provided in Appendix B.

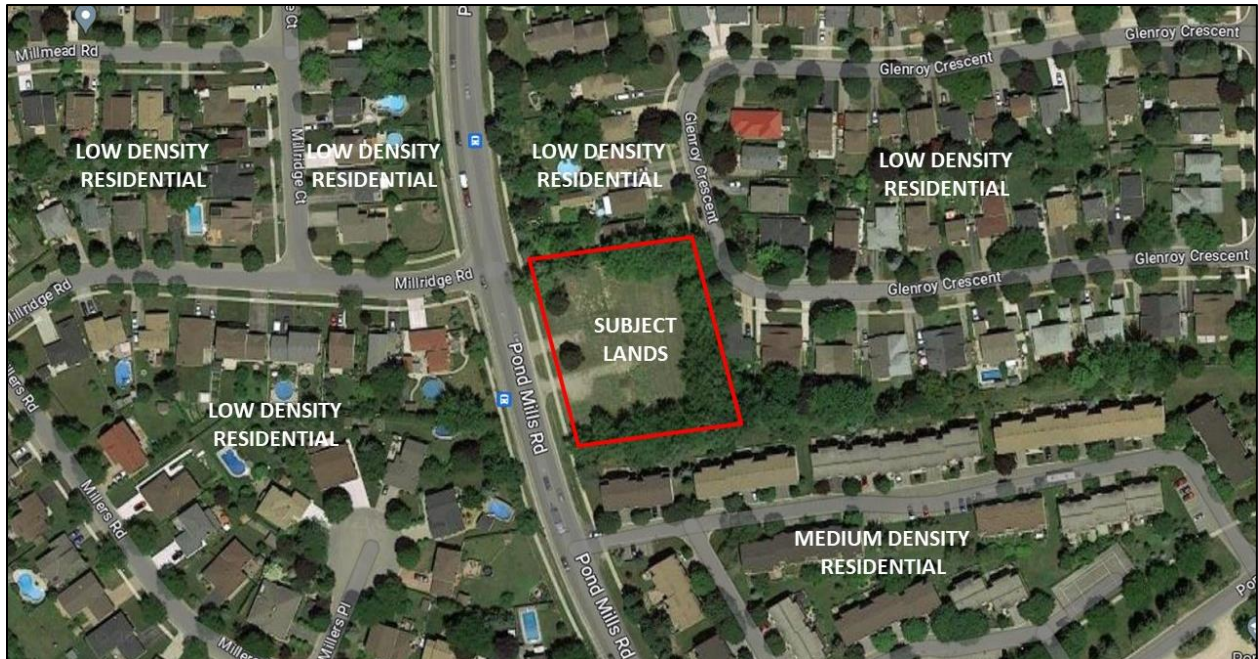


Figure 1- Aerial Photo of 488-492 Pond Mills Road and surrounding lands



Figure 2 - Streetview of 488-492 Pond Mills Road (view looking east)

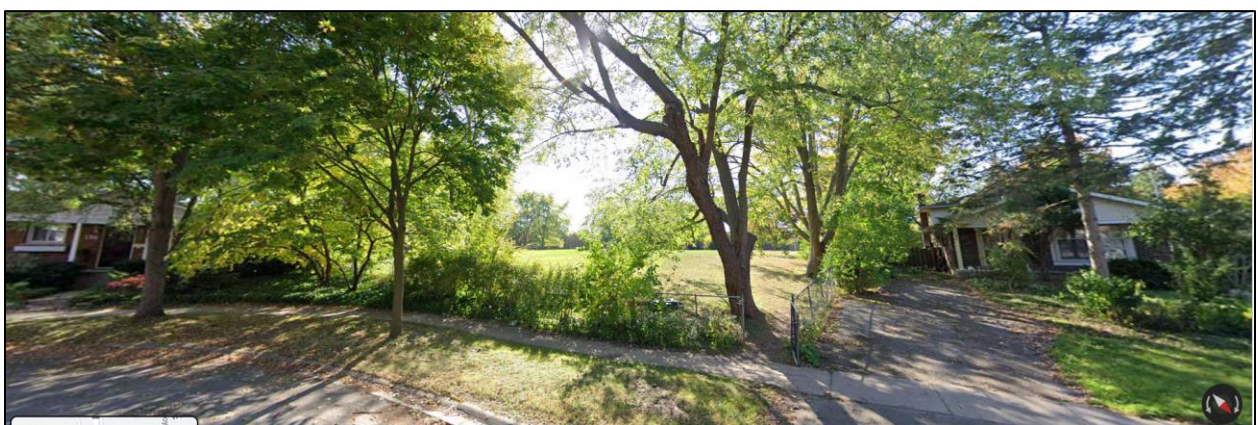


Figure 3 - Streetview of 488-492 Pond Mills Road from Glenroy Crescent (view looking west)

2.0 Discussion and Considerations

2.1 Development Proposal (May 2023)

In May 2023, the City accepted a complete zoning by-law amendment application. The development proposal is comprised of a 4-storey, 39-unit, residential apartment building with a maximum density of 100 uph. The application included a conceptual site plan,

shown below as Figure 5. Building rendering and elevations are shown in Figures 4-7 below.

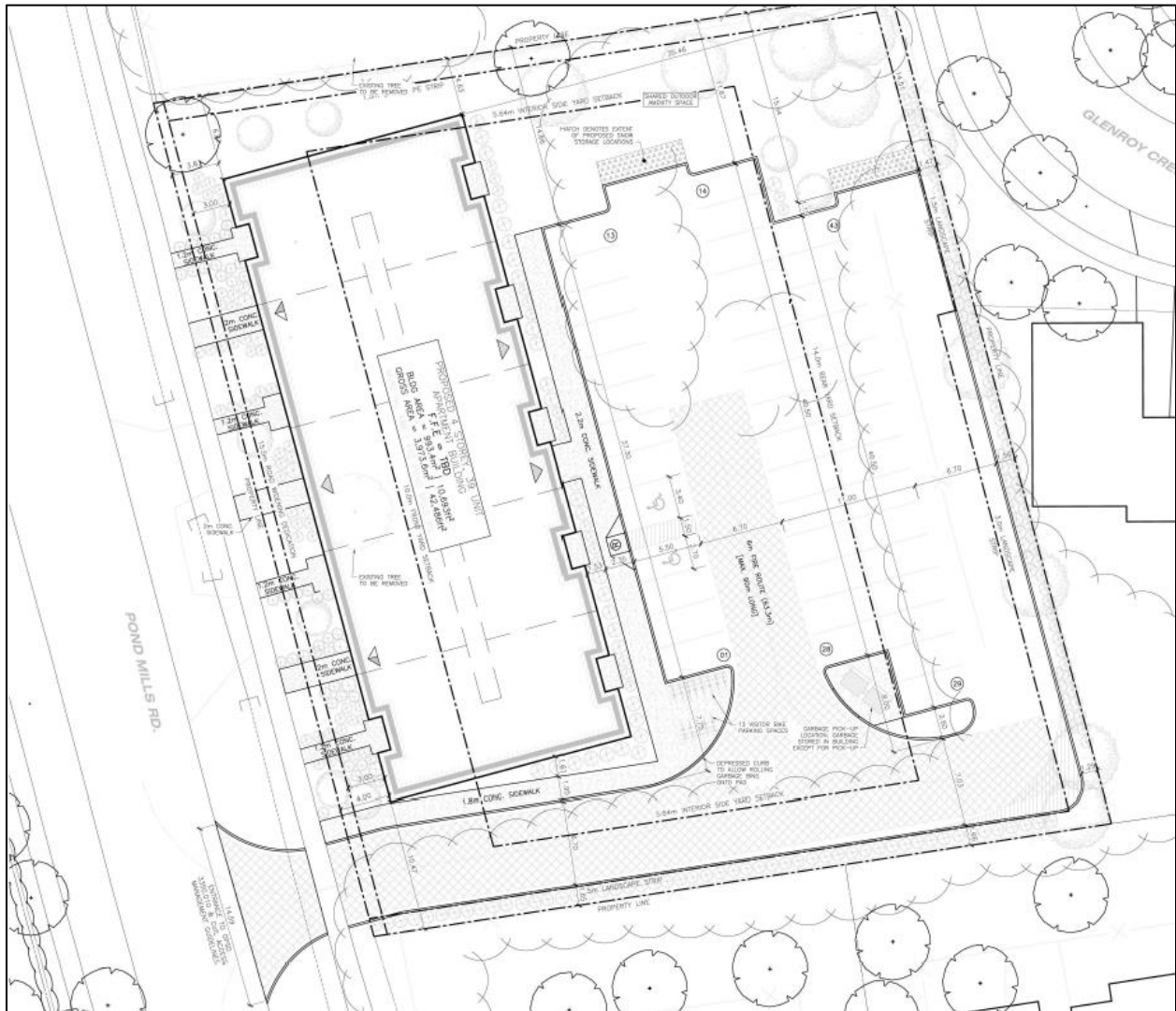


Figure 4 - Conceptual Site Plan (received May 2023)



Figure 5 –Renderings of proposed apartment building at 488-492 Pond Mills Road (received May 2023)



Figure 6 – West and South view building façade (received May 2023)



Figure 7 – East and North view of building façade (received May 2023)

2.2 Revised Development Proposal (October 2023)

Based on comments provided by Staff, the applicant submitted a revised conceptual site plan, shown in Figure 8 below. The revised development proposal continues to comprise of a 4-storey, 39-unit, residential apartment building with a maximum density of 100uph. Revisions to the development proposal include a reconfiguration of the building footprint and movement northwards in order to provide for the new vehicle layby (at the southerly end of the building), the main drive aisle, a new bank of parking spaces as well as the 3m landscaping setback along the southerly property line and alteration to the built form to increase the yard setbacks. The revised conceptual site plan is shown in Figure 8 below.

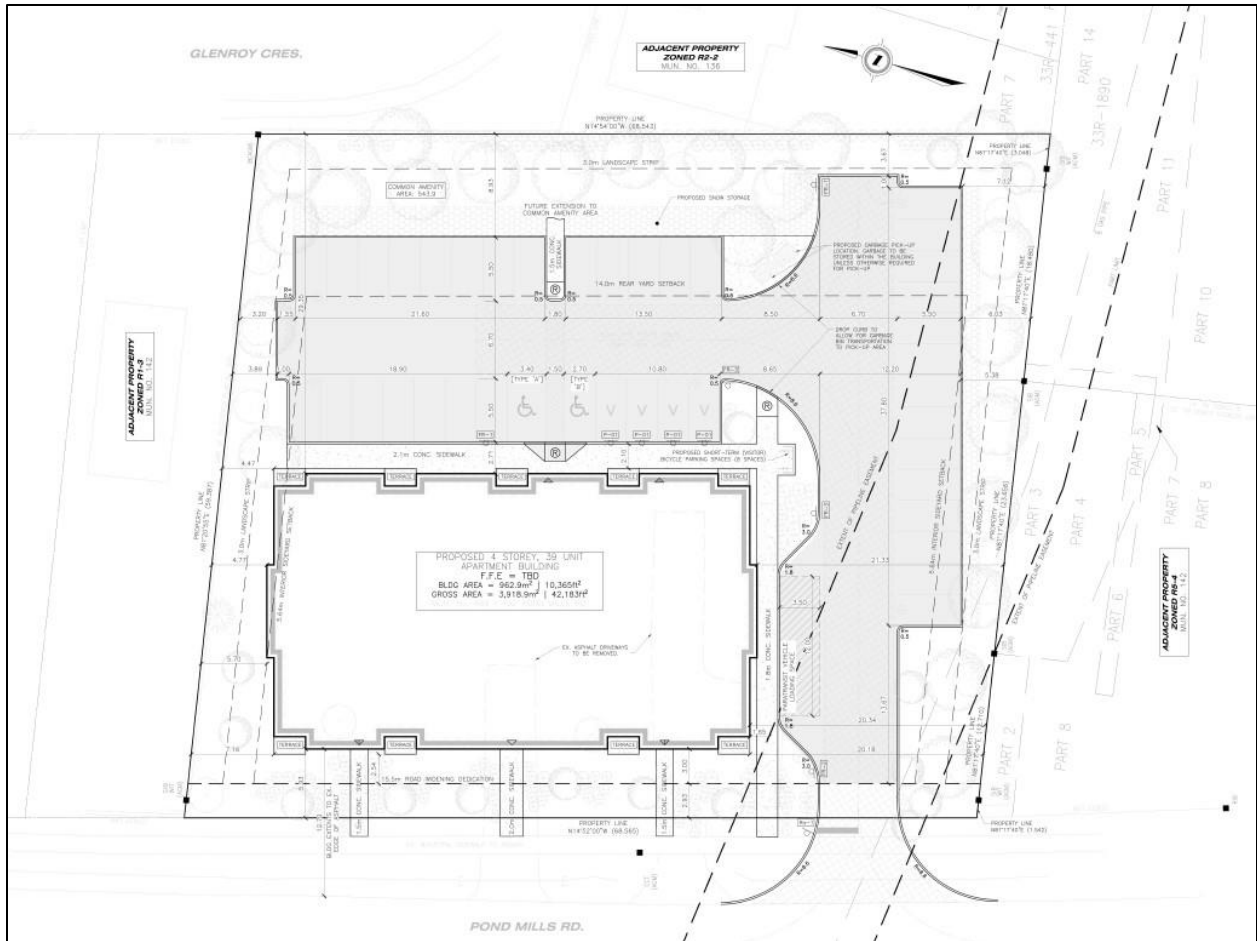


Figure 8 – Revised Conceptual Site Plan (received October 2023)

The proposed development includes the following features:

- Land use: Residential
- Form: Low-rise apartment building
- Height: 4 storeys (14.1m)
- Residential units: 39 units
- Density: 98 units per hectare
- Gross floor area: 3,973m²
- Building coverage: 24.5%
- Parking spaces: 43 surface parking spaces
- Bicycle parking spaces: 48 spaces
 - External bicycle parking is proposed at the southeasterly corner of the building, and long-term spaces are to be provided internally.
- Landscape open space: 32.4%
- Functional amenity space: 624.8m²

Additional proposal information and context is provided in Appendix B and C.

2.2 Requested Amendment(s)

The applicant has requested an amendment to the Zoning Bylaw Z.-1 to rezone the property from a Residential R1 (R1-3) Zone to a Residential R9 Special-Provision (R9-3(_)) Zone.

The following table summarizes the special provisions that have been proposed by the applicant and those that are being recommended by staff.

Regulation (R9-3)	Required (m)	Proposed (m)	Staff Recommended
Maximum Building Height (metres)		14.1	15
Minimum Front Yard Depth (metres)	10.0	3.0	3.0
Minimum Interior Side Yard Depth (metres)	5.64	4.47 (northern lot line)	4.4

Regulation (R9-3)	Required (m)	Proposed (m)	Staff Recommended
Minimum Rear Yard Parking Setback (metres)	1.5	1.56	3.0

2.3 Internal and Agency Comments

The application and associated materials were circulated for internal comments and public agencies to review. Comments received were considered in the review of this application and are addressed in Section 4.0 of this report.

Key issues identified by staff and agencies included:

- Tree preservation
- Parking setbacks
- Connecting pedestrian pathway from Pond Mills Road to Glenroy Crescent
- Screen proposed parking from Glenroy Crescent
- Discrepancies between concept plan and Urban Design Brief

Detailed internal and agency comments are included in Appendix “D” of this report.

2.4 Public Engagement

On June 14, 2023, Notice of Application was sent to 294 property owners and residents in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on June 15, 2023. A “Planning Application” sign was also placed on the site.

There were 16 responses received during the public consultation period. A petition with 41 signatures was also received. All comments received were considered in the review of this application and are addressed in Section 4.0 of this report.

Concerns expressed by the public relate to:

- Increased traffic and congestion
- Reduced setbacks – too close to the road and existing homes
- Doesn’t fit within existing neighbourhood
- Loss of trees
- Site concerns - privacy/noise/lighting/fencing/lack of amenity space
- Not enough parking
- Site too small/too intense proposal
- Stormwater impacts
- Construction impacts
- Location of driveway
- Tenancy of building
- Affect property values

Detailed public comments are included in Appendix “E” of this report.

2.5 Policy Context

The Planning Act and the Provincial Policy Statement, 2020

The Provincial planning policy framework is established through the *Planning Act* (Section 3) and the *Provincial Policy Statement, 2020 (PPS)*. The *Planning Act* requires that all municipal land use decisions affecting planning matters shall be consistent with the *PPS*.

The mechanism for implementing Provincial policies is through the Official Plan, The London Plan. Through the preparation, adoption, and subsequent Ontario Land Tribunal (OLT) approval of The London Plan, the City of London has established the local policy framework for the implementation of the Provincial planning policy framework. As such,

matters of provincial interest are reviewed and discussed in The London Plan analysis below.

As the application for a Zoning By-law amendment complies with The London Plan, it is staff's opinion that the application is consistent with the *Planning Act* and the *PPS*.

The London Plan, 2016

The London Plan (TLP) includes evaluation criteria for all planning and development applications with respect to use, intensity and form, as well as with consideration of the following (TLP 1577-1579):

1. Consistency with the Provincial Policy Statement and all applicable legislation.
2. Conformity with the Our City, Our Strategy, City Building, and Environmental policies.
3. Conformity with the Place Type policies.
4. Consideration of applicable guideline documents.
5. The availability of municipal services.
6. Potential impacts on adjacent and nearby properties in the area and the degree to which such impacts can be managed and mitigated.
7. The degree to which the proposal fits within its existing and planned context.

Staff are of the opinion that all the above criteria have been satisfied.

3.0 Financial Impact/Considerations

3.1 Financial Impact

There are no direct municipal financial expenditures with this application.

3.2 Climate Emergency

On April 23, 2019, Council declared a Climate Emergency. Through this declaration the City is committed to reducing and mitigating climate change. Details on the characteristics of the proposed application related to the City's climate action objectives are included in Appendix B of this report.

4.0 Key Issues and Considerations

4.1 Use

The proposed residential use is supported by the policies of the *Provincial Policy Statement, 2020 (PPS)* and contemplated in the Neighbourhoods Place Type along a Civic Boulevard.

Policy 916_3 of the Neighbourhoods Place Type identifies key elements for achieving the vision for neighbourhoods, which includes a diversity of housing choices allowing for affordability and giving people the opportunity to remain in their neighbourhoods as they age if they choose to do so. Furthermore, policy 918_2 states that neighbourhoods will be planned for a diversity and mix of unit types and should avoid the broad segregation of different housing types, intensities, and forms. The development of the proposed four (4) storey apartment building would contribute to the existing mix of housing types currently available in the area.

The subject site is in the Neighbourhoods Place Type located on a Civic Boulevard. Table 10 – Range of Permitted Uses in Neighbourhoods Place Type, shows the range of primary and secondary permitted uses that may be allowed within the Neighbourhoods Place Type, by street classification (921_). At this location, uses permitted include a range of low-rise residential uses including single, semi-detached, duplex, triplex, and fourplex dwellings, townhouses, stacked townhouses, and low-rise apartments (Table 10 – Range of Permitted Uses in Neighbourhoods Place Type). The proposed residential use aligns with the goals of the Neighbourhoods Place Type by providing and adding a diversity and mix of housing types that are compatible with the

existing neighbourhood character (Policy 918_2 and 13).

4.2 Intensity

The proposed residential intensity is consistent with the policies of the *PPS* that encourage residential intensification, redevelopment, and compact form (1.1.3.4), an efficient use of land (1.1.1 a), and a diversified mix of housing types and densities (1.4.1). The proposed residential intensity also conforms with the Neighbourhoods Place Type in The London Plan which contemplates a standard maximum height of 4-storeys where a property has frontage onto a Civic Boulevard (Table 11). The proposed residential intensity will be directed towards the Pond Mills frontage, and setbacks, parking and additional landscaping will be provided from the existing homes along Glenroy Crescent to the proposed development. The policies of the London Plan require intensification to respect existing neighbourhood character, while providing for strategic ways to accommodate development to improve our environment, support local businesses, enhance our physical and social health, and create dynamic, lively, and engaging places to live (Policy 918_13). Furthermore, the development will facilitate the efficient use of the land on existing municipal services (Policy 953_2 and 3).

4.3 Form & Design

The proposed development is consistent with the Neighbourhoods Place Type policies and the City Design Policies in The London Plan by facilitating an appropriate form and scale of residential intensification that provides a mix of housing types within the area and is compatible with the existing neighbourhood character (Policy 953_2).

Specifically, the proposed built form supports a positive pedestrian environment, a mix of housing types to support ageing in place and affordability, is supportive of all types of active mobility and universal accessibility and is designed to be a good fit and compatible within its context/neighbourhood character (Policy 193_).

The location and massing of the proposed building is consistent with urban design goals within The London Plan. The building is proposed to be situated close to the street (Pond Mills), defining the street edge and encouraging a street-oriented design with individual ground floor entrances facing the street.

Parking areas will also be located internally, shielded from the street to maintain visual aesthetic and safety, while encouraging a pedestrian oriented streetscape (Policy 936_4). The parking area is located within the rear yard and does not extend beyond the building façade. Adequate space is provided to allow for appropriate screening of the parking from the street and adjacent to abutting properties.

Based on comments received through public consultation, Staff are also recommending that a pedestrian pathway/walkway be provided on site along the north side of the property (1.5m). This access will further enhance the pedestrian connectivity of the subject site and connect residents along Glenroy Crescent to Pond Mills Road. An easement for public access would be sought at the time of site plan.

Although the proposed building is taller than the surrounding single detached dwellings, the proposed building placement provides for a suitable separation between the proposed development and existing homes, mitigating compatibility concerns including loss of privacy. Sufficient space is available to provide for appropriate fencing and/or vegetative screening along the north, south and east property boundary adjacent to the existing single detached and townhouse dwellings.

Staff have identified additional site plan matters that are included and are noted requiring additional consideration at the site plan approval stage, as follows:

Comments for Zoning

- Provide a minimum 3m setback from the property line to provide adequate space to screen the proposed parking from Glenroy Crescent
- Orient the primary communal entrance of the building towards Pond Mills Road.

Items to be addressed at Site Plan

- Provide a pedestrian walkway along the north portion of the property, where a desire line exists between Glenroy Crescent and Pond Mills Road. (TLP, 255).
- As indicated by the UDPRP, the primary communal residential entrance should be a prominent feature along the Pond Mills Road elevation. Clearly outlined the main entrance canopy in the site plan.
 - Differentiate the primary communal residential entrances from the private ground floor residential entrances.
- Incorporate patios or courtyard spaces that spill out into the setback along Pond Mills Road or the communal amenity area to further activate the space and provide additional amenity space for the residents.
 - Utilize landscaping and/or low-rise decorative fencing to distinguish the threshold between public amenity and private amenity for each unit.
 - Retain the direct walkway access from ground floor units to the public sidewalk.
 - Use lockable (from the exterior and interior) swing doors for any private residential ground floor units facing the public street or internal roadway to encourage walkability, activate the streetscape, and provide direct access to the units from the sidewalk.
- Reduce the amount of parking spaces to expand the outdoor communal amenity space with enhanced landscaping and to better frame the interface between the amenity space and Glenroy Crescent. (TLP, 295).
 - The proposal should take into consideration any existing significant mature trees on the site and along the property boundaries. Where possible, retain existing significant mature trees. (TLP, 210).
- Screen any surface parking exposed to a public street with enhanced all-season landscaping, including low landscape walls, shrubs, and street trees. (TLP, 278).

Additional primary entrance and setback requirements will be added to the zoning bylaw. Additional site plan requirements will be addressed at site plan. In Staff's opinion the proposed development is of a suitable form and through the recommended zoning provisions and direction to the Site Plan approval authority the development will meet the high-level urban design goals. The end result will be a development that is compatible with, and a good fit, with the existing and planned context of the area.

4.4 Zoning

The applicant has requested an amendment to Zoning By-law Z.-1 to rezone the subject site from a Residential R1 (R1-3) Zone to a Residential R9 Special Provision (R9-3(_)*H15) Zone.

The 'R9' Zone is intended to permit and regulate medium to high-density development in various forms of apartment buildings. The 'R9-3' Zone permits apartment buildings and special population's accommodations, in the form of lodging house class 2, senior citizens apartment buildings, handicapped persons apartment buildings, and continuum-of-care facilities. The R9-3 Zone variation would permit 100 units per hectare which would facilitate the proposed 4-storey apartment building at a density of 98uph.

The proposed R9-3 Zone requires a minimum lot area of 1,000m² and a minimum lot frontage of 30 metres. The application satisfies the lot frontage and area requirements; however, additional special provisions are required. The following summarizes the special provisions that have been proposed by the applicant and additional special provisions recommended by staff:

- a) *A minimum front yard depth of 3.0 metres whereas 10.0 metres is required.*

The intent of a front yard depth is to ensure sufficient space between the buildings and front lot line to accommodate all site functions while still facilitating a pedestrian oriented development. In this case, the reduced front yard depth will help facilitate a pedestrian oriented development by establishing a strong street edge, with individual unit entrances to help establish a positive interface with the public realm.

Several comments from the public indicated that a larger setback from the street should be required, however, from an urban design perspective, the location and massing of the proposed building is consistent with urban design goals within The London Plan. The building is proposed to be situated close to the street, thus defining the street edge and encouraging a street-oriented design with ground floor entrances facing the street. By bringing the building forward, it increases the overall setback from existing low rise housing forms to the east and south.

- b) *A minimum north interior side yard depth of 4.4 metres whereas 5.6 metres is required.*

The intent of interior yard depths is to ensure all aspects of the built form are located at an appropriate distance away from adjacent properties to mitigate impacts, including stormwater runoff and privacy concerns. In this case the applicant is proposing a reduction of the north interior side yard depth that is not anticipated to impact the adjacent single detached dwelling. Within this reduced side yard, staff will explore opportunities to provide a pedestrian connection between Glenroy Crescent and Pond Mills Road.

- c) *A minimum rear yard parking setback of 1.56m whereas 1.5/3.0m is required.*

The requirement for parking area setbacks from property lines comes from the City's Site Plan By-law. The intent is to ensure adequate setbacks for privacy, plantings, and fencing.

For the subject site, the requirement would be 1.5m abutting a property line, and 3.0m abutting a street (Glenroy). The applicant has requested a 1.5m setback for the rear property line. The request for the reduced rear yard parking setback (1.56m) is not supported by staff. The development is proposing to remove all of the trees on site, staff has identified the need for enhanced tree planting, which cannot occur in areas with a 1.5m setback. As such, staff is recommending the addition of a 3.0m rear yard parking setback, to ensure a sufficient area for tree plantings, and to help mitigate the development from adjacent homes. A parking area redesign will likely be required.

- d) *A maximum building height of 15 metres.*

There is no specified maximum building height in the Residential 'R9' Zone. The intent is to allow flexibility in the height of the various forms and intensity of apartment buildings. In this case, a building height of 15 meters will facilitate a development that is compatible with, and a good fit within the existing and planned context of the area. The proposed residential intensity will also be directed towards the Pond Mills frontage, and setbacks, parking and additional landscaping will be provided from the existing homes along Glenroy Crescent to the proposed development.

- e) *Entrance orientation/Pond Mills Road frontage.*

Staff are also recommending a provision within the zone to ensure the main entrance for the building is oriented to Pond Mills Road, and to recognize Pond Mills Road as the main building frontage, as per the policies of the London Plan.

Staff are of the opinion that the above-recommended special provisions comply with The London Plan and are consistent with the *Planning Act* and the *PPS*.

4.5 Enbridge Gas Pipeline

The property contains a gas pipeline, located along the southwest corner of the site. Through the circulation for this application, Enbridge indicated that the pipeline is not high pressure, which would typically require a 20m setback, as per the policies of the London Plan. The pipeline is running through an Enbridge easement, and as such no buildings or permanent structures are permitted within that area. Any work being done in the vicinity of the pipeline should adhere to the guidelines outlined in the Third-Party Requirements in the Vicinity of Natural Gas Facilities Standard. During construction, should any heavy equipment be required to cross the pipeline, Enbridge will review and

provide approvals as required.

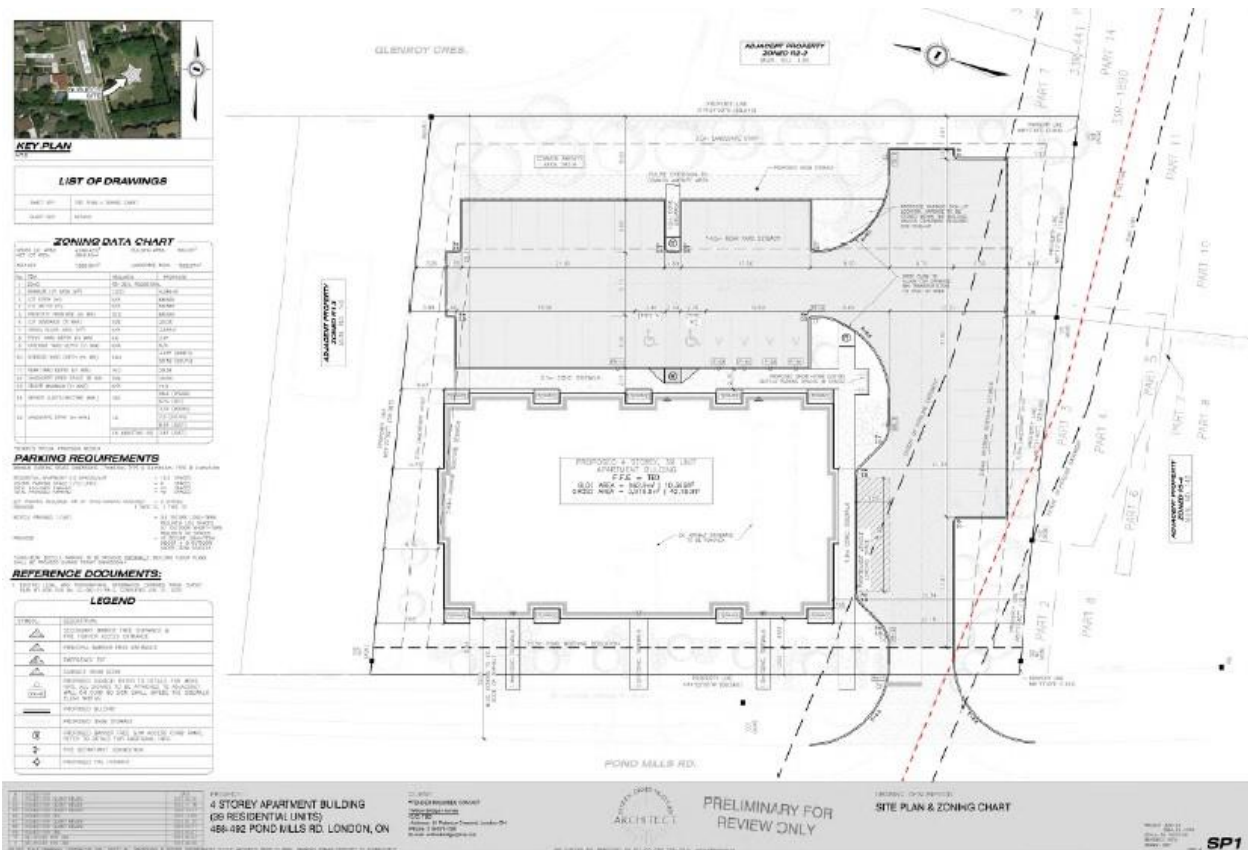


Figure 9 - Conceptual Site Plan Denoting the location of the gas pipeline in red.

4.6 Neighbourhood Concerns

Although many issues have been raised by the residents, many of the concerns can be generally grouped under several key headings - Traffic Impacts and Parking, Site Concerns, Tree Removal, Stormwater, Construction and Property Value Impacts, and Type of Tenancy.

Comments related to height, form, density, and incompatibility have been addressed in section 4.1-4.4. of this report.

Traffic Impacts and Parking

Concerns were raised about the amount of traffic that would be generated by this development. Residents in the area are concerned about negative impacts on the neighbourhood in terms of increased traffic and safety, and the loss of a direct connection to Pond Mills Road from Glenroy Crescent, the lack of parking available for the site, and the location of the driveway.

As part of the complete application, no traffic study (TIA) was required by Transportation. Pond Mills Road is a Civic Boulevard and there should not be any traffic issues generated by the proposed site plan.

Residents were also concerned about the amount of parking, and possible overflow parking on local streets as a result. The City’s recent parking by-law changes allow for 0.5 parking spaces for unit. The proposed application shows 43 parking spaces currently for the site, which is just over one space per unit. The site will also have easy access to transit along Pond Mills Road.

The Transportation Division had no initial concerns with respect to the proposed driveway location. This will be further addressed at site plan.

Site Concerns

Neighbourhood concerns with the development include issues with privacy, noise, lighting, fencing, and lack of amenity space.

With respect to privacy, the building is proposed to be set back 3 metres from the road, which allows the building to be pushed towards the street, thus increasing the setbacks from the adjacent properties. Enhanced landscaping and tree planting will also help to minimize privacy concerns.

The proposed development is not expected to generate any unacceptable noise impacts on surrounding properties.

Directional lighting and fencing will be addressed at site plan.

The amenity space provided and shown on the proposed concept plan is of an adequate size for the site. To ensure connectivity staff will work with the applicant through the site plan process to secure a public walkway through the site, connecting Pond Mills Road to Glenroy Crescent.

Tree Removal

Members of the public expressed concerns about the removal of trees proposed for the site. As shown through the Tree Preservation Plan and Report, nearly all of the on-site trees will be removed. However, staff are recommending additional side and rear yard setbacks for the parking lot to ensure more robust landscaping and to allow for trees to be planted.

Stormwater

Stormwater is proposed to be conveyed on site to the existing stormwater pipes that are located on Glenroy Crescent. As with all applications, they are required to control all stormwater runoff including overland flow to the satisfaction of our Engineering Department.

Construction

Construction impacts can be anticipated for this development; however, they will be temporary. Construction traffic will access the site via Pond Mills Road which is not anticipated to affect nearby local streets.

Type of Tenancy/Tenure

Several comments were made with respect to who will be living in the proposed development. The applicant has indicated the building will likely be rentals. It's important to note though that planning considerations the type of tenancy and tenure (owner vs. rental) are not planning considerations when analyzing planning applications.

Conclusion

The applicant has requested an amendment to Zoning By-law Z.-1 to rezone the subject site from a Residential R1 (R1-3) Zone to a Residential R9 Special Provision (R9-3()*H15) Zone. Staff are recommending approval of the requested Zoning By-law Amendment with special provisions.

The recommended action is consistent with the *Provincial Policy Statement, 2020 (PPS)*, conforms to The London Plan and will permit a 4-storey residential apartment building with 39 units and a maximum density of 100 units per hectare. The recommendation will facilitate an appropriate infill development that will help broaden the range and mix of housing options within the area.

Prepared by: **Michaella Hynes**
 Planner

Reviewed by: **Mike Corby, MCIP, RPP**
 Manager, Planning Implementation

Recommended by: Heather McNeely, MCIP, RPP
Director, Planning and Development

Submitted by: Scott Mathers, MPA, P.Eng.
Deputy City Manager, Planning and Economic
Development

Copy: Nancy Pasato, Manager, Planning Policy (Research)
Chloe Cernanec, Planner, Planning Implementation
Britt O'Hagan, Manager, Current Development
Michael Pease, Manager, Site Plans
Ismail Abushehada, Manager, Development Engineering

Appendix A – Zoning Bylaw Amendment

Bill No. (number to be inserted by Clerk's Office)
2023

By-law No. Z.-1-

A by-law to amend By-law No. Z.-1 to rezone an area of land located at 488-492 Pond Mills Road.

WHEREAS Willow Bridge Homes Ltd. c/o Zelinka Priamo Ltd. has applied to rezone an area of land located at 488-492 Pond Mills Road, as shown on the map attached to this by-law, as set out below;

AND WHEREAS this rezoning conforms to the Official Plan;

THEREFORE, the Municipal Council of The Corporation of the City of London enacts as follows:

1. Schedule "A" to By-law No. Z.-1 is amended by changing the zoning applicable to lands located at 488-492 Pond Mills Road, as shown on the attached map comprising part of Key Map No.112, **FROM** a Residential R1 (R1-3) Zone **TO** a Residential R9 Special Provision (R9-3(_)*H15) Zone.
2. Section Number 13.4 of the Residential R9 Zone is amended by adding the following Special Provisions:

R9-3 (_) 488-492 Pond Mills Road

a. Regulations

- i) For the purposes of Zoning, Pond Mills Road is considered to be the front lot line.
 - ii) Front Yard Setback (Minimum) 3.0 metres (9.8 feet)
 - iii) North Interior Side Yard Setback (Minimum) 4.4 metres (14.8 feet)
 - iv) Rear Yard Setback For Parking Lot to Property Line (Minimum) 3.0 metres (9.8 feet)
 - v) Height (Maximum) 15 metres (or 4 Storeys)
 - vi) Balcony Encroachment on Apartment Buildings: Projection permitted in the required yard (Maximum) 1.5 metres provided the projection is no closer than 2.5 metres to the front lot line.
 - vii) The main building entrance shall be oriented to Pond Mills Road.
- 3) This Amendment shall come into effect in accordance with Section 34 of the *Planning Act, R.S.O. 1990, c. P13*, either upon the date of the passage of this by-law or as otherwise provided by the said section.

The inclusion in this By-law of imperial measure along with metric measure is for the purpose of convenience only and the metric measure governs in case of any discrepancy between the two measures.

This By-law shall come into force and be deemed to come into force in accordance with Section 34 of the *Planning Act, R.S.O. 1990, c. P13*, either upon the date of the passage of this by-law or as otherwise provided by the said section.

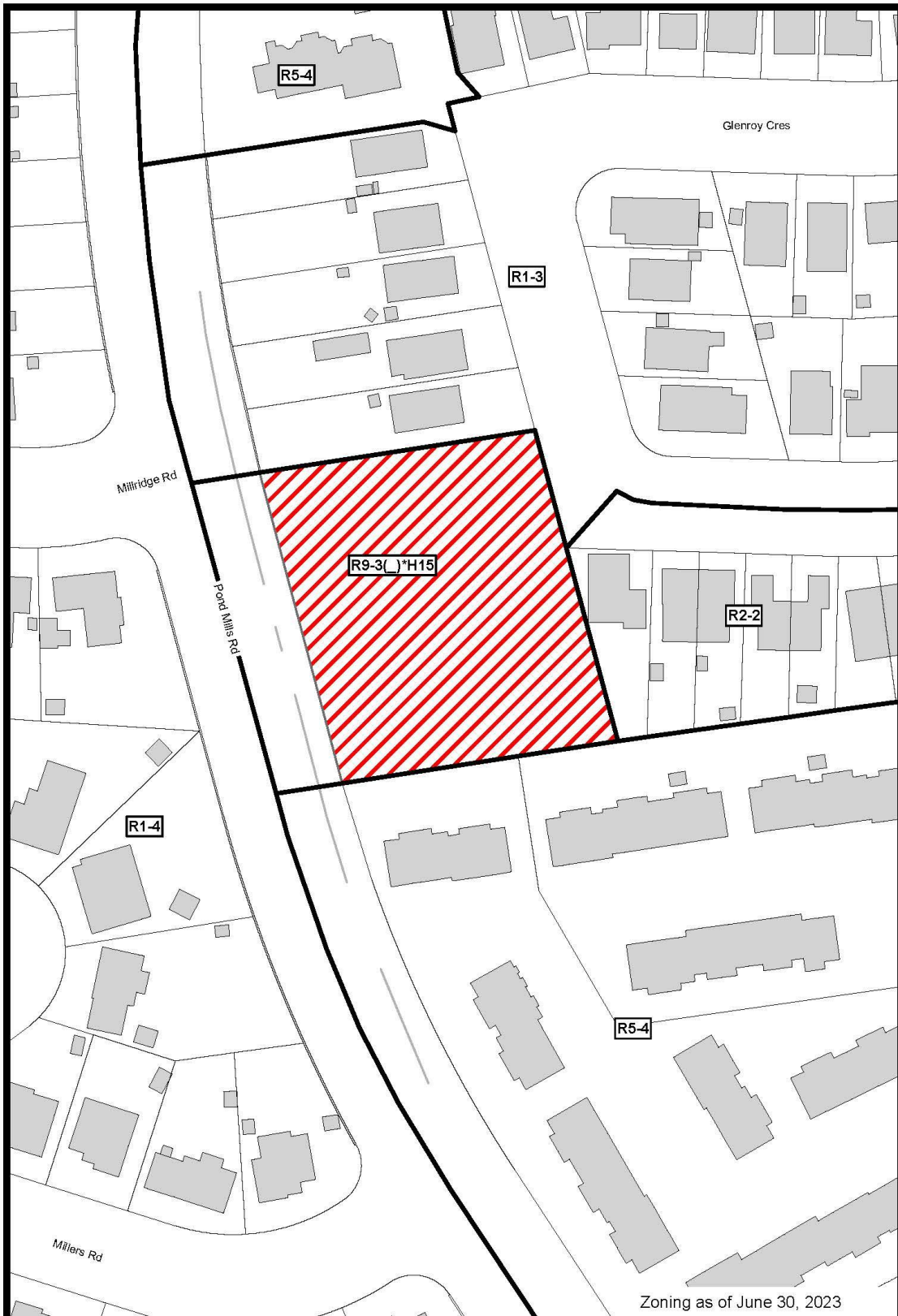
PASSED in Open Council on December 19, 2023.

Josh Morgan
Mayor

Michael Schulthess
City Clerk

First Reading – December 19, 2023
Second Reading – December 19, 2023
Third Reading – December 19, 2023

AMENDMENT TO SCHEDULE "A" (BY-LAW NO. Z.-1)



File Number: Z-9625
Planner: NP
Date Prepared: 2023/8/10
Technician: JI
By-Law No: Z.-1-

SUBJECT SITE 

1:1,000

0 5 10 20 30 40 Meters



Geodatabase

Appendix B - Site and Development Summary

A. Site Information and Context

Site Statistics

Current Land Use	Vacant
Frontage	68.6 metres (225 feet)
Depth	59.3 metres (194.5 feet)
Area	0.4 hectares (1 acre)
Shape	Regular (rectangle)
Within Built Area Boundary	Yes
Within Primary Transit Area	Yes

Surrounding Land Uses

North	Low-Density Residential: Single-detached dwellings
East	Low-Density Residential: Single-detached dwellings
South	Medium-Density Residential: 2-storey townhouses
West	Low-Density Residential: Single-detached dwellings

Proximity to Nearest Amenities

Major Intersection	Commissioners Road East & Pond Mills Road, 1,317m
Dedicated cycling infrastructure	Pond Mills Road - bike lane, 0m
London Transit stop	Pond Mills Road, 70m
Public open space	Westminster Ponds, 250m
Commercial area/use	Southdale Road East & Adelaide Street South Commercial Area, 2200m; Devron/Glenroy commercial plaza, 1.0km
Food store	Tazza Fresh, 500m
Community/recreation amenity	Southeast Optimist Park, 1.1km

B. Planning Information and Request

Current Planning Information

Current Place Type	Neighbourhoods, fronting a Civic Boulevard
Current Special Policies	Primary Transit Area
Current Zoning	Residential R1 (R1-3)

Requested Designation and Zone

Requested Place Type	Neighbourhoods, fronting a Civic Boulevard
Requested Special Policies	N/A
Requested Zoning	Residential R9 (R9-3(_))

Requested Special Provisions

Regulation (R9-3)	Required	Proposed
Maximum height		14.1 metres (15m)
Front yard depth	10.0 metres	3.0 metres
Interior side yard depth (north)	5.64 metres	4.47 metres
Rear yard parking setback	3.0 metres	1.56 metres

C. Development Proposal Summary

Development Overview

The subject lands are proposed to be developed for a 4-storey apartment building comprised of 39 dwelling units, for a density of 98 units per hectare (“UPH”).

Proposal Statistics

Land use	Residential
Form	Low-rise apartment
Height	4 storeys (14.1m)
Residential units	39
Density	95 UPH (gross); 97 UPH (net)
Gross floor area	3,973.6m ²
Building coverage	24.5%
Landscape open space	32.4%
Functional amenity space	624.8m ²
New use being added to the local community	No

Mobility

Parking spaces	43 surface parking spaces
Vehicle parking ratio	1.1 surface parking spaces per unit
New electric vehicles charging stations	N/A
Secured bike parking spaces	35 secure long-term spaces, 13 outdoor short-term spaces
Secured bike parking ratio	0.9 secure long-term spaces per unit, 0.3 outdoor short-term spaces per unit
Completes gaps in the public sidewalk	N/A
Connection from the site to a public sidewalk	Yes
Connection from the site to a multi-use path	N/A

Environmental Impact

Tree removals	20 trees proposed for removal
Tree plantings	123 replacement trees are required to be planted on site
Tree Protection Area	No
Loss of natural heritage features	No
Species at Risk Habitat loss	No
Minimum Environmental Management Guideline buffer met	N/A
Existing structures repurposed or reused	No
Green building features	Unknown

Appendix C – Additional Plans and Drawings



Figure 8 – Rendering of proposed apartment building – view from Pond Mills Road (received May 2023)



Figure 9 – Rendering of proposed apartment building – rear view (received May 2023)



Figure 10 – Rendering of proposed apartment building – pedestrian view (received May 2023)

Appendix D – Internal and Agency Comments

Parks Planning – Received June 14, 2023

- Parkland dedication is required in the form of cash in lieu, pursuant to By-law CP-25 and will be finalized at the time of site plan approval.

London Hydro – Received June 15, 2023

- Any new or relocation of the existing service will be at the expense of the owner.
- A pedestrian connection (not necessarily a multi-use pathway) here would be beneficial, but that it shouldn't use parkland dedication. Ideally, it would not be Parks to maintain as it doesn't connect to the park system. Perhaps just a standard City walkway block or easement.

UTRCA – Received June 19, 2023

- The subject lands are not affected by any regulations (Ontario Regulation 157/06) made pursuant to Section 28 of the *Conservation Authorities Act*.
- The UTRCA has no objections to the application, and we have no Section 28 approval requirements.

Landscape Architecture – Received July 4, 2023

- Major Issues
 - No potential grounds for refusal, or issues that could require significant changes to the proposal.
- Matters for OPA/ZBA
 - Two off-site trees will suffer serious impacts from the development, #9 and #24. Number 9 will lose approximately 38% of its critical root mass and will become structurally unsound and will probably die. To protect tree a 6m no disturbance setback from the east property line would be required. Tree 24 will suffer a 16% loss of critical root mass and will most likely survive. To protect the trees critical root mas, a 2m no disturbance setback from north property line would be required.
- Matters for Site Plan
 - Site currently has 37% canopy coverage. All trees within site are proposed for removal, a loss of 1,234.6cm dbh. In accordance with LP Policy 399.4, 123 replacement trees are required to be planted on site. Replacement trees to be recommendation to Site Plan Review. Only 17 trees have been proposed on LP. Landscape strips are the minimum required at 1.8m. This limited provision of soil will impact development of canopy regrowth. An increase landscape setback of 3m along north, south and east property lines would better support tree growth and reestablish canopy lost to development.
 - Three City of London trees are proposed for removal from the Pond Mills Road boulevard. These trees are protected by the City's Tree Protection Bylaw. To request the removal of a city tree or to request consent to damage the root system of a City tree, contact Forestry Dispatcher at trees@london.ca [Proof of payment issued by Forestry Operations requirement of Site Plan approval. A recommendation for proof of payment will be forwarded for Site Plan review.](#)
- Complete Application Requirements
 - No further reports required.

Urban Design – Received July 4, 2023

- The proposed development is located within the Neighbourhood Place Type, abutting Ponds Mills Road, a Civic Boulevard, and Glenroy Crescent, a local Road. Urban Design is generally supportive of the proposed four storey development if a pedestrian pathway is provided between Glenroy Crescent and Pond Mills Road. As a through-lot ensure that the interface between the proposed development and Glenroy Crescent is also addressed with enhanced landscaping and programming.

Urban Design Peer Review Panel (UDPRP)

- Provide a response to the **June 2023 UDPRP memo**. After attending the UDPRP, the applicant received a formal memo from the UDPRP Chair, or their designate. A Comment Response Table outlining in detail the applicant's response to the UDPRP is required as part of the UDPRP process.
- Provide updated drawings that reflect the revisions made to address the UDPRP comments.

Comments for Zoning

- Provide a minimum 3m setback from the property line to provide adequate space to screen the proposed parking from Glenroy Crescent
- Orient the primary communal entrance of the building towards Pond Mills Road.

Items to be addressed at Site Plan

- Provide a pedestrian walkway along the north portion of the property, where a desire line exists between Glenroy Crescent and Pond Mills Road. (TLP, 255).
- As indicated by the UDPRP, the primary communal residential entrance should be a prominent feature along the Pond Mills Road elevation. Clearly outlined the main entrance canopy in the site plan.
 - Differentiate the primary communal residential entrances from the private ground floor residential entrances.
- Incorporate patios or courtyard spaces that spill out into the setback along Pond Mills Road or the communal amenity area to further activate the space and provide additional amenity space for the residents.
 - Utilize landscaping and/or low-rise decorative fencing to distinguish the threshold between public amenity and private amenity for each unit.
 - Retain the direct walkway access from ground floor units to the public sidewalk.
 - Use lockable (from the exterior and interior) swing doors for any private residential ground floor units facing the public street or internal roadway to encourage walkability, activate the streetscape, and provide direct access to the units from the sidewalk.
- Reduce the amount of parking spaces to expand the outdoor communal amenity space with enhanced landscaping and to better frame the interface between the amenity space and Glenroy Crescent. (TLP, 295).
 - The proposal should take into consideration any existing significant mature trees on the site and along the property boundaries. Where possible, retain existing significant mature trees. (TLP, 210).
- Screen any surface parking exposed to a public street with enhanced all-season landscaping, including low landscape walls, shrubs, and street trees. (TLP, 278).

Heritage – Received July 5, 2023

- The archaeological requirements were addressed through the Site Plan process. No further heritage or archaeological concerns for this application.

Engineering – Received July 10, 2023

Major Issues

- None

Matters for OPA/ZBA

- Confirm road dedication shown on the conceptual site plan is 2.394m.

Matters for Site Plan

The following items are to be considered during a future development application stage:

Transportation:

- A TMP is required for any work in the City ROW, including any servicing, restoration, proposed construction, etc. To be reviewed as part of a PAW submission;
- Provide Engineering Plans showing existing infrastructure, include utility poles/boxes, fire hydrants, light standards, etc.;
- As per Site Plan control by-law and City's Access Management Guideline (AMG) minimum 6.7m width, and 6.0m curb radii is required;
- Presently the width from centerline for Pond Mills Road adjacent to this property is 13.106m as shown on RP 998. Therefore a widening of 2.394m is required to attain 15.5m from c/l.

Water:

- Water is available via the municipal 200mm DI watermain on Pond Mills Road at Millridge Road. This watermain is part of the Westmount/Pond Mills high-level water system (HGL=335.0m). If this watermain is to be used, the Owner will be required to construct/extend (at no cost to the City) the municipal watermain to the subject site, in accordance with City Standards.
- The City is supportive of a water service connection to Glenroy Crescent as long as fire flow/protection requirements are met. Also note that a cut in tee will be required for size on size connections.

Wastewater:

- The municipal sanitary sewer available is the 200mm diameter sewer on Glenroy Crescent, city drawing no. 7959 shows information to the municipal sewer and existing 100mm PDC.
- The applicant's engineer is to provide site servicing drawings with the connection to the municipal sewer. A new adequately sized PDC is required as per City of London Standards with the existing PDC to be properly abandoned/removed.

Stormwater:

Comments Specific to the Site

- The site falls within the Dingman Subwatershed. The Dingman EA requires design for the stormwater control hierarchy for the 25 mm event in new development design. This approach and LID design is included in the Section 6 Stormwater Management of the Design Specifications & Requirements manual.
- As per attached as-constructed 7957, the site at C=0.50 is tributary to the existing 525mm storm sewer on Glenroy Cres. The applicant should be aware that any future changes to the C-value will require the applicant to demonstrate sufficient capacity in this pipe and downstream systems to service the proposed development as well as provide on-site SWM controls. On-site SWM controls design should include, but not be limited to required storage volume calculations, flow restrictor sizing, proposed stormwater controls, etc... It should also be noted that the C-value determined by the consultant must reflect the proposed development's landcover (i.e. impervious cover, slope and soil type).
- It is SWED's expectation that the proposed reduced setbacks shall not impede self-containment and safe conveyance of the site's storm water flows. As part of the storm servicing strategy for this land during the development application stage, the applicant must demonstrate how stormwater flows will be contained, and safely conveyed on this site without impacting adjacent properties.
- As per the Drainage By-law, the consultant would be required to provide for a storm PDC ensuring existing peak flows from the 2 through 100 year return period storms are maintained pre to post development with any increase in flow being managed onsite. The servicing report should also confirm capacity in the existing sewers.
- The number of proposed parking spaces exceeds 29, the owner shall be required to have a consulting Professional Engineer confirming how the water quality will be addressed to the standards of the Ministry of the Environment, Conservation

and Parks (MECP) with a minimum of 80% TSS removal, as per the Dingman EA and to the satisfaction of the City Engineer. Applicable options are outlined in the Stormwater Design Specifications & Requirements Manual.

- Any proposed LID solutions should be supported by a Geotechnical Report and/or a Hydrogeological Assessment report prepared with a focus on the type(s) of soil present at the Site, measured infiltration rate, hydraulic conductivity (under field saturated conditions), and seasonal high groundwater elevation. Please note that the installation of monitoring wells and data loggers may be required to properly evaluate seasonal groundwater fluctuations. The report(s) should include geotechnical and hydrogeological recommendations of any preferred/suitable LID solution. All LID proposals are to be in accordance with Section 6 Stormwater Management of the Design Specifications & Requirements manual.
- The proposed land use of a medium/high density residential will trigger the application of design requirements of Permanent Private Storm System (PPS) as approved by Council resolution on January 18, 2010.
- A standalone Operation and Maintenance manual document for the proposed SWM system is to be included as part of the system design and submitted to the City for review.
- Additional SWM related comments will be provided upon future review of this site.

•

General comments for sites within Dingman Creek Subwatershed

- The Owner agrees to promote the implementation of SWM Best Management Practices (BMP's) within the plan, including Low Impact Development (LID) where possible, to the satisfaction of the City Engineer.
- The owner is required to provide a lot grading plan for Stormwater flows and major overland flows on site and ensure that Stormwater flows are self-contained on site, up to the 100 year event and safely conveys up to the 250 year storm event, all to be designed by a Professional Engineer for review
- The Owner shall allow for conveyance of overland flows from external drainage areas that naturally drain by topography through the subject lands.
- Stormwater run-off from the subject lands shall not cause any adverse effects to adjacent or downstream lands.
- An erosion/sediment control plan that will identify all erosion and sediment control measures for the subject site shall be prepared to the specification and satisfaction of the City Engineer and shall be in accordance with City of London, MECP, and current industry standards and requirements. This plan is to include measures to be used during all phases of construction (i.e. site grading, site servicing, and construction). These measures shall be identified in a standalone ESC plan.

UDPRP – Received July 12, 2023

General Comments

- The panel notes that generally the increased density and the proposed scale and use of the building seems appropriate for the neighbourhood and fits within the intent of the City of London Official Plan. Please consider the comments and recommendations below.

Site Strategy, Building Layout

- While the effort to address the street frontage along Pond Mills Road is appreciated, the panel has concerns that once the road widening is implemented the front yard setback of 3.0M will be too close to the high traffic along Pond Mills Road and will be too tight to provide adequate privacy and grading accommodations. Consider working within the existing front yard setback or retaining minimum 6.0M setback.
- If grade-related units are included along Pond Mills Road, a more generous front yard setback is required. Grade-related units should also be grade-separated from the public realm. Consider a few steps up (above the sidewalk). If setback is not to be significantly increased the panel suggest that due to the proximity to Pond Mills Road, individual sidewalk entrances to units are not appropriate.

- The panel notes that the development has two street frontages: one faces Pond Mills Road, and another one faces Glenroy Crescent. We suggest that both frontages need to be addressed. Consider revising the building to an L-shaped building with a 4-6 storey wing along Pond Mills Road, stepping town to a 2-4 storey wing along the north end of the site and fronting onto Glenroy Crescent.
- The panel suggests that by revising to an L-shaped building footprint as noted above, a paved and landscaped amenity space could be included at the interior corner of the building. This could be more integrated with the interior of the building, more sheltered, and generally more meaningful and useful for building occupants. The sod amenity space at the north edge of the site as currently shown does not appear to be very useful.

Site Circulation, Landscaping

- The applicant is highly encouraged to establish a pedestrian connection through the lot to connect Pond Mills Road and Glenroy Crescent so that the entire neighbourhood can become more connected for pedestrians.
- The panel suggest providing a minimum 3M landscape setback along the full perimeter of the property to retain as many existing mature trees as possible, and to allow for continuous and generous landscape buffers. As noted above, we recommend providing a building frontage along Glenroy Crescent and providing a pedestrian connection through the site to Glenroy Crescent. At a minimum we suggest the proposed fence along Glenroy should be removed and replaced with extensive landscape planting and trees to provide a landscape frontage that is friendly to the neighbourhood while also providing screening to the parking lot. If a fence must be included, we suggest it should be low and transparent.

Architectural Expression

- The panel notes that the main entrances from Pong Mills Road and from the parking lot should be further articulated with architectural elements such as increased glazing, material differentiation, larger or more prominent canopies and signage, etc. to make the entrances more prominent and functional for way-finding. We suggest the entrances along the west and east elevations should be in line to make a clear and visible common circulation path through the building.
- The panel suggest that more work on the building design details and articulation is required at the site plan stage to ensure a high quality of this development. We recommend consideration of the following:
 - a) While the change in material from brick to siding is understood to help break up the mass of the building, consider more variation to the datum line where this transition occurs. When the top 2 stories of a brick building are clad with continuous siding, it can look like many old brick buildings that have been capped with siding at the top as a repair to conceal aging brick/mortar. We suggest introducing lower and high brick massing elements to further articulate smaller masses within the elevations.
 - b) The exit doors at grade along the west elevation currently read like service doors and the windows above are treated the same as apartment windows. We suggest the stairs could be an opportunity to break up the massing of the building by providing more glazing or breaks in the brick massing that could be curtainwall or a panel system.
 - c) Consider providing balconies at the corners of the building.
 - d) Depending on the building massing resolutions noted above, sloped or mansard roofs for some or all of the building could be considered to help reduce the appearance of height relative to the adjacent residential neighbourhood.

Concluding comments:

This UDPRP review is based on City planning and urban design policy, the submitted brief, and the noted presentation. It is intended to inform the ongoing planning and design process. Subject to incorporation of the comments and recommendations noted above, the proposed redevelopment of this site will make a positive contribution to the evolving neighbourhood. Consider the panel's recommendations as noted above for

future refinements to the project in the interest of enhanced experience of the public realm and for current and future residents. The Panel looks forward to the proponent's response.

Site Plan – Received July 14, 2023

- A lay-by is required for the proposed development given the number of residential units exceeds 24 units. A lay with a minimum dimension of 3.5m x 12.0m is required to be provided internal to the site.
- In accordance with the Site Plan Control By-law, a minimum 1.5 metre landscape buffer is required along the rear property boundary and 3.0 metres is required where parking abuts a street (the rear portion along Glenroy Cres). In order to provide additional buffering from the parking area and existing off-site landscaping, staff are recommending a minimum 3.0 metre buffer be provided along the entire rear property boundary. To accommodate this, consider 1-way parking through the interior parking area (maintaining a 6.7m drive-aisle width for the site access and garbage collection).
- A special provision should be included for Pond Mills Road to be deemed the front lot line.
- Identify any at-grade patio spaces fronting Pond Mills Road that project beyond the main building wall.
- The applicant should dimension all walkways to at least 1.5 metres or 2.1 metres if abutting parking spaces, with at least a 1-metre setback from parking area(s) (C.P.-1455-541 Table 7.1).
- Beyond pedestrian circulation, landscaping could affect the site layout if they need extra space along the perimeter.

Appendix E – Public Engagement

Community Engagement

Public liaison: On Wednesday, June 14, 2023, Notice of Application was sent to property owners and tenants in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on Thursday, June 15, 2023. A “Planning Application” sign was also placed on the site.

Nature of Liaison: The purpose and effect of this zoning change is to permit a 4-storey, 39-unit, residential building with parking. Possible change to the Zoning By-law Z.-1 **FROM** a Residential R1 (R1-3) Zone **TO** a Residential R9 Special Provision (R9-7(_)) Zone to permit a 4-storey, 39-unit, residential building with parking. Special provisions include: a minimum front yard depth of 3.0 metres whereas 10.0 metres is required; a minimum north interior side yard depth of 4.6 metres whereas 5.6 metres is required; and a minimum rear yard parking setback of 1.56 metres whereas 3.0 metres is required; a height of 14.1 metres (4 storeys). The City may also consider additional special provisions, and the use of holding provisions, in addition to the above.

Public Responses: 17 responses received. 1 petition was received with 41 signatures.

1. Petition with 41 signatures

Dear Ms Pasato,

My name is Shannon Hart and I am writing on behalf of the residents of 900 Pond View Road in regards to the proposed zoning by-law amendment for 488-492 Pond Mills Road. We are concerned about the proposed development and the potential impact it may have on our properties specifically in regards to environmental impact. We are also concerned that we were provided so little time to review collectively and provide meaningful feedback, particularly in light of the number of property owners and residents affected by the change.

The area in which we live is environmentally sensitive, within steps of Miller's Pond and the abutting green space. As such we feel that the environmental impact of any potential zoning change must be responsibly addressed. As our properties are low lying we have historically experienced issues with flooding and inadequate drainage that have required remediation. In allowing for decreased front, rear and side yard setbacks in addition to the large building and paved parking this will severely limit the greenspace and exacerbate drainage issues. Several trees will also be required to be removed which provide erosion protection, improved soil condition as well as sun and heat diversion.

Increased traffic is also a significant concern as Pond Mills Road is already a busy thoroughfare despite narrowing over the pond. The addition of 38 residents will substantially increase road traffic and will require additional road amendments such as a turning lane. Based on the size of the lot and the proposed building it seems unlikely that sufficient visitor parking will be available for the proposed apartment complex, resulting in increased congestion with street parking as well as illegal parking on neighbouring properties, such as within our condominium complex.

In 2021 a public notice was sent out in regards to a public hearing involving a planning adjustment for a property on the opposing side of Pond View Rd. The notice was in regard to an addition that was being planned that would result in a reduced setback from the road on the second story. We were provided a month of notice to voice any concerns and were invited to the hearing. The work has since been completed and has not resulted in any meaningful impact on the community. To compare, I received this notice of planning application with only ten business days to consult with my neighbours and propose feedback. From our perspective it appears that the city cares much more about a single window addition than then potential impact that a large apartment complex will have on our community. We simply ask that the same level of procedural fairness be applied in this case.

We appreciate your attention in this matter,

2. C. Richter – email response

Please accept the following two considerations regarding the above zoning amendment application. I witness/drive with the Pond Mills road traffic every day. This proposed building, so close to Pond Mills, does not allow for much visual or physical clearance when entering/exiting the property. This is a potential safety hazard. Any building should be constructed within the city's standing 10 m front yard depth to allow for proper site lines and access -- not 3 m as suggested in the proposed revised plan. The proposal reports re-iterate the natural beauty of the ponds in the neighbourhood and access to

these local wholistic sites for local residents. And yet, this proposal includes the removal of numerous trees on the property, trees that provide shade, wildlife habitat, and enjoyment to local residents. The proposed landscaping does not supplant the existing benefits of the existing trees. I am all for the development of affordable housing, but to jam this proposed building in this space presents as a desperate move to get residential use of every square inch of this property. Please consider the 'flavour' of the neighbourhood and recognize we are not in the downtown - we live in Pond Mills, where nature is what has drawn us here to begin with. Perhaps the building plan should be re-worked to exist within the R1 (R3). Thank you kindly for your time and consideration. Please feel free to contact me should there be a requirement to do so.

3. M. Wagenman – email response

I'm writing about the potential zoning change at 488-492 Pond Mills Road. I am very concerned about the already high activity in the area due to the higher density housing nearby. I am also unhappy with how close this new development will be to existing homes. The traffic on Pond Mills Road is already a concern and this new development will only cause this problem to increase. As such, I am opposed to the proposed rezoning and development. I wanted you to know this and also ask how I can more formally file my opposition to the by-law amendment. Thank you!

4. D. Jonas – phone call

- Questions on application
- Nothing like it in area
- Can they even fit this building on the site?
- Traffic will increase in area
- Additional lights/noise impacts
- Will follow up with written objection
- Questions on how to object

5. D. McKellar – phone call & email response

This email is regarding the proposed zoning amendments for the 488-492 Pond Mills development. In talking to our neighbours, we are not the only ones having concerns regarding the scope of this project.

Reducing the setback on the front of the property from 10.0 metres to 3.0 metres is significant and would make it too close to the road. No other property on Pond Mills Road, or in the local area, is located that close to the road and doing so would have a negative impact on the value of the properties in the area.

The height of the building is too high and size of the building is too large for the lot and the area. Both issues are a major concern as there are no buildings in the area that are remotely close to this height or size. The number of occupants increases the density significantly relative to the neighbourhood and this would also have a negative impact on the value of the properties in the area.

There will be several full-grown trees removed to make way for this project. Removing the trees and replacing them with a relatively small green space is a concern.

The size of the development also raises a concern regarding water runoff. The natural slope of the lot runs towards the southeast corner of the property. The size of the parking lot and the decreased setback would create a drainage issue for the adjacent properties.

The footprint of the parking area, combined with the number of units, means the neighbours will have a high volume of car traffic in and out of the property. There is already a high volume of car traffic on Pond Mills and the development will add to an issue that is already a problem. The city is currently installing a crosswalk at the corner

of Pond Mills and Pond View Road to help pedestrians cross the road safely but adding more care traffic to an already busy area creates more safety concerns. This would especially be a concern if the occupants are handicapped or senior citizens as per the permitted occupants.

The proposal also mentions the city may also consider special provisions. Do you have an example of what these special provisions could be?

6. B. Chiodo – phone call

- Generally support high density
- However this doesn't make sense here
- Frustrating why we cant stop urban sprawl
- Doesn't fit in area
- Need community gardens or a park here – none in area
- Should be infilling old commercial or brownfields not building here
- Lot not big enough for residential
- Quite area near natural heritage

7. S. Dixon – email response

In reference to the proposed development at 488-492 Pond Mills Road, what is the makeup of the units? Is it single family, subsidized or geared to income housing? Thx in advance for any info you can provide.

8. S. Schomburg – email response

As per the mailing we received about the proposed bldg of a 4 story low-rise unit at 489-492 pond Mills Rd , I'm emailing to express our disapproval. We feel this low rise apt would be disruptive to our area with increased traffic and congestion. Will there be a general mtg? Where we can express our opinions and discuss the project.

9. S. Penn – phone call

- Do not support
- Too dense
- Roads are already too busy

10. L. Marzec – email response

I have received notice of the zone change application for the property at 488-492 Pond Mills Road. My concerns are the following:

- What is the plan for the current walkway/opening that currently gives people direct access from Glenroy Crescent to Pond Mills?
- It looks like the parking lot is planned on the rear of the property that backs onto Glenroy Crescent, what is the plan for the back of the property?
- Is there a plan to open Glenroy Crescent to the parking lot?
- As this is planned to be a senior/disabled home, there will be an increase in noise such as ambulances, police etc. What is the plan to reduce this noise for the current community?
- What is the plan for the construction workers that will be working on this building?
- Parking plan
- Noise plan (community respect)

Please notify me of all public participation meetings and any decisions made with respect to this property plan.

11. Tanya – email response

I have great concerns of this being built.

Concerns are: noise, construction being disruptive, disruption to wild life including birds, rabbits, ducks that yearly build their home in that yard, wood peckers. The fact that most of the trees will have to come down to accommodate such a large structure is very concerning. This will not only affect nature & the wildlife but it will take away shade for all of us, privacy, increase the noise factor. These trees are enjoyed by our community. People come and sit and enjoy time outside.

The property as it sits is not kept up well by the current property owners. There is garbage on the property, hazardous materials, and a fence that is falling down causing risk to the residents here. The grass is not cut regularly to the point that the police were called as a child with special needs was in the grass & the grass was so tall that she could not be seen. With ticks an issue in the city this is also a concern. We also had a crime committed here recently and the assailants were hiding out in the tall grass and police dogs were needed to rid them from hiding out there any longer. If the current owners of the property Zelinka Priamo Ltd can't maintain things now how are they ever going to maintain a building once build especially the size they want to build it.

Then there is the issue of privacy. The fact that we are 3 storey units and they are 4 there will be no privacy for anyone especially without the trees. There is going to be more landscape needed for us without those trees and more cost to the people that live here now & that isn't fair. Also they are proposing the driveway being on the side of the building closest to 900 pond view road which is a huge issue having that many cars coming in and out all day and night with highlights shining into everyone's windows. Where as on the other side it's one house only and it would only shine into their back yard. Also with two driveways close together those coming and going from 900 pond view and them coming and going from their driveway is an accident waiting to happen. This property backs onto so many others properties and this needs to be seriously thought about. Also this is going add so many people in one very small area. We also want more info as to what kind of units would these be, for example how many bedrooms, low income, seniors, rentals or owned. It is not fair to leave this info vague and to also say here is the info so far however it could change. So how are people supposed to decide on things when a potential owner can change things & disrupt all of our lives for the rest of the time we live here. Please do not allow this building to be built. We are a beautiful community who would be drastically disrupted in permanent ways. We need a park there, a place where our children can be kids and parents & caregivers can join together. We need this more than ever.

12.R. Cartwright – email response

I am writing to express my concern with the proposed zoning by-law amendments for 488-492 pond mills road.

I have several concern that I would like addressed before a final decision is made by the planning department such as.

1. the proposed density is far to high for the size of the property.
2. Pond Mills is already a busy road and adding a building with the proposed density will make it even busier (it's already difficult to exit millers cove complex onto pond mills road at certain times.)
3. who are the target population for the proposed apartment. (Ie. senior, low income, people with disabilities, general population)
4. will the units be owned or rental units?
5. grading to the rear of the property (so the people on Glenroy cres don't get heavy run off from the property)
6. the location of the proposed driveway into the site is not appropriate. (Entrance to site the would be where people heading north can merge into a left turning lane to turn onto Millridge rd and people heading south can merge into a left turning lane to enter the Miller's cove complex, the sight lines looking south are limited when leaving the property especially if there is someone from Miller's cove trying to leave their complex.
7. lack of space for snow removal (ie space is not adequate and will possibly end up in the neighbouring house backyard.

8. the removal a valuable green space.

I look forward to receiving the answers to these concerns in the near future.

13.K. Carter – email response

I am writing in regards to a planning application for 488-492 Pondmills Road. I have read through the planning application we received. I am concerned about a few things. First is the reduced setbacks proposed. The existing trees on the property permitter are mature, and have quite large root bulbs. It seems the reduced setbacks along the North, South and East sides of the property would comprise the health of the mature trees. It would be a huge loss to the surrounding properties to have the trees removed.

My second concern is the view from Glenroy Crescent. It is hard to see from the proposed plans if new wooden fences or large shrubs are planned to reduce noise and car lights shining into the homes along Glenroy Crescent? Can a fence be requested for these reasons?

My third concern is the property has become a thoroughfare from Pondmills Road and Glenroy Crescent. It is used daily by many people in the community to access the bus routes, and for students to walk to Laurier Secondary School. Will the gate at the North East corner of the property be maintained?

Finally, how will water drainage be maintained for the property when the current grass is removed? The sidewalk on Glenroy Crescent that runs adjacent to the property tends to puddle when it rains, I am very concerned removing the grass and any trees will contributing to flooding on the two Glenroy Crescent properties that are next to the proposed plan.

14.I. Klassen – email response

I am commenting on the proposed changes to the empty lot on Pond mills rd. While I am not opposed to infill, my concerns are about the change in the neighborhood. Particularly the requested reduced frontage. As much of the road in that area is either the back yard fences of subdivisions or town homes, having a huge building so close to the road would be out of place. I would like to see the front yard setback remain at 10 m , which is less than my 2 story home has. This would be better for both residents to enjoy their balconies or walk out decks , and pedestrians who pass by; a little green space between the sidewalk and the building gives personal space and not an "in your face " feeling. I don't like the idea of a street wall as the proposal keeps saying, this isn't downtown or a new neighborhood.

My other big concern is the amount of trees to be removed. Right now there is a lovely green perimeter to the lot , which provides privacy and shade. Also 3 new trees were recently planted by the city on city property yet they are slated for removal? Razing it all to the ground just to suit the builder is wasteful. How many years did it take to grow all those trees. We call London the Forest City and yet keep cutting down trees. Trees add value to properties and cost money to cut down. Aside from the old willow and the 2 maples in the centre, being removed, only some stray branches need trimming. I'd like to see the greenery maintained, which requires a greater setback than requested. However , if the number of units are reduced by limiting the height to 3 stories, parking spaces would also be reduced, possibly fitting inside the required space. Reducing setbacks along the back parking would likely mean limited room for trees, even if they offered to plant any.

If this was my back neighbor, I wouldn't like a 4 story building with windows on the side, overlooking my yard. No fence could be tall enough to obscure that view. In a neighborhood of 2 story homes , doubling that height is invasive. And then seeking to remove all the trees that might help block out the building on top of that?

In conclusion, I would like to see the proposal reduce it's footprint and units and stick closer to the existing zones around the lot.

15. S. Hart – email response with attached petition

I have attached a letter that I have circulated amongst my neighbours regarding the planned zoning change for the vacant lot on Pond Mills Road. Many have expressed concerns to me regarding the development. I would also like to call attention to the letter to the editor from the London Free Press (June 30, 2023) entitled "infill too tall". If you require a copy I would be happy to forward that along to you.

16. K. Nagle – email response

Regarding file Z-9625, Kate and myself request to be kept informed in any updates regarding the planned proposal as we are the adjacent property and share a property line. Please keep us both informed of any information going forward.

17. A. Srivastava – email response with attached letter

We are in receipt of the Notice of Planning Application for the property located at 488-492 Pond Mills Road, London, ON ("488-492 Pond Mills"). 488-492 Pond Mills and xxxxxxxx are located across the street from one another across Pond Mills Road.

On initial review, the proposed Zoning By-Law Amendment at 488-492 Pond Mills appears inconsistent with the local neighbourhood and may present a concern. We are in the process of retaining a land use planner to prepare an opinion about proposed Zoning By-Law Amendment at 488-492 Pond Mills. Based on this forthcoming expert input, we may request that Municipal Council not approve the Zoning By-Law Amendment at 488-492 Pond Mills.

To this end, we initially request that:

- this letter be provided to the City's Planning and Environment Committee and City Council
- subsequent letters and their enclosure(s), including expert reports, be considered and included in the staff's recommendation to the City's Planning and Environment Committee and City Council
- the City provide us with the opportunity to make oral submissions about the foregoing, and
- the City continue to update us about the planning application at 488-492 Pond Mills.

18. I. Klassen – email response

I have the following concerns

1. Height. I find it excessive at nearly double any existing structure nearby or on this section of the street. At maximum a 3 storey apartment, or better yet a two storey townhouse would be acceptable. Perhaps the builder could use the slope to their advantage and sink the building down and move it back from the street as well. Mature trees should be able to block it from sight, particularly my backyard, which I prefer to be private not a fishbowl. I purchased my home based on the existing skyline which did not have a 4 storey apartment in view from my backyard.

2. Density. Placing 39 units and 39 plus vehicles on a property originally zoned for 2 homes is pushing the limits. Pond Mills is not a major road, being only 2 lanes of traffic, so this density seems out of place for the footage. Half that might be acceptable. Zelinka should consider a lower zoning change.

3. Trees. I object to cutting everything down to the ground for the convenience of the builder. Replacing them and adding fencing does not equal the amount of height they've already achieved- which took a good 20 years or more. This is an established neighbourhood, not new housing, so leave the trees with minimal, judicious pruning to limbs (not trunks) that present hazards. If the neighbors haven't complained about the trees: I suggest that they like them as a green scene and buffer.

4. Green space . Another concern of mine is the very limited green space of the plan and its proximity to parking and cars. Little kids need places to run but not near traffic. There is a reason that residential homes have parking in the front and backyards in the back away from vehicles. Even seniors can enjoy some green space that's quiet (no car noises) behind or beside the building to relax in . In this day of mental health awareness, builders should also consider those needs of potential buyers. Blue sky and green scenery help ground us and keep us healthy in a busy urban landscape.

5. Parking. I have issue with the parking in the back of the property as it will negatively affect the direct neighbours with both traffic noise and light pollution at night. That belongs near the street where people expect traffic and streetlights at night . Moving the parking to the front of the property would be an improvement in my opinion. And also reduce the amount of pavement required, thus reducing cost to the builder. Any security concerns can be dealt with lighting and fencing, and some landscaping. There are examples further north on the road with existing townhomes.

Letter:

My wife, Kimberly Srivastava, and I are the registered owners of xxxxxxxxx, London, ON (xxxxxxxxxxx).

We are in receipt of the Notice of Planning Application for the property located at 488-492 Pond Mills Road, London, ON ("488-492 Pond Mills"). 488-492 Pond Mills and 2 Millridge Road are located across the street from one another across Pond Mills Road.

On initial review, the proposed Zoning By-Law Amendment at 488-492 Pond Mills appears inconsistent with the local neighbourhood and may present a concern. We are in the process of retaining a land use planner to prepare an opinion about proposed Zoning By-Law Amendment at 488-492 Pond Mills. Based on this forthcoming expert input, we may request that Municipal Council not approve the Zoning By-Law Amendment at 488-492 Pond Mills.

To this end, we initially request that:

1 this letter be provided to the City's Planning and Environment Committee and City Council

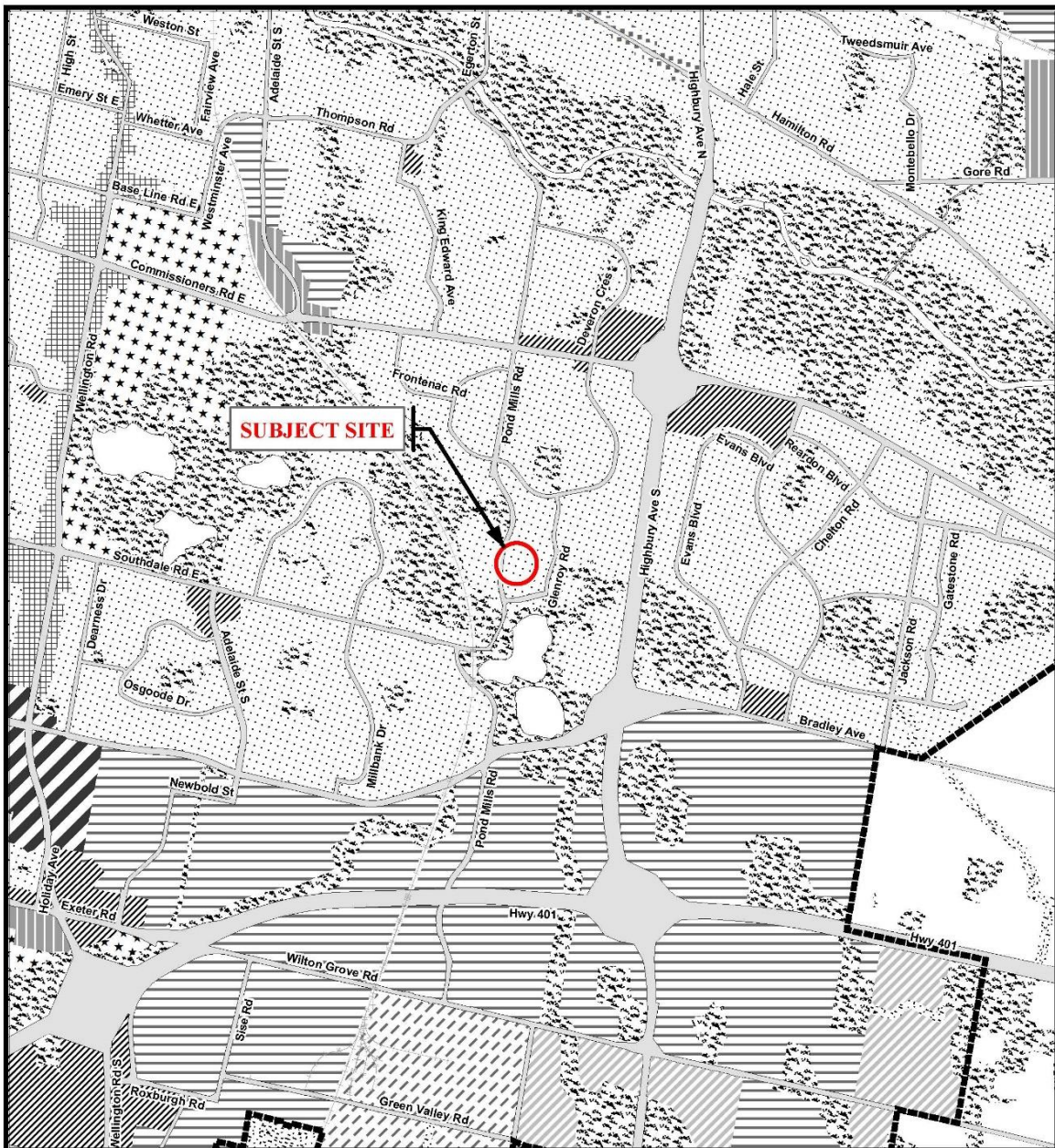
2 subsequent letters and their enclosure(s), including expert reports, be considered and included in the staff's recommendation to the City's Planning and Environment Committee and City Council

3 the City provide us with the opportunity to make oral submissions about the foregoing, and

4 the City continue to update us about the planning application at 488-492 Pond Mills.

We intend to fully participate in the land use planning process.

Appendix F – Relevant Background



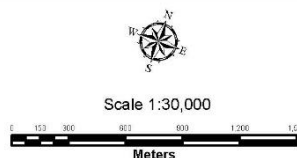
Legend

- | | | |
|------------------------|--------------------------|---|
| Downtown | Future Community Growth | Environmental Review |
| Transit Village | Heavy Industrial | Farmland |
| Shopping Area | Light Industrial | Rural Neighbourhood |
| Rapid Transit Corridor | Future Industrial Growth | Waste Management Resource Recovery Area |
| Urban Corridor | Commercial Industrial | Urban Growth Boundary |
| Main Street | Institutional | |
| Neighbourhood | Green Space | |

This is an excerpt from Planning & Development's working consolidation of Map 1 - Place Types of the London Plan, with added notations.

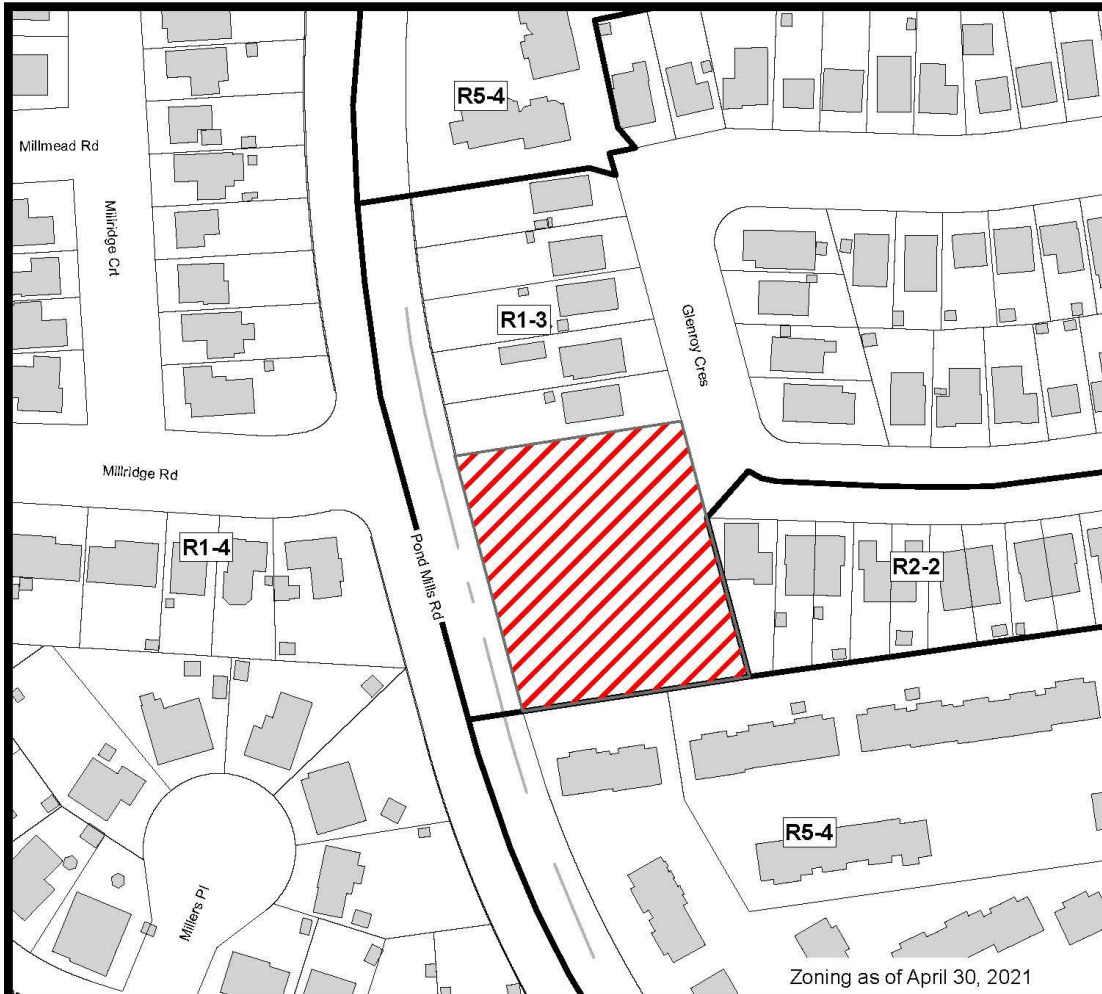
CITY OF LONDON
Official Plan
LONDON PLAN MAP 1
- PLACE TYPES -

PREPARED BY: Planning & Development



File Number: Z-9625
Planner: NP
Technician: JI
Date: 2023/8/10

Project Location: \\cfile1\giswork\Planning\Projects\p_officialplan\workconsol00\excerpts_LondonPlan\mxds\Z-9625_Map1_PlaceTypes.mxd



COUNCIL APPROVED ZONING FOR THE SUBJECT SITE: R1-3

1) **LEGEND FOR ZONING BY-LAW Z-1**

- | | |
|---|-----------------------------------|
| R1 - SINGLE DETACHED DWELLINGS | RF - REGIONAL FACILITY |
| R2 - SINGLE AND TWO UNIT DWELLINGS | CF - COMMUNITY FACILITY |
| R3 - SINGLE TO FOUR UNIT DWELLINGS | NF - NEIGHBOURHOOD FACILITY |
| R4 - STREET TOWNHOUSE | HER - HERITAGE |
| R5 - CLUSTER TOWNHOUSE | DC - DAY CARE |
| R6 - CLUSTER HOUSING ALL FORMS | |
| R7 - SENIOR'S HOUSING | OS - OPEN SPACE |
| R8 - MEDIUM DENSITY/LOW RISE APTS. | CR - COMMERCIAL RECREATION |
| R9 - MEDIUM TO HIGH DENSITY APTS. | ER - ENVIRONMENTAL REVIEW |
| R10 - HIGH DENSITY APARTMENTS | |
| R11 - LODGING HOUSE | OB - OFFICE BUSINESS PARK |
| | LI - LIGHT INDUSTRIAL |
| DA - DOWNTOWN AREA | GI - GENERAL INDUSTRIAL |
| RSA - REGIONAL SHOPPING AREA | HI - HEAVY INDUSTRIAL |
| CSA - COMMUNITY SHOPPING AREA | EX - RESOURCE EXTRACTIVE |
| NSA - NEIGHBOURHOOD SHOPPING AREA | UR - URBAN RESERVE |
| BDC - BUSINESS DISTRICT COMMERCIAL | |
| AC - ARTERIAL COMMERCIAL | AG - AGRICULTURAL |
| HS - HIGHWAY SERVICE COMMERCIAL | AGC - AGRICULTURAL COMMERCIAL |
| RSC - RESTRICTED SERVICE COMMERCIAL | RRC - RURAL SETTLEMENT COMMERCIAL |
| CC - CONVENIENCE COMMERCIAL | TGS - TEMPORARY GARDEN SUITE |
| SS - AUTOMOBILE SERVICE STATION | RT - RAIL TRANSPORTATION |
| ASA - ASSOCIATED SHOPPING AREA COMMERCIAL | |
| OR - OFFICE/RESIDENTIAL | "h" - HOLDING SYMBOL |
| OC - OFFICE CONVERSION | "D" - DENSITY SYMBOL |
| RO - RESTRICTED OFFICE | "H" - HEIGHT SYMBOL |
| OF - OFFICE | "B" - BONUS SYMBOL |
| | "T" - TEMPORARY USE SYMBOL |

CITY OF LONDON

PLANNING SERVICES / DEVELOPMENT SERVICES

**ZONING
BY-LAW NO. Z-1
SCHEDULE A**



THIS MAP IS AN UNOFFICIAL EXTRACT FROM THE ZONING BY-LAW WITH ADDED NOTATIONS

FILE NO:

Z-9625

NP

MAP PREPARED:

2023/8/10

Jl

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0 5 10 20 30 40

Meters

Q3 DEFERRED MATTERS

**PLANNING AND ENVIRONMENT COMMITTEE
(AS OF NOVEMBER 27, 2023)**

File No.	Subject	Request Date	Requested/ Expected Reply Date	Person Responsible	Status
1	Inclusionary Zoning for the delivery of affordable housing - the Civic Administration BE DIRECTED to report back to the Planning and Environment Committee outlining options and approaches to implement Inclusionary Zoning in London, following consultation with the London Home Builders Association and the London Development Institute.	August 28/18 (2.1/13/PEC)	Q4 2023	McNeely/Adema	Council approved Terms of Reference in January, 2021 for the Inclusionary Zoning review. In February, 2022 Council submitted a request to the Province to allow for the consideration of Inclusionary Zoning polices that apply City-wide. Work is currently underway to update the analysis, with recommended policies anticipated in Q4, 2023.
2	Draft City-Wide Urban Design Guidelines – Civic Admin to report back at a future PPM of the PEC	Oct 29/19 (2.1/18/PEC)	Q4 2024	McNeely/Edwards	Staff are working to incorporate the contents of the draft Urban Design Guidelines into the Site Plan Control By-law update (expected Q2 2024) as well as the new Zoning By-law (expected Q4 2024). The need for additional independent UDG will be assessed after those projects are complete.
3	Homeowner Education Package – 3 rd Report of EEPAC - part c) the Civic Administration BE REQUESTED to report back at a future Planning and Environment Committee meeting with respect to the feasibility of continuing with the homeowner education package as part of Special Provisions or to replace it with a requirement to post	May 4/21 (3.1/7/PEC)	Q4 2023	McNeely/Davenport/ Edwards	Through the EIS Monitoring Project, staff are assessing the efficacy and implementation of EIS recommendations across a number of now assumed developments. Following the completion of this project, a more detailed review of the

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	descriptive signage describing the adjacent natural feature; it being noted that the Environmental and Ecological Planning Advisory Committee (EEPAC) was asked to undertake research on best practices of other municipalities to assist in determining the best method(s) of advising new residents as to the importance of and the need to protect, the adjacent feature; and,				recommendations made in the EIS and overall best practices will be reviewed.
4	Medway Valley Heritage Forest ESA – c) the portion of the pathway and trail system from Gloucester Road (Access A11) to its connection with the pathway in the Valley shown on “Appendix B” of the Medway Valley Heritage Environmentally Significant Area (South) Conservation Master Plan BE DEFERRED to be considered at a future meeting of the Planning and Environment Committee following further consultation and review with the adjacent neighbours, the Upper Thames River Conservation Authority, the Environmental and Ecological Planning Advisory Committee and the Accessibility Advisory Committee	August 10/21 (3.9/11/PEC)	Q4 2023	McNeely/Edwards	Staff are resolving the detailed design aspects of the project in advance of initiating consultation with the adjacent neighbours, UTRCA, ECAC and ACAAC. Following the detailed design recommendations of the retained consultants and community consultation, staff will recommend a preferred alternative.
5	Food Based Businesses – Regulations in Zoning By-law Z-1 for home occupations as it relates to food based businesses	Nov 16/21 (4.2/16/PEC)		McNeely/Adema	Issue to be addressed via ReThink Zoning.

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6	Global Bird Rescue – update Site Plan Control By-law and Guidelines for Bird Friendly Buildings; CA to contact London Bird Team to finalize bird-friendly pamphlet; pamphlet to be circulated to EEPAC and AWAC when completed	Nov 16/21 (4.3/16/PEC)	Q2 2024 Q3 2023	McNeely/O’Hagan Bennett/Tucker	<p>Staff are working to update the Site Plan Control by-law (expected Q2 2024), which will include Bird Friendly standards and guidelines.</p> <p>Staff have prepared a printable Bird-Friendly pamphlet that can be distributed to homeowners. The preparation of an online version of the pamphlet is underway and will be circulated to the advisory committees once complete (expected Q3 2023).</p> <p>Overall, being managed via different project.</p> <p>The preparation of a pamphlet is underway that will be circulated to the Advisory group for feedback. Expected completion by Q3 2022.</p>
7	Community Improvement Plan (CIP) Financial Incentive Programs 5-Year Review - the Civic Administration BE DIRECTED to report back with a comprehensive review, including a sensitivity analysis, of the City’s existing Community Improvement Plans and associated financial incentives; and, the Civic Administration BE DIRECTED to report back at a future meeting with preliminary	May 24/22 (2.2/10/PEC)	Q2 2023	S. Thompson/ Yanchula	Staff at the May 23, 2023 PEC meeting submitted its comprehensive review of the existing Community Improvement Plans and Financial Incentive programs, including recommendations for changes to Community Improvement Plans and preliminary impacts of recommended changes to

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	<p>information for the 2024-2027 multi-year Budget.</p> <p>Civic Administration to review existing and consider in future housing-related CIPs opportunities to include and incentivize the creation of affordable housing units and report back no later than Q2 of 2024, including but not limited to the introduction of mandatory minimums to access CIP funds; and, options to include affordable housing units in existing buildings</p>	June 27, 2023 (3.2/10/PEC)	Q2 2024		<p>Financial Incentives ahead of the upcoming 2024-2027 budget.</p> <p>Final approval of all recommended changes is anticipated to be completed Q3 2023.</p>
8	Additional Residential Units – Civic Administration to review current five-bedroom limit and report back; Review of the current parking and driveway widths policies in additional residential units and report back;	June 6, 2023 (3.4/9/PEC)	Q1 2024	H. McNeely/J. Adema	Under review.
9	Byron Gravel Pits Secondary Plan – Civic Administration to report back on consultation process, and the outcome of supporting studies that will inform the Final Byron Gravel Pits Secondary Plan and implementing an OPA	July 25, 2023 (2.2/12/PEC)	Q1 2024	H. McNeely/P. Kavcic	Public consultation anticipated October 2023.